



CITY OF BOULDER
PLANNING BOARD MEETING AGENDA
DATE: January 22, 2015
TIME: 6 p.m.
PLACE: 1777 Broadway, Council Chambers

1. **CALL TO ORDER**
2. **APPROVAL OF MINUTES**
3. **PUBLIC PARTICIPATION**
4. **DISCUSSION OF DISPOSITIONS, PLANNING BOARD CALL-UPS/CONTINUATIONS**
 - A. **Call Up Item: [Site Review Minor Amendment \(LUR2014-00031\)](#) and [Final Plat \(TEC2014-00031\)](#):** Request to to subdivide one 32,510 sq. ft. developed lot within the Carrie Subdivision PUD located at 593 Lee Hill Rd. into to three new residential lots. Call-up expires **January 23, 2015**.
 - B. **Call Up Item: [Knapp Subdivision \(TEC2013-00057\)](#):** Final Plat to subdivide one 0.5-acre developed lot at 3050 15th St. in the Garden Home Subdivision to create 2 new residential lots: Lot 1 (9,605 s.f.) and Lot 2 (12,176 s.f.). Lot 1 will contain the existing single family home. The call up period expires on **January 26, 2015**.
5. **PUBLIC HEARING ITEMS**
 - A. **[Public hearing and consideration of a Site and Use Review \(LUR2014-00057\)](#) to construct one new 2,850 square foot, single story Bank of America building** with a drive thru facility on the pad site at 1965 28th St. The proposal also includes improvements to the existing parking area serving the pad site as well to the parking area adjacent to the Hazels liquor store. The project site is zoned Business – Regional 1 (BR-1).

Applicant: Bruce Dierking
Owner: Andre Family Partnership, RLLLP
 - B. **[Public hearing and consideration of a Minor Amendment to an Approved Site Review \(LUR2014-00088\)](#)** for a 1,950 square foot addition to an existing single-family residence partially located in the rear yard setback at 3059 6th St. The project site is zoned Residential - Low 1 (RL-1).

Applicant: Coburn Development Inc.
Owner: Kara Goucher.
6. **MATTERS FROM THE PLANNING BOARD, PLANNING DIRECTOR, AND CITY ATTORNEY**
 - A. **[Envision East Arapahoe project update and scenarios analysis](#)**
7. **DEBRIEF MEETING/CALENDAR CHECK**
8. **ADJURNMENT**

**CITY OF BOULDER PLANNING BOARD
MEETING GUIDELINES**

CALL TO ORDER

The Board must have a quorum (four members present) before the meeting can be called to order.

AGENDA

The Board may rearrange the order of the Agenda or delete items for good cause. The Board may not add items requiring public notice.

PUBLIC PARTICIPATION

The public is welcome to address the Board (3 minutes* maximum per speaker) during the Public Participation portion of the meeting regarding any item not scheduled for a public hearing. The only items scheduled for a public hearing are those listed under the category PUBLIC HEARING ITEMS on the Agenda. Any exhibits introduced into the record at this time must be provided in quantities of ten (10) to the Board Secretary for distribution to the Board and admission into the record.

DISCUSSION AND STUDY SESSION ITEMS

Discussion and study session items do not require motions of approval or recommendation.

PUBLIC HEARING ITEMS

A Public Hearing item requires a motion and a vote. The general format for hearing of an action item is as follows:

1. Presentations

- a. Staff presentation (5 minutes maximum*)
- b. Applicant presentation (15 minute maximum*). Any exhibits introduced into the record at this time must be provided in quantities of ten (10) to the Board Secretary for distribution to the Board and admission into the record.
- c. Planning Board questioning of staff or applicant for information only.

2. Public Hearing

Each speaker will be allowed an oral presentation (3 minutes maximum*). All speakers wishing to pool their time must be present, and time allotted will be determined by the Chair. No pooled time presentation will be permitted to exceed ten minutes total.

- Time remaining is presented by a Green blinking light that means one minute remains, a Yellow light means 30 seconds remain, and a Red light and beep means time has expired.
- Speakers should introduce themselves, giving name and address. If officially representing a group, homeowners' association, etc., please state that for the record as well.
- Speakers are requested not to repeat items addressed by previous speakers other than to express points of agreement or disagreement. Refrain from reading long documents, and summarize comments wherever possible. Long documents may be submitted and will become a part of the official record.
- Speakers should address the Land Use Regulation criteria and, if possible, reference the rules that the Board uses to decide a case.
- Any exhibits introduced into the record at the hearing must be provided in quantities of ten (10) to the Secretary for distribution to the Board and admission into the record.
- Citizens can send a letter to the Planning staff at 1739 Broadway, Boulder, CO 80302, two weeks before the Planning Board meeting, to be included in the Board packet. Correspondence received after this time will be distributed at the Board meeting.

3. Board Action

- d. Board motion. Motions may take any number of forms. With regard to a specific development proposal, the motion generally is to either approve the project (with or without conditions), to deny it, or to continue the matter to a date certain (generally in order to obtain additional information).
- e. Board discussion. This is undertaken entirely by members of the Board. The applicant, members of the public or city staff participate only if called upon by the Chair.
- f. Board action (the vote). An affirmative vote of at least four members of the Board is required to pass a motion approving any action. If the vote taken results in either a tie, a vote of three to two, or a vote of three to one in favor of approval, the applicant shall be automatically allowed a rehearing upon requesting the same in writing within seven days.

MATTERS FROM THE PLANNING BOARD, DIRECTOR, AND CITY ATTORNEY

Any Planning Board member, the Planning Director, or the City Attorney may introduce before the Board matters which are not included in the formal agenda.

ADJOURNMENT

The Board's goal is that regular meetings adjourn by 10:30 p.m. and that study sessions adjourn by 10:00 p.m. Agenda items will not be commenced after 10:00 p.m. except by majority vote of Board members present.

*The Chair may lengthen or shorten the time allotted as appropriate. If the allotted time is exceeded, the Chair may request that the speaker conclude his or her comments.

MEMORANDUM

TO: Planning Board
FROM: Chandler Van Schaack, Case Manager
DATE: January 22, 2015
SUBJECT: **Call Up Item:** Site Review Minor Amendment (LUR2014-00031) and Final Plat (TEC2014-00031): Request to to subdivide one 32,510 sq. ft. developed lot within the Carrie Subdivision PUD located at 593 Lee Hill Rd. into to three new residential lots. Call-up expires **January 23, 2015**.

Background. The 32,510 sq. ft. project site is located in North Boulder at the northwest corner of the intersection of Lee Hill Dr. and 6th St., as shown below in **Figure 1**. The site is zoned RL-2 (Residential – Low 2), which is defined as “Medium density residential areas primarily used for small-lot residential development, including without limitation, duplexes, triplexes, or townhouses, where each unit generally has direct access at ground level” per section 9-5-2(c)(1)(B), B.R.C. 1981. The site is located within the Carrie Subdivision PUD, which was originally approved in 2002 as an 18-lot subdivision and PUD for single family residential development. At that time, the subject lot contained a single-family dwelling constructed in 1963; however, the dwelling unit has since been demolished and the site is currently vacant. The original PUD approval created building envelopes for future development and allowed for the aggregation of the required open space; however, there were no other development standards or design guidelines included with the approval, thereby allowing for the lots to essentially be developed “by-right” under the RL-2 zone district standards.



Figure 1: Vicinity Map

Currently, the character of the area surrounding the subject site in general consists of large, traditional style single family detached homes with attached garages most representative of construction in the 1990s and early 2000s. The homes within the Carrie Subdivision that lie to the north along 6th Street range in size from approximately 4,200 to 6,800 square feet in floor area, including garage space, based on city permit records. The Northbriar Estates subdivision lies to the south of the site, and consists of single family detached homes ranging from approximately

2,900 to 6,700 square feet in area. The Dakota Ridge East and Dakota Ridge West developments are located to the east, on the north side of Lee Hill Drive, and are characterized by a variety of housing types with attached garages. The homes in Dakota Ridge East and Dakota Ridge West range from approximately 1,770 to 6,060 square feet in area. Dakota Ridge Village, located north of these developments, is a mixed density residential development with a diversity of housing types, including apartment, townhomes, and single family homes. The area to the west of the subject site is a mix of very low-density residential, agricultural and open space uses, including the Four Mile Creek Trailhead lying immediately across Lee Hill Drive.

Proposed Project. The current proposal is to subdivide the existing 32,510 sq. ft. lot at 593 Lee Hill to create three new single-family residential lots: Lot 19 (9,917 sq. ft.), Lot 20 (11,581 sq. ft.), and Lot 21 (11,014 sq. ft.). No modifications to the land use regulations are proposed as part of this development. The future homes have not yet been designed; however, development of the proposed lots would be subject to detailed design guidelines found in [Attachment C](#), the intent of which is to ensure high quality design standards consistent with the “modern craftsman” style typical of surrounding and adjacent single-family homes. While the Compatible Development standards do not apply to properties zoned RL-2 that are within an existing PUD, the applicant has proposed to limit floor area to 4,900 sq. ft. per lot in order to be consistent with the surrounding residences. In addition, the Design Guidelines contain requirements pertaining to building materials, roofs, architectural elements, landscaping and fences that will help to ensure high quality development consistent with the Site Review criteria. All new homes will be required to meet the city residential “[Green Points](#)” program. Refer to [Attachment C](#) for the Applicant’s proposed plans and design guidelines.

Public Comment. Required public notice was provided in the form of written notifications to property owners within 600 feet of the subject property. In addition, a public notice sign was posted on the property and therefore, all public notice requirements of section 9-4-3, “*Public Notice Requirements*,” B.R.C. 1981 were met. Staff fielded questions from one neighbor and comments from another neighbor opposed to the proposed subdivision due to concerns about potential on-street parking impacts. The Carrie Court HOA, which is the homeowner’s association for the Carrie Subdivision, has indicated support for the proposed project.

Project Analysis/ Conclusion. Staff finds that this application is consistent with the intent of the Subdivision standards found in Chapter 9-12, B.R.C. 1981 and meets all applicable Final Plat criteria set forth in section 9-12-8(b), B.R.C. 1981. Staff has reviewed the plat and determined that the proposed subdivision meets all applicable zoning standards as well as the “Standards for Lots and Public Improvements” as set forth in section 9-12-12, B.R.C. The proposal was also found to be consistent with the criteria for Minor Amendments to Approved Site Plans found in section 9-2-14(l), B.R.C. 1981. Please refer to [Attachment B](#) for staff’s complete analysis of the review criteria. This proposal was approved by Planning and Development Services staff on **January 9, 2015** (see [Attachment A](#)) and the decision may be called up before Planning Board on or before **January 23, 2015**. There is one Planning Board meeting within the 14-day call up period, on **January 22, 2015**. Questions about the project or decision should be directed to Chandler Van Schaack at (303) 441-3137 or vanschaackc@bouldercolorado.gov.

Attachments

- A. [Signed Dispositions](#)
- B. [Analysis of Review Criteria](#)
- C. [Applicant’s Proposed Plans](#)



CITY OF BOULDER
Community Planning and Sustainability

1739 Broadway, Third Floor • P.O. Box 791, Boulder, CO 80306-0791
 phone 303-441-1880 • fax 303-441-3241 • www.bouldercolorado.gov

CITY OF BOULDER PLANNING DEPARTMENT
NOTICE OF DISPOSITION

You are hereby advised that the following action was taken by the Planning Department based on the standards and criteria of the Land Use Regulations as set forth in Chapter 9-2, B.R.C. 1981, as applied to the proposed development.

DECISION: **Approved with Conditions**
 PROJECT NAME: **Carrie Subdivision Site Review Minor Amendment**
 DESCRIPTION: **SITE REVIEW to allow for the replat of Lot 14, Carrie Subdivision Filing No. 2 into three lots (Lots 19, 20, and 21). This is an amendment to Site Review #LUR2001-00036. See TEC2014-00031 for the associated subdivision.**
 LOCATION: **593 Lee Hill Dr.**
 COOR: **N09W07**
 LEGAL DESCRIPTION: **Lot 14, Carrie Subdivision Filing No. 2, County of Boulder, State of Colorado**
 APPLICANT: **Michael Bosma**
 OWNER: **Lee Hill Investment Group, LLC**
 APPLICATION: **LUR2014-00031**
 ZONING: **RL-2**
 CASE MANAGER: **Chandler Van Schaack**
 VESTED PROPERTY RIGHT: **NO; the owner has waived the opportunity to create such right under Section 9-2-19, B.R.C. 1981.**

FOR CONDITIONS OF APPROVAL, SEE THE FOLLOWING PAGES OF THIS DISPOSITION.

Approved on: _____

Date

By: _____

David Driskell, Executive Director of Community Planning and Sustainability

This decision may be appealed to the Planning Board by filing an appeal letter with the Planning Department within two weeks of the decision date. If no such appeal is filed, the decision shall be deemed final fourteen days after the date above mentioned.

Appeal to Planning Board expires: _____

IN ORDER FOR A BUILDING PERMIT APPLICATION TO BE PROCESSED FOR THIS PROJECT, A SIGNED DEVELOPMENT AGREEMENT AND FINAL PLANS FOR CITY SIGNATURE MUST BE SUBMITTED TO THE PLANNING DEPARTMENT WITH DISPOSITION CONDITIONS AS APPROVED SHOWN ON THE FINAL PLANS, IF THE DEVELOPMENT AGREEMENT IS NOT SIGNED WITHIN NINETY (90) DAYS OF THE FINAL DECISION DATE, THE PLANNING DEPARTMENT APPROVAL AUTOMATICALLY EXPIRES.

Pursuant to Section 9-2-12 of the Land Use Regulations (Boulder Revised Code, 1981), the applicant must begin and substantially complete the approved development within three years from the date of final approval. Failure to "substantially complete" (as defined in Section 9-2-12) the development within three years shall cause this development approval to expire.

CONDITIONS OF APPROVAL

1. The Applicant shall ensure that the **development shall be in compliance with all approved plans** dated October 6, 2014 and the Design Guidelines for Carrie Subdivision Filing No. 2 Replat A dated October 6, 2014 on file in the City of Boulder Planning Department, except to the extent that the development may be modified by the conditions of this approval.
2. The Applicant shall **comply with all previous conditions** contained in any previous approvals, except to the extent that any previous conditions may be modified by this approval, including, but not limited to, the following recorded in the records of the Boulder County Clerk and Recorder:
 - Development Agreement recorded May 24, 2002 at Reception No. 2291030;
 - Subdivision Agreement recorded May 24, 2002 at Reception No. 2291029; and
 - Subdivision Agreement recorded August 1, 2003 at Reception No. 2481602.
3. Prior to issuance of a building permit, the Applicant shall submit a Technical Document Review application for the following items, subject to the approval of the City Manager:
 - a. A **final storm water report and plan**, including Grading and Drainage and Erosion Control plans, meeting the City of Boulder Design and Construction Standards.
 - b. A **detailed landscape plan**, including size, quantity, and type of plants existing and proposed; type and quality of non-living landscaping materials; any site grading proposed; and any irrigation system proposed, to insure compliance with this approval and the City's landscaping requirements. Removal of trees must receive prior approval of the Planning Department. Removal of any tree in City right of way must also receive prior approval of the City Forester.
4. Prior to approval of **Carrie Subdivision Filing No. 2 Replat A (Technical Document Review application TEC2014-00031)**, the Applicant shall ensure that Outlot A, Carrie Subdivision, City of Boulder, County of Boulder, State of Colorado is conveyed into the ownership of Carrie Subdivision Owners Association.
5. This approval is **contingent upon approval Carrie Subdivision Filing No. 2 Replat A** (Technical Document Review application TEC2014-00031).



CITY OF BOULDER
Community Planning and Sustainability

1739 Broadway, Third Floor • P.O. Box 791, Boulder, CO 80306-0791
phone 303-441-1880 • fax 303-441-3241 • web www.bouldercolorado.gov

CITY OF BOULDER PLANNING DEPARTMENT
NOTICE OF DISPOSITION

You are hereby advised that the following action was taken by the Planning Department based on the standards and criteria of the Land Use Regulations as set forth in Section 9-12, B.R.C. 1981, as applied to the proposed development.

DECISION: **Approved with Condition**
PROJECT NAME: **Carrie Subdivision Filing No. 2 Replat A**
DESCRIPTION: **Final Plat for the replat of Lot 14, Carrie Subdivision Filing No. 2 into three lots (Lots 19, 20, and 21).**
COOR: **N09W07**
LEGAL DESCRIPTION: **Lot 14, Carrie Subdivision Filing No. 2, County of Boulder, State of Colorado**
APPLICANT: **Michael Bosma**
OWNER: **Lee Hill**
APPLICATION: **TEC2014-00031**
ZONING: **RL-2**
CASE MANAGER: **Chandler Van Schaack**

THIS IS NOT A SITE SPECIFIC DEVELOPMENT PLAN APPROVAL AND NO VESTED PROPERTY RIGHT IS CREATED BY THIS APPROVAL.

Approved On: 1.9.15
Date

By: 
David Driskell, Executive Director of Community Planning & Sustainability

This decision may be appealed to the Planning Board by filing an appeal letter with the Planning Department within two weeks of the decision date. If no such appeal is filed, the decision shall be deemed final fourteen days after the date above mentioned.

Appeal to Planning Board expires: 1.23.15

1. The subdivision is approved subject to the terms of the Subdivision Agreement.

Case #: LUR2014-00031

Project Name: 593 Lee Hill

Date: January 22, 2015

CRITERIA FOR REVIEW

No site review application shall be approved unless the approving agency finds that:

(1) Boulder Valley Comprehensive Plan:

✓ (A) The proposed site plan is consistent with the land use map and the service area map and, on balance, the policies of the Boulder Valley Comprehensive Plan.

The proposed project is a low-density residential development. The parcel is zoned RL-2, and has an underlying land use designation of Low Density Residential.

Additional BVCP policies that the proposed project is consistent with include:

2.10 Preservation and Support for Residential Neighborhoods

2.30 Sensitive Infill and Redevelopment

✓ (B) The proposed development shall not exceed the maximum density associated with the Boulder Valley Comprehensive Plan residential land use designation. Additionally, if the density of existing residential development within a three-hundred-foot area surrounding the site is at or exceeds the density permitted in the Boulder Valley Comprehensive Plan, then the maximum density permitted on the site shall not exceed the lesser of:

The subject property has a BVCP land use designation of Low Density Residential, which anticipates a density of two to six units per acre. The current proposal is to subdivide the existing 32,510 square foot (.75-acre) lot into three new lots for single family residential development. The proposed project is therefore in keeping with the underlying land use designation for the site. The existing residential development within three hundred feet of the site is also detached single family with an underlying land use designation of Low Density Residential, and is therefore in keeping with density permitted by the underlying land use designation.

N/A (i) The density permitted in the Boulder Valley Comprehensive Plan, or,

N/A (ii) The maximum number of units that could be placed on the site without waiving or varying any of the requirements of [chapter 9-8](#), "Intensity Standards," B.R.C. 1981.

✓ (C) The proposed development's success in meeting the broad range of BVCP policies considers the economic feasibility of implementation techniques require to meet other site review criteria.

The proposed project sensitively utilizes an infill site in providing an appropriate addition to the established residential uses to the north and east. This is achieved by context-sensitive design guidelines that will ensure compatibility with the surrounding area. The use and density are consistent with the BVCP plan, meet housing needs, and utilizes an infill site where utilities, roads, and other infrastructure exist.

(2) Site Design: Projects should preserve and enhance the community's unique sense of place through creative design that respects historic character, relationship to the natural environment, multi-modal transportation connectivity and its physical setting. Projects should utilize site design techniques which are consistent with the purpose of site review in subsection (a) of this section and enhance the quality of the project. In determining whether this subsection is met, the approving agency will consider the following factors:

✓ **(A) Open Space:** Open space, including, without limitation, parks, recreation areas, and playgrounds:

✓ **(i)** Useable open space is arranged to be accessible and functional and incorporates quality landscaping, a mixture of sun and shade and places to gather;

The proposed development is three single family lots. The development is subject to a set of Design Guidelines intended to allow for flexibility of design while ensuring a high quality design outcome. In terms of open space, the design guidelines include a requirement that "Rear yard living areas shall be incorporated in the form of wood decks, concrete, stone, or paver patios, courtyards, etc. Components, materials, and colors shall be integral to the overall house design and be of a durable lasting material."

✓ **(ii)** Private open space is provided for each detached residential unit;

The proposed site plan includes building envelopes that are consistent with the underlying zone district as well as a FAR limitation of 0.45 for each unit. These standards, as well as the requirement to provide rear yard living area as described in the response above, will ensure that each single family lot has ample private open space.

N/A **(iii)** The project provides for the preservation of or mitigation of adverse impacts to natural features, including, without limitation, healthy long-lived trees, significant plant communities, ground and surface water, wetlands, riparian areas, drainage areas and species on the federal Endangered Species List, "Species of Special Concern in Boulder County" designated by Boulder County, or prairie dogs (*Cynomys ludovicianus*), which is a species of local concern, and their habitat;

Not applicable, as the existing site is currently developed with a single family residence, and does not contain any significant natural features.

✓ **(iv)** The open space provides a relief to the density, both within the project and from surrounding development;

Each unit will have landscaped open space on all sides of the main structure consistent with the underlying zoning requirements. In addition, it is worth noting that the units are directly across Lee Hill Dr. from City of Boulder open space and the Foothills South trail. The on-site open space, the low density character of the surrounding area and the

presence of open space nearby will all serve to provide a relief from any perceived density within the development.

N/A (v) Open space designed for active recreational purposes is of a size that it will be functionally useable and located in a safe and convenient proximity to the uses to which it is meant to serve;

Not applicable, as the open space within the proposed development is intended primarily for passive recreational uses.

N/A (vi) The open space provides a buffer to protect sensitive environmental features and natural areas; and

Not applicable, as the area surrounding the subject site is already developed.

✓ (vii) If possible, open space is linked to an area- or city-wide system.

While not directly linked, the proposed development is immediately adjacent to City of Boulder open space and the South Foothills trail.

N/A (B) Open Space in Mixed Use Developments (Developments that contain a mix of residential and non-residential uses)

Not applicable, as the proposed development will consist of three single family residential units only, and will not contain a mix of other uses.

N/A (i) The open space provides for a balance of private and shared areas for the residential uses and common open space that is available for use by both the residential and non-residential uses that will meet the needs of the anticipated residents, occupants, tenants, and visitors of the property; and

N/A (ii) The open space provides active areas and passive areas that will meet the needs of the anticipated residents, occupants, tenants, and visitors of the property and are compatible with the surrounding area or an adopted plan for the area.

___(C) Landscaping

✓ (i) The project provides for aesthetic enhancement and a variety of plant and hard surface materials, and the selection of materials provides for a variety of colors and contrasts and the preservation or use of local native vegetation where appropriate;

Eight street trees are provided in the landscape strip along Lee Hill, with additional street trees to complement the landscaping on the east and west borders along 6th and 5th Street, respectively. The design guidelines also include a requirement that all homeowners provide a professional landscape plan at time of building permit to insure appropriate landscaping on each lot.

N/A (ii) Landscape design attempts to avoid, minimize, or mitigate impacts to important native species, plant communities of special concern, threatened and endangered species and habitat by integrating the existing natural environment into the project;

Not applicable, as the subject lot and surrounding area are already developed as single family residential, and as such does not contain any sensitive environmental features, species or natural areas.

✓ (iii) The project provides significant amounts of plant material sized in excess of the landscaping requirements of [sections 9-9-12](#), "Landscaping and Screening Standards" and [9-9-13](#), "Streetscape Design Standards," B.R.C. 1981; and

Landscaping in the proposed development is proposed to meet the current landscape requirements. In addition, given the proposed building envelopes and FAR limitations, each lot will exceed the required amount of open space, which will provide the opportunity for additional landscaping beyond the minimum code requirements.

✓ (iv) The setbacks, yards, and useable open space along public rights-of-way are landscaped to provide attractive streetscapes, to enhance architectural features, and to contribute to the development of an attractive site plan.

As discussed above, new street trees are proposed along all public frontages, and homeowners are required to provide a landscape plan at the time of building permit per the proposed design guidelines.

(D) Circulation: Circulation, including, without limitation, the transportation system that serves the property, whether public or private and whether constructed by the developer or not:

✓ (i) High speeds are discouraged or a physical separation between streets and the project is provided;

Each lot within the project is separated from surrounding streets by a landscaped strip with street trees. No new streets are proposed as part of this project.

✓ (ii) Potential conflicts with vehicles are minimized;

Units are separated from existing streets via new landscaping as well as a new sidewalk along Lee Hill Dr. Site access will be required to meet city of boulder design and construction standards regarding separation of access points and minimum sight triangles.

✓ (iii) Safe and convenient connections are provided that support multi-modal mobility through and between properties, accessible to the public within the project and between the project and the existing and proposed transportation systems, including, without limitation, streets, bikeways, pedestrianways and trails;

A new sidewalk is proposed along Lee Hill Dr., which will facilitate pedestrian travel past the site where currently no sidewalk exists. There are no adopted trails or multi-use

connections through the subject property, so no additional new connections through the property are required.

✓ (iv) Alternatives to the automobile are promoted by incorporating site design techniques, land use patterns, and supporting infrastructure that supports and encourages walking, biking, and other alternatives to the single-occupant vehicle;

A new sidewalk is proposed along Lee Hill Dr., which will facilitate pedestrian travel past the site where currently no sidewalk exists.

N/A (v) Where practical and beneficial, a significant shift away from single-occupant vehicle use to alternate modes is promoted through the use of travel demand management techniques;

Not applicable. The proposed project is located within an existing developed low-density single family residential area and will meet city of boulder parking standards, therefore, a TDM plan is not required.

✓ (vi) On-site facilities for external linkage are provided with other modes of transportation, where applicable;

A new sidewalk is proposed along Lee Hill Dr., which will facilitate pedestrian travel past the site where currently no sidewalk exists.

✓ (vii) The amount of land devoted to the street system is minimized; and

No new streets are proposed as part of this development.

✓ (viii) The project is designed for the types of traffic expected, including, without limitation, automobiles, bicycles, and pedestrians, and provides safety, separation from living areas, and control of noise and exhaust.

The site is served by existing roadways, and will provide a new sidewalk along Lee Hill Dr. to facilitate pedestrian movement to and across the site. Separation of living areas will be achieved by the minimum landscaped setbacks provided for in the site plan.

(E) **Parking**

✓ (i) The project incorporates into the design of parking areas measures to provide safety, convenience, and separation of pedestrian movements from vehicular movements;

Parking is anticipated to be provided via attached or detached garages which will be set back from the street.

✓ (ii) The design of parking areas makes efficient use of the land and uses the minimum amount of land necessary to meet the parking needs of the project;

Parking will be provided via individual garages for each of the three detached units, and will be required to meet the FAR limitations and setbacks for the underlying zone.

✓ (iii) Parking areas and lighting are designed to reduce the visual impact on the project, adjacent properties, and adjacent streets; and

Garages will be subject to the design guidelines, which require that they be architecturally compatible with the principal structure and constructed of high quality materials.

N/A (iv) Parking areas utilize landscaping materials to provide shade in excess of the requirements in Subsection [9-9-6](#) (d), "Parking Area Design Standards," and Section [9-9-14](#), "Parking Lot Landscaping Standards," B.R.C. 1981.

Not Applicable, as the above-referenced landscape standards do not apply to single family residential uses.

(F) Building Design, Livability, and Relationship to the Existing or Proposed Surrounding Area

✓ (i) The building height, mass, scale, orientation, and configuration are compatible with the existing character of the area or the character established by an adopted plan for the area;

While there is an existing Site Review approval associated with the Carrie Subdivision, the previous approval did not include design of the buildings, and did not establish any design guidelines to shape development in the area. As such, the other lots within the Carrie Subdivision have been developed without any architectural controls, and thus represent a variety of styles and sizes. In order to ensure compatibility with the surrounding area in terms of massing, the applicant has included lot size, floor area and FAR data for 11 homes in the immediate vicinity, and has determined that the average floor area is 5,327 square feet with an average FAR of 0.67. The applicant has limited the size of each of the new homes to 4,900 square feet, which generates an average FAR of 0.45 for the three new homes. This approach ensures that the new homes will be appropriately scaled while remaining on the smaller side of existing home sizes in the surrounding area. Also included in the design guidelines are precedent images of homes in the surrounding vicinity as well as homes constructed by the applicant that are in keeping with the intent of the guidelines. All of the proposed homes will meet the zoning standards in terms of building height and setbacks, which is consistent with the other homes in the surrounding area.

✓ (ii) The height of buildings is in general proportion to the height of existing buildings and the proposed or projected heights of approved buildings or approved plans for the immediate area;

The proposed homes will not exceed the 35' height limitation for the RL-2 zone district, which is consistent with the surrounding by-right single-family context.

✓ (iii) The orientation of buildings minimizes shadows on and blocking of views from adjacent properties;

The new homes will be required to meet city of Boulder solar access standards for Solar Area I and will be evaluated for compliance with the code at the time of building permit submittal; thus, shading will be minimized to the extent required by the land use code. The building envelopes and height will meet by-right standards, ensuring that views are also protected.

✓ (iv) If the character of the area is identifiable, the project is made compatible by the appropriate use of color, materials, landscaping, signs, and lighting;

The character of the area can be identified as primarily large, modern craftsman-style single family homes, although many exceptions to this exist throughout the neighborhood. The overall mix is quite eclectic, as the surrounding homes were developed by-right and thus were not subject to any uniform design guidelines. In order to ensure general compatibility with the surrounding area, the applicant has included precedent images in the design guidelines of nearby homes, and has included standards regarding architecture and building materials, building height, building and roof forms and FAR.

✓ (v) Projects are designed to a human scale and promote a safe and vibrant pedestrian experience through the location of building frontages along public streets, plazas, sidewalks and paths, and through the use of building elements, design details and landscape materials that include, without limitation, the location of entrances and windows, and the creation of transparency and activity at the pedestrian level;

The proposed design guidelines include standards intended to encourage the creation of transparency and activity at the pedestrian level, including the following language: "Homes shall be oriented with their front yards, front porches, and front doors visible from the street. Front porches per Sec. 9-7-4 B.R.C. 1981 are encouraged." The guidelines also include minimum requirements with regards to building materials and architectural elements in order to encourage four-sided architectural interest.

✓ (vi) To the extent practical, the project provides public amenities and planned public facilities;

The project provides for a new sidewalk along Lee Hill Dr. No other public facilities are planned for the subject site and thus none are provided.

✓ (vii) For residential projects, the project assists the community in producing a variety of housing types, such as multifamily, townhouses and detached single family units, as well as mixed lot sizes, number of bedrooms and sizes of units;

The proposed project is comprised of three detached single family homes, and thus will add to the overall variety of housing types within the city.

✓ (viii) For residential projects, noise is minimized between units, between buildings, and from either on-site or off-site external sources through spacing, landscaping, and building materials;

The proposed project will meet the underlying zoning district standards with regards to building setbacks and landscaping. The size of the proposed lots will ensure that there is adequate space between the homes to minimize noise.

✓ (ix) A lighting plan is provided which augments security, energy conservation, safety, and aesthetics;

Lighting plans will be required for each of the new homes as they are developed to ensure compliance with city outdoor lighting standards.

N/A (x) The project incorporates the natural environment into the design and avoids, minimizes, or mitigates impacts to natural systems;

Not applicable, as the subject site and surrounding area are already fully developed.

✓ (xi) Buildings minimize or mitigate energy use; support on-site renewable energy generation and/or energy management systems; construction wastes are minimized; the project mitigates urban heat island effects; and the project reasonably mitigates or minimizes water use and impacts on water quality.

The new homes will be required to meet the 2012 IBC building and energy code requirements as well as the city's residential "Green Points" program.

✓ (xii) Exteriors or buildings present a sense of permanence through the use of authentic materials such as stone, brick, wood, metal or similar products and building material detailing;

The design guidelines include standards to ensure high quality building materials are used in the construction of the three new homes. Specifically, the standards state: "Exterior wall finishes will be of a high quality that is durable and long lasting. The maximum numbers of finish materials will be limited to four. Stucco will be used as an accent material and not the primary building material."

✓ (xiii) Cut and fill are minimized on the site, the design of buildings conforms to the natural contours of the land, and the site design minimizes erosion, slope instability, landslide, mudflow or subsidence, and minimizes the potential threat to property caused by geological hazards;

The site is already developed as a single family home, and thus has already been graded. The proposed project does not include significant grading changes, and will maintain the historic drainage pattern across the site.

N/A (xiv) In the urbanizing areas along the Boulder Valley Comprehensive Plan boundaries between Area II and Area III, the building and site design provide for a well-defined urban edge; and

N/A (xv) In the urbanizing areas located on the major streets shown on the map in Appendix A of this title near the Boulder Valley Comprehensive Plan boundaries between Area II and Area III, the buildings and site design establish a sense of entry and arrival to the City by creating a defined urban edge and a transition between rural and urban areas.

(G) Solar Siting and Construction: For the purpose of ensuring the maximum potential for utilization of solar energy in the City, all applicants for residential site reviews shall place streets, lots, open spaces, and buildings so as to maximize the potential for the use of solar energy in accordance with the following solar siting criteria:

✓ (i) *Placement of Open Space and Streets:* Open space areas are located wherever practical to protect buildings from shading by other buildings within the development or from buildings on adjacent properties. Topography and other natural features and constraints may justify deviations from this criterion.

Streets are already existing. All of the proposed buildings will meet the solar access protection and solar siting requirements of [section 9-9-17](#), "Solar Access," B.R.C. 1981 to allow for renewables

✓ (ii) *Lot Layout and Building Siting:* Lots are oriented and buildings are sited in a way which maximizes the solar potential of each principal building. Lots are designed to facilitate siting a structure which is unshaded by other nearby structures. Wherever practical, buildings are sited close to the north lot line to increase yard space to the south for better owner control of shading.

The three lots are well oriented to maximize solar potential, with the two of the lots (Lots 20 & 21) being located immediately north of Lee Hill Dr and thus having unobstructed southern exposure. In addition, due to the unique shape of the existing lot, the shapes of Lots 19 (northeast side of project) and 20 (south of Lot 19) are irregular and therefore allow for homes to be staggered to the east or west to maximize southern exposure.

✓ (iii) *Building Form:* The shapes of buildings are designed to maximize utilization of solar energy. Buildings shall meet the solar access protection and solar siting requirements of [section 9-9-17](#), "Solar Access," B.R.C. 1981.

All of the proposed buildings will meet the solar access protection and solar siting requirements of [section 9-9-17](#), "Solar Access," B.R.C. 1981.

✓ (iv) *Landscaping:* The shading effects of proposed landscaping on adjacent buildings are minimized.

All of the proposed buildings will meet the solar access protection and solar siting requirements of [section 9-9-17](#), "Solar Access," B.R.C. 1981.

N/A (H) Additional Criteria for Poles Above the Permitted Height: No site review application for a pole above the permitted height will be approved unless the approving agency finds all of the following:

Not applicable, as no request for a pole above the permitted height is included with this proposal.

N/A (I) Land Use Intensity Modifications:

Not applicable.

N/A (J) Additional Criteria for Floor Area Ratio Increase for Buildings in the BR-1 District:

Not applicable.

N/A (K) Additional Criteria for Parking Reductions: The off-street parking requirements of [section 9-9-6](#), "Parking Standards," B.R.C. 1981, may be modified as follows:

Not applicable.

N/A (L) Additional Criteria for Off-Site Parking: The parking required under [section 9-9-6](#), "Parking Standards," B.R.C. 1981, may be located on a separate lot if the following conditions are met:

Not applicable.

Section 9-12-12, B.R.C. 1981

Standards for Lots and Public Improvements

(a) Conditions Required: Except as provided in subsection (b) of this section, subdivision plats shall comply with section 9-9-17, "Solar Access," B.R.C. 1981, and meet the following conditions:

(1) Standards for Lots: Lots meet the following conditions:

(A) Each lot has access to a public street.

Standard met.

(B) Each lot has at least thirty feet of frontage on a public street.

Standard met.

(C) No portion of a lot is narrower than thirty feet.

Standard met.

(D) Lots meet all applicable zoning requirements of this title and section 9-9-17, "Solar Access," B.R.C. 1981.

The project site is located in the RL-2 zone district, which requires a minimum of 6,000 sq. ft. of open space per dwelling unit. The proposed lots are between 9,917 sq. ft. and 11,581 sq. ft. in size, which will allow ample room for a single dwelling unit to meet the minimum open space requirements. In addition, while there are no FAR requirements for the project site, the applicant has voluntarily restricted FAR on the subject lots to 0.45 to ensure consistency with the surrounding properties. No modifications to the land use regulations have been requested, so all of the RL-2 zoning standards will apply at time of building permit for each of the three lots. Standard met.

(E) Lots with double frontage are avoided, except where necessary to provide separation from major arterials or incompatible land uses or because of the slope of the lot.

The existing 32,510 sq. ft. lot is irregularly shaped and has three frontages, on 5th Street, 6th Street and Lee Hill Rd. Any subdivision of the existing lot would require the new lots to have more than one frontage. Lots 20 and 21 are corner lots, with frontage on Lee Hill Dr. and 6th Street and Lee Hill Dr. and 5th Street, respectively. Lot 19 has only one frontage on 6th St. Each of the lots will be required to take access off the lowest category street.

(F) Side lot lines are substantially at right angles or radial to the centerline of streets, whenever feasible.

The existing lot is irregularly shaped, so it is not feasible to subdivide using right angles; however, the proposed configuration is logical and simple. Standard met.

(G) Corner lots are larger than other lots to accommodate setback requirements of section 9-7-1, "Schedule of Form and Bulk Standards," B.R.C. 1981.

Standard met. Lots 20 and 21, the two corner lots, are proposed to be 11,581 sq. ft. and 11,014 sq. ft. , respectively, while Lot 19 is proposed to be 9, 917 sq. ft. in size.

(H) Residential lots are shaped so as to accommodate a dwelling unit within the setbacks prescribed by the zoning district.

Standard met. The applicant has demonstrated that the proposed new lots are large enough to meet or exceed all applicable setbacks prescribed by the zoning district.

(I) Lots shall not be platted on land with a ten percent or greater slope, unstable land, or land with inadequate drainage unless each platted lot has at least one thousand square feet of buildable area, with a minimum dimension of twenty-five feet. The city manager may approve the platting of such land upon finding that acceptable measures, submitted by a registered engineer qualified in the particular field, eliminate or control the problems of instability or inadequate drainage.

Standard met. Each lot has at least one thousand square feet of buildable area, and a preliminary drainage and grading plan has been approved by staff.

(J) Where a subdivision borders an airport, a railroad right-of-way, a freeway, a major street, or any other major source of noise, the subdivision is designed to reduce noise in residential lots to a reasonable level and to retain limited access to such facilities by such measures as a parallel street, a landscaped buffer area, or lots with increased setbacks.

Standard met. None of the uses described above is associated with this subdivision.

(K) Each lot contains at least one deciduous street tree of two-inch caliper in residential subdivisions, and each corner lot contains at least one tree for each street upon which the lot fronts, located so as not to interfere with sight distance at driveways and chosen from the list of acceptable trees established by the city manager, unless the subdivision agreement provides that the subdivider will obtain written commitments from subsequent purchasers to plant the required trees.

Standard met. The proposal includes adding new street trees in conformance with the land use regulations.

(L) The subdivider provides permanent survey monuments, range points, and lot pins placed by a Colorado registered land surveyor.

Standard met.

(M) Where an irrigation ditch or channel, natural creek, stream, or other drainage way crosses a subdivision, the subdivider provides an easement sufficient for drainage and maintenance.

Standard met.

(N) Lots are assigned street numbers by the city manager under the city's established house numbering system, and before final building inspection the subdivider installs numbers clearly visible and made of durable material.

Standard met.

(O) For the purpose of ensuring the potential for utilization of solar energy in the city, the subdivider places streets, lots, open spaces, and buildings so as to maximize the potential for the use of solar energy in accordance with the following solar siting criteria:

The three lots are well oriented to maximize solar potential, with the two of the lots (Lots 20 & 21) being located immediately north of Lee Hill Dr and thus having unobstructed southern exposure. In addition, due to the unique shape of the existing lot, the shapes of Lots 19 (northeast side of project) and 20 (south of Lot 19) are irregular and therefore allow for homes to be staggered to the east or west to maximize southern exposure.

(i) Placement of Open Space and Streets: Open space areas are located wherever practical to protect buildings from shading by other buildings within the development or from buildings on adjacent properties. Topography and other natural features and constraints may justify deviations from this criterion.

Standard met.

(ii) Lot Layout and Building Siting: Lots are oriented and buildings sited in a way which maximizes the solar potential of each principal building. Lots are designed so that it would be easy to site a structure which is unshaded by other nearby structures and so as to allow for owner control of shading. Lots also are designed so that buildings can be sited so as to maximize the solar potential of adjacent properties by minimizing off-site shading.

Standard met.

(iii) Building Form: The shapes of buildings are designed to maximize utilization of solar energy. Existing and proposed buildings shall meet the solar access protection and solar siting requirements of section 9-9-17, "Solar Access," B.R.C. 1981.

Standard met.

(iv) Landscaping: The shading impact of proposed landscaping on adjacent buildings is addressed by the applicant. When a landscape plan is required, the applicant shall indicate the plant type and whether the plant is coniferous or deciduous.

Standard met.

(2) Transportation Standards for Streets, Alleys, and Sidewalks: Streets, curb and gutters, sidewalks, alleys, and the public rights-of-way therefor, are provided in conformity with the standards in the City of Boulder Design and Construction Standards, and meet the following conditions:

There are no new streets proposed as part of this subdivision. However, the existing streets adjacent to the proposed subdivision currently meet all applicable City of Boulder Design and Construction Standards, and will continue to meet these standards following the proposed subdivision. The applicant will also be required to provide a new sidewalk along Lee Hill.

(A) Streets are aligned to join with planned or existing streets.

Standard met. No new streets are being constructed as part of this subdivision.

(B) Streets are designed to bear a relationship to the topography, minimizing grade, slope, and fill.

Standard met. No new streets are being constructed as part of this subdivision.

(C) There are no dead-end streets without an adequate turnaround and appropriate barriers.

Standard met. No new streets are being constructed as part of this subdivision.

(D) Access to freeway, arterial, or collector street occurs only at intersections approved by the city manager, if the manager finds that the access provides efficient traffic movement and safety for drivers and pedestrians.

Standard met. Access is to be taken off existing local streets only.

(E) A street of only one-half width is not dedicated to or accepted by the city.

Standard met. No new streets are being constructed as part of this subdivision.

(F) When the plat dedicates a street that ends on the plat or is on the perimeter of the plat, the subdivider conveys that last foot of the street on the terminal end or outside border of the plat to the city in fee simple, and it is designated by using an outlot.

Standard met. No new streets are being constructed as part of this subdivision.

(G) Streets are provided as prescribed by the Boulder Valley Comprehensive Plan, adopted subcommunity or area plans, or the Transportation Master Plan.

Standard met. No new streets are being constructed as part of this subdivision.

(H) Alleys are encouraged and should be provided. If they are provided, they are paved or otherwise appropriately surfaced with a material approved by the city manager for the specific application and location.

Standard met. No new alleys are being constructed as part of this subdivision.

(I) Sidewalks are provided in all subdivisions, unless the city manager determines that no public need exists for sidewalks in a certain location.

Standard met. A new sidewalk will be provided along Lee Hill Dr.

(J) Signs for street names (subject to approval of the city manager), directions, and hazards are provided.

Standard met. No new streets are being constructed as part of this subdivision.

(K) Traffic control signs are provided, as required by the city manager for control of traffic.

Standard met. No new streets are being constructed as part of this subdivision.

(L) Pedestrian crosswalks are provided, as required by the city manager for traffic control and, at a minimum, between streets where the distance between intersecting streets exceeds one thousand feet.

Standard met. No crosswalks will be required.

(M) Bike paths or lanes are provided in conformity with the City of Boulder Comprehensive Plan for bicycle facilities and are dedicated to the city.

Standard met. No bicycle lanes will be required.

(N) Private streets are not permitted.

Standard met. No private streets are being constructed as part of this subdivision.

(3) Standards for Water and Wastewater Improvements: Water and wastewater utilities are provided in conformity with the construction and design standards in the City of Boulder Design and Construction Standards, and meet the following conditions:

(A) Water and sanitary sewer mains are provided as necessary to serve the subdivision.

Standard met.

(B) Easements are provided for city utilities as prescribed by the City of Boulder Design and Construction Standards.

Standard met.

(C) Easements for utilities other than city utilities are provided as required by the applicable private utility.

Standard met.

(D) Newly installed telephone, electric, and cable television lines and other similar utility service are placed underground. Existing utilities are also placed underground unless the subdivider demonstrates to the manager that the cost substantially outweighs the visual benefit from doing so. But transformers, switching boxes, terminal boxes, meter cabinets, pedestals, ducts, electric transmission and distribution feeder lines, communication long distance trunk and feeder lines, and other facilities necessarily appurtenant to such facilities and to underground utilities may be placed above ground within dedicated easements or public rights-of-way.

Standard met.

(4) Standards for Flood Control and Storm Drainage: Flood control and storm drainage measures are provided as required by the city's master drainage plan and in conformity with the construction and design standards in the City of Boulder Design and Construction Standards, and meet the following conditions:

(A) The measures retain existing vegetation and natural features of the drainageway where consistent with the master drainage plan.

Standard met.

(B) Any land subject to flooding by a one hundred-year flood conforms to the requirements of chapter 11-5, "Storm Water and Flood Management Utility," B.R.C. 1981.

Standard met.

(C) Storm drainage improvements and storm sewers are maintained to collect drainage from the subdivision and convey it off-site into a city right of way or drainage system without adversely affecting adjacent property.

Standard met.

(D) Bridges, culverts, or open drainage channels are provided when required by the flood control utility master drainage plan.

Standard met.

(E) All subdivisions shall be designed to minimize flood damage.

Standard met.

(F) All subdivisions shall have public utilities and facilities, including, without limitation, sewer, gas, electrical, and water systems, located and constructed to prevent flood damage.

Standard met.

(G) All subdivisions shall have adequate drainage provided to reduce exposure to flood damage.

Standard met.

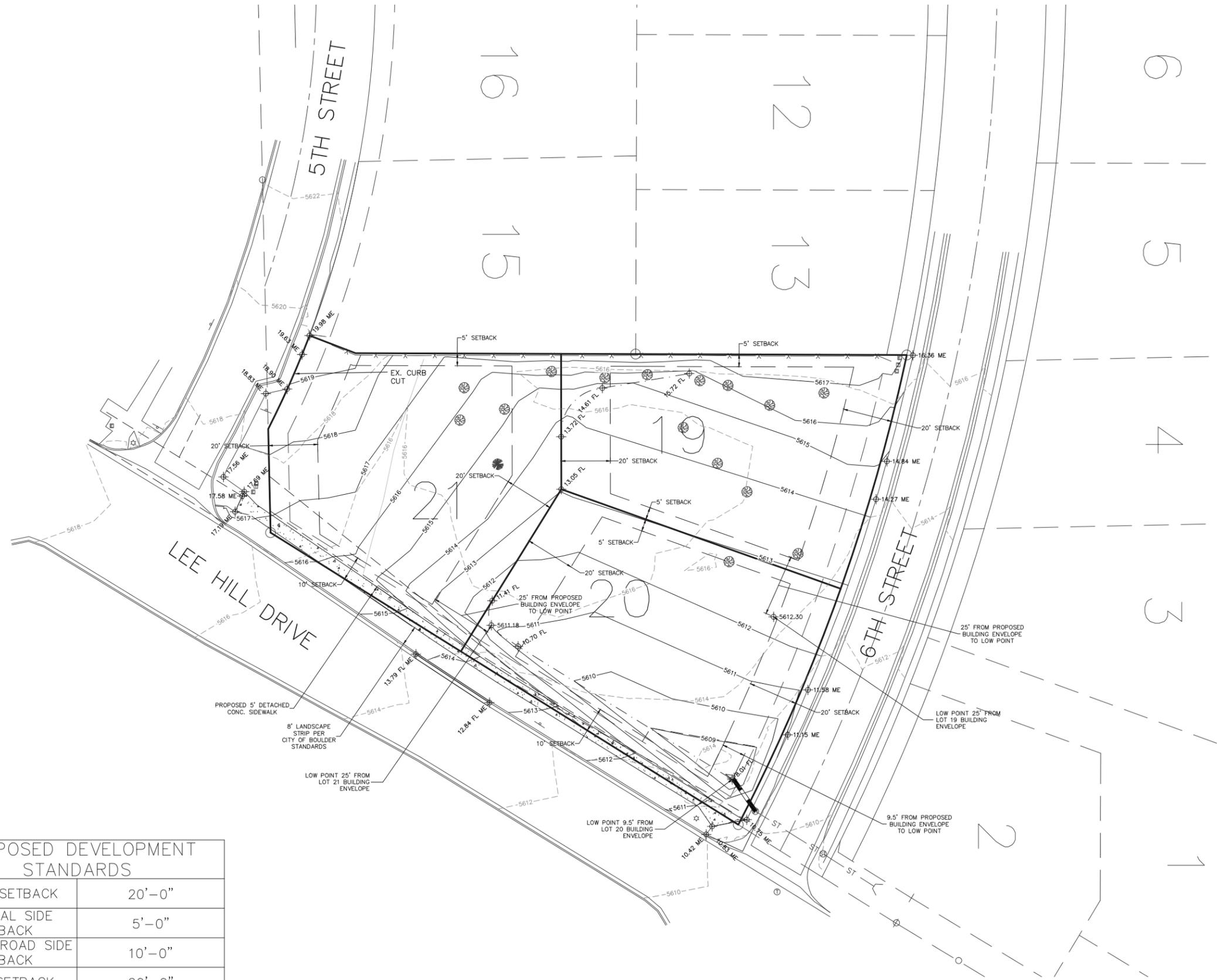
(5) Standards for Fire Protection: Fire protection measures meet the following conditions:

(A) Fire hydrants are provided as required by chapter 10-8, "Fire Prevention Code," B.R.C. 1981.

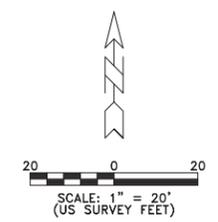
Standard met.

(B) Fire lanes are provided where necessary to protect the area; an easement at least sixteen feet wide for fire lanes is dedicated to the city, remains free of obstructions, and permits emergency access at all times.

Standard met.



PROPOSED DEVELOPMENT STANDARDS	
FRONT SETBACK	20'-0"
INTERNAL SIDE SETBACK	5'-0"
LEE HILL ROAD SIDE SETBACK	10'-0"
REAR SETBACK	20'-0"
BUILDING HEIGHT	35' MAX
OPEN SPACE	6,000 SF



ACCURATE Engisury
 A MANHARD CONSULTING DIVISION
 10155 WESTMOOR DRIVE, SUITE 120
 WESTMINSTER, COLORADO 80021
 (303)468-5505 FAX: (303)468-5160

No	Revisions	Date	By
6			
5			
4			
3			
2			
1			

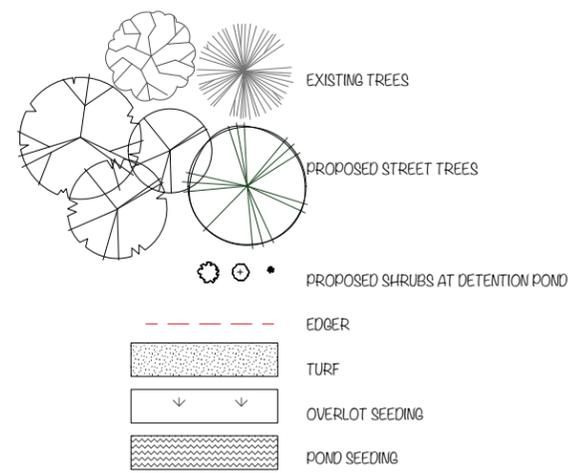
SITE DEVELOPMENT PLAN
 CARRIE SUBDIVISION FILING 2 REPLAT A
 CITY OF BOULDER, COLORADO

Date of Dwg:	10-6-2014
Drawn By:	AJD
Designed By:	DAC
Approved By:	DAC
Project No:	1AGRO201
Scale:	
Horiz:	AS SHOWN
Vert:	N/A
Sheet	

SOUTH 1/4 CORNER SECTION 12, T1N R71W.



LEGEND:



LANDSCAPE REQUIREMENTS: 03/17/14

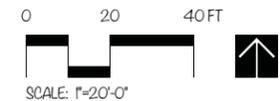
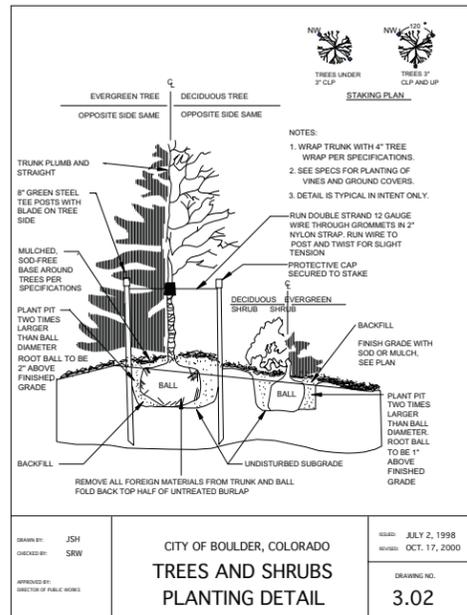
OVERALL SITE	REQUIRED	PROVIDED/COMMENTS
TOTAL LOT AREA	32,510 SF	
PARKING:		
TOTAL NUMBER OF PARKING STALLS	N/A	0 - residential lots
TOTAL NUMBER BIKE RACKS	N/A	0 - residential lots
PARKING LOT SCREENING:		
FROM ADJACENT PROPERTIES:		
Height & Opacity	Landscape Material 42" ht.	
Width	6' Buffer	N/A - residential lots
Trees	1 tree/25 lf	N/A - residential lots
STREETSCAPE:		
REQUIRED		
Attached Sidewalk - Lee Hill	1 tree/30' - 40' - 224 LF = 6-8 trees	8 provided
Attached Walk - 5th Street	1 tree/30' - 40' - 67 LF = 2-3 trees	2 provided
Attached Walk - 6th Street	1 tree/30' - 40' - 203 LF = 6-7 trees	5 provided (extra on Lee Hill due to driveway and utility conflicts)
MINIMUM PLANT SIZES:		
Deciduous Trees	1 tree & 5 shrubs/1500 sf = 127,661 sf = 59 trees + 425 shrubs	15 trees
Evergreen Trees		0
Ornamental Trees		0
Shrubs		39 5 gallon shrubs + 33 1-gal ornamental grasses

PLANT LIST: 08/27/14

KEY	QTY	COMMON NAME	SCIENTIFIC NAME	SIZE	SPACING	O.D.
SHADE TREES:						
BO	2	Burr Oak	<i>Quercus macrocarpa</i>	2" cal.	30' o.e.	
HB	5	Hackberry	<i>Celtis occidentalis</i>	2" cal.	30' o.e.	
LPT	3	London Planetree	<i>Platanus acerifolia</i>	2" cal.	30' o.e.	
WC	5	Western Catalpa	<i>Catalpa speciosa</i>	2" cal.	30' o.e.	
TOTAL:	15					
DECIDUOUS SHRUBS:						
BMS	15	Blue Mist Bluebeard	<i>Caryopteris x clandonensis 'Blue Mist'</i>	5 gal.	4' o.e.	
GLS	24	Oro-Low Fragrant Sumac	<i>Rhus aromatica 'Oro-Low'</i>	5 gal.	5' o.e.	
TOTAL:	39					
ORNAMENTAL GRASSES:						
DFG	33	Dwarf Fountain Grass	<i>Pennisetum alopecuroides 'Hameln'</i>	1 gal.	15' o.e.	
TOTAL:	33					

PLANT NOTES:

- All plant material shall meet specifications of the American Association of Nurserymen (AAN) for number one grade. All trees shall be balled and burlapped or equivalent. All plant materials shall have all wire, twine or other containment materials, except for burlap, removed from trunk and root ball of the plant prior to planting.
- Trees shall not be planted closer than 10 feet to any sewer or water line. Tree planting shall be coordinated with Xcel Energy. Locations of all utilities shall be verified in the field prior to planting.
- All shrubs shall be planted no closer than 3' from any walk or road edge.
- Grades shall be set to allow for proper drainage away from structures. Grades shall maintain smooth profiles and be free of surface debris, bumps, and depressions.
- Developers shall ensure that the landscape plan is coordinated with the plans done by other consultants so that the proposed grading, storm drainage, or other constructions does not conflict nor preclude installation and maintenance of landscape elements on this plan.
- All shrub beds adjacent to turf or seed areas shall be edged with Ryerson or approved equivalent steel edger.
- All shrub bed areas shall be mulched with a 4" layer of wood mulch. Perennials and groundcover areas shall be mulched with a 3" layer of wood mulch. Landscape fabric to be used in shrub beds only, **do not install fabric below ornamental grasses, perennials or groundcover areas.**
- Prior to installation of plant materials, areas that have been compacted or disturbed by construction activity shall be thoroughly loosened; organic soil amendments shall be incorporated at the rate of at least three (3) cubic yards per 1000 square feet of landscape area.
- All seeded areas to be seeded with a dryland seed mixture or approved equal from Arkansas Valley Seed. All slopes steeper than 3:1 will have erosion control fabric.
- All landscape (plant materials and grass) will be irrigated with an automatic system. Turf areas will have a spray zone, shrubs will have a drip zone and perennials/groundcovers (part of the drip zone) will have a combination of drip and micro-jet sprays. Micro-jet spray will be limited to plants that respond better to spray than drip. If budget allows, we will extend a drip line to all trees so they may be watered in the event of drought conditions and the turf areas are turned off. Plants with like water requirements are shown together in order to have an efficient use of water. Irrigation plans will be submitted during T.E.C. Doc that meet the City's requirements.
- Contractor shall verify all material quantities prior to installation. Actual number of plant symbols shall have priority over the quantity designated.
- Refer to the City of Boulder Design and Construction Streetscaping Standards for all work within public areas, and **Planting/Construction Requirements/Schedule (10.03.C.2) for planting season specifications.**
- Refer to the Civil Engineer Drawings for Grading and Utility information.
- This plan meets or exceeds City of Boulder landscape code requirements.



Rev	Date	By
1	August 29, 2014	outs

LANDSCAPE PLAN
CARRIE SUBDIVISION FILING 2 REPLAT 1
CITY OF BOULDER, COLORADO
6/10/2014

Date of Dwg:	10/6/2014
Drawn By:	SG
Designed By:	OSLA
Approved By:	OSLAA
Project No:	1AGR0201
Scale:	
Horiz:	1"=20'-0"
Vert:	N/A
Sheet	



593 Lee Hill Drive Boulder, CO 80302
LUR2014-00031
10/06/2014

Design Guidelines

Carrie Subdivision Filing No. 2 Replat A

Design Theme and Philosophy

The current design theme for the subject properties is to provide single-family residences with a high quality design standard consistent with the surrounding and adjacent single-family homes. We feel a “modern craftsman” theme is best suited as a compliment to the surrounding area. We have attached examples of surrounding homes that have been built as examples of what is typical for the neighborhood. In addition we have attached pictures of homes previously built by the applicant, which would best describe the level of design considerations expected for the subject property. As can easily be seen in the photos a wide variety of exterior finishes and textures will be expected on all sides of the proposed residences. Not only will the future homes be bound by this set of design criteria, but must also be consistent with the previous site-review criteria which was previously approved for the subject parcel.

A. Site Considerations

The basic objective is to achieve compatibility of the future homes and other improvements, with the subject lot and the immediate surrounding homes. Location of the principal structures should consider:

1. Location of future trees, landscaping, and site accessibility.
2. All four elevations must reflect mass and scale proportional to the overall design. All four elevations must include architectural detailing appropriate for the style of the surrounding homes.
3. Site grading and drainage, which minimize required natural grade alterations.
4. Homes shall be oriented with their front yards, front porches, and front doors visible from the street. Front porches per Sec. 9-7-4 B.R.C. 1981 are encouraged.
5. Garage doors will be criteria of design. Single bay garage doors will be encouraged and will be required to be of a high quality. Wood and or other higher quality garage doors will be required. Traditional steel garage door will not be allowed.

B. Building

1. Building Height

The maximum building height for buildings is 35 feet to the highest point of the roof measured from the lowest point of the original grade 25' away per requirements set forth in Sec. 9-7-5 B.R.C. Building height will be further regulated by the City of Boulder Solar Access Guidelines.

2. Floor Area Requirements

The subject property is Compatible Development Exempt. However, floor area ratios will be kept consistent with surrounding and adjacent properties. As can be seen in Table 1.0 at the end of this exhibit, a sample of the closest 12 houses within the Carrie subdivision it reveals a FAR of **.51**. Using the above logic to keep the homes consistent with the neighborhood, the max allowable floor area for each lot would be as follows:

- Lot 21-5,570 SF
- Lot 19-5,010 SF
- Lot 20-5,850 SF

For the purposes of the design review criteria for these lots our proposal is to cap the allowable floor area at a maximum of **4,900 SF**. This equates to a **FAR of .45**. This will create neighborhood compatibility without overbuilding the new proposed lots.

3. Roofs

In general, the architectural style of the homes will allow for an eclectic mix of roof types. Flat roofs will be discouraged to encourage compatibility of surrounding area. The following guidelines should be observed:

- Principal roof forms will be primarily gabled and moderately sloped (4:12-6:12)
- Extensive roof elements should be “interrupted” by intersection roofs, wall elements, parapets, chimneys, etc.
- Exposed roof elements such as flues shall be colored to match the surrounding material, or enclosed by decorative elements when possible.

4. Exterior Walls

Visual breaks in larger wall masses shall be incorporated. This may be accomplished by punctuated or projected building elements, accent roofs, and balconies. Multiple wall finish materials should be incorporated. The presence of varied materials and textures shall occur on all sides of the structure in order to generate “4-sided” architectural interest. Exterior wall finishes will be of a high quality that is durable and long lasting. The maximum numbers of finish materials we be limited to four. Stucco will be used as an accent material and not the primary building material.

5. Window and Door Fenestrations

Windows and doors generally should match surrounding homes. Vinyl window components will not be allowed. Wood, fiberglass, aluminum, and aluminum clad window systems are acceptable.

6. Architectural Elements

Architectural designs shall maintain a consistent level of architectural interest in all elevations. Detail drawings showing architectural elements must be provided for the following items prior to building permit issuance:

- Both front and rear porch details
- Any exterior handrails
- Windows and door trim details
- Garage door design and trim
- Window design/style
- Exterior

7. Foundations

Building foundation walls and site retaining walls should be designed to visually link the structure to the finished grade, designed so that the building appears to emerge from the ground.

C. Other Improvements

1. Driveway and Private Lanes

All driveways must be concrete or hard surface. Limiting impervious ground areas with minimal driveway areas is encouraged. No shared driveways will be allowed.

2. Exterior Mechanical Equipment

All exterior mechanical equipment will be located in inconspicuous locations to be in compliance with all BRC sound regulations.

3. Accessory Structures

No accessory structures are planned at this time

4. Exterior Lighting

All exterior lighting to be in compliance with Section 9-9-16, Outdoor Lighting of the City of Boulder Land Use Code. The exterior lighting incorporated in each residence must avoid impact on adjacent lots and the surrounding areas. The intention is for the development to blend in with the existing character by not creating a brightly lit complex contrasting with the adjacent open space and streetscape. Where the homeowner desires direct source lighting, low voltage fixtures are preferred.

5. Landscaping

Each single-family homeowner shall be required to landscape and maintain their landscaping on their individual lot.

Each single family home shall have a landscape plan prepared by a landscape design professional with their title block included on the drawing. The plan should be sensitive to using appropriate plantings, and an appropriate transition to all city right-of-way.

The Landscape Plan may be subject to review and modification by the City of Boulder to insure compliance with Sec. 9-9-12 B.R.C., and best landscape practices.

6. Rear Yard Living Area

Rear yard living areas shall be incorporated in the form of wood decks, concrete, stone, or paver patios, courtyards, etc. Components, materials, and colors shall be integral to the overall house design and be of a durable lasting material.

7. Fences

Fences are to be consistent with Sec. 9-9-15 B.R.C, and be also consistent with the style of the adjacent and surrounding homes.

The following are existing adjacent and surrounding homes consistent with the subject design guidelines.

4852 5th Street



4888 5th Street



4855 6th Street



4864 5th Street



The following are photos of current homes built by the applicant, consistent with the subject design guidelines.

1465 Sunset Blvd.



512 Valley View Dr.



440 Alpine Ave.



515 Forest Ave.



Table 1 FAR Calculation

Neighborhood FAR Calculation Table

Address	Lot	Lot size SF	Above Grade House SF	FAR For each house
4855 6th Street	13	7,779	6,883	0.88
4865 6th Street	12	7,830	4,592	0.59
4875 6th Street	11	7,855	4,204	0.54
4852 5th street	15	9,641	5,960	0.62
4864 5th street	16	8,575	5,494	0.64
4876 5th street	17	8,358	5,878	0.70
4852 6th street	5	8,152	6,086	0.75
4840 6th street	4	8,025	5,364	0.67
4834 6th street	3	9,723	4,743	0.49
4828 6th street	1	9,227	4,578	0.50
4822 6th street	2	8,252	4,815	0.58
Average FAR for the neighborhood		93,417	58,597	0.63

*Lot area data from Final Plat of Carrie Subdivision, First and Second Filings

**Floor area data derived from City of Boulder construction permit records

New Lot #	New Lot SF	Allowable SF Consistent with Neighborhood FAR	Max Above Grade SF of Each House Inclusive of Garage	Proposed FAR
21	11,026	6,916.20	4,900.00	0.44
19	9,917	6,220.56	4,900.00	0.49
20	11,581	7,264.33	4,900.00	0.42
Average FAR of New Proposed Houses				0.45

FINAL PLAT CARRIE SUBDIVISION FILING NO. 2 REPLAT A

BEING A REPLAT OF LOT 14 CARRIE SUBDIVISION FILING NO. 2
SITUATE IN THE NORTHWEST 1/4 OF THE SOUTHEAST 1/4 OF SECTION 12,
TOWNSHIP 1 NORTH, RANGE 71 WEST OF THE 6TH PRINCIPAL MERIDIAN
CITY OF BOULDER, COUNTY OF BOULDER, STATE OF COLORADO

TOTAL AREA=32,510 S.F.
SHEET 1 OF 2

DEDICATION:

KNOW ALL PERSONS BY THESE PRESENTS THAT THE UNDERSIGNED IS THE OWNER OF THE LAND SITUATE IN THE NORTHWEST ONE-QUARTER (NW1/4) OF THE SOUTHEAST ONE-QUARTER (SE1/4), OF SECTION TWELVE (12), TOWNSHIP ONE NORTH, RANGE 71 WEST OF THE SIXTH PRINCIPAL MERIDIAN; AND LOCATED IN THE CITY BOULDER, STATE OF COLORADO;

BEING ALL OF LOT 14, OF BOULDER, COUNTY OF CARRIE SUBDIVISION FILING NO. 2, AS RECORDED IN THE BOULDER COUNTY CLERK AND RECORDER'S OFFICE AT RECEPTION NO. 2481601, ON AUGUST 1, 2003; AND BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHWEST CORNER OF SAID LOT 14, CARRIE SUBDIVISION FILING NO. 2 WHENCE THE NORTHEAST CORNER OF THE SOUTHEAST ONE-QUARTER OF SECTION 12, TOWNSHIP 1 NORTH, RANGE 71 WEST, OF THE 6TH PRINCIPAL MERIDIAN BEARS N46°41'26"W 1202.64 FEET;

THENCE S68°59'50"E 20.00 FEET ALONG THE EXTERIOR OF SAID LOT 14 FOR THIS AND THE NEXT 5 COURSES;

THENCE S89°48'44"E 224.46 FEET TO A POINT OF NON-TANGENT CURVE CONCAVE WESTERLY, WHOSE RADIUS POINT BEARS N77°53'38"W 760.00 FEET;

THENCE SOUTHERLY 203.36 FEET ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 15°19'51" TO A POINT OF NON-TANGENCY;

THENCE N58°04'31"W 224.36 FEET;

THENCE N01°20'00"W 383.17 FEET TO A POINT OF NON-TANGENT CURVE CONCAVE WESTERLY, WHOSE RADIUS POINT BEARS N63°07'10"W 404.00 FEET;

THENCE NORTHERLY 41.57 FEET ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 5°53'43" TO THE POINT OF BEGINNING.

CONTAINING 32,510 SQUARE FEET OR 0.746 ACRES, MORE OR LESS.

THAT WE HAVE CAUSED SAID REAL PROPERTY TO BE LAID OUT AND SURVEYED AS "CARRIE SUBDIVISION FILING NO. 2, REPLAT A", A SUBDIVISION IN THE CITY OF BOULDER, COUNTY OF BOULDER, STATE OF COLORADO, AND DO HEREBY GRANT TO THE CITY OF BOULDER THOSE PORTIONS OF SAID REAL PROPERTY DESIGNATED AS "UTILITY AND DRAINAGE EASEMENT", AN EASEMENT FOR THE CONSTRUCTION, INSTALLATION, OPERATION, MAINTENANCE, REPAIR AND REPLACEMENT FOR ALL SERVICES, INCLUDING, WITHOUT LIMITING THE GENERALITY OF THE FOREGOING: TELEPHONE AND ELECTRIC LINES, WORKS, POLES AND UNDERGROUND CABLES, GAS PIPELINES, WATER PIPELINES, DRAINAGE DITCHES AND DRAINS AND ALL APPURTENANCES THERETO, IT BEING EXPRESSLY UNDERSTOOD AND AGREED BY THE UNDERSIGNED THAT ALL EXPENSES AND COSTS INVOLVED IN CONSTRUCTING AND INSTALLING SANITARY SEWER SYSTEM WORKS AND LINES, GAS SERVICE LINES, ELECTRICAL SERVICE WORKS AND LINES, GRADING AND LANDSCAPING, CURBS, STREET PAVEMENT, SIDEWALKS, AND OTHER SUCH UTILITIES AND SERVICES SHALL BE GUARANTEED AND PAID FOR BY THE SUBDIVIDER OR ARRANGEMENTS MADE BY THE SUBDIVIDER THEREOF WHICH ARE APPROVED BY THE CITY OF BOULDER, AND SUCH SUMS SHALL NOT BE PAID BY THE CITY OF BOULDER, COLORADO, AND THAT ANY ITEM SO CONSTRUCTED OR INSTALLED AND INSPECTED BY THE CITY OF BOULDER, COLORADO, SHALL BECOME THE PROPERTY AND MAINTENANCE RESPONSIBILITY OF THE CITY OF BOULDER, EXCEPT THOSE ITEMS OWNED BY MUNICIPALLY FRANCHISED OR PERMITTED UTILITIES AND/OR OTHER COMMUNICATIONS, WHEN CONSTRUCTED OR INSTALLED, REMAIN THE PROPERTY AND MAINTENANCE RESPONSIBILITY OF THE OWNER OR PUBLIC UTILITY, AND SHALL NOT BECOME THE PROPERTY OF THE CITY OF BOULDER, COLORADO.

THE UNDERSIGNED DOES FURTHER GRANT TO THE CITY OF BOULDER THOSE PORTIONS OF REAL PROPERTY DESIGNATED AS "PUBLIC ACCESS EASEMENT" ON THE ACCOMPANYING PLAT AS EASEMENTS FOR PUBLIC INGRESS AND EGRESS, AND FOR THE CONSTRUCTION, INSTALLATION, OPERATION, MAINTENANCE, REPAIR AND REPLACEMENT OF TRANSPORTATION IMPROVEMENTS AND LANDSCAPING AND UTILITIES AND APPURTENANCES THERETO, IT IS EXPRESSLY UNDERSTOOD AND AGREED BY THE UNDERSIGNED THAT ALL EXPENSES AND COSTS INVOLVED IN CONSTRUCTING AND INSTALLING SAID IMPROVEMENTS SHALL BE GUARANTEED AND PAID FOR BY THE SUBDIVIDER OR ARRANGEMENTS MADE BY THE SUBDIVIDER THEREOF WHICH ARE APPROVED BY THE CITY OF BOULDER, AND SUCH SUMS SHALL NOT BE PAID BY THE CITY OF BOULDER, COLORADO, AND THAT ANY ITEM SO CONSTRUCTED OR INSTALLED AND ACCEPTED BY THE CITY OF BOULDER, COLORADO, SHALL BECOME THE SOLE PROPERTY OF THE CITY OF BOULDER.

FOR THE APPROVAL OF "CARRIE SUBDIVISION FILING NO. 2, REPLAT A" AND THE DEDICATIONS AND CONDITIONS WHICH APPLY THERETO THIS _____ DAY OF _____, 20____.

LEE HILL INVESTMENT GROUP LLC
A COLORADO LIMITED LIABILITY COMPANY

BY _____
NAME: ROGER C. GROW
TITLE: MANAGER

ACKNOWLEDGMENT

STATE OF COLORADO)
) ss.
COUNTY OF BOULDER)

THE FOREGOING INSTRUMENT WAS ACKNOWLEDGED BEFORE ME THIS _____ DAY OF _____, 2014, BY ROGER C. GROW, AS MANAGER OF LEE HILL INVESTMENT GROUP LLC, A COLORADO LIMITED LIABILITY COMPANY.

WITNESS MY HAND AND OFFICIAL SEAL.

NOTARY PUBLIC
MY COMMISSION EXPIRES: _____

GENERAL NOTES

1. NOTICE: ACCORDING TO COLORADO LAW YOU MUST COMMENCE ANY LEGAL ACTION BASED UPON ANY DEFECT IN THIS SURVEY WITHIN THREE YEARS AFTER YOU DISCOVER SUCH DEFECT. IN NO EVENT MAY LEGAL ACTION BASED UPON ANY DEFECT IN THIS SURVEY BE COMMENCED MORE THAN TEN YEARS FROM THE DATE OF THE CERTIFICATION SHOWN HEREON.
2. THIS SURVEY DOES NOT CONSTITUTE A TITLE SEARCH BY ACCURATE TO DETERMINE TITLE OR EASEMENTS OF RECORD. RESEARCH FOR THIS SURVEY WAS PERFORMED IN ACCORDANCE WITH CRS 38-51-106 AND THE RULES OF PROCEDURE AND BOARD POLICY STATEMENTS OF THE STATE BOARD OF LICENSURE FOR PROFESSIONAL ENGINEERS AND PROFESSIONAL LAND SURVEYORS, SPECIFICALLY THOSE BOARD RULES AND POLICY STATEMENTS RELATING TO THE DEPICTION OF EASEMENTS AND RIGHTS OF WAY ON SUBDIVISION PLATS. TITLE COMMITMENT NUMBER 1505090, DATED JANUARY 3, 2014, PREPARED BY CHICAGO TITLE GUARANTEE COMPANY WAS RELIED UPON FOR ALL INFORMATION REGARDING EASEMENTS OF RECORD, RIGHTS OF WAY, TITLE OF RECORD AND CIVIL COURT ACTIONS OF RECORD.
3. BASIS OF BEARINGS: BEARINGS USED HEREIN ARE BASED ON THE WEST LINE OF SAID CARRIE SUBDIVISION FINAL PLAT, AS FILED IN BOULDER COUNTY CLERK AND RECORDER'S OFFICE AT RECEPTION #2291028, BEING N01°20'00"W AS OCCUPIED AND MEASURED.
4. THE SUBJECT PROPERTY IS LOCATED WITHIN ZONE X, DEFINED AS "AREAS DETERMINED TO BE OUTSIDE 500 YEAR FLOOD PLAIN", AS SHOWN ON FLOOD INSURANCE RATE MAP (FIRM), MAP NUMBER 0801300391J, REVISED DECEMBER 18, 2012.



VICINITY MAP
NOT TO SCALE

SURVEYOR'S CERTIFICATE:

I, DOUGLAS H. ORT III, A PROFESSIONAL LAND SURVEYOR IN THE STATE OF COLORADO, ON THE BASIS OF MY KNOWLEDGE, INFORMATION AND BELIEF, DO HEREBY CERTIFY THAT IN MARCH 2014, A SUBDIVISION PLAT WAS MADE UNDER MY SUPERVISION. THAT THIS SUBDIVISION PLAT AND ALL NOTES SHOWN HEREON ARE AN ACCURATE REPRESENTATION OF THE PROPERTY AS DETERMINED BY THIS SURVEY. ENCROACHMENTS, EASEMENTS, RIGHTS OF WAY OR PASSAGEWAYS ACROSS SAID PROPERTY THAT ARE IN EVIDENCE OR KNOWN TO ME, ARE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF.

DOUGLAS H. ORT III, PLS 37066

APPROVALS:

DIRECTOR OF PLANNING

DIRECTOR OF PUBLIC WORKS AND UTILITIES

CITY MANAGER'S CERTIFICATE:

IN WITNESS WHEREOF, THE SAID CITY OF BOULDER HAS CAUSED ITS SEAL TO BE HEREUNTO AFFIXED BY ITS CITY MANAGER THIS _____ DAY OF _____, 20____.

ATTEST:

CITY CLERK

CITY MANAGER

CLERK AND RECORDER'S CERTIFICATE:

STATE OF COLORADO)
) ss
COUNTY OF BOULDER)

I HEREBY CERTIFY THAT THIS INSTRUMENT WAS FILED IN MY OFFICE AT _____ O'CLOCK _____ M., THIS _____ DAY OF _____, 20____, AND IS RECORDED AT RECEPTION # _____.

FEES PAID: \$ _____

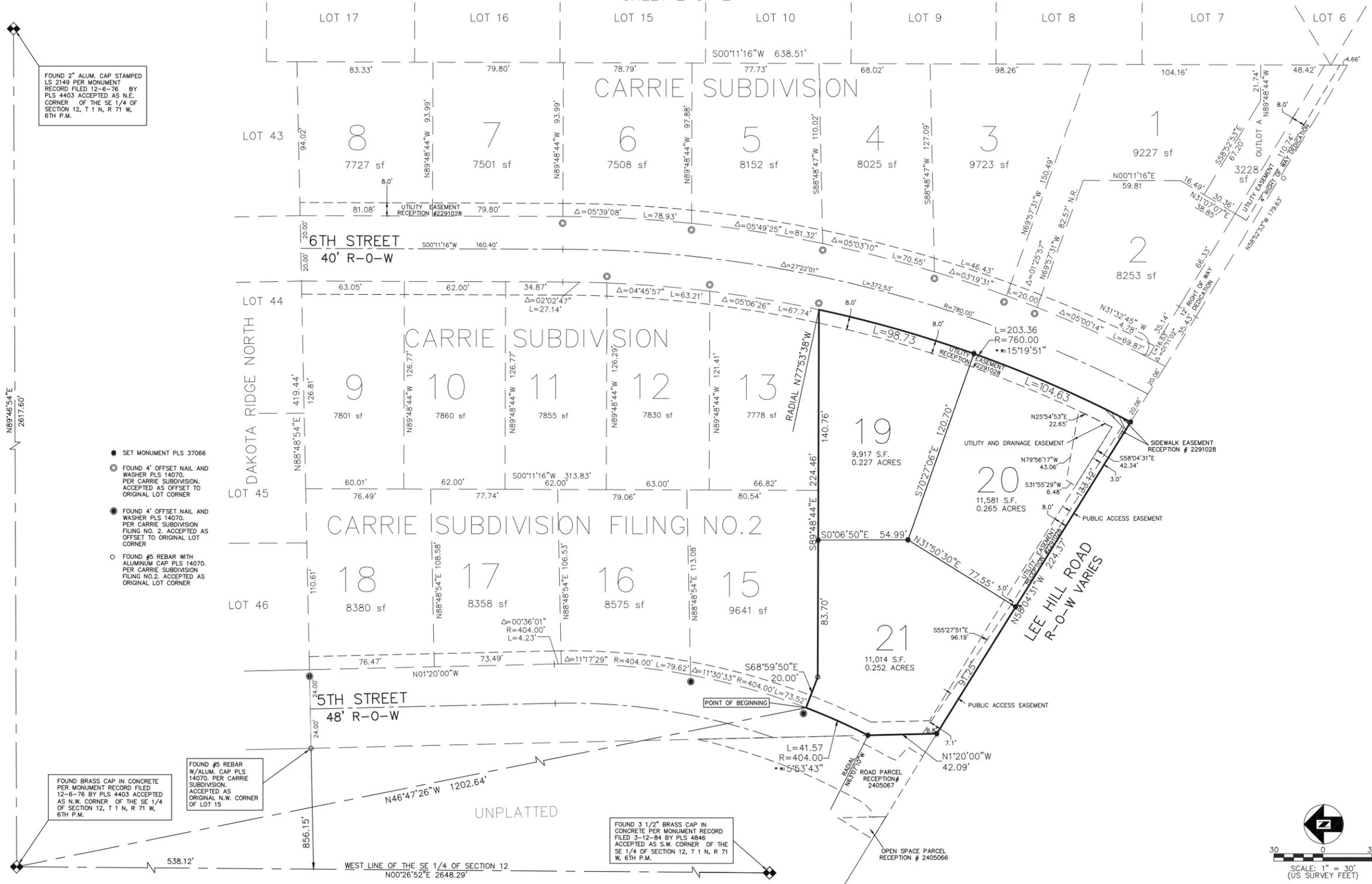
CLERK AND RECORDER

DEPUTY

FINAL PLAT CARRIE SUBDIVISION FILING NO. 2 REPLAT A

BEING A REPLAT OF LOT 14 CARRIE SUBDIVISION FILING NO. 2
SITUATE IN THE NORTHWEST 1/4 OF THE SOUTHEAST 1/4 OF SECTION 12,
TOWNSHIP 1 NORTH, RANGE 71 WEST OF THE 6TH PRINCIPAL MERIDIAN
CITY OF BOULDER, COUNTY OF BOULDER, STATE OF COLORADO

TOTAL AREA=32,510 S.F.
SHEET 2 OF 2



FOUND 2" ALUM. CAP STAMPED
LS 2149 PER MONUMENT
RECORD FILED 12-6-76 BY
PLS 4403 ACCEPTED AS N.E.
CORNER OF THE SE 1/4 OF
SECTION 12, T 1 N, R 71 W,
6TH P.M.

- SET MONUMENT PLS 37066
- FOUND 4" OFFSET NAIL AND WASHER PLS 14070. PER CARRIE SUBDIVISION, ACCEPTED AS OFFSET TO ORIGINAL LOT CORNER
- FOUND 4" OFFSET NAIL AND WASHER PLS 14070. PER CARRIE SUBDIVISION FILING NO. 2, ACCEPTED AS OFFSET TO ORIGINAL LOT CORNER
- FOUND #5 REBAR WITH ALUMINUM CAP PLS 14070. PER CARRIE SUBDIVISION FILING NO.2, ACCEPTED AS ORIGINAL LOT CORNER

FOUND BRASS CAP IN CONCRETE
PER MONUMENT RECORD FILED
12-6-76 BY PLS 4403 ACCEPTED
AS N.W. CORNER OF THE SE 1/4
OF SECTION 12, T 1 N, R 71 W,
6TH P.M.

FOUND #5 REBAR
W/ALUM. CAP PLS
14070. PER CARRIE
SUBDIVISION,
ACCEPTED AS
ORIGINAL N.W. CORNER
OF LOT 15

FOUND 3 1/2" BRASS CAP IN
CONCRETE PER MONUMENT RECORD
FILED 3-12-84 BY PLS 4846
ACCEPTED AS S.W. CORNER OF THE
SE 1/4 OF SECTION 12, T 1 N, R 71
W, 6TH P.M.



SHEET 2 OF 2	REVISIONS	DRAWN BY: TJB	APPROVED BY: DHO
		FIELD WORK BY: DP	DATE OF SURVEY: 2-27-14
		DATE OF Dwg: 10-6-2014	PROJECT NO: 1AGR0201

MEMORANDUM

TO: Planning Board
FROM: Chandler Van Schaack, Case Manager
DATE: January 22, 2015
SUBJECT: **Call Up Item:** Knapp Subdivision (TEC2013-00057): Final Plat to subdivide one 0.5-acre developed lot at 3050 15th St. in the Garden Home Subdivision to create 2 new residential lots: Lot 1 (9,605 s.f.) and Lot 2 (12,176 s.f.). Lot 1 will contain the existing single family home. The call up period expires on **January 26, 2015**.

Attached is the disposition for the conditional approval (see [Attachment A](#)) for a review of the Final Plat for the proposed Knapp Subdivision within the RL-1 (Residential- Low 1) zoning district. As indicated in [Attachment B](#), this approval will result in the replat of one existing lot to create two new residential lots: Lot 1 (9,605 s.f.) will contain an existing single family home, and Lot 2 (12,176 s.f.) will be sold as a vacant, developable lot. No modifications to the development code or minimum lot standards have been requested as a part of this application.

Process.

Due to the removal of a portion of an existing structure and the dedication of a public access easement for a shared driveway, the proposed subdivision exceeds the limitations of a Minor Subdivision. Pursuant to Chapter 9-12, B.R.C. 1981, any proposed subdivision of land in a residential zone district which exceeds the limitations of a Minor Subdivision requires approval of a Preliminary and Final Plat. Pursuant to section 9-12-10, B.R.C. 1981, approval of a final plat is subject to call-up by the planning board. If the decision is not called up by the planning board then it will become final fourteen days after the date of the initial approval.

Background.

As shown above in **Figure 1**, 3050 15th St. is located in North Boulder on 15th Street north of Elder Ave. The property is zoned RL-1 (Residential- Low 1), which is defined as "Single-family detached residential dwelling units at low to very low residential densities" per section 9-5-2(c)(1)(A), B.R.C. 1981. The



Figure 1: Vicinity Map and Current Lot Configuration



Figure 2: Proposed Lot Configuration

Address: 3050 15th St.

surrounding neighborhood is also zoned RL-1. Pursuant to section 9-8-1, Table 8-1, "Intensity Standards," the minimum lot area for the RL-1 zone district is 7,000 square feet; however, the lots located along 15th Street on this block range in size from approximately 10,000 square feet to 22,400 square feet.

The subject property is 21,781 sq. ft. (0.5-acres) in size and currently contains a detached single-family dwelling unit, constructed in 1950. As indicated above, the proposed subdivision will result in the replat of the existing lot to create two new residential lots: Lot 1 (9,605 s.f.) will contain the existing single family home, and Lot 2 (12,176 s.f.) will be sold as a vacant developable lot. The proposed subdivision is in a flag lot configuration, with Lot 2 being located behind Lot 1 to the east and accessed via a 30' portion of lot that runs along the south edge of the proposed Lot 1 (See **Figure 2** above for the proposed subdivision layout. Both lots will share access using the existing driveway, which will be located on Lot 2 following the subdivision of the lots and subject to a shared access easement. In order for the existing home on Lot 1 to continue to meet the minimum side yard setback requirements for the RL-1 zone, a portion of the existing home, which was added in 2008, will be demolished prior to building permit issuance for either lot.

Analysis / Conclusion.

Staff finds that this application is consistent with the intent of the Subdivision standards found in Chapter 9-12, B.R.C. 1981 and meets all applicable Final Plat criteria set forth in section 9-12-8(b), B.R.C. 1981. Both of the new lots will exceed the minimum lot size required by the RL-1 zone district (7,000 square feet).

Public Comment and Process:

The required public notice was given in the form of written notification mailed to all property owners within 600 feet of the subject property and a sign posted on the property for at least 10 days. All notice requirements of Section 9-4-2, B.R.C. 1981 have been met. Staff initially received comments from several neighbors who opposed the proposed subdivision; however, following additional notification regarding the subdivision approval, staff has not received any further comments.

This proposal was approved by Planning and Development Services staff on January 12, 2015, and the decision may be called up before Planning Board on or before **January 26, 2015**. There is one Planning Board meeting within the 14-day call up period on **January 22, 2015**. Questions about the project or decision should be directed to Chandler Van Schaack at (303) 441-3137 or vanschaackc@bouldercolorado.gov.

Attachments:

- A. Signed Disposition
- B. Approved Final Plat for Knapp Subdivision
- C. Staff's Analysis of Lot Standards for Subdivision



CITY OF BOULDER
Community Planning and Sustainability

1739 Broadway, Third Floor • P.O. Box 791, Boulder, CO 80306-0791
 phone 303-441-1880 • fax 303-441-3241 • web www.bouldercolorado.gov

CITY OF BOULDER PLANNING DEPARTMENT
NOTICE OF DISPOSITION

You are hereby advised that the following action was taken by the Planning Department based on the standards and criteria of the Land Use Regulations as set forth in Section 9-12, B.R.C. 1981, as applied to the proposed development.

DECISION: **Approved with Condition**
 PROJECT NAME: **Knapp Subdivision**
 DESCRIPTION: **Final Plat to subdivide one 0.5-acre developed lot in the Garden Home Subdivision to create 2 new residential lots: Lot 1 (9,605 s.f.) and Lot 2 (12,176 s.f.). Lot 1 will contain the existing single family home.**
 LOCATION: **3050 15th St.**
 COOR: **N05W06**
 LEGAL DESCRIPTION: **The North Half of Lot 8, Garden Home Subdivision, City of Boulder, County of Boulder, State of Colorado**
 APPLICANT: **James McCutcheon**
 OWNER: **Charles L. Knapp & Ellen C. Smith-Knapp**
 APPLICATION: **TEC2013-00057**
 ZONING: **RL-1**
 CASE MANAGER: **Chandler Van Schaack**

THIS IS NOT A SITE SPECIFIC DEVELOPMENT PLAN APPROVAL AND NO VESTED PROPERTY RIGHT IS CREATED BY THIS APPROVAL.

Approved On: 1.12.15
 Date

By: 
 David Driskell, Executive Director of Community Planning & Sustainability

This decision may be appealed to the Planning Board by filing an appeal letter with the Planning Department within two weeks of the decision date. If no such appeal is filed, the decision shall be deemed final fourteen days after the date above mentioned.

Appeal to Planning Board expires: 1.26.15

1. The subdivision is approved subject to the terms of the Subdivision Agreement.

KNAPP SUBDIVISION FINAL PLAT

A REPLAT OF THE NORTH 1/2 OF LOT 8, GARDEN HOME SUBDIVISION, LOCATED IN THE SW 1/4 OF SECTION 19, T1N, R70W OF THE 6TH P.M. CITY OF BOULDER, COUNTY OF BOULDER, STATE OF COLORADO

TOTAL AREA = 0.500 ACRES
SHEET 1 OF 1

DEDICATION

KNOW ALL MEN BY THESE PRESENTS: THAT ELLEN C. SMITH-KNAPP AND CHARLES L. KNAPP ARE THE OWNERS OF THAT PARCEL OF LAND SITUATED IN THE CITY OF BOULDER, AND LYING WITHIN THE SW 1/4 OF SECTION 19, T1N, R70W OF THE 6TH P.M., DESCRIBED AS FOLLOWS:

THE NORTH HALF OF LOT 8, GARDEN HOME SUBDIVISION, CITY OF BOULDER, COUNTY OF BOULDER, STATE OF COLORADO.

HAVE CAUSED SAID PROPERTY TO BE LAID OUT, SURVEYED, SUBDIVIDED AND PLATTED UNDER THE NAME OF "KNAPP SUBDIVISION" IN THE CITY OF BOULDER, COUNTY OF BOULDER, STATE OF COLORADO, AS SHOWN ON THE ACCOMPANYING PLAT.

THE UNDERSIGNED DO GRANT TO THE CITY OF BOULDER THAT PORTION OF SAID REAL PROPERTY DESIGNATED AS "SHARED ACCESS EASEMENT" FOR ITS USE AND THE USE OF THE PUBLIC FOREVER, AS AN EASEMENT FOR INGRESS AND EGRESS OVER AND ACROSS THAT PORTION OF SAID REAL PROPERTY, SO SHOWN HEREON FOR THE BENEFIT OF LOT 1. SAID EASEMENT SHALL REMAIN THE MAINTENANCE RESPONSIBILITY OF SAID OWNERS AND SHALL NOT BECOME THE MAINTENANCE RESPONSIBILITY OF THE CITY.

THE UNDERSIGNED DO GRANT TO THE CITY OF BOULDER THAT PORTION OF SAID REAL PROPERTY DESIGNATED AS "UTILITY EASEMENT" ON THE ACCOMPANYING PLAT AS EASEMENTS FOR THE CONSTRUCTION, INSTALLATION, OPERATION, MAINTENANCE, REPAIR AND REPLACEMENT FOR ALL SERVICES, INCLUDING WITHOUT LIMITING THE GENERALITY OF THE FOREGOING: TELEPHONE AND ELECTRIC LINES, WORKS, POLES, UNDERGROUND CABLES, GAS PIPELINES, WATER PIPELINES, SANITARY SEWER LINES, STREET LIGHTS, CULVERTS, HYDRANTS, DRAINAGE DITCHES AND DRAINS AND ALL APPURTENANCES THERETO. IT IS EXPRESSLY UNDERSTOOD AND AGREED BY THE UNDERSIGNED THAT ALL EXPENSES AND COSTS INVOLVED IN CONSTRUCTING AND INSTALLING WATER, PIPELINES AND APPURTENANCES, SANITARY SEWER WORKS AND LINES, GAS SERVICE LINES, ELECTRICAL SERVICE WORKS AND LINES, STORM SEWERS AND DRAINS, STREET LIGHTING, GRADING AND LANDSCAPING, CURBS, GUTTERS, STREET PAVEMENT, SIDEWALKS, AND OTHER SUCH UTILITIES AND SERVICES SHALL BE GUARANTEED AND PAID FOR BY THE SUBDIVIDER OR ARRANGEMENTS MADE BY THE SUBDIVIDER THEREFORE WHICH ARE APPROVED BY THE CITY OF BOULDER, AND SUCH SUMS SHALL NOT BE PAID BY THE CITY OF BOULDER, COLORADO, AND THAT ANY ITEM SO CONSTRUCTED OR INSTALLED AND ACCEPTED BY THE CITY OF BOULDER, COLORADO, SHALL BECOME THE SOLE PROPERTY OF THE CITY OF BOULDER, EXCEPT ITEMS OWNED BY MUNICIPALLY FRANCHISED OR PERMITTED UTILITIES, WHICH ITEMS, WHEN CONSTRUCTED OR INSTALLED, SHALL REMAIN THE PROPERTY OF THE OWNER OR THE PUBLIC UTILITY, AND SHALL NOT BECOME THE PROPERTY OF THE CITY OF BOULDER.

FOR THE APPROVAL OF "KNAPP SUBDIVISION" AND THE DEDICATIONS AND CONDITIONS WHICH APPLY THERETO THIS ____ DAY OF _____, 20____

BY: ELLEN C. SMITH-KNAPP BY: CHARLES L. KNAPP

STATE OF COLORADO)
) SS.
COUNTY OF BOULDER)

THE FOREGOING INSTRUMENT WAS ACKNOWLEDGED BEFORE ME THIS ____ DAY OF _____, 20____, BY ELLEN C. SMITH-KNAPP AND CHARLES L. KNAPP.

WITNESS MY HAND AND SEAL.
MY COMMISSION EXPIRES: _____

[SEAL]

NOTARY PUBLIC

LENDER'S CONSENT AND SUBORDINATION

THE UNDERSIGNED, A BENEFICIARY UNDER A CERTAIN DEED OF TRUST ENCUMBERING THE PROPERTY, HEREBY EXPRESSLY CONSENTS TO AND JOINS IN THE EXECUTION AND RECORDING OF THIS SUBDIVISION PLAT, DEDICATION AND EASEMENTS SHOWN HEREON AND MAKES THE DEED OF TRUST SUBORDINATE HERETO. THE UNDERSIGNED REPRESENTS THAT HE OR SHE HAS FULL POWER AND AUTHORITY TO EXECUTE THIS LENDER'S CONSENT AND SUBORDINATION ON BEHALF OF THE LENDER STATED BELOW.

RESOURCE MORTGAGE CORPORATION

BY: _____
AUTHORIZED REPRESENTATIVE

ACKNOWLEDGEMENT

STATE OF _____)
) SS.
COUNTY OF _____)

THE FOREGOING INSTRUMENT WAS ACKNOWLEDGED BEFORE ME THIS ____ DAY OF _____, 20____, BY _____ AS AUTHORIZED REPRESENTATIVE OF MORTGAGE ELECTRONIC REGISTRATION SYSTEMS, INC. A SEPARATE CORPORATION THAT IS ACTING SOLELY AS A NOMINEE FOR LENDER AND LENDER'S SUCCESSORS AND ASSIGNS FOR RESOURCE MORTGAGE CORPORATION.

WITNESS MY HAND AND SEAL.
MY COMMISSION EXPIRES: _____

[SEAL]

NOTARY PUBLIC

LENDERS CONSENT AND SUBORDINATION

THE UNDERSIGNED, A BENEFICIARY UNDER A CERTAIN DEED OF TRUST ENCUMBERING THE PROPERTY, HEREBY EXPRESSLY CONSENTS TO AND JOINS IN THE EXECUTION AND RECORDING OF THIS SUBDIVISION PLAT, DEDICATION AND EASEMENTS SHOWN HEREON AND MAKES THE DEED OF TRUST SUBORDINATE HERETO. THE UNDERSIGNED REPRESENTS THAT HE OR SHE HAS FULL POWER AND AUTHORITY TO EXECUTE THIS LENDER'S CONSENT AND SUBORDINATION ON BEHALF OF THE LENDER STATED BELOW.

BOULDER MUNICIPAL EMPLOYEES FEDERAL CREDIT UNION

BY: _____
AUTHORIZED REPRESENTATIVE

ACKNOWLEDGEMENT

STATE OF _____)
) SS.
COUNTY OF _____)

THE FOREGOING INSTRUMENT WAS ACKNOWLEDGED BEFORE ME THIS ____ DAY OF _____, 20____, BY _____ AS _____ OF BOULDER MUNICIPAL EMPLOYEES FEDERAL CREDIT UNION.

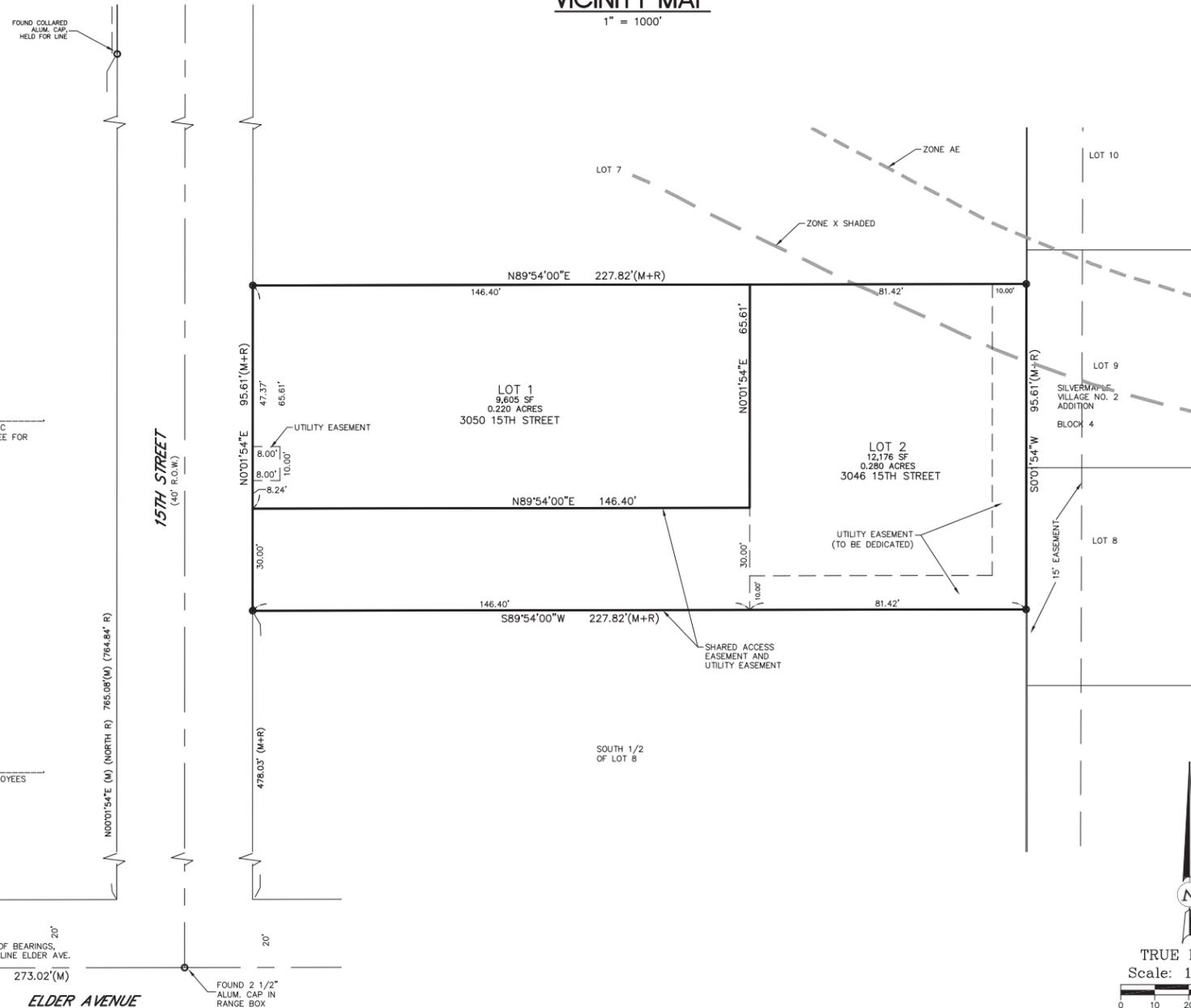
WITNESS MY HAND AND SEAL.
MY COMMISSION EXPIRES: _____

[SEAL]

NOTARY PUBLIC



VICINITY MAP
1" = 1000'



SURVEY NOTES

1. THE BASIS OF BEARINGS IS THE CENTER LINE OF ELDER AVE. AND BEARS S89°54'W PER GARDEN HOME SUBDIVISION PLAT.
2. THE SIZE AND TYPE OF MONUMENTS FOUND ARE SHOWN HEREON.
3. THE SURVEY FIELD WORK ON THIS SITE WAS COMPLETED ON 7/08/13.
4. NOTICE: ACCORDING TO COLORADO LAW, YOU MUST COMMENCE ANY LEGAL ACTION BASED ON ANY DEFECT IN THIS SURVEY WITHIN THREE YEARS AFTER YOU FIRST DISCOVERED SUCH DEFECT. IN NO EVENT, MAY ANY ACTION BASED UPON ANY DEFECT IN THIS SURVEY BE COMMENCED MORE THAN TEN YEARS FROM THE DATE OF THE CERTIFICATION SHOWN HEREON. CRS-13-80-105 (3)(a)
5. FIRST COLORADO TITLE COMPANY, LLC, POLICY NO. 113-00-880032, DATED SEPTEMBER 3, 2013 WAS SOLELY RELIED UPON FOR RECORDED RIGHTS-OF-WAY, EASEMENTS AND ENCUMBRANCES IN THE PREPARATION OF THIS SURVEY.
6. LAND SURVEY PLATS REFERENCED OR USED FOR THIS SURVEY: FLAGSTAFF SURVEYING INC., LAND SURVEY PLAT, LS-12-0300; FLATIRON, INC., IMPROVEMENT SURVEY PLAT, LS-13-0068; BONSALL SUBDIVISION, REC. NO. 549483; GARDEN HOME SUBDIVISION, REC. NO. 441958; IMPROVEMENT SURVEY PLAT BY SCOTT COX AND ASSOC., 7-03-12.
7. LOTS ARE TO BE USED FOR SINGLE FAMILY RESIDENCES.
8. THE NORTHEAST PORTION OF THIS PROPERTY LIES WITHIN ZONE X, SHADED (AREAS OF 0.2% ANNUAL CHANCE FLOOD) AND THE REST LIES WITHIN ZONE X, (AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN) AS SHOWN ON FLOOD INSURANCE RATE MAP, CITY OF BOULDER COLORADO, BOULDER COUNTY FIRM MAP NUMBER 080130392 J EFFECTIVE DATE: DECEMBER 18, 2012.
9. THE PROPERTY IS ZONED RL-1.

SURVEYOR'S CERTIFICATE

I, A. JOHN BURI, A DULY REGISTERED PROFESSIONAL LAND SURVEYOR IN THE STATE OF COLORADO, DO HEREBY CERTIFY THAT THIS PLAT WAS PREPARED UNDER MY SUPERVISION AND IS BASED ON A BOUNDARY SURVEY MADE BY SCOTT, COX & ASSOCIATES, INC., AND TO THE BEST OF MY KNOWLEDGE AND BELIEF CORRECTLY SHOWS THE LOCATION AND DIMENSIONS OF THE LOTS, IN SUBSTANTIAL COMPLIANCE WITH C.R.S. 38-51-106.

A. JOHN BURI, PLS 24302
FOR AND ON BEHALF OF
SCOTT, COX & ASSOCIATES, INC.

APPROVALS

DIRECTOR OF PLANNING

DIRECTOR OF PUBLIC WORKS AND UTILITIES

CITY MANAGER'S CERTIFICATE

IN WITNESS WHEREOF, THE CITY OF BOULDER HAS CAUSED ITS SEAL TO BE HEREUNTO AFFIXED BY ITS CITY MANAGER THIS ____ DAY OF _____, 20____

ATTEST:

CITY CLERK

CITY MANAGER

CLERK AND RECORDER'S CERTIFICATE

STATE OF COLORADO)
) SS.
COUNTY OF BOULDER)

I HEREBY CERTIFY THAT THIS INSTRUMENT WAS FILED IN MY OFFICE AT _____ O'CLOCK ____ M., THIS ____ DAY OF _____, 20____, AND IS RECORDED AT RECEPTION # _____ FEES PAID: \$ _____

CLERK AND RECORDER

DEPUTY



Designed by	AJB	Date	02/14/14	Scale	1" = 20'	Drawing no.	13330B-1	Sheet	1
Drawn by	JAS	Revision		Description		Date		Project no.	13330B
Checked by	AJB								

Section 9-12-12, "Standards for Lots and Public Improvements," B.R.C. 1981

Section 9-12-12, "Standards for Lots and Public Improvements," B.R.C. 1981 includes all of the substantive regulatory requirements that need to be met in order to have an approvable final plat. The proposed subdivision meets all of the standards set forth in Section 9-12-12, B.R.C. 1981. Below is a summary of the staff findings on each of the standards.

(a) Conditions Required: Except as provided in subsection (b) of this section, subdivision plats shall comply with section 9-9-17, "Solar Access," B.R.C. 1981, and meet the following conditions:

(1) Standards for Lots: Lots meet the following conditions:

(A) Each lot has access to a public street.

Standard met. The proposed new lot is in a flag lot configuration, with 30' of frontage on 15th St. A Shared Access Easement will be dedicated through the Final Plat which will allow both lots to utilize the existing curb cut.

(B) Each lot has at least thirty feet of frontage on a public street.

Standard met.

(C) No portion of a lot is narrower than thirty feet.

Standard met.

(D) Lots meet all applicable zoning requirements of this title and section 9-9-17, "Solar Access," B.R.C. 1981.

Both of the proposed new lots meet the 7,000 s.f. minimum lot size requirement for the RL-1 zone district, with Lot 1 being 9,605 square feet and Lot 2 being 12,176 square feet, respectively. In order for the existing home on Lot 1 to continue to meet the minimum side yard setback requirements for the RL-1 zone, a portion of the existing home will be demolished prior to building permit issuance for either lot. Following the demolition of the portion of the existing home, Lot 1 will comply with all applicable zoning standards, including FAR. Any new development on Lot 2 will be subject to compatible development standards. Standard met.

(E) Lots with double frontage are avoided, except where necessary to provide separation from major arterials or incompatible land uses or because of the slope of the lot.

Standard met. Both lots will front on 15th Street only.

(F) Side lot lines are substantially at right angles or radial to the centerline of streets, whenever feasible.

Standard met.

(G) Corner lots are larger than other lots to accommodate setback requirements of section 9-7-1, "Schedule of Form and Bulk Standards," B.R.C. 1981.

Not applicable, as neither of the proposed lots will be a corner lot.

(H) Residential lots are shaped so as to accommodate a dwelling unit within the setbacks prescribed by the zoning district.

Standard met. Both of the proposed new lots are large enough to accommodate the setback requirements of section 9-7-1.

(I) Lots shall not be platted on land with a ten percent or greater slope, unstable land, or land with inadequate drainage unless each platted lot has at least one thousand square feet of buildable area, with a minimum dimension of twenty-five feet. The city manager may approve the platting of such land upon finding that acceptable measures, submitted by a registered engineer qualified in the particular field, eliminate or control the problems of instability or inadequate drainage.

Not Applicable, as the subject lot does not contain slopes greater than ten percent, is not unstable, and will provide adequate drainage. Regardless, each lot has at least one thousand square feet of buildable area.

(J) Where a subdivision borders an airport, a railroad right-of-way, a freeway, a major street, or any other major source of noise, the subdivision is designed to reduce noise in residential lots to a reasonable level and to retain limited access to such facilities by such measures as a parallel street, a landscaped buffer area, or lots with increased setbacks.

Not applicable, as the subject property borders a residential street that terminates a half-block to the north. There is no thru-traffic on 15th St., so noise levels are minimal.

(K) Each lot contains at least one deciduous street tree of two-inch caliper in residential subdivisions, and each corner lot contains at least one tree for each street upon which the lot fronts, located so as not to interfere with sight distance at driveways and chosen from the list of acceptable trees established by the city manager, unless the subdivision agreement provides that the subdivider will obtain written commitments from subsequent purchasers to plant the required trees.

Standard will be met at time of building permit application.

(L) The subdivider provides permanent survey monuments, range points, and lot pins placed by a Colorado registered land surveyor.

Standard met.

(M) Where an irrigation ditch or channel, natural creek, stream, or other drainage way crosses a subdivision, the subdivider provides an easement sufficient for drainage and maintenance.

Not applicable, as the proposed subdivision is not crossed by any irrigation ditch or channel, natural creek, stream, or other drainage way.

(N) Lots are assigned street numbers by the city manager under the city's established house numbering system, and before final building inspection the subdivider installs numbers clearly visible and made of durable material.

Standard met.

(O) For the purpose of ensuring the potential for utilization of solar energy in the city, the subdivider places streets, lots, open spaces, and buildings so as to maximize the potential for the use of solar energy in accordance with the following solar siting criteria:

The applicant has demonstrated that following subdivision any new development on the new lots will be able to meet all applicable solar access standards for the RL-1 zone district.

(i) Placement of Open Space and Streets: Open space areas are located wherever practical to protect buildings from shading by other buildings within the development or from buildings on adjacent properties. Topography and other natural features and constraints may justify deviations from this criterion.

Standard met.

(ii) Lot Layout and Building Siting: Lots are oriented and buildings sited in a way which maximizes the solar potential of each principal building. Lots are designed so that it would be easy to site a structure which is unshaded by other nearby structures and so as to allow for owner control of shading. Lots also are designed so that buildings can be sited so as to maximize the solar potential of adjacent properties by minimizing off-site shading.

Standard met.

(iii) Building Form: The shapes of buildings are designed to maximize utilization of solar energy. Existing and proposed buildings shall meet the solar access protection and solar siting requirements of section 9-9-17, "Solar Access," B.R.C. 1981.

Standard met.

(iv) Landscaping: The shading impact of proposed landscaping on adjacent buildings is addressed by the applicant. When a landscape plan is required, the applicant shall indicate the plant type and whether the plant is coniferous or deciduous.

A Landscape Plan will be required at time of redevelopment of the new lot.

(2) Transportation Standards for Streets, Alleys, and Sidewalks: Streets, curb and gutters, sidewalks, alleys, and the public rights-of-way therefore, are provided in conformity with the standards in the City of Boulder Design and Construction Standards, and meet the following conditions:

There is an existing sidewalk in front of the subject property, as well as an existing curb cut. No additional transportation improvements are required as part of the proposed subdivision.

(A) Streets are aligned to join with planned or existing streets.

Not applicable, as there are no new streets proposed.

(B) Streets are designed to bear a relationship to the topography, minimizing grade, slope, and fill.

Not applicable, as there are no new streets proposed.

(C) There are no dead-end streets without an adequate turnaround and appropriate barriers.

Not applicable, as there are no new streets proposed.

(D) Access to freeway, arterial, or collector street occurs only at intersections approved by the city manager, if the manager finds that the access provides efficient traffic movement and safety for drivers and pedestrians.

Not applicable, as both lots take access from 15th Street, which is a local street.

(E) A street of only one-half width is not dedicated to or accepted by the city.

Standard met.

(F) When the plat dedicates a street that ends on the plat or is on the perimeter of the plat, the subdivider conveys that last foot of the street on the terminal end or outside border of the plat to the city in fee simple, and it is designated by using an outlot.

Not applicable, as no street is being dedicated to the city through this subdivision.

(G) Streets are provided as prescribed by the Boulder Valley Comprehensive Plan, adopted subcommunity or area plans, or the Transportation Master Plan.

Standard met.

(H) Alleys are encouraged and should be provided. If they are provided, they are paved or otherwise appropriately surfaced with a material approved by the city manager for the specific application and location.

Standard met. No new alleys are being constructed as part of this subdivision.

(I) Sidewalks are provided in all subdivisions, unless the city manager determines that no public need exists for sidewalks in a certain location.

Standard met. There is an existing sidewalk along 15th St.

(J) Signs for street names (subject to approval of the city manager), directions, and hazards are provided.

Standard met. Existing street signs for 15th St. are already in place.

(K) Traffic control signs are provided, as required by the city manager for control of traffic.

Standard met. No new traffic control signs are required.

(L) Pedestrian crosswalks are provided, as required by the city manager for traffic control and, at a minimum, between streets where the distance between intersecting streets exceeds one thousand feet.

Standard met. No crosswalks will be required.

(M) Bike paths or lanes are provided in conformity with the City of Boulder Comprehensive Plan for bicycle facilities and are dedicated to the city.

Standard met. No new bicycle lanes are required.

(N) Private streets are not permitted.

Standard met. No private streets are being constructed as part of this subdivision.

(3) Standards for Water and Wastewater Improvements: Water and wastewater utilities are provided in conformity with the construction and design standards in the City of Boulder Design and Construction Standards, and meet the following conditions:

(A) Water and sanitary sewer mains are provided as necessary to serve the subdivision.

Standard met.

(B) Easements are provided for city utilities as prescribed by the City of Boulder Design and Construction Standards.

Standard met.

(C) Easements for utilities other than city utilities are provided as required by the applicable private utility.

Standard met.

(D) Newly installed telephone, electric, and cable television lines and other similar utility service are placed underground. Existing utilities are also placed underground unless the subdivider demonstrates to the manager that the cost substantially outweighs the visual benefit from doing so. But transformers, switching boxes, terminal boxes, meter cabinets, pedestals, ducts, electric transmission and distribution feeder lines, communication long distance trunk and feeder lines, and other facilities necessarily appurtenant to such facilities and to underground utilities may be placed above ground within dedicated easements or public rights-of-way.

Standard met. All new utilities will be underground, and the existing overhead powerline serving the existing home on Lot 1 will be removed.

(4) Standards for Flood Control and Storm Drainage: Flood control and storm drainage measures are provided as required by the city's master drainage plan and in conformity with the construction and design standards in the City of Boulder Design and Construction Standards, and meet the following conditions:

(A) The measures retain existing vegetation and natural features of the drainageway where consistent with the master drainage plan.

Standard met.

(B) Any land subject to flooding by a one hundred-year flood conforms to the requirements of chapter 11-5, "Storm Water and Flood Management Utility," B.R.C. 1981.

Not applicable. The subject property is not located within a floodplain.

(C) Storm drainage improvements and storm sewers are maintained to collect drainage from the subdivision and convey it off-site into a city right of way or drainage system without adversely affecting adjacent property.

Standard met.

(D) Bridges, culverts, or open drainage channels are provided when required by the flood control utility master drainage plan.

Not applicable.

(E) All subdivisions shall be designed to minimize flood damage.

Not applicable.

(F) All subdivisions shall have public utilities and facilities, including, without limitation, sewer, gas, electrical, and water systems, located and constructed to prevent flood damage.

Not applicable.

(G) All subdivisions shall have adequate drainage provided to reduce exposure to flood damage.

Standard met.

(5) Standards for Fire Protection: Fire protection measures meet the following conditions:

(A) Fire hydrants are provided as required by chapter 10-8, "Fire Prevention Code," B.R.C. 1981.

Standard met.

(B) Fire lanes are provided where necessary to protect the area; an easement at least sixteen feet wide for fire lanes is dedicated to the city, remains free of obstructions, and permits emergency access at all times.

Not applicable, as no new fire lanes are required.

CITY OF BOULDER
PLANNING BOARD AGENDA ITEM
MEETING DATE: January 22, 2015

AGENDA TITLE: Public hearing and consideration of a Site and Use Review (LUR2014-00057) to construct one new 2,850 square foot, single story Bank of America building with a drive thru facility on the pad site at 1965 28th St. The proposal also includes improvements to the existing parking area serving the pad site as well to the parking area adjacent to the Hazels liquor store. The project site is zoned Business – Regional 1 (BR-1).

Applicant: Bruce Dierking
Owner: ANDRE FAMILY PARTNERSHIP, RLLLP

REQUESTING DEPARTMENT:

Community Planning & Sustainability
David Driskell, Executive Director
Susan Richstone, Deputy Director
Charles Ferro, Development Review Manager
Chandler Van Schaack, Planner I

OBJECTIVE:

Define the steps for Planning Board consideration of this request:

1. Hear Applicant and Staff presentations
2. Hold Quasi-Judicial Public Hearing
3. Planning Board discussion
4. Planning Board action to approve, approve with conditions or deny

SUMMARY:

Proposal: LAND USE REVIEW: Public hearing and consideration of a Site and Use Review (LUR2014-00057) to construct one new 2,850 square foot, single story Bank of America building with a drive thru facility on the pad site at 1965 28th St. The proposal also includes improvements to the existing parking area serving the pad site as well to the parking area adjacent to the Hazels liquor store. The project site is zoned Business – Regional 1 (BR-1).

Project Name: Bank of America
Location: 1955-1965 28th Street
Size of Tract: 162,106 square feet (3.72-acres)
Zoning: BR-1 (Business - Regional 1)
Comprehensive Plan: Regional Business

KEY ISSUES:

1. Is the proposed Site Review Amendment consistent with the criteria for Amendments to Approved Site Plans as set forth in section 9-2-14(m), B.R.C. 1981?

2. Does the proposed project meet the Use Review criteria as set forth in section 9-2-15(e), B.R.C. 1981?
3. Does the proposed project meet the standards for Drive-Thru Uses set forth in section 9-6-9(c), BRC, 1981?

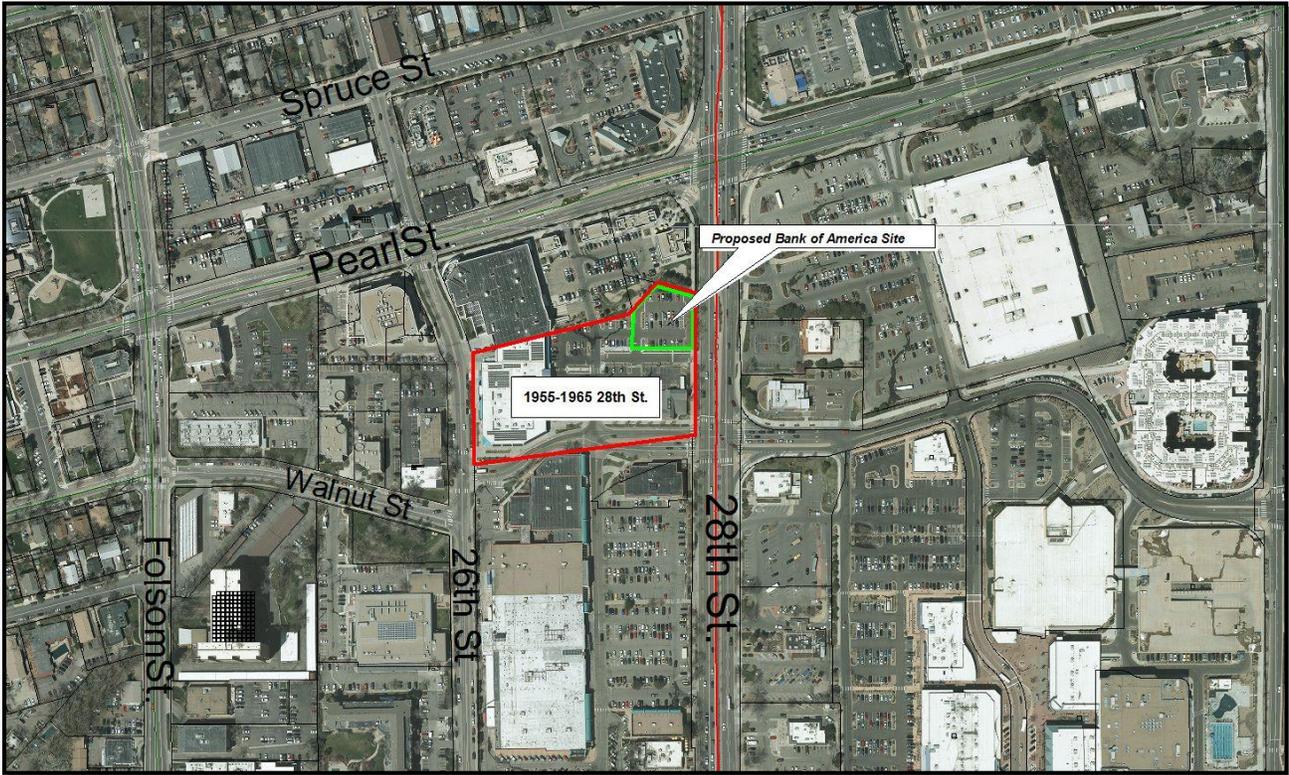


Figure 1: Vicinity Map

BACKGROUND:

Project Description

The intent of this proposal is to amend the 1955 28th St. PUD to construct a new 2,850 square foot, single story Bank of America branch with a drive-thru facility on the former Wendy’s pad site at 1965 28th Street (depicted in green in **Figure 1** above). The Bank massing has been designed to be similar to its surroundings, with a 20’4” roof height which is lower than neighboring buildings and below the maximum height of 35’ allowed by the zone district. The Bank proposal places the drive-through on the west side of the building, where it is least visible from 28th St. and allows the building to be positioned closer to the street, consistent with the existing buildings to the south. The Bank materials consist of a mix of stone, brick, metal panel and stucco. The applicant is proposing a modification to the minimum side yard setback to allow for a 9’ setback where 12’ is the minimum required by the BR-1 zone district standards (See **Figure 2** below for the proposed modification).

The proposal includes reconfiguration of the entire 1955 28th St. parking area and drive aisles in order to reduce vehicular speeds and maintain the previously approved number of parking spaces, and the addition of a new 7’ concrete pedestrian multi-use path running north-south across the site consistent with the adopted BVRC Connections Plan.

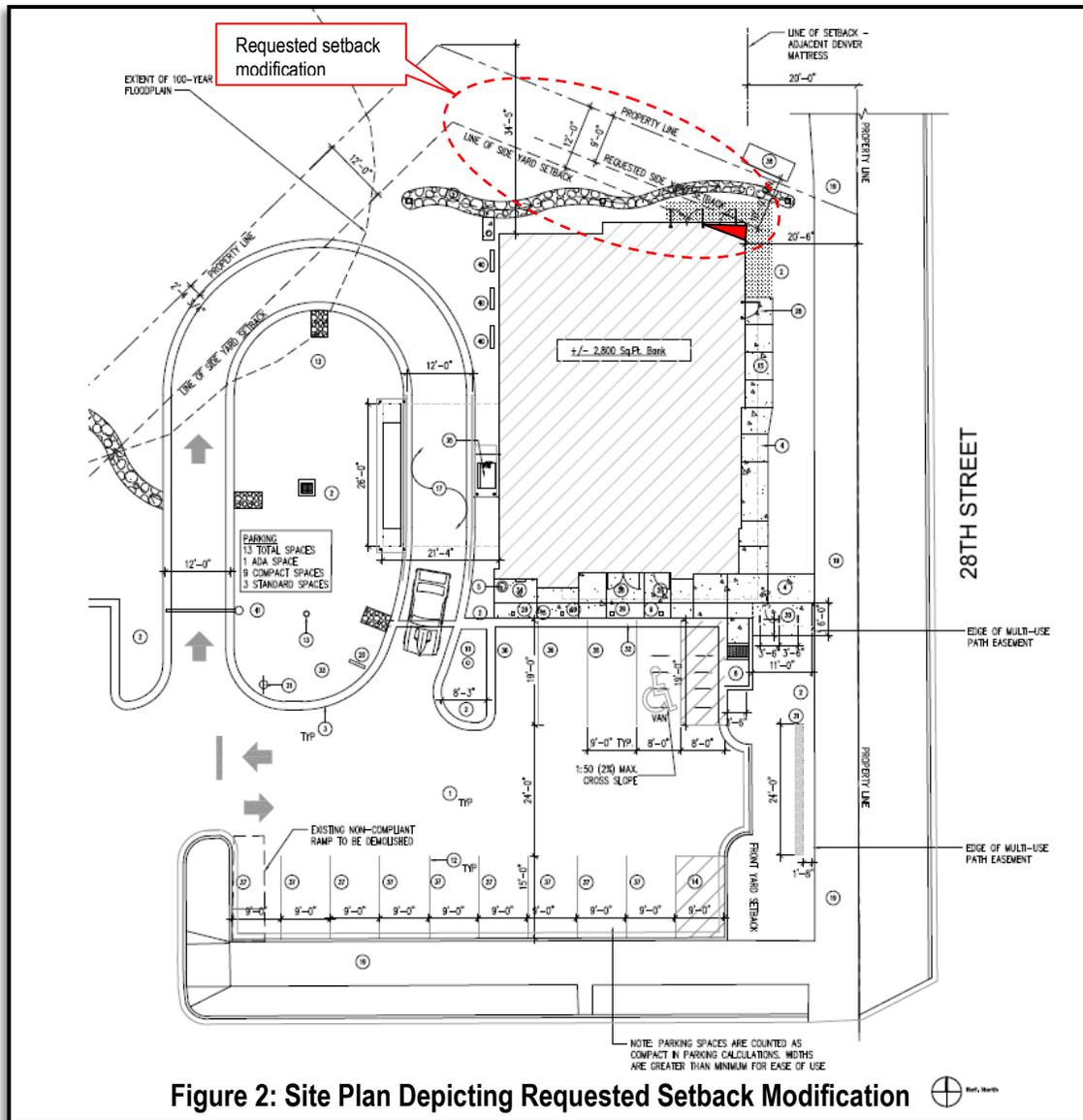


Figure 2: Site Plan Depicting Requested Setback Modification

Parking lot landscaped areas would also be reconfigured, and new interior parking lot landscaping added to the southern portion of the lot in conformance with the parking lot landscaping standards. The proposal adds landscaping buffers in excess of the required size to the perimeter of the Bank of America site, and provides significant landscaping within and around the proposed drive-thru loop. Additional site improvements include providing pedestrian access from the 28th St. sidewalk to the bank and adding colored (red) concrete to the drive aisle in front of Hazel's to improve pedestrian safety. The 13 existing bicycle racks located on the site will be maintained, and a total of 14 new bicycle parking spaces will be added to the site (6 spaces in front of Bank of America and 8 spaces in front of Hazel's). Please see **Figure 3** for further details on proposed improvements, and refer to **Attachment A** for complete plans.

The original approvals allowed for 135 parking spaces to serve all of the retail uses on-site. Under the current proposal, the parking area will be reconfigured and re-stripped in order to maintain 134 parking spaces following construction of the proposed Bank of America building.

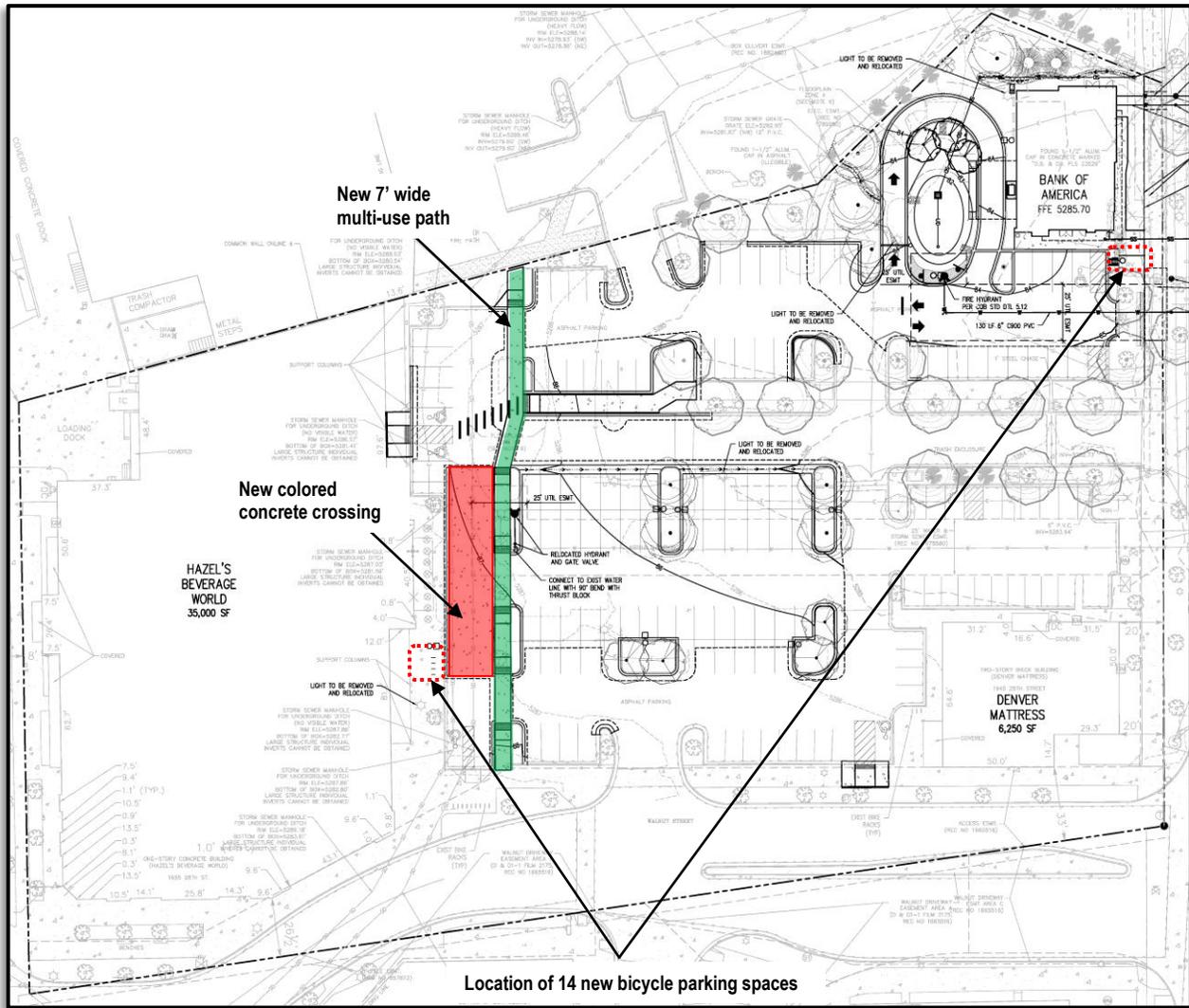


Figure 3: Site Plan Depicting Pedestrian/Bicycle Improvements

Zoning Description

The project is zoned Business Regional One (BR-1), which is defined in the land use code as, *“Business centers of the Boulder Valley, containing a wide range of retail and commercial operations, including the largest regional-scale businesses, which serve outlying residential development; and where the goals of the Boulder Urban Renewal Plan are implemented.”*

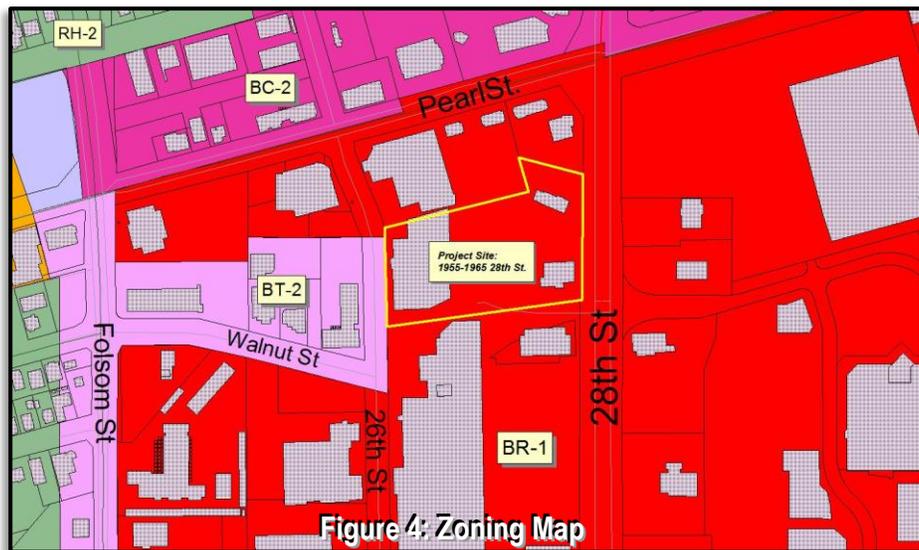


Figure 4: Zoning Map

Refer to **Figure 4** for a Zoning Map. The project site is surrounded primarily by BR-1 zoning, with the

exception of the adjacent properties to the west which are zoned Business Transitional Two (BT-2).

Boulder Valley Comprehensive Plan (BVCP) Designation

As shown in the map below, the subject property is designated Regional Business in the [BVCP](#), which is defined as follows: *“Within these areas are located the major shopping facilities, offices, financial institutions, and government and cultural facilities serving the entire Boulder Valley and neighboring communities. These areas will continue to be refurbished and upgraded and will remain the dominant focus for major business activities in the region.”*

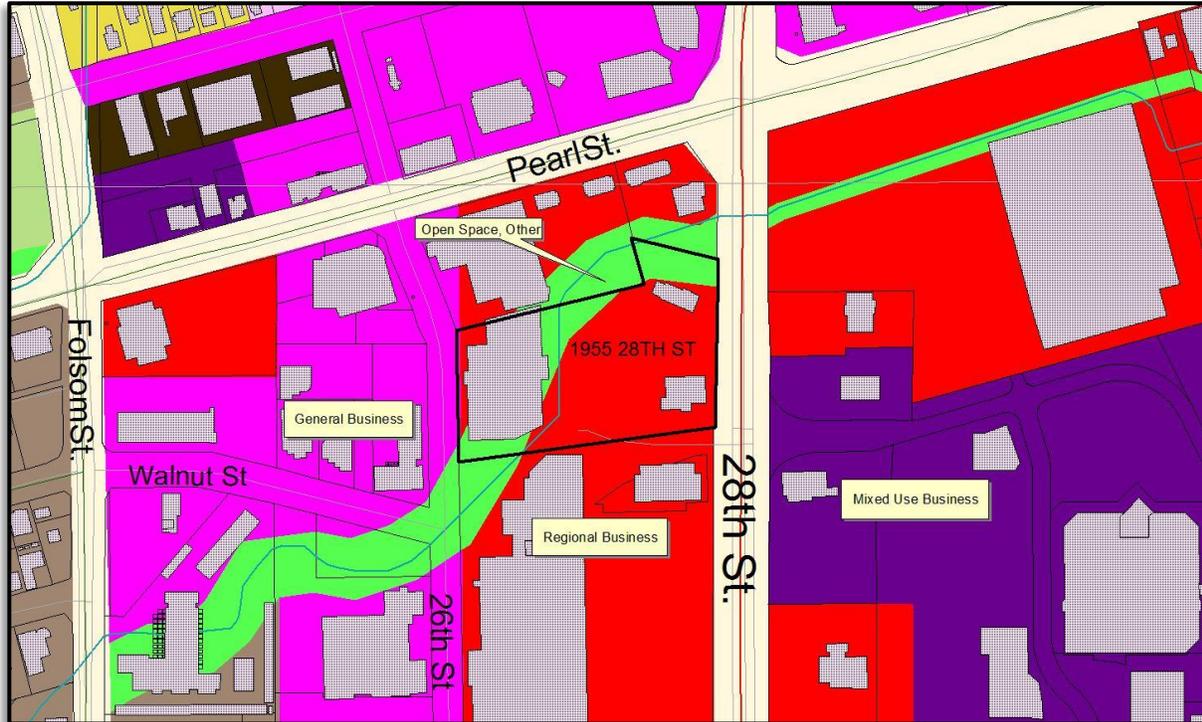


Figure 5: BVCP Land Use Map

There is a strip of land within the property with a land use designation of Open Space – Other, which is defined as *“public and private land designated prior to 1981 that the city and county would like to preserve through various preservation methods including but not limited to intergovernmental agreements, dedications or acquisitions.”* There are no development restrictions associated with this designation; rather, the designation indicates *“that the long-term use of the land is planned to serve one or more open space functions. However, Open Space designations may not reflect the current use of the land while in private ownership.”* In this case, the open space designation surrounds the North Boulder Farmer’s Ditch, which runs under the site. Because the subject property and neighboring property are privately owned and already fully developed, the Open Space land use designation does not impact the types of development allowed on those parcels. See **Figure 5** above for Land Use Designation Map.

Existing Site / Area Context.

The subject site is located within the Boulder Valley Regional Center (BVRC) on the west side of 28th Street between Walnut Street and Pearl Street, and as such is subject to the [BVRC Design Guidelines](#) (the Guidelines). The character of this area is predominantly commercial and retail oriented, with Target and the 29th Street Shopping Center located immediately across 28th Street to the east. To the north is the existing Google office building (formerly Circuit City) and pad restaurant and retail shops. To the south is the Marshall’s Plaza shopping center including Marshall’s, Office Depot, REI and Bed Bath & Beyond.

The 1955 28th St. PUD where the project site is located is comprised of three parcels held under common ownership. Currently, the PUD is developed with a large retail liquor store (Hazel's) and a smaller retail mattress store (Denver Mattress), and includes a large surface parking area that is shared between uses. The proposed Bank of America site is located at the northeast corner of the PUD, and currently contains a surface parking lot providing parking for the two existing retail uses.

The proposed Bank of America site was the previously location of a drive-thru Wendy's restaurant, which was originally approved in 1977 through a Special Review. In 1995, a separate Site Review was approved (as a part of the 1955 28th St. PUD), which included a 35,980 square foot retail building (currently Hazel's Liquors) as well as an additional 4,000 square foot pad site (currently Denver Mattress) to the west and south of the Wendy's site. In 2012, staff approved a Site Review Amendment for the demolition and removal of the existing Wendy's restaurant pad building and the temporary reconfiguration of the parking lot, vehicular access, and landscaping and lighting to serve the Hazel's retail liquor store. The proposed Bank of America building would be located where the former Wendy's stood.

Review Process.

Pursuant to section 9-2-14(m), B.R.C. 1981, a proposal to modify, structurally enlarge, or expand any approved site review that is found to exceed the Minor Amendment standards regarding changes to the intended design character and site arrangement of the development requires an Amendment to the Approved Site Plan in conformance with the Site Review criteria found in section 9-2-14(h), B.R.C. 1981. In addition, pursuant to section 9-6-1, B.R.C. 1981, a Use Review is required for drive-thru uses to operate in the BR-1 zone district. In addition to the Use Review standards found in section 9-2-15(e), B.R.C. 1981, a request for a drive-thru use must meet the conditional use standards for drive-thru uses found in section 9-6-9(c), B.R.C. 1981. Both the Site Review Amendment and Use Review are staff-level decisions subject to call-up by the Planning Board. The subject application was called up by the Planning Board on December 4, 2014.

KEY ISSUES:

Staff has identified the following key issues for the board's consideration:

1. Is the proposed Site Review Amendment consistent with the criteria for Amendments to Approved Site Plans as set forth in section 9-2-14(m), B.R.C. 1981?

Section 9-2-14(m), "Amendments to Approved Site Plans," B.R.C. 1981 includes the procedures and review criteria for approval of an amendment to an approved site review development. The proposal was found to be consistent with the criteria for Amendments to Approved Site Plans found in section 9-2-14(m), B.R.C. 1981. Please refer to [Attachment B](#) for staff's complete analysis of the review criteria.

2. Does the proposed project meet the Use Review criteria as set forth in section 9-2-15(e), B.R.C. 1981?

Section 9-2-15(e), B.R.C. 1981 includes the procedures and review criteria for approval of a Use Review. The proposal was found to be consistent with the criteria for Use Review found in section 9-2-15(e), B.R.C. 1981. Please refer to [Attachment B](#) for staff's complete analysis of the review criteria.

3. Does the proposed project meet the standards for drive thru uses set forth in section 9-6-9(c), BRC, 1981?

The proposal was found to be consistent with the criteria for Drive-Thru Uses found in section 9-6-9(c), B.R.C. 1981. Please refer to **Attachment B** for staff's complete analysis of the review criteria.

PUBLIC COMMENT AND PROCESS:

The required public notice was given in the form of written notification mailed to all property owners within 600 feet of the subject property and a sign posted on the property for at least 10 days. All notice requirements of Section 9-4-2, B.R.C. 1981 have been met. Staff has not received any comments from the public regarding this proposal.

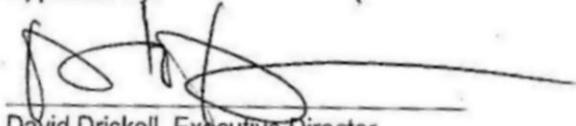
STAFF FINDINGS AND RECOMMENDATION

Staff recommends that the Planning Board approve the Site and Use Review application LUR2014-00057, adopting the staff memorandum as findings of fact, including the attached analysis of review criteria, and subject to the recommended conditions of approval.

RECOMMENDED CONDITIONS OF APPROVAL:

1. The Applicant shall ensure that the **development shall be in compliance with all approved plans** prepared by the Applicant on November 6, 2014 on file in the City of Boulder Planning Department, except to the extent that the development may be modified by the conditions of this approval.
2. The Applicant shall **comply with all previous conditions** contained in any previous approvals, except to the extent that any previous conditions may be modified by this approval, including, but not limited to, the following: the Development Agreement recorded May 16, 1996 at Reception No. 1608284, the Amended Development Agreement recorded August 22, 1997 at Reception No. 1724916 and the Development Agreement recorded May 22, 2012 at Reception No. 03224469.
3. Prior to building permit issuance, the Applicant shall submit a Technical Document Review application for the following items, subject to the approval of the City Manager:
 - a. **Final architectural** plans, including material samples and colors, to insure compliance with the intent of this approval and compatibility with the surrounding area. The architectural intent shown on the approved plans dated November 6, 2014 is acceptable. Planning staff will review plans to assure that the architectural intent is performed.
 - b. A **final site plan** which includes detailed floor plans and section drawings.
 - c. A **final utility plan** meeting the City of Boulder Design and Construction Standards.
 - d. A **final storm water report and plan** meeting the City of Boulder Design and Construction Standards.
 - e. A **detailed landscape** plan, including size, quantity, and type of plants existing and proposed; type and quality of non-living landscaping materials; any site grading proposed; and any irrigation system proposed, to insure compliance with this approval and the City's landscaping requirements. Removal of trees must receive prior approval of the Planning Department. Removal of any tree in City right of way must also receive prior approval of the City Forester.

Approved By:

A handwritten signature in black ink, appearing to read 'David Driskell', written over a horizontal line.

David Driskell, Executive Director
Department of Community Planning and Sustainability

ATTACHMENTS:

- A: [Proposed Plans](#)
- B: [Staff Analysis of Review Criteria](#)
- C: [Staff's Development Review Comments](#)



JVA, Incorporated 1319 Spruce Street
 Boulder, CO 80302 Phone: 303.444.1951
 Fax: 303.444.1957 E-mail: info@jva.com

SITE REVIEW AMENDMENT 1945-1965 28TH STREET BOULDER, COLORADO

AUGUST 4, 2014

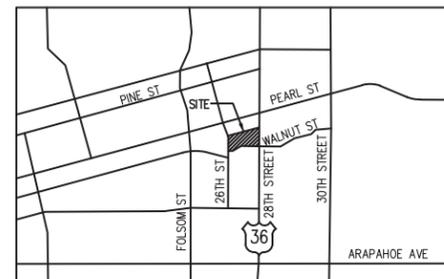
CONTACTS

APPLICANT: BRUCE DIERKING (303) 447-0450
 c/o PACKARD AND DIERKING, LLC
 2595 CANYON BLVD, SUITE 200
 BOULDER, CO 80302-5620

CIVIL ENGINEER: JVA, INC (303) 444-1951
 1319 SPRUCE STREET
 BOULDER, CO 80302
 CHARLES R. HAGER, IV

LANDSCAPE ARCHITECT: STUDIO TERRA, INC. (303) 494-9138
 758 CLUB CIRCLE
 LOUISVILLE, CO 80027
 CAROL ADAMS

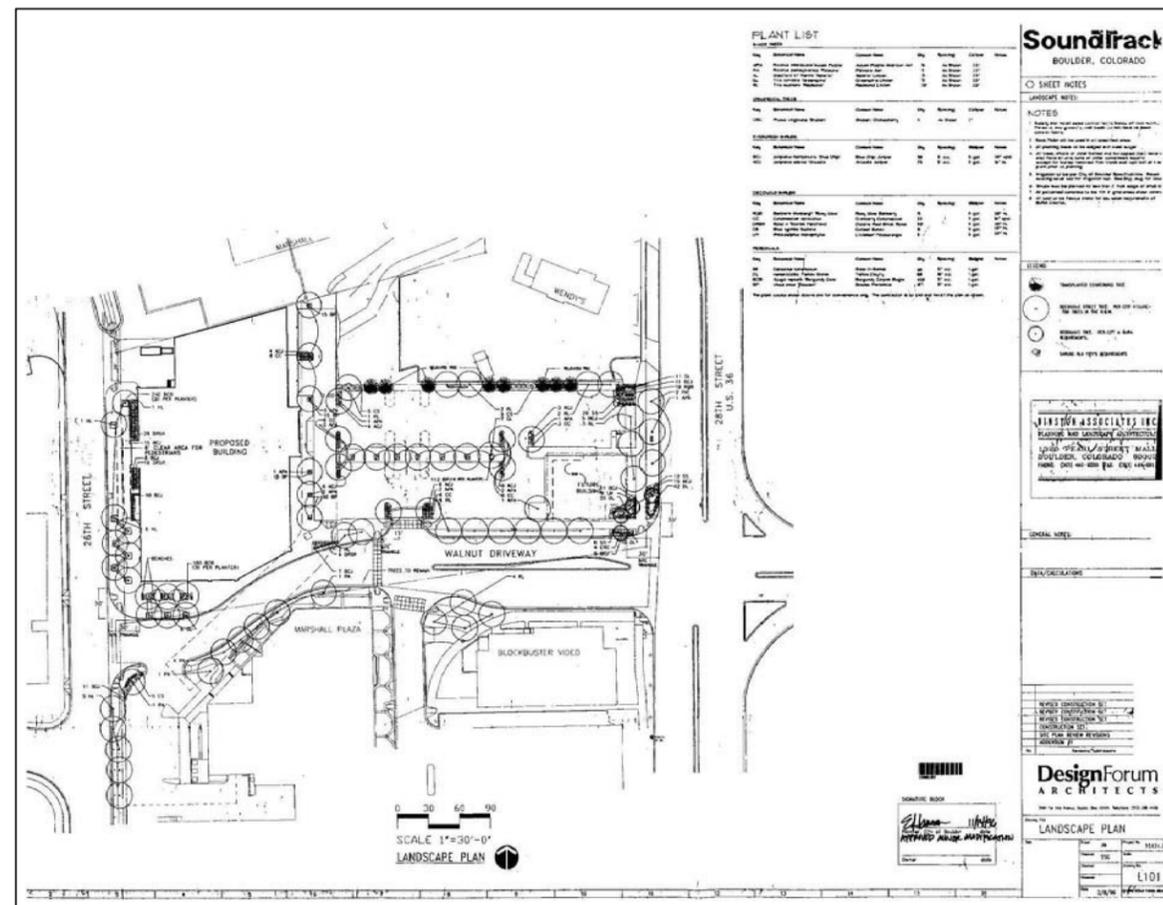
ARCHITECT: GENSLER (303) 595-8585
 1625 BROADWAY, SUITE 400
 DENVER, CO 80202
 JIM LARSON



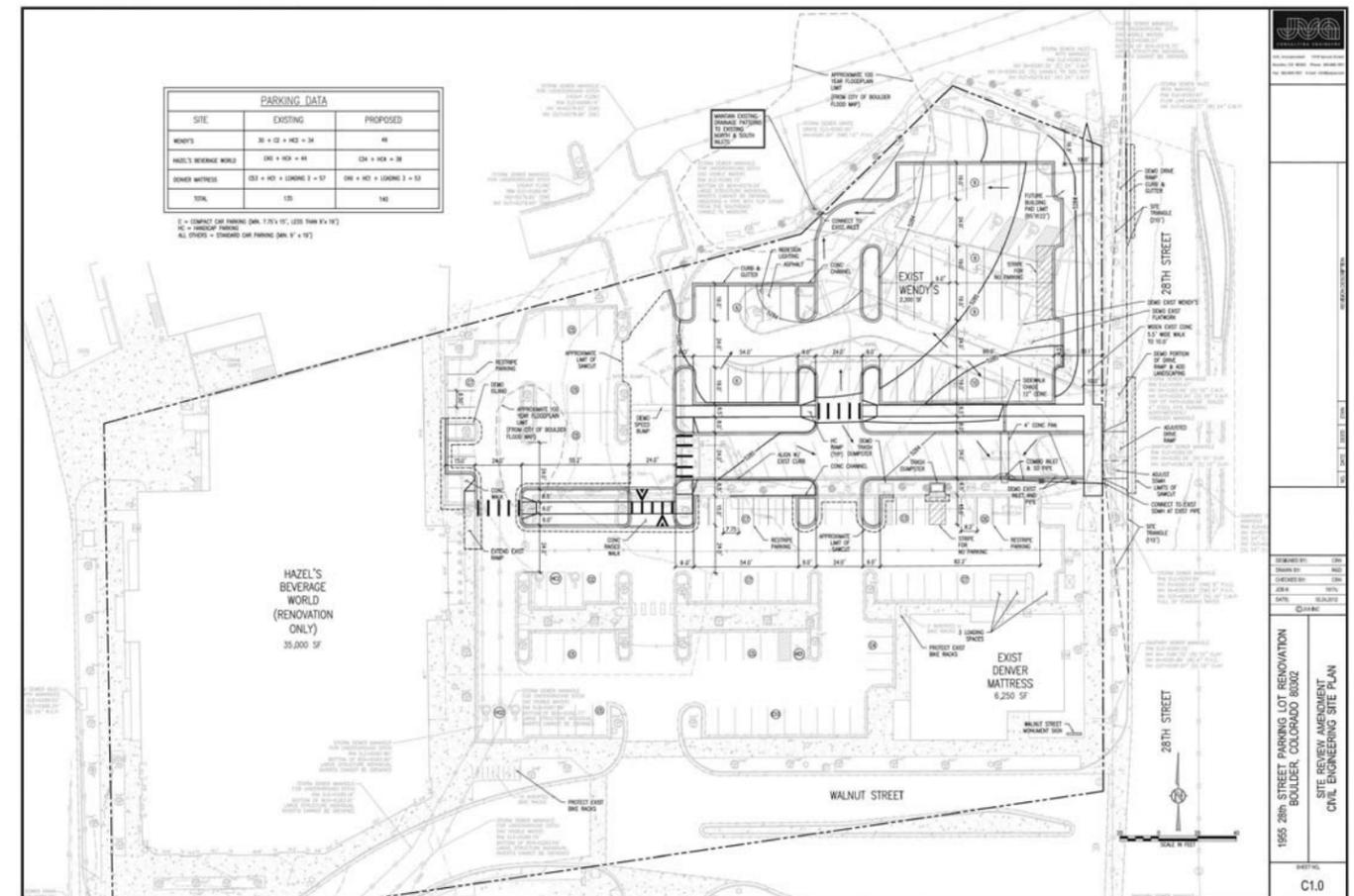
VICINITY MAP
 NTS

DRAWING INDEX

SHEET NO.	TITLE
C0.0	COVER SHEET
C0.1	DEMOLITION PLAN
C1.0	GRADING PLAN
C2.0	UTILITY PLAN
C3.0	HORIZONTAL CONTROL PLAN
L1.0	LANDSCAPE PLAN AND TREE INVENTORY
L2.0	PLANT DETAILS, SCHEDULE AND COMPLIANCE CHART
E1.0	LIGHTING PLAN



PREVIOUS MINOR MODIFICATION
 (APPROVED 11/14/96)



PREVIOUS SITE PLAN AMENDMENT
 (APPROVED 04/13/12)

SITE REVIEW RESUBMITTAL
 NO. DATE DESD CWN
 1 10/06/14 DNS REP

DESIGNED BY: DNS
 DRAWN BY: REP
 CHECKED BY: CRH
 JOB #: 1917.3c
 DATE: 09.04.14
 © JVA INC

1945-1965 28th STREET PARKING LOT RENOVATION
 BOULDER, COLORADO 80302
 SITE AMENDMENT
 COVER SHEET

J:\1917.3c\Drawings\1917.3c - C0.0 - SITE REVIEW.dwg, 11/19/2014 4:31:57 PM, cwg, 1:1



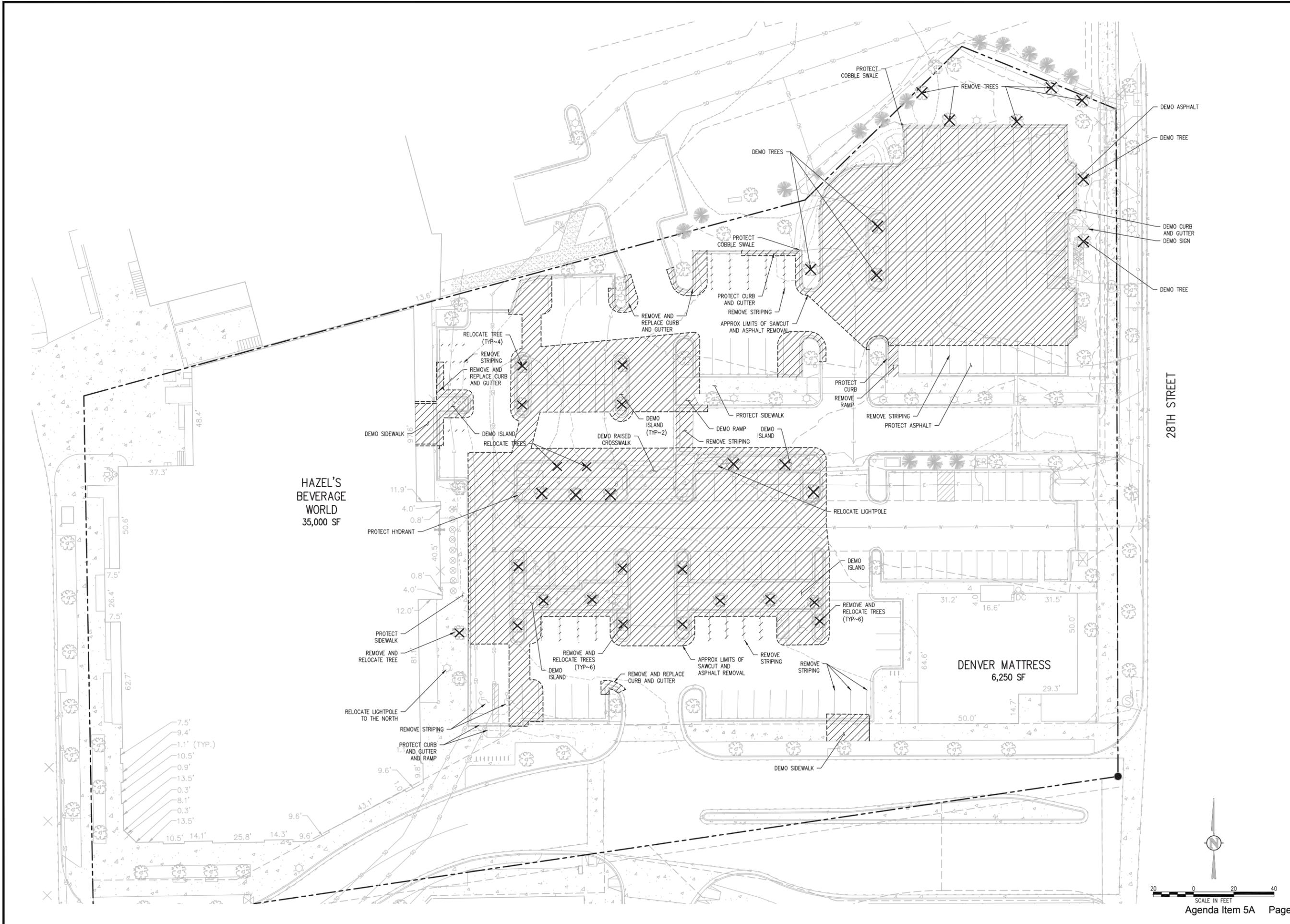
JVA, Incorporated 1319 Spruce Street
 Boulder, CO 80302 Phone: 303.444.1951
 Fax: 303.444.1957 E-mail: info@jva.com

1 10/06/14 DNS REP
 SITE REVIEW RESUBMITTAL
 NO. DATE DESD DWN
 REVISION DESCRIPTION

DESIGNED BY: DNS
 DRAWN BY: REP
 CHECKED BY: CRH
 JOB #: 1917.3c
 DATE: 09.04.14
 © JVA INC

1945-1965 28th STREET PARKING LOT RENOVATION
 BOULDER, COLORADO 80302
 DEMOLITION PLAN

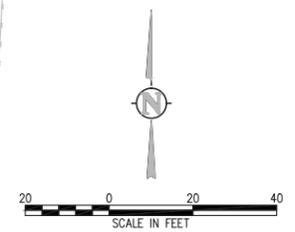
SHEET NO.



28TH STREET

HAZEL'S BEVERAGE WORLD
 35,000 SF

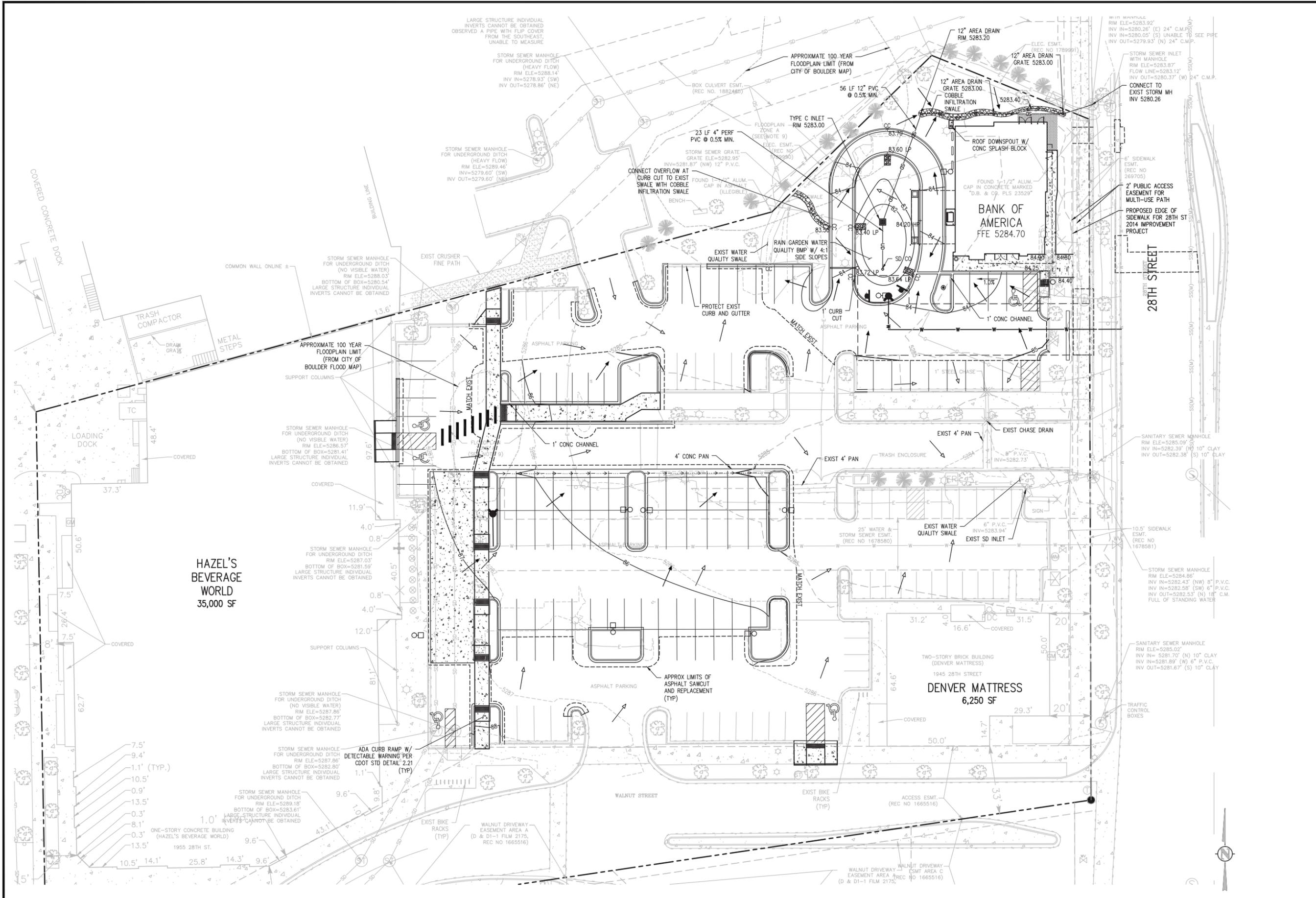
DENVER MATTRESS
 6,250 SF



J:\1917.3c\Drawings\1917.3c - C02.dwg, 11/5/2014, 4:32:10 PM, rrp, 1:1



JVA, Incorporated 1319 Spruce Street
 Boulder, CO 80302 Phone: 303.444.1951
 Fax: 303.444.1957 E-mail: info@jva.com



SITE REVIEW RESUBMITTAL

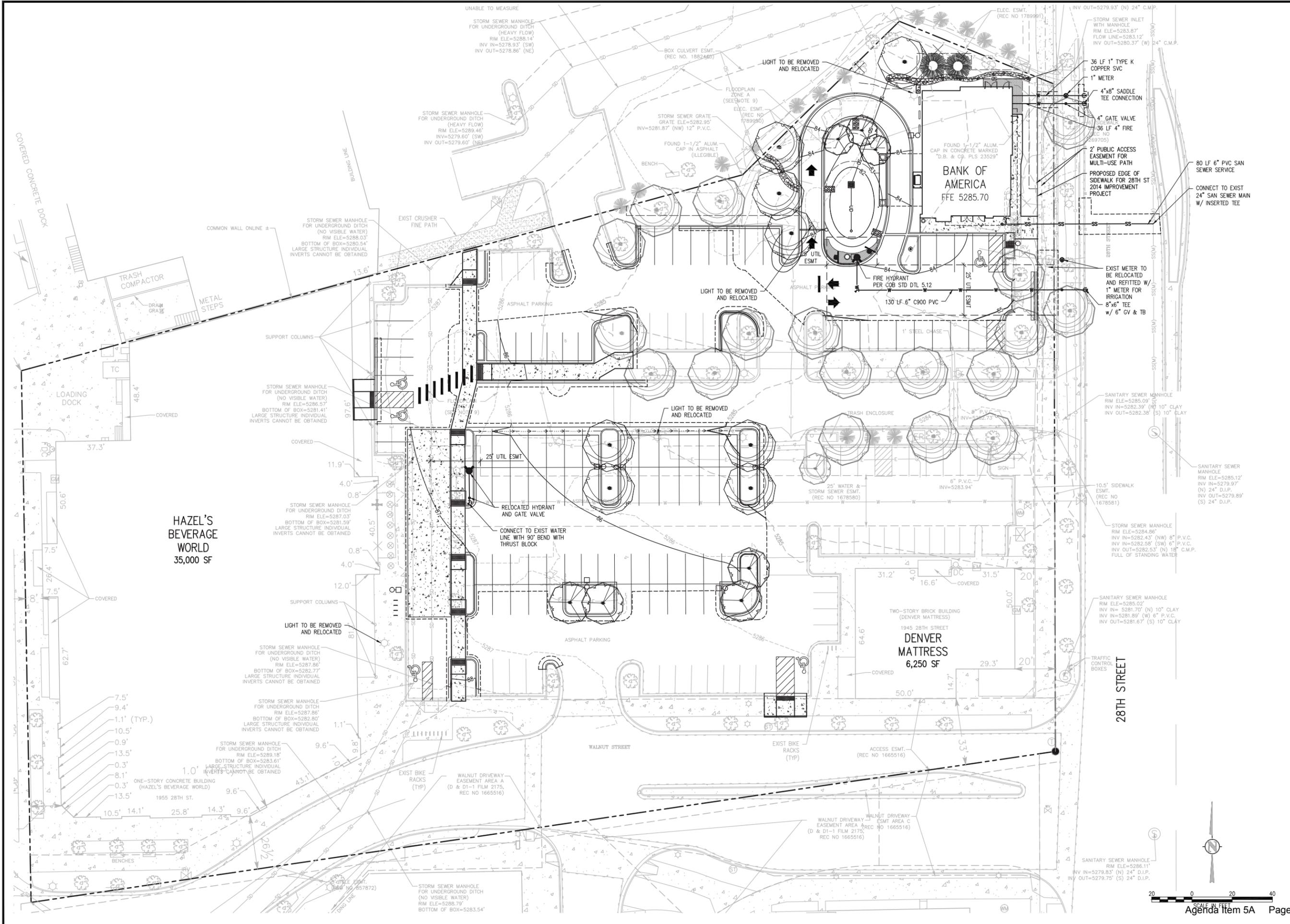
DESIGNED BY:	DNS
DRAWN BY:	REP
CHECKED BY:	CRH
JOB #:	1917.3c
DATE:	09.04.14

1945-1965 28th STREET PARKING LOT RENOVATION
 BOULDER, COLORADO 80302

GRADING AND DRAINAGE PLAN

SHEET NO.

d:\1917.3c\Drawings\1917.3c - C10.dwg, 11/9/2014 4:32:29 PM, rep, 1:1



1 1006/14 DNS REP SITE REVIEW RESUBMITTAL

DESIGNED BY: DNS
DRAWN BY: REP
CHECKED BY: CRH
JOB #: 1917.3c
DATE: 09.04.14

1945-1965 28th STREET PARKING LOT RENOVATION
BOULDER, COLORADO 80302

UTILITY PLAN

SHEET NO.



JVA, Incorporated 1319 Spruce Street
 Boulder, CO 80302 Phone: 303.444.1951
 Fax: 303.444.1957 E-mail: info@jva.com

SITE REVIEW/RESUBMITTAL

REP

DNS

1 10/06/14

INC.

DATE

DESIGN

DRAWN

DESCRIPTION

DESIGNED BY:	DNS
DRAWN BY:	REP
CHECKED BY:	CRH
JOB #:	1917.3c
DATE:	09.04.14

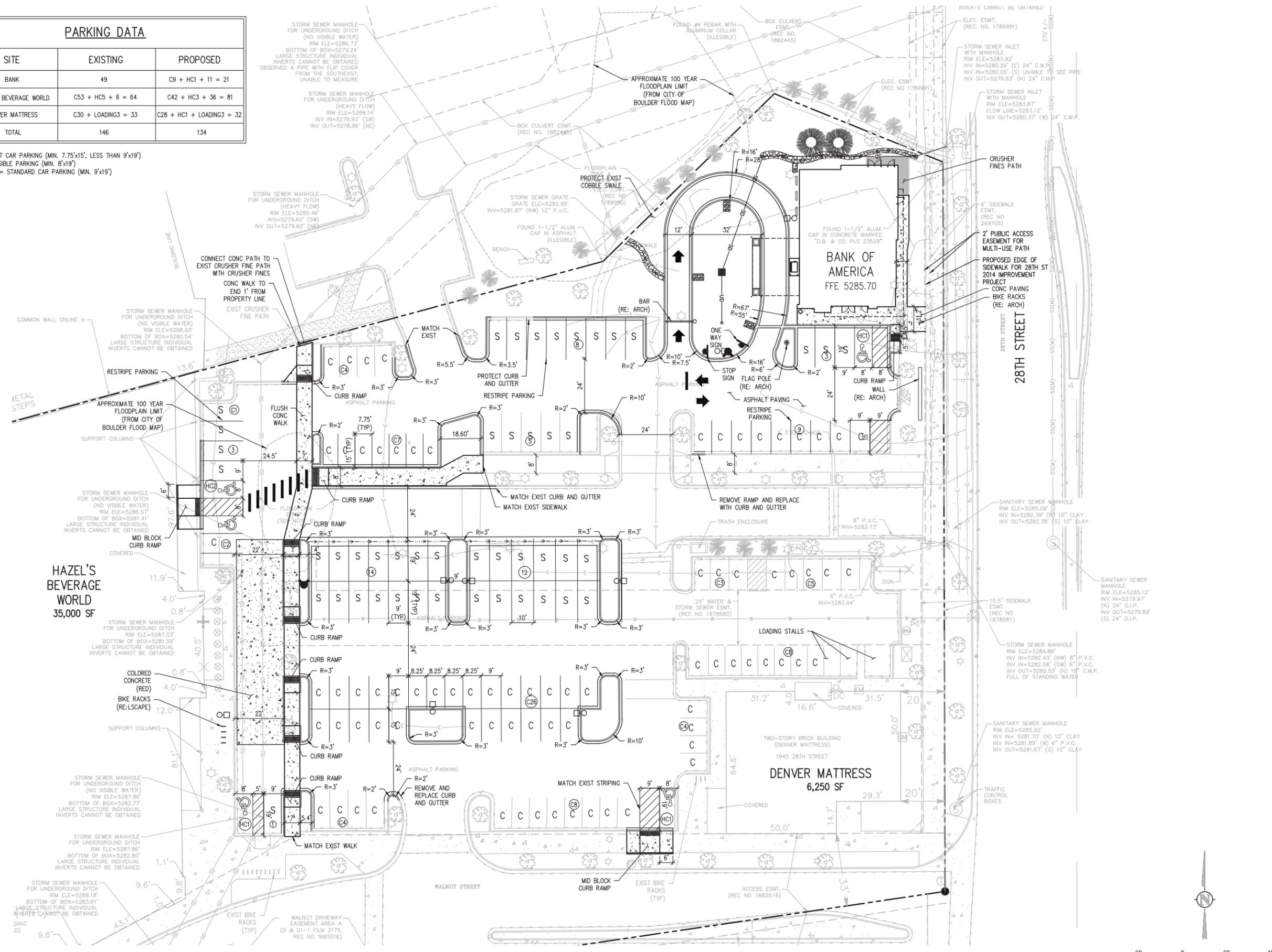
1945-1965 28th Street Parking Lot Renovation
 BOULDER, COLORADO 80302

CIVIL SITE PLAN

SHEET NO. 13 of 53.0

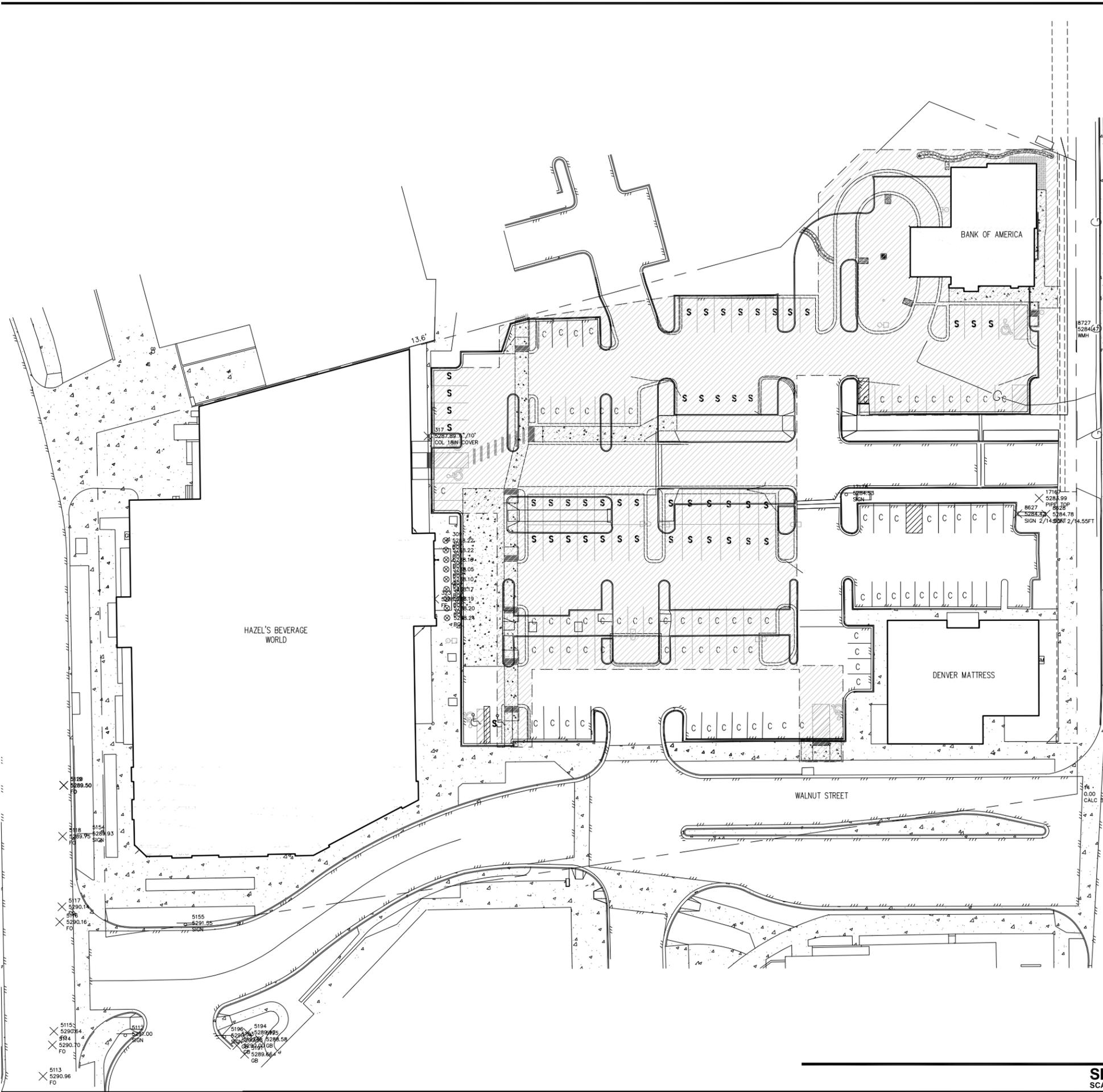
PARKING DATA		
SITE	EXISTING	PROPOSED
BANK	49	C9 + HC1 + 11 = 21
HAZEL'S BEVERAGE WORLD	C53 + HC5 + 6 = 64	C42 + HC3 + 36 = 81
DENVER MATTRESS	C30 + LOADING3 = 33	C28 + HC1 + LOADING3 = 32
TOTAL	146	134

C = COMPACT CAR PARKING (MIN. 7.75'x15', LESS THAN 9'x19')
 HC = ACCESSIBLE PARKING (MIN. 8'x19')
 ALL OTHERS = STANDARD CAR PARKING (MIN. 9'x19')



J:\1917.3c\Drawings\1917.3c - C 3.0.dwg, 11/5/2014, 4:33:05 PM, rps, 1:1

SCALE IN FEET
 Agenda Item 5A Page 13 of 53.0



KEY NOTES

SITE FLOOR AREA TABLE

	EXISTING (SF)	PROPOSED (SF)
DENVER MATTRESS	6,250	6,250
HAZEL'S	35,000	35,000
BANK OF AMERICA	0	2,850
TOTAL	41,250	44,100

PARKING DATA

SITE	EXISTING	PROPOSED
BANK	49	C9+HC1+11=21
HAZEL'S BEVERAGE WORLD	C53+HC5+6=64	C42+HC3+36 = 81
DENVER MATTRESS	C30+L3=33	C28+HC1+L3 = 32
TOTAL	146	134

HATCH INDICATES AREAS OF PROPOSED ADJUSTMENTS



JVA
CIVIL ENGINEERING
1319 SPRUCE STREET
BOULDER, CO 80302
303-444-1951

ANDERSON HASTINGS
STRUCTURAL ENGINEERING
12596 W. BAYAUD AVE.
LAKEWOOD, CO 80228
303-433-8486

SWANSON RINK
MECHANICAL ENGINEERING
1120 LINCOLN STREET
DENVER, CO 80223
303-832-2666

STUDIO TERRA
LANDSCAPE ARCHITECT
758 CLUB CIRCLE
LOUISVILLE, CO 80027
303-494-9138

No.	Revision	Date	By

BANK OF AMERICA BOULDER
CITY OF BOULDER SITE PLAN REVIEW & USE REVIEW
RE-SUBMITTAL
1965 28TH STREET, BOULDER 80301
SITE PLAN

SHEET NOTES

DATE: NOVEMBER 06, 2014
JOB NUMBER: 03.7330.000
SCALE: 1:25

SHEET
A00.00
Page 14 of 52



JVA, Incorporated 1319 Spruce Street
 Boulder, CO 80302 Phone: 303.444.1951
 Fax: 303.444.1957 E-mail: info@jva.com



Gensler
 1625 Broadway
 Ste. 400
 Denver, CO 80202
 303.595.8585

SITE REVIEW RESUBMITTAL
 SITE REVIEW CORRECTIONS

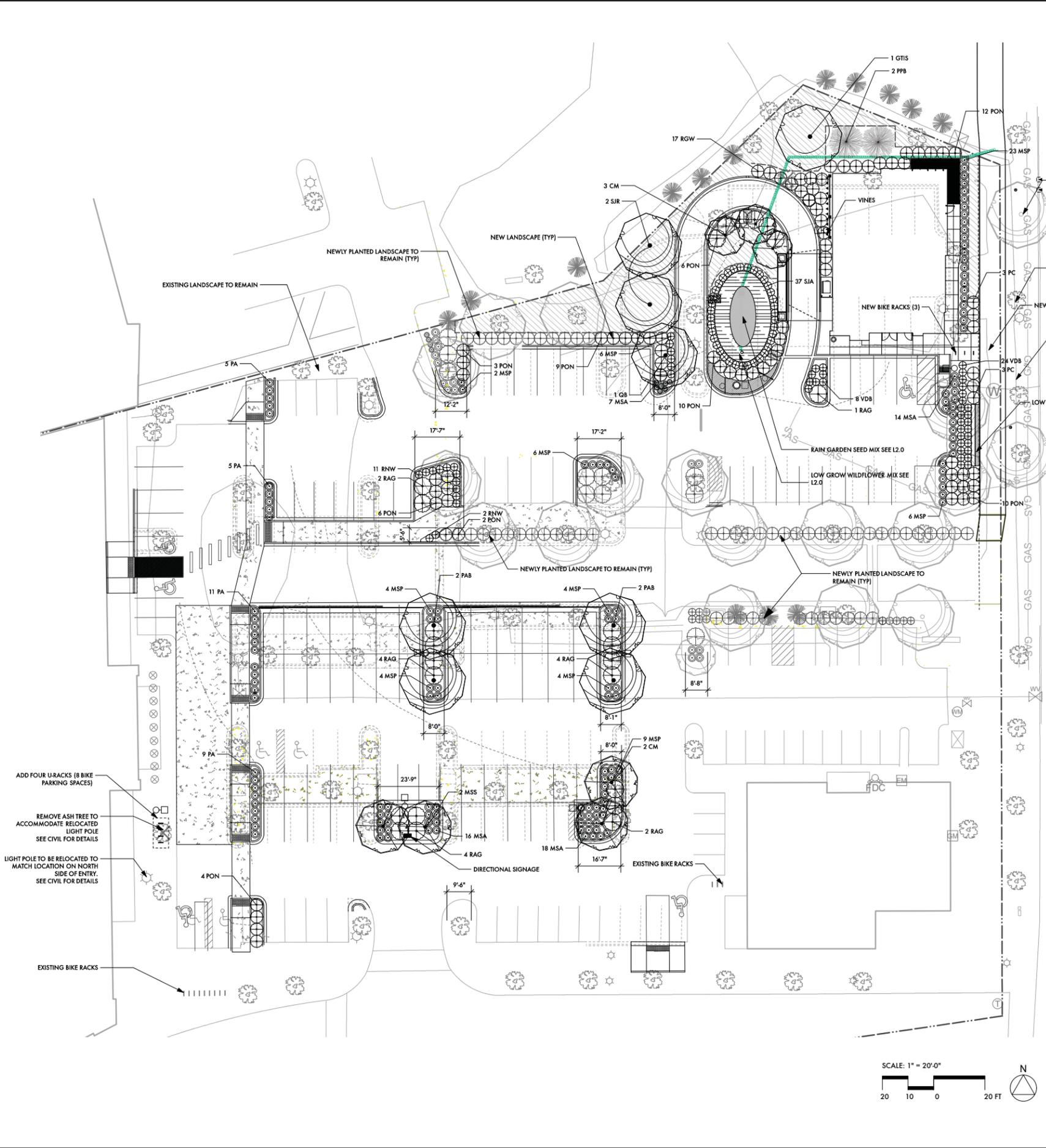
1	10/06/14	CA	CA
2	11/06/14	CA	CA

DESIGNED BY:	
DRAWN BY:	
CHECKED BY:	
JOB #:	
DATE:	8/4/2014
© STUDIOTERRA, INC. 2014	

1945-1965 28TH STREET PARKING LOT
 RENOVATION
 BOULDER, COLORADO 80302

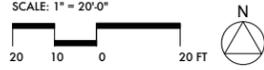
LANDSCAPE PLAN
 SITE REVIEW AMENDMENT

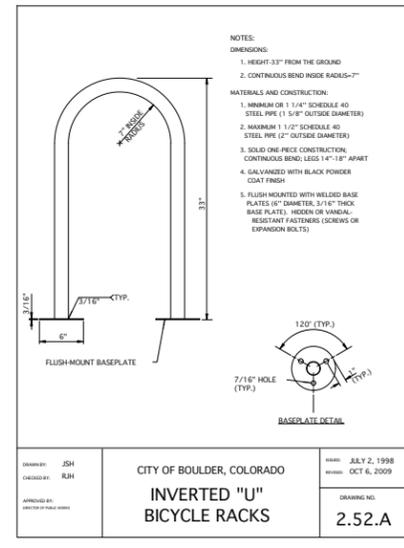
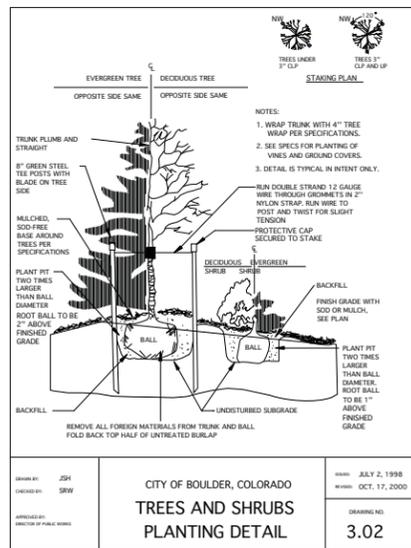
SHEET NO.
L 1.0



- LEGEND:**
- ⊗ EXISTING TREES TO REMAIN OR BE REMOVED - SEE TREE INVENTORY FOR FURTHER DETAIL
 - ⊙ DECIDUOUS TREES - 3" CAL
 - ⊙ ORNAMENTAL TREES - 2" CAL
 - ⊕ SHRUBS - 5 GAL
 - ⊙ ORNAMENTAL GRASSES - 1 GAL
 - ⊙ GROUNDCOVERS - 1 GAL
 - ▨ LOW WATER DEMAND TURF GRASS - SOD
 - ▨ LOW GROW WILDFLOWER SEED MIX
 - ▨ RAIN GARDEN SEED MIX
 - ⊙ U-RACK BIKE RACK
 - LANDSCAPE EDGER

- NOTES:**
- ALL PLANT MATERIAL SHALL MEET SPECIFICATIONS OF THE AMERICAN ASSOCIATION OF NURSEYMEN (AAN) FOR NUMBER ONE GRADE. ALL TREES SHALL BE BALLED AND BURLAPPED OR EQUIVALENT. ALL PLANT MATERIALS SHALL HAVE ALL WIRE, TWINE OR OTHER CONTAINMENT MATERIALS, EXCEPT FOR BURLAP, REMOVED FROM TRUNK AND ROOT BALL OF THE PLANT PRIOR TO PLANTING.
 - TREES SHALL NOT BE PLANTED CLOSER 10 FEET TO ANY PUBLIC SEWER OR WATER LINE (WITH THE EXCEPTION OF WATER SERVICE LINES WHICH MAY BE AS CLOSE AS 5'). TREE PLANTING SHALL BE COORDINATED WITH PUBLIC SERVICE COMPANY. LOCATIONS OF ALL UTILITIES SHALL BE VERIFIED IN THE FIELD PRIOR TO PLANTING.
 - ALL SHRUBS SHALL BE PLANTED NO LESS THAN 3' FROM ANY SIDEWALK OR CURB.
 - GRADES SHALL BE SET TO ALLOW FOR PROPER DRAINAGE AWAY FROM STRUCTURES. GRADES SHALL MAINTAIN SMOOTH PROFILES AND BE FREE OF SURFACE DEBRIS, BUMPS, AND DEPRESSIONS.
 - OWNERS SHALL ENSURE THAT THE LANDSCAPE PLAN IS COORDINATED WITH THE PLANS DONE BY OTHER CONSULTANTS SO THAT THE PROPOSED GRADING, STORM DRAINAGE, OR OTHER CONSTRUCTIONS DOES NOT CONFLICT NOR PRECLUDE INSTALLATION AND MAINTENANCE OF LANDSCAPE ELEMENTS ON THIS PLAN.
 - ALL SHRUB BEDS ADJACENT TO TURF AREAS SHALL BE EDGED WITH RYERSON OR APPROVED EQUIVALENT STEEL EDGER.
 - ALL SHRUB BED AREAS, PERENNIALS AND GROUNDCOVER SHALL BE MULCHED WITH A 4" LAYER OF SHREDDED BARK MULCH.
 - PRIOR TO INSTALLATION OF PLANT MATERIALS, AREAS THAT HAVE BEEN COMPACTED OR DISTURBED BY CONSTRUCTION ACTIVITY SHALL BE THOROUGHLY LOOSENEED; ORGANIC SOIL AMENDMENTS SHALL BE INCORPORATED AT THE RATE OF AT LEAST FOUR (4) CUBIC YARDS PER 1000 SQUARE FEET OF LANDSCAPE AREA.
 - ALL LANDSCAPE (PLANT MATERIALS AND GRASS) WILL BE IRRIGATED WITH AN AUTOMATIC SYSTEM. TURF AREAS WILL HAVE A SPRAY ZONE OR SUB-SURFACE DRIP. SHRUBS AND TREES WILL HAVE A DRIP ZONE AND PERENNIALS/GROUNDCOVERS (PART OF THE DRIP ZONE) WILL HAVE MICRO-JET SPRAYS OR DRIP.
 - PLANTS ARE GROUPED BY WATER USE ZONE TO CONSERVE WATER.
 - CONTRACTOR SHALL VERIFY ALL MATERIAL QUANTITIES PRIOR TO INSTALLATION. ACTUAL NUMBER OF PLANT SYMBOLS SHALL HAVE PRIORITY OVER THE QUANTITY DESIGNATED.
 - REFER TO THE CITY OF BOULDER DESIGN AND CONSTRUCTION STREETSCAPING STANDARDS FOR ALL WORK WITHIN PUBLIC AREAS.
 - REFER TO THE CIVIL ENGINEER DRAWINGS FOR GRADING AND UTILITY INFORMATION.
 - THIS PLAN MEETS OR EXCEEDS CITY OF BOULDER LANDSCAPE CODE REQUIREMENTS.
 - REFER TO THE CITY OF BOULDER DESIGN AND CONSTRUCTION STANDARDS FOR TREE PROTECTION REQUIREMENTS.
 - REFER TO THE TREE INVENTORY FOR INFORMATION ON TREES TO REMAIN AND TREES TO BE REMOVED. (FOUR INVENTORIED TREES TO BE REMOVED)
 - REUSE EXISTING PLANT MATERIAL WHERE POSSIBLE. IF EXISTING MATERIAL IS NOT ABLE TO BE RETAINED, PROVIDE REPLACEMENT PLANTS AT SIZES INDICATED IN THE PLANT SCHEDULE.





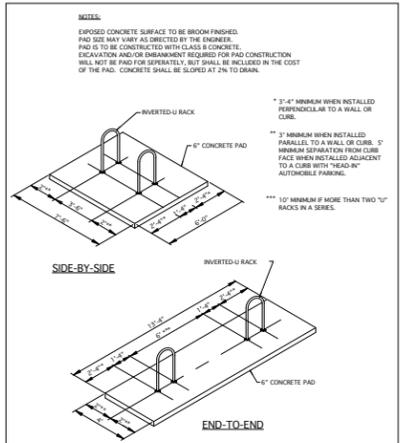
PLANT SCHEDULE

QUANT	PERENNIALS/VINES BOTANICAL NAME	COMMON NAME	HEIGHT	SPREAD	WATER USE	EXPOSURE	FLOWER COLOR	SEASON
CP	CERATOSTIGMA PLUMBAGINOIDES	PLUMBAGO	8-12"	18-24"	LOW	ADAPTABLE	BLUE	MID TO LATE SUMMER
PN	POTENTILLA NEUMANNIANA 'NANA'	DWARF SPRING CINQUEFOIL	12-18"	2-4"	LOW	SUN TO FS	BUTTER YELLOW	LATE SPRING
VM	VINCA MINOR 'BOWLES VARIETY'	BOWLES PERIWINKLE	12-18"	4-6"	LOW	ADAPTABLE	BLUE	EARLY SPRING TO MID-SUMMER
PQ	PARTHENOCISSUS QUINQUEFOLIA	VIRGINIA CREEPER	N/A	N/A	LOW	SUN TO FS	N/A	FALL
PT	PARTHENOCISSUS TRICUSPIDATA	BOSTON IVY	N/A	N/A	MEDIUM	SHADE	N/A	FALL
CT	CAMPIS X TAGLIABUANA 'MADAME GLEN'	MADAME GALEN TRUMPET VINE	N/A	N/A	LOW	SUN	ORANGE	SUMMER
0								
QUANT	ORNAMENTAL GRASSES BOTANICAL NAME	COMMON NAME	HEIGHT	SPREAD	WATER USE	EXPOSURE	FLOWER COLOR	SEASON
MSA	MISCANTHUS SINENSIS 'ADAGIO'	COMPACT MAIDEN GRASS	2-3'	2-3'	MEDIUM	SUN	PINK	LATE SUMMER
MSL	MISCANTHUS SINENSIS 'MORNING LIGHT'	MORNING LIGHT MAIDEN GRASS	4-5'	2-3'	MEDIUM	SUN	BRONZE	LATE SUMMER
MSP	MISCANTHUS SINENSIS PURPURASCENS	FLAME (PURPLE MAIDEN) GRASS	3-4'	2-3'	MEDIUM	SUN	BRONZE TO SILVERY	LATE SUMMER
PA	FENNISETUM ALOPECUROIDES	FOUNTAIN GRASS	3-4'	24-30"	LOW	SUN	TAN	LATE SUMMER
0								
QUANT	CONTAINER SHRUBS BOTANICAL NAME	COMMON NAME	HEIGHT	SPREAD	WATER USE	EXPOSURE	FLOWER COLOR	SEASON
PON	PHYSOCARPUS OPULIFOLIUS NANUS	NINEBARK, DWARF	4-5'	4-5'	LOW	SUN TO FS	WHITE	LATE SPRING
PC	PRUNUS X CISTENA	PLUM, PURPLE LEAF	6-8'	4-6'	MEDIUM	SUN	PALE PINK	MID-SPRING
RAG	RHUS APOMATICA GRO-LOW	SUMAC, DWARF FRAGRANT	2-3'	6-8'	LOW	SUN TO FS	YELLOW	EARLY SPRING
RA	RIBES ALPNUM	CURRIANT, ALPINE	3-6'	3-6'	LOW	SUN TO FS	YELLOWISH-GREEN	MID-SPRING
RGW	ROSA X GOLDEN WINGS	ROSE, SINGLE YELLOW SHRUB	3-5'	4-6'	LOW	SUN	YELLOW	EARLY SUMMER
RNW	ROSA X NEARLY WILD	ROSE, SINGLE PINK SHRUB	2-3'	2-3'	LOW	SUN	PINK	EARLY TO LATE SUMMER
SJA	SPIREA JAPONICA ANTHONY WATERER	SPIREA, ANTHONY WATERER	2-3'	2-4'	MEDIUM	SUN TO FS	ROSE RED	EARLY SUMMER
VDB	VIBURNUM DENTATUM BLUE MUFFIN	VIBURNUM, BLUE MUFFIN	3-5'	3-4'	MEDIUM	ADAPTABLE	WHITE	SPRING
0								
QUANT	DECIDUOUS TREES BOTANICAL NAME	COMMON NAME	HEIGHT	SPREAD	WATER USE	EXPOSURE	FLOWER COLOR	SEASON
GTIS	1 GLEDITSIA TRIACANTHOS INERMIS SHADEMASTER	HONEYLOCUST, SHADEMASTER	40-50'	30-40'	LOW	SUN	N/A	N/A
PAB	4 PLATANUS X ACERIFOLIA BLOODGOOD	PLANETREE, BLOODGOOD	70-100'	65-80'	MEDIUM	SUN	N/A	N/A
SJR	2 SOPHORA JAPONICA 'REJENT'	REGENT JAPANESE PAGODA TREE	40-50'	30-40'	MEDIUM	SUN TO FS	CREAMY WHITE	SUMMER
7								
QUANT	ORNAMENTAL TREES BOTANICAL NAME	COMMON NAME	HEIGHT	SPREAD	WATER USE	EXPOSURE	FLOWER COLOR	SEASON
CM	5 CRATAEGUS X MORDENENSIS SNOWBIRD	HAWTHORN, SNOWBIRD	15-20'	15-20'	LOW	SUN	WHITE	SPRING
MSS	2 MALUS SPRING SNOW	CRABAPPLE, SPRING SNOW	20-25'	20-25'	MEDIUM	SUN	WHITE	SPRING
5								
QUANT	EVERGREEN TREES BOTANICAL NAME	COMMON NAME	HEIGHT	SPREAD	WATER USE	EXPOSURE	FLOWER COLOR	SEASON
PPB	2 PICEA PUNGENS 'BABY BLUE EYES'	BABY BLUE EYES SPRUCE	20-30'	10-15'	MEDIUM	SUN	N/A	N/A
2								

THIS PLANT SCHEDULE ASSUMES FULL PLANTINGS SHOULD EXISTING PLANTS NOT SURVIVE TRANSPLANTING OR PLANTS TO REMAIN IN PLACE DO NOT SURVIVE CONSTRUCTION.

1 **TREE AND SHRUB PLANTING DETAIL**
SCALE: not to scale

2 **INVERTED - U BIKE RACKS**
SCALE: not to scale



LANDSCAPE COMPLIANCE

LANDSCAPE REQUIREMENTS

STREETSCAPE	LF	TREES		NOTES
		REQUIRED	PROVIDED	
28TH STREET	165 LF	5	5	STREET TREES NOT AFFECTED WITH SITE REVIEW AMENDMENT
TOTAL		5	5	

SITE LANDSCAPE

TOTAL SITE AREA 72,224 SF

	SF	TREES		SHRUBS	
		REQUIRED	PROVIDED	REQUIRED	PROVIDED
BUILDINGS, DRIVES, PARKING	57,860 SF	10	16	50	100 PLUS
NET SITE AREA	14,364 SF				

TREES PROVIDED DOES NOT INCLUDE STREETSCAPE OR PARKING LOT TREES OR PARKING LOT SHRUBS OF THE 16 TREES PROVIDED, 9 ARE NEW AND 7 ARE EXISTING TO REMAIN.

PARKING LOT LANDSCAPE

140 SPACES EXISTING, 138 PROPOSED (REQUIRES 5% LANDSCAPE AREA)

	TOTAL AREA	SF LANDSCAPE		TREES		COVERAGE/SCREENING
		REQUIRED	PROVIDED	REQUIRED	PROVIDED	
NEW LOT - INTERIOR LANDSCAPE	57,310 SF	2,866 SF	3,222 SF	15	15	FULL PLANT COVERAGE IN ISLANDS AND FULL 42" SCREENING ALONG 28TH STREET AND NORTH EDGE OF LOT
TOTAL				15	15	

DOES NOT INCLUDE "NON-COMPLIANT" PARKING LOT LANDSCAPING THAT TOTALS AN ADDITIONAL 1,015 SF OF LANDSCAPE



4 **LANDSCAPE CALCULATIONS**
Scale: 1" = 40 ft

3 **INVERTED - U BIKE RACKS - LAYOUT**
SCALE: not to scale

Arkansas Valley Seed
Supplying QUALITY Seed WITH INTEGRITY Since 1945

Low Grow Wildflower Mix

The Low Grow Wildflower Mix offers a combination of both annuals and perennials that will be lower growing. Grows from 8 to 20 inches high and is very drought tolerant.

Characteristics:
 > Grows 8-20 inches tall
 > Annuals & Perennials

Seeding Rate:
 > 6-8 lbs per Acre
 > 1 lb per 6,000 Sq Ft
 > 1/2 lb per 3,000 Sq Ft

Mix contains:

12% Cornflower	5% Sweet William
10% Baby's Breath	5% Dwarf Lance-Leaf Coreopsis
10% Blue Flax	3% Dwarf Plains Coreopsis
8% Sweet Alyssum	3% Annual Candy Tuft
8% Chinese Forget-Me-Not	2% Dwarf Red Coneflower
8% Dwarf Godetia	2% Gallardia Aristata
8% Wall Flower, Siberian	1% Tussock Bellflower
8% California Poppy	1% Johnny Jump-Up
5% Shasta Daisy	1% Snow-in-Summer

RAIN GARDEN SEED MIX

COMMON NAME	SPECIES	VARIETY	PLS LBS PER ACRE	OUNCES PER ACRE
Sand bluestem	Andropogon hallii	Garden	3.5	
Sideoats grama	Bouteloua curtipendula	Butte	3	
Western wheatgrass	Pascopyrum smithii	Ariba	3	
Little bluestem	Schizachyrium scoparium	Patura	3	
Alkali sacaton	Sporobolus airoides		3	
Sand dropseed	Sporobolus cryptandrus		3	
Pasture sage	Artemisia frigida			2
Blue aster	Aster laevis			4
Blanket flower	Gaillardia aristata			8
Prairie coneflower	Ratibida columnifera			4
Purple prairieclover	Dalea (Petaloestemum) purpurea			4
Subtotals			18.5	22
Total lbs/acre			18.5	22

5 **SEED MIXES**

CONSULTING ENGINEERS
JVA, Incorporated 1319 Spruce Street
Boulder, CO 80302 Phone: 303.444.1951
Fax: 303.444.1957 E-mail: info@jvape.com

STUDIOTERRA
THE CLIVE CLINE & ASSOCIATES
BOULDER, CO 80502
303.484.3138
CLIVE@STUDIOTERRA.COM

Gensler
1625 Broadway
Ste. 400
Denver, CO 80202
303.595.8585

SITE REVIEW RESUBMITTAL
SITE REVIEW CORRECTIONS

NO. DATE DES'D DWN

1 10/06/14 CA CA
2 11/05/14 CA CA

DESIGNED BY:
DRAWN BY:
CHECKED BY:
JOB #:
DATE: 8/4/2014
© STUDIOTERRA, INC. 2014

1945-1965 28TH STREET PARKING LOT RENOVATION BOULDER, COLORADO 80302

PLANT DETAILS, SCHEDULE AND COMPLIANCE CHART SITE REVIEW AMENDMENT

SHEET NO.
L 2.0

LEGEND	
[Symbol]	MAIN DISTRIBUTION CENTER
[Symbol]	PANELBOARD
[Symbol]	ELECTRIC SERVICE METER
[Symbol]	CURRENT TRANSFORMER
[Symbol]	TRANSFORMER
[Symbol]	CONCEALED CIRCUIT
[Symbol]	UNDERFLOOR / UNDERGROUND CIRCUIT
[Symbol]	EXPOSED CIRCUIT
[Symbol]	WM WIREMOLD (SURFACE WIREWAY)
[Symbol]	PM PUSHMOLD
[Symbol]	HOMERUN TO PANELBOARD (ONE ARROW / CNT, PNL & CNT #S SHOWN)
[Symbol]	CRUIT NUMBER(S) FOR SPECIFIED PANEL
[Symbol]	CONDUIT TURNS UP
[Symbol]	CONDUIT TURNS DOWN
[Symbol]	JUNCTION BOX
[Symbol]	PORCELAIN LAMP HOLDER (PC PULL CHAIN)
[Symbol]	LIGHT FIXTURE: SURFACE MOUNTED
[Symbol]	RECESSED LIGHT FIXTURE
[Symbol]	WALL MOUNTED LIGHT FIXTURE
[Symbol]	EXIT LIGHT: DIRECTIONAL ARROW
[Symbol]	BATTERY PACK
[Symbol]	SURFACE FLUORESCENT FIXTURE
[Symbol]	RECESSED FLUORESCENT FIXTURE
[Symbol]	SINGLE OUTLET: C-CLOCK (+70)
[Symbol]	DUPLEX RECEPTACLE (G: ISOLATED GROUND S: SAFETY)
[Symbol]	DUPLEX RECEPTACLE - GFCI
[Symbol]	SPLIT WIRE DUPLEX
[Symbol]	QUADRAPLEX (DOUBLE DUPLEX)
[Symbol]	COMB. SWITCH / RECEPTACLE
[Symbol]	FLOOR MOUNTED RECEPTACLE
[Symbol]	SPECIAL PURPOSE (AS NOTED)
[Symbol]	TELEVISION OUTLET
[Symbol]	MOTOR OUTLET (sfc - switched thermal overload)
[Symbol]	TELEPHONE TERMINAL
[Symbol]	TELEPHONE OUTLET
[Symbol]	FLOOR MTD. TELEPHONE OUTLET
[Symbol]	COMPUTER OUTLET
[Symbol]	COMB. TELE/COMPUTER OUTLET
[Symbol]	TOGGLE SWITCH (switching p-pilot light 2-2 pole 4-wired 3-S way 1p-thermal overload 4-4 way 1-miner)
[Symbol]	MOTION DETECTOR
[Symbol]	DIMMER
[Symbol]	THERMOSTAT
[Symbol]	TIME SWITCH
[Symbol]	PHOTOCELL
[Symbol]	PUSHBUTTON STATION
[Symbol]	SAFETY SWITCH
[Symbol]	MOTOR STARTER / LIGHTING CONTACTOR
[Symbol]	COMBINATION MOTOR STARTER
[Symbol]	RELAY
[Symbol]	FIRE ALARM CONTROL PANEL
[Symbol]	ANNUNCIATOR
[Symbol]	PULL STATION
[Symbol]	HORN
[Symbol]	HORN / LIGHT COMBINATION
[Symbol]	HORN / STROBE
[Symbol]	STROBE
[Symbol]	BELL
[Symbol]	OS & Y VALVE
[Symbol]	FLOW SWITCH
[Symbol]	ROOM DETECTOR (SMOKE)
[Symbol]	ROOM DETECTOR (THERMAL)
[Symbol]	DUCT DETECTOR
[Symbol]	REMOTE INDICATING LIGHT / TEST SWITCH
[Symbol]	DOOR HOLDER
[Symbol]	CHIME
[Symbol]	FUSED SWITCH
[Symbol]	CIRCUIT BREAKER
[Symbol]	GROUND CONNECTOR
[Symbol]	MECHANICAL EQUIPMENT
[Symbol]	DETAIL NOTE
[Symbol]	KITCHEN / MEDICAL EQUIPMENT
[Symbol]	EXISTING TO REMAIN
[Symbol]	EXISTING TO BE REPLACED
[Symbol]	EXISTING TO BE DEMOLISHED

COMcheck Software Version 3.9.4
Exterior Lighting Compliance Certificate

2012 IECC

Section 1: Project Information

Project Type: New Construction
 Project Title: Parking Lot Renovation
 Exterior Lighting Zone: 2 (Light industrial area with limited nighttime use)

Section 2: Exterior Lighting Area/Surface Power Calculation

A	B	C	D	E	F
Exterior Area/Surface	Quantity	Allowed Watts / Unit	Tradable Wattage	Allowed Watts (B x C)	Proposed Watts
Parking area	95850 ft ²	0.06	Yes	5751	3870
		Total Tradable Watts** =		5751	3870
		Total Allowed Watts =		5751	
		Total Allowed Supplemental Watts** =		600	

* Wattage tradeoffs are only allowed between tradable areas/surfaces.
 ** A supplemental allowance equal to 600 watts may be applied toward compliance of both non-tradable and tradable areas/surfaces.

Section 3: Exterior Lighting Fixture Schedule

A	B	C	D	E
Fixture ID : Description / Lamp / Wattage Per Lamp / Ballast	Lamps/ Fixture	# of Fixtures	Fixture Watt.	(C X D)
Parking area (95850 ft ²): Tradable Wattage				
Single Head, Type IV: AA2: Metal Halide: Standard:	1	12	165	1980
Twin Head, Type IV: AA3: Metal Halide: Standard:	2	6	315	1890
		Total Tradable Proposed Watts =		3870

Section 4: Compliance Statement

Compliance Statement: The proposed exterior lighting design represented in this document is consistent with the building plans, specifications and other calculations submitted with this permit application. The proposed lighting system has been designed to meet the 2012 IECC requirements in COMcheck Version 3.9.4 and to comply with the mandatory requirements of the 2012 IECC Checklist.

Gerald Novotny, P.E.
 Name - Title
 Signature: *Gerald Novotny*
 Date: 9/11/14

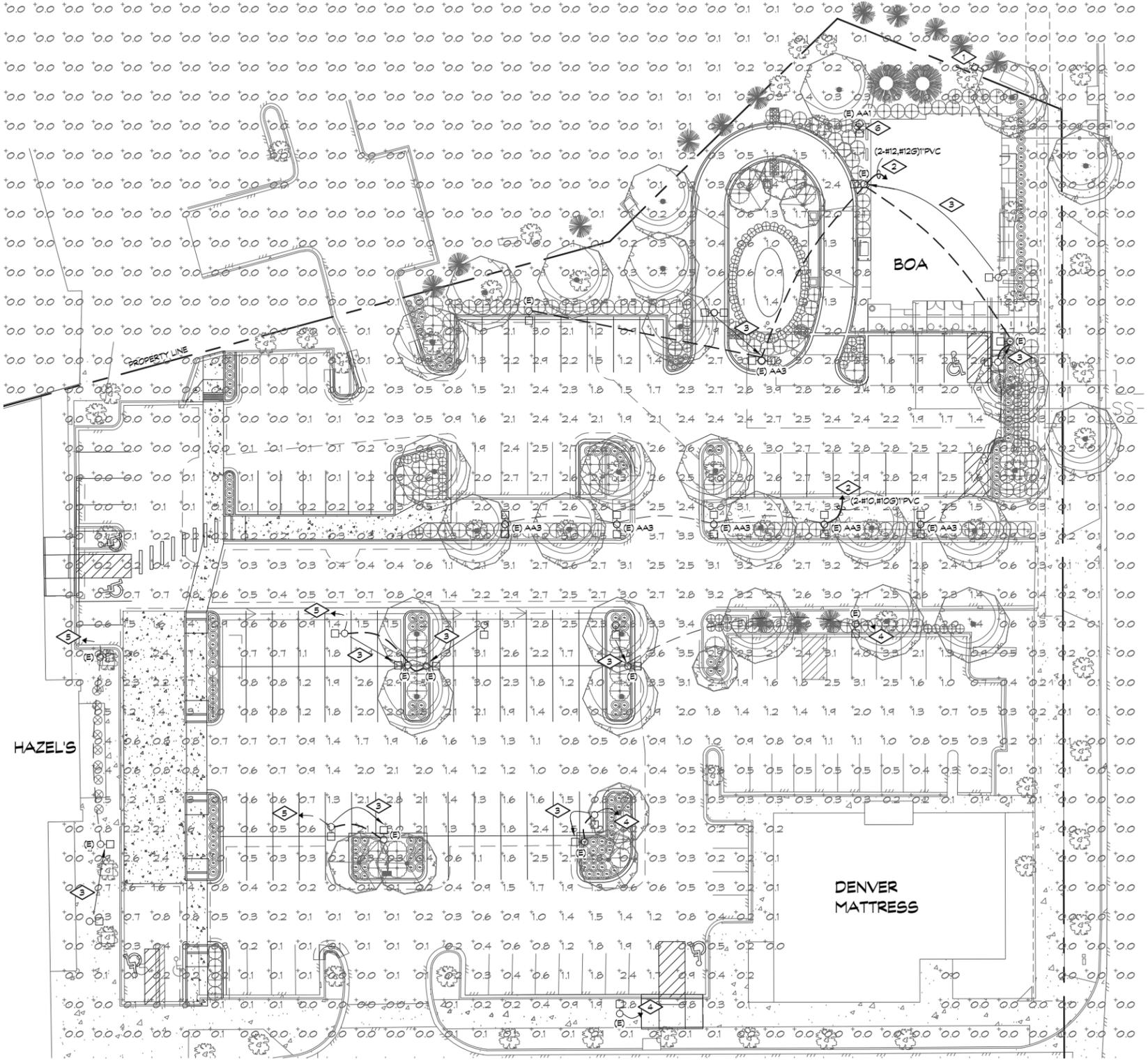
LUMINAIRE SCHEDULE

KEY	LAMP	DESCRIPTION	CELG (DEPTH)	MANUFACTURER#	VOLT
(E) AA1	M50/HOR (3000 LUM, 85 CR)	FULL CUTOFF LUMINAIRE, SINGLE HEAD, TYPE III DIST, CAST ALUMINUM, SQUARE POLE DARK BRONZE FINISH, HOUSE SIDE SHIELD	20' POLE (TOTAL HEIGHT W/ 30" H BASE)	LITHONIA KSE1 150M R3 120 SPO4 - HS	120
(E) AA3	M50/HOR (3000 LUM, 85 CR)	FULL CUTOFF LUMINAIRE, TWIN HEAD, TYPE IV DIST, CAST ALUMINUM, SQUARE POLE DARK BRONZE FINISH	20' POLE (TOTAL HEIGHT W/ 30" H BASE)	LITHONIA KSE1 150M R4SC 120 SPO4	120
(E)	M50/HOR (3000 LUM, 85 CR)	EXISTING FULL CUTOFF LUMINAIRE, SINGLE HEAD, TYPE IV DIST, CAST ALUMINUM, SQUARE POLE DARK BRONZE FINISH	20' POLE (TOTAL HEIGHT W/ 30" H BASE)	EXISTING	120

NOTES: *NOTIFY ENGINEER OF ANY DISCREPANCIES BETWEEN MODEL NUMBERS AND DESCRIPTIONS PRIOR TO ORDERING
 **VERIFY CEILING INSULATION W/ GC AND NOTIFY ENGINEER OF ANY IC RATINGS CONFLICTS PRIOR TO ORDERING

DETAIL NOTES THIS SHEET

- EXISTING PEDESTAL MOUNTED ELECTRICAL SERVICE.
- EXISTING CIRCUIT FED FROM PEDESTAL MOUNTED ELECTRICAL SERVICE, RE-FEED FROM BANK.
- RELOCATE EXISTING POLE TO NEW LOCATION. EXTEND EXISTING WIRING TO NEW LOCATION FROM INDICATED CIRCUIT.
- LIGHTING CIRCUIT FED FROM DENVER MATTRESS.
- LIGHTING CIRCUIT FED FROM HAZEL'S.
- EXISTING POLE MOUNTED LIGHT TO BE REMOVED.



SITE LIGHTING PLAN

T = 20

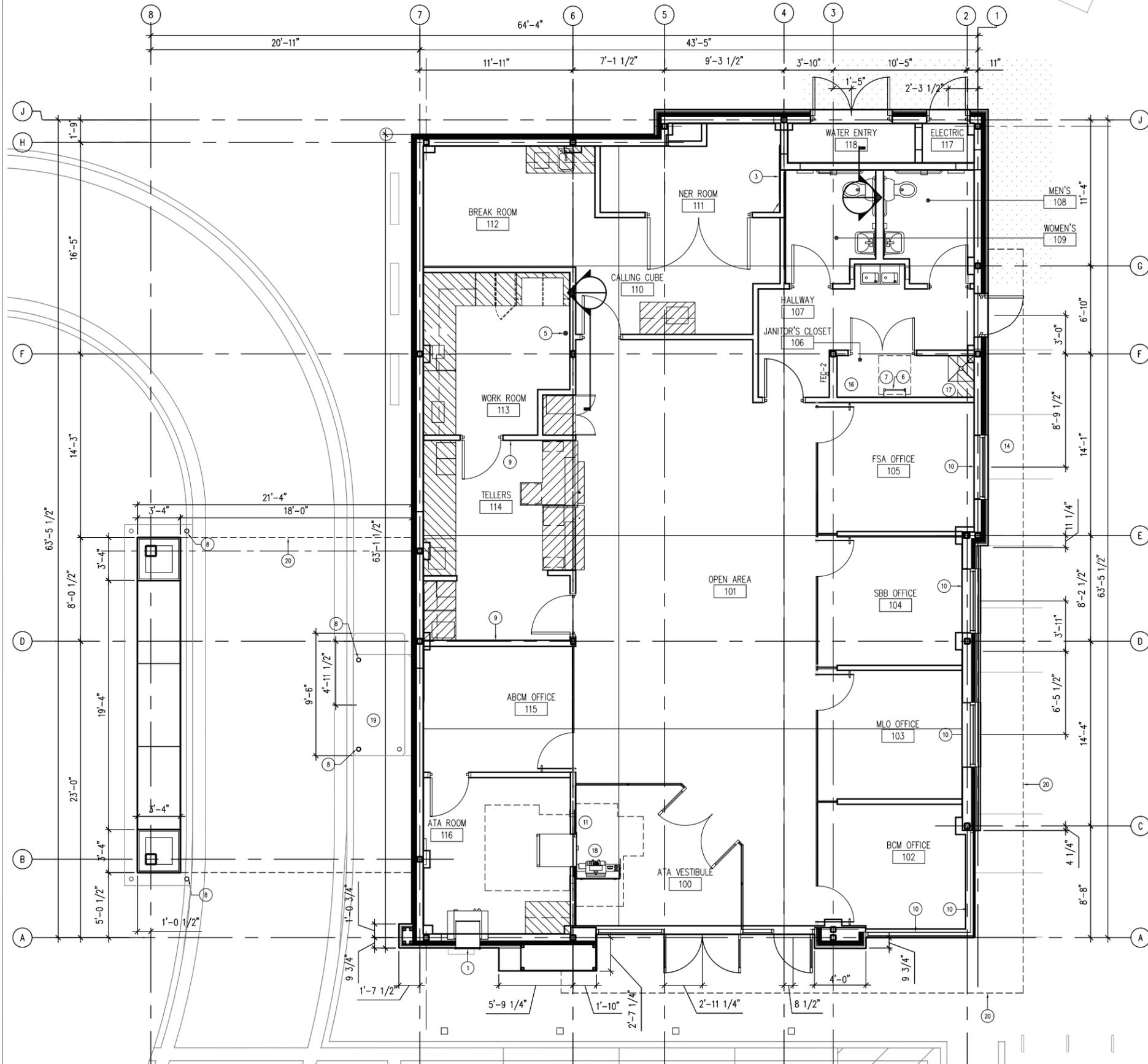


JVA, Incorporated 1319 Spruce Street
 Boulder, CO 80302 Phone: 303.444.1951
 Fax: 303.444.1957 E-mail: info@jva.com

boulder engineering
 plumbing, mechanical and electrical
 1717 15th Street
 Boulder, CO 80302
 303.444.6038 phone
 303.442.1172 fax
 staff@boulderengineering.com

DESIGNED BY:	EM
DRAWN BY:	ER
CHECKED BY:	GFN
JOB #:	14098
DATE:	09.04.14

1945-1965 28th STREET PARKING LOT RENOVATION
 BOULDER, COLORADO 80302



KEY NOTES

- ① WALK-UP NIGHT DEPOSITORY
- ② FIRE PROTECTION EQUIPMENT, REFER TO ELECTRICAL DRAWINGS
- ③ PROVIDE FIRE-RATED 3/4" PLYWOOD BACKBOARD ON THIS WALL, H=8'-0".
- ④ NOT USED.
- ⑤ PROVIDE HEAVY DUTY FLOOR STOP PER SPEC- COORDINATE LOCATION WITH CASH VAULT DOOR.
- ⑥ LADDER TO ROOF HATCH - SEE DETAIL 12/A09.32
- ⑦ ROOF HATCH ABOVE - SEE DETAIL 8/A09.30
- ⑧ BOLLARD - SEE DETAIL 5/A09.30
- ⑨ PREP WALLS FOR FUTURE BANDIT BARRIER INSTALLATION.
- ⑩ ROLLER SHADE
- ⑪ REFER TO MANUFACTURER'S REQUIREMENTS FOR WALL FRAMING AROUND ATAs
- ⑫ NOT USED
- ⑬ NOT USED
- ⑭ CLEAN OUT LOCATION, REFER TO PLUMBING DRAWINGS.
- ⑮ REFER TO CIVIL DRAWINGS FOR SLOPE DIRECTION.
- ⑯ LOCATION OF THE TRASH CONTAINER
- ⑰ LOCATION OF THE MOP BUCKET
- ⑱ ACCESSIBLE ATA SHALL COMPLY WITH AMERICAN DISABILITY ACT 2010.
- ⑲ DRIVE UP ATA WITH TELLER ASSIST.
- ⑳ LINE OF OVERHANG ABOVE.

SHEET NOTES



JVA
CIVIL ENGINEERING
1319 SPRUCE STREET
BOULDER, CO 80302
303-444-1951

ANDERSON HASTINGS
STRUCTURAL ENGINEERING
12596 W. BAYAUD AVE.
LAKEWOOD, CO 80228
303-433-8486

SWANSON RINK
MECHANICAL ENGINEERING
1120 LINCOLN STREET
DENVER, CO 80223
303-832-2666

STUDIO TERRA
LANDSCAPE ARCHITECT
758 CLUB CIRCLE
LOUISVILLE, CO 80027
303-494-9138

No.	Revision	Date	By

BANK OF AMERICA BOULDER
CITY OF BOULDER SITE PLAN REVIEW & USE REVIEW
RE-SUBMITTAL
1965 28TH STREET, BOULDER 80301
CONSTRUCTION PLAN

DATE: NOVEMBER 06, 2014
JOB NUMBER: 03.7330.000
SCALE: 1/4" = 1'-0"

SHEET
A00.02
Page 19 of 52



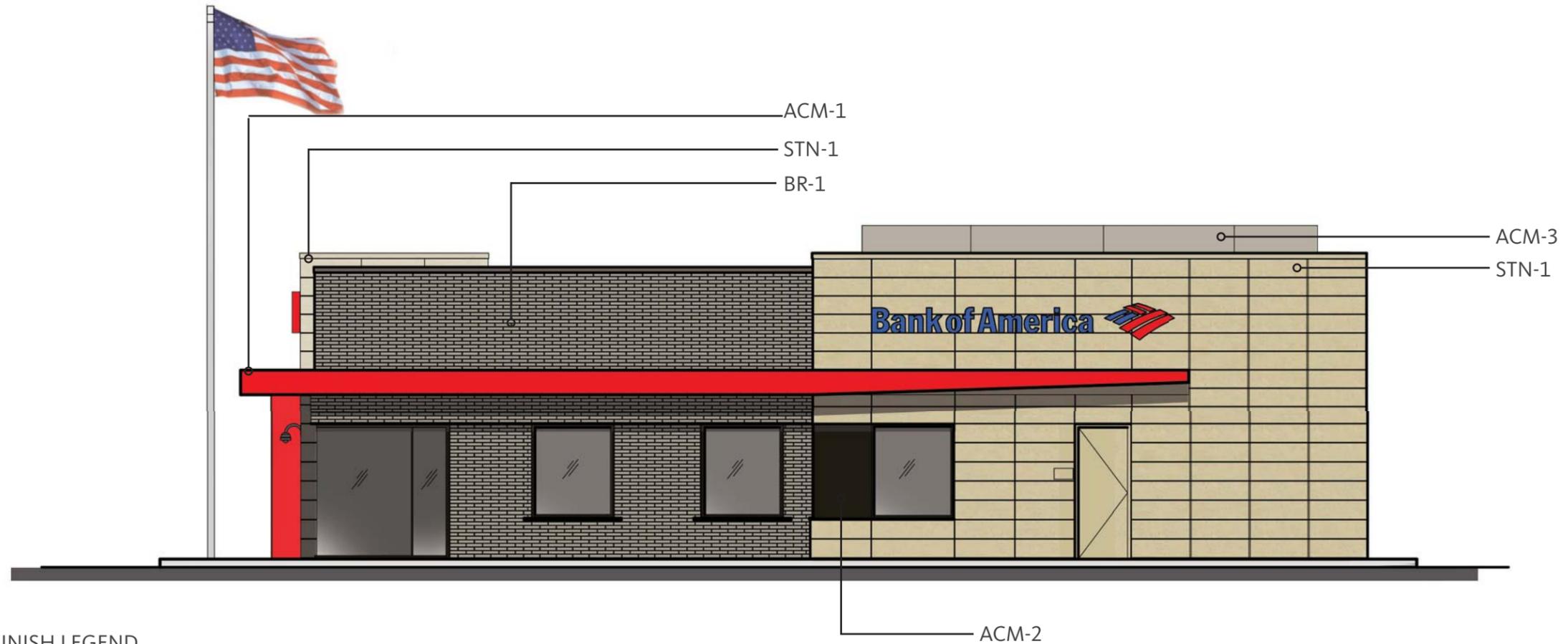
FINISH LEGEND

STN-1	STONE - STONE SOURCE CARPINO - HONED
ST-1	STUCCO - PAREX 3021L 'CAVERN'
BR-1	BRICK - ACME SLATE GRAY, NORMAN SIZE
ACM-1	ALUMINUM COMPOSITE METAL PANEL - 'BRITE RED'
ACM-2	ALPOLIC JBR BRONZE
ACM-3	MBX MEDIUM BRONZE METALLIC



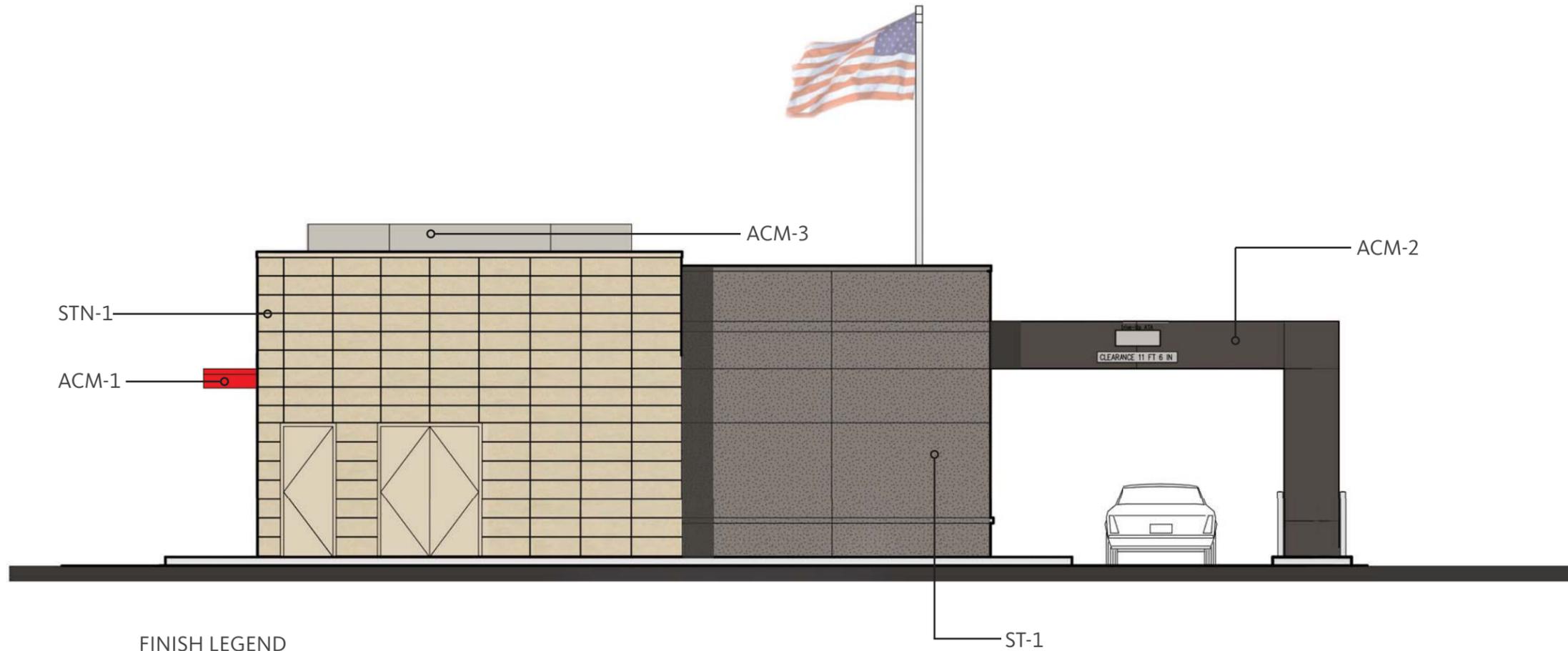
FINISH LEGEND

STN-1	STONE - STONE SOURCE CARPINO - HONED
ST-1	STUCCO - PAREX 3021L 'CAVERN'
BR-1	BRICK - ACME SLATE GRAY, NORMAN SIZE
ACM-1	ALUMINUM COMPOSITE METAL PANEL - 'BRITE RED'
ACM-2	ALPOLIC JBR BRONZE
ACM-3	MBX MEDIUM BRONZE METALLIC
TR-1	GREEN SCREEN TRELIS SYSTEM



FINISH LEGEND

STN-1	STONE - STONE SOURCE CARPINO - HONED
ST-1	STUCCO - PAREX 3021L 'CAVERN'
BR-1	BRICK - ACME SLATE GRAY, NORMAN SIZE
ACM-1	ALUMINUM COMPOSITE METAL PANEL - 'BRITE RED'
ACM-2	ALPOLIC JBR BRONZE
ACM-3	MBX MEDIUM BRONZE METALLIC



FINISH LEGEND

STN-1	STONE - STONE SOURCE CARPINO - HONED
ST-1	STUCCO - PAREX 3021L 'CAVERN'
BR-1	BRICK - ACME SLATE GRAY, NORMAN SIZE
ACM-1	ALUMINUM COMPOSITE METAL PANEL - 'BRITE RED'
ACM-2	ALPOLIC JBR BRONZE
ACM-3	MBX MEDIUM BRONZE METALLIC

Case #: LUR2014-00057

Project Name: Bank of America

Date: November 28, 2014

CRITERIA FOR REVIEW

No site review application shall be approved unless the approving agency finds that:

(1) Boulder Valley Comprehensive Plan:

✓ (A) The proposed site plan is consistent with the land use map and the service area map and, on balance, the policies of the Boulder Valley Comprehensive Plan.

The site is located at 1955-1965 28th St. within the Boulder Valley Regional Center (BVRC) and within the city limits. The BVRC is one of the city's three regional centers, along with the Historic Downtown and the University of Colorado (CU) with the University Hill business district. These three regional centers represent the highest level of land use intensity within the city, and each center has a distinct function and character, provides a wide range of activities and draws from the entire city as well as the region. Within this context, staff has found the application for this project to add a drive-thru bank to be consistent with the existing 2010 Boulder Valley Comprehensive Plan (BVCP) land use designation for the site of Regional Business. The Regional Business land use designation applies to the Downtown and BVRC areas, which are described in the 2010 BVCP as follows:

"Within these areas are located the major shopping facilities, offices, financial institutions, and government and cultural facilities serving the entire Boulder Valley and neighboring communities. These areas will continue to be refurbished and upgraded and will remain the dominant focus for major business activities in the region."

In addition, staff has found the proposal to be consistent with the following BVCP policies:

- 2.14 Mix of Complementary Land Uses*
- 2.17 Variety of Activity Centers*
- 2.18 Role of the Central Area*
- 2.30 Sensitive Infill and Redevelopment*
- 2.37 Enhanced Design for Private Sector Projects*

N/A (B) The proposed development shall not exceed the maximum density associated with the Boulder Valley Comprehensive Plan residential land use designation. Additionally, if the density of existing residential development within a three-hundred-foot area surrounding the site is at or exceeds the density permitted in the Boulder Valley Comprehensive Plan, then the maximum density permitted on the site shall not exceed the lesser of:

Not applicable. There are no residential units in the existing development and no new residential units are proposed.

N/A (i) The density permitted in the Boulder Valley Comprehensive Plan, or,

N/A (ii) The maximum number of units that could be placed on the site without waiving or varying any of the requirements of chapter 9-8, "Intensity Standards," B.R.C. 1981.

✓ (C) The proposed development's success in meeting the broad range of BVCP policies considers the economic feasibility of implementation techniques required to meet other site review criteria.

While the proposed project is to construct one new drive-through bank on the subject site, the project is part of a larger PUD managed by the applicant that includes two existing retail businesses, (Hazel's and Denver Mattress). The improvements proposed to the site as part of this project will not only allow for the creation of a new drive-thru bank, but will also benefit the existing retail establishments and will serve to support the economic health of the PUD overall. The project meets a broad range of BVCP policies as well as other site review criteria in an economically feasible manner.

(2) Site Design: Projects should preserve and enhance the community's unique sense of place through creative design that respects historic character, relationship to the natural environment, multi-modal transportation connectivity and its physical setting. Projects should utilize site design techniques which are consistent with the purpose of site review in subsection (a) of this section and enhance the quality of the project. In determining whether this subsection is met, the approving agency will consider the following factors:

(A) Open Space: Open space, including, without limitation, parks, recreation areas, and playgrounds:

✓ (i) Useable open space is arranged to be accessible and functional and incorporates quality landscaping, a mixture of sun and shade and places to gather;

The original approval for SI-94-29 included a pedestrian area to the southwest of the Soundtrack building along the north side of the Walnut Driveway, which will be maintained following approval of this proposal. In addition, the proposal includes landscaping improvements to the proposed Bank of America site, including a new pedestrian access from the existing 10' multi-use path along 28th Street.

N/A (ii) Private open space is provided for each detached residential unit;

Not applicable, as there are no residential units included in this project.

N/A (iii) The project provides for the preservation of or mitigation of adverse impacts to natural features, including, without limitation, healthy long-lived trees, significant plant communities, ground and surface water, wetlands, riparian areas, drainage areas and species on the federal Endangered Species List, "Species of Special Concern in Boulder County" designated by Boulder County, or prairie dogs (*Cynomys ludovicianus*), which is a species of local concern, and their habitat;

Not applicable, as the subject site is already fully developed in an urban context and as such does not contain any significant natural features.

N/A (iv) The open space provides a relief to the density, both within the project and from surrounding development;

Not applicable, as there are no residential units included in this project.

N/A (v) Open space designed for active recreational purposes is of a size that it will be functionally useable and located in a safe and convenient proximity to the uses to which it is meant to serve;

Not applicable, as the proposal is commercial, not recreational or residential.

N/A (vi) The open space provides a buffer to protect sensitive environmental features and natural areas; and

Not applicable, as the subject site is already fully developed and urban in character, as is the surrounding area. There are currently no sensitive natural features located on or adjacent to the site.

✓ (vii) If possible, open space is linked to an area- or city-wide system.

The proposed redevelopment of the former Wendy's site includes adding a pedestrian access to the site from the existing 10' multi-use path along 28th St. In addition, the proposal includes adding a 7' multi-use path connecting the existing sidewalk along Walnut St. on the south side of the site to the existing crusher fines path on the adjacent property to the north, consistent with the adopted BVRC Connections Plan. The new path across the site will facilitate pedestrian and bicycle movement between properties as well as between existing city transportation facilities to the north and south.

N/A (B) Open Space in Mixed Use Developments (Developments that contain a mix of residential and non-residential uses)

Not applicable. There are no residential units in the existing development and no new residential units are proposed.

N/A (i) The open space provides for a balance of private and shared areas for the residential uses and common open space that is available for use by both the residential and non-residential uses that will meet the needs of the anticipated residents, occupants, tenants, and visitors of the property; and

N/A (ii) The open space provides active areas and passive areas that will meet the needs of the anticipated residents, occupants, tenants, and visitors of the property and are compatible with the surrounding area or an adopted plan for the area.

(C) Landscaping

The proposal includes upgrades to the existing landscaping on the former Wendy's site. The existing parking lot landscaping in the Hazel's parking area will be reconfigured, and will continue to meet city landscaping requirements.

✓ (i) The project provides for aesthetic enhancement and a variety of plant and hard surface materials, and the selection of materials provides for a variety of colors and contrasts and the preservation or use of local native vegetation where appropriate;

The proposal includes several landscaping improvements on the Bank of America site and provides for a variety of plant and hard surfaces. A new 7' multi-use path running north-south across the site is also proposed.

N/A (ii) Landscape design attempts to avoid, minimize, or mitigate impacts to important native species, plant communities of special concern, threatened and endangered species and habitat by integrating the existing natural environment into the project;

Not applicable, as the subject site is already fully developed and as such does not contain any endangered species or habitat.

✓ (iii) The project provides significant amounts of plant material sized in excess of the landscaping requirements of sections 9-9-12, "Landscaping and Screening Standards" and 9-9-13, "Streetscape Design Standards," B.R.C. 1981; and

The proposal includes reconfiguring parking lot landscaped areas and adding new interior parking lot landscaping to the southern portion of the lot in conformance with the parking lot landscaping standards. The proposal also adds landscaping buffers in excess of the required size to the perimeter of the Bank of America site, and provides significant landscaping within and around the proposed drive-thru loop.

✓ (iv) The setbacks, yards, and useable open space along public rights-of-way are landscaped to provide attractive streetscapes, to enhance architectural features, and to contribute to the development of an attractive site plan.

The proposal includes adding new landscaping to all of the building setbacks around the proposed Bank of America building, and provides a new low site wall to the 28th St. frontage. In addition, the proposed pedestrian access off of the new 28th St. multi-use path will be landscaped to frame both the entrance feature as well as the front of the bank to passers-by.

(D) Circulation: Circulation, including, without limitation, the transportation system that serves the property, whether public or private and whether constructed by the developer or not:

✓ (i) High speeds are discouraged or a physical separation between streets and the project is provided;

The intent of the proposed parking lot reconfiguration is to further reduce vehicle speeds and improve pedestrian safety. While the 2012 Site Review Amendment improved the previous parking lot by adding pedestrian crosswalks to all of the major pedestrian walkways and widening the multi-use path along 28th St. from 5.5 feet to 10 feet, since that time the applicant has noted that the straight drive aisle in the middle of the site has not been effective at reducing vehicular speeds. Per the applicant, there have been several "close calls" between pedestrians and vehicles; thus, the applicant is proposing to bend the main drive aisle to the east, thereby making it so that vehicles must make two small turns in order to pass through the site from south to north. In addition, the applicant is proposing to add a large area of colored concrete in front of the Hazel's store (as currently exists at the 28th St. Whole Foods) to act as a traffic calming measure. Overall, the proposed parking lot improvements will serve to reduce vehicular speeds and improve pedestrian circulation and safety.

✓ (ii) Potential conflicts with vehicles are minimized;

As mentioned above, the proposal includes several measures to slow down cars and improve pedestrian safety. In addition to the change in circulation and addition of colored concrete at the main store entry to Hazel's, the applicant is proposing to add a new 7' wide colored concrete multi-use path running north-south across the site. This will improve safety for pedestrians and bicyclists travelling within and across the site by creating a visual break in the drive aisles and providing a designated travel route where currently there is not one.

✓ (iii) Safe and convenient connections are provided that support multi-modal mobility through and between properties, accessible to the public within the project and between the project and the existing and proposed transportation systems, including, without limitation, streets, bikeways, pedestrianways and trails;

As previously discussed, a new 7' multi-use path will be provided running north to south across the site consistent with the adopted Boulder Valley Regional Center (BVRC) Connections Plan, and a new pedestrian access will be provided to the bank building from the existing 28th St. multi-use path. Currently there are 13 inverted U bike racks on site. All of the existing bicycle and pedestrian facilities will be maintained, and an additional 14 bike parking spaces will be provided (4 in front of the Bank of America building and 8 in front of Hazel's).

✓ (iv) Alternatives to the automobile are promoted by incorporating site design techniques, land use patterns, and supporting infrastructure that supports and encourages walking, biking, and other alternatives to the single-occupant vehicle;

Site design techniques that support alternate modes of transportation include the addition of colored concrete at the main store entry to Hazel's, the addition of a new 7' wide colored concrete multi-use path running north-south across the site, the creation of a new

pedestrian access to the bank site from the 28th St. path and the addition of 14 new bicycle parking spaces.

✓ (v) Where practical and beneficial, a significant shift away from single-occupant vehicle use to alternate modes is promoted through the use of travel demand management techniques;

Site design techniques that support alternate modes of transportation include the addition of colored concrete at the main store entry to Hazel's, the addition of a new 7' wide colored concrete multi-use path running north-south across the site, the creation of a new pedestrian access to the bank site from the 28th St. path and the addition of 14 new bicycle parking spaces.

✓ (vi) On-site facilities for external linkage are provided with other modes of transportation, where applicable;

As mentioned above, the proposal includes the addition of a new 7' multi-use path running north-south across the site as shown in the BVRC Connections Plan as well as a new pedestrian access from the 28th St. multi-use path.

N/A (vii) The amount of land devoted to the street system is minimized; and

Not applicable, as there are no new streets or right-of-way being dedicated through this proposal.

✓ (viii) The project is designed for the types of traffic expected, including, without limitation, automobiles, bicycles, and pedestrians, and provides safety, separation from living areas, and control of noise and exhaust.

The project is well-designed to accommodate both vehicular and bike/pedestrian traffic. The proposal includes maintaining 134 out of 135 previously approved car parking spaces in order to meet the high demand for parking generated by the existing and proposed uses, and also provides a total of 27 bike parking spaces across the site.

(E) Parking

✓ (i) The project incorporates into the design of parking areas measures to provide safety, convenience, and separation of pedestrian movements from vehicular movements;

As mentioned above, the proposal includes several measures to slow down cars and improve pedestrian safety. In addition to the change in circulation and addition of colored concrete at the main store entry to Hazel's, the applicant is proposing to add a new 7' wide colored concrete multi-use path running north-south across the site. This will improve safety for pedestrians and bicyclists travelling within and across the site by creating a visual break in the drive aisles and providing a designated travel route where currently

there is not one. All of the existing pedestrian walkways and crosswalks will be maintained following the proposed reconfiguration.

✓ (ii) The design of parking areas makes efficient use of the land and uses the minimum amount of land necessary to meet the parking needs of the project;

The proposed parking layout represents an efficient use of the land, and uses the minimum amount of land necessary to meet the parking requirements of the development by maximizing the number of compact spaces.

✓ (iii) Parking areas and lighting are designed to reduce the visual impact on the project, adjacent properties, and adjacent streets; and

The parking area will meet city landscaping standards, reducing the visual impact of the parking areas, and all new lighting will be compliant with current lighting standards.

✓ (iv) Parking areas utilize landscaping materials to provide shade in excess of the requirements in Subsection 9-9-6 (d), "Parking Area Design Standards," and Section 9-9-14, "Parking Lot Landscaping Standards," B.R.C. 1981.

The proposal includes reconfiguring parking lot landscaped areas and adding new interior parking lot landscaping to the southern portion of the lot in conformance with the parking lot landscaping standards. The proposal also adds landscaping buffers in excess of the required size to the perimeter of the Bank of America site, and provides significant landscaping within and around the proposed drive-thru loop.

(F) Building Design, Livability, and Relationship to the Existing or Proposed Surrounding Area

✓ (i) The building height, mass, scale, orientation, and configuration are compatible with the existing character of the area or the character established by an adopted plan for the area;

The subject site is located within the Boulder Valley Regional Center (BVRC) on the west side of 28th Street between Walnut Street and Pearl Street, and as such is subject to the BVRC Design Guidelines (the Guidelines). The character of this area is predominantly commercial and retail oriented, with Target and the 29th Street Shopping Center located immediately across 28th Street to the east. To the north is the Google office building (formerly Circuit City) and pad restaurant and retail shops. To the south is the Marshall's Plaza shopping center including Marshall's, Office Depot, REI and Bed Bath & Beyond.

The Bank massing has been designed to be sensitive and appropriate to its surroundings, with a 20'4" roof height which is lower than neighboring buildings. The Bank proposal places the drive-through on the west side of the building, where it has the least visibility to the adjacent roadway and allows the building to be pushed closer to the street consistent with the existing Denver Mattress building to the south. The proposed building will support a lively street presence, placing the majority of glazed areas on the south and east sides of

the building adjacent to 28th street. The Bank materials will be a mix of high quality stone, brick, metal panel and stucco to complement the character of surrounding developments. In addition, a new pedestrian access from the existing 28th Street multi-use path, landscaped and with new bike parking nearby, will be provided so that pedestrians and bicyclists can access the bank without having to enter the parking lot.

Staff finds the proposed building to be in keeping with the goals of the BVRC Design Guidelines (the Guidelines) to continue to upgrade the BVRC through high-quality redevelopment, make the BVRC a memorable, people-oriented place, develop a more fine-grained and complete transportation network and incorporate a greater diversity of land uses. Specifically, staff finds the proposed project to be consistent with the following policies contained in the Guidelines:

Overall Site Layout

- 3.1.B. Locate buildings close to the street
- 3.1.D. Maximize street-frontage of buildings

The proposal places the Bank of America building roughly 20' from the property line, which is consistent with the neighboring Denver Mattress building to the south. The drive-through has been placed on the west side of the building so that it is fully screened from 28th Street. A new pedestrian access will create a visual connection to the building entrance from the 28th Street multi-use path, and will enhance visual interest to passers-by.

Circulation

- 3.1.E. Lay out site to support pedestrian circulation
- 3.1.K. Provide vehicular and pedestrian links
- 3.2.B. Connect with adjacent parking lots or drives
- 3.3.B. Provide interior pedestrian links to adjacent properties
- 3.3.D. Use distinctive paving
- 3.3.G. Provide bicycle facilities shown on Connections Plan

As previously discussed, the proposal includes reconfiguring the parking lot and drive aisles in order to slow down vehicular traffic and improve pedestrian circulation. In addition, a new 7' multi-use path will be provided running north to south across the site consistent with the adopted Boulder Valley Regional Center (BVRC) Connections Plan, a new pedestrian access will be provided to the bank building from the existing 28th St. multi-use path, and colored concrete will be added to the main drive aisle in front of Hazel's in order to slow down cars and improve pedestrian safety.

Bicycle Parking

- 3.4.A. Ensure bicycle parking is ample and secure

Currently there are 13 inverted U bike racks on site. All of the existing bicycle and pedestrian facilities will be maintained, and an additional 14 bike parking spaces

will be provided (4 in front of the Bank of America building and 8 in front of Hazel's).

Automobile Parking

3.5.D. Screen parking from the street

3.5.E. Landscape the interior and perimeter of parking lots

The proposal includes reconfiguring parking lot landscaped areas and adding new interior parking lot landscaping to the southern portion of the lot in conformance with the parking lot landscaping standards. The proposal also adds landscaping buffers in excess of the required size to the perimeter of the Bank of America site, and provides significant landscaping within and around the proposed drive-thru loop.

Building Design

5.2.A. Orient the building to the street

5.2.C. Emphasize building entrances

5.2.E. Provide pedestrian interest on the ground level

5.2.J. Select high-quality exterior materials

The proposed building will support a lively street presence, placing the main entrance on the southern elevation and the majority of glazed areas on the south and east sides of the building adjacent to 28th street. The Bank materials will be a mix of high quality stone, brick, metal panel and stucco to complement the character of surrounding developments and reflect vernacular building materials in the Boulder area. In addition, a new pedestrian access from the existing 28th Street multi-use path, landscaped and with new bike parking nearby, will be provided which will help to add visual interest and frame the entrance to passers-by.

The BVRC Guidelines also include the following policy:

5.1.F. Drive-throughs are discouraged:

“Free-standing drive-through buildings (e.g., fast food or banking) are discouraged. If drive-through service is found to be appropriate, consider incorporating the service into a larger building with other uses.”

Staff finds that this site is an appropriate location for drive-thru service due to the fact that the drive-thru is located to the west of the proposed building, which maximizes the building frontage along 28th Street and minimizes the visual impacts associated with automobile queues. The building's location on the northern edge of the site also minimizes impacts to surrounding uses and provides a buffer for the proposed drive-thru use, as the area immediately adjacent to the drive-thru is mainly undeveloped land serving as storm water detention for the neighboring property and bordered on the north by a parking lot. Additionally, access to and from the proposed drive-thru would be entirely contained within an existing parking area, meaning that no new curb cuts or traffic impacts would be

generated on 28th Street. Finally, the proposed drive-thru facility would not affect pedestrian or bicycle access, as a new access point off the 28th Street multi-use path will allow bicyclists and pedestrians to access the bank without having to cross the drive-thru lanes, and the new proposed 7' multi-use path would provide a safe way for bicyclists and pedestrians to get across the site without interacting with traffic entering and exiting the bank. Overall, given the regional context of the site and surrounding area and the relatively low impacts of the proposed facility, staff finds that the subject site is uniquely appropriate for a new drive-thru use.

✓ (ii) The height of buildings is in general proportion to the height of existing buildings and the proposed or projected heights of approved buildings or approved plans for the immediate area;

The proposed Bank of America building will be a single story building with a height of 20'4", which is lower than adjacent buildings and is well within the 35' maximum height limit for the BR-1 zone.

✓ (iii) The orientation of buildings minimizes shadows on and blocking of views from adjacent properties;

The Bank site has an existing, well established grove of trees on the north side of the site. It is anticipated that any shading from the proposed bank will shade the trees and have no impact on neighboring buildings to the north. In addition, the project is within Solar Access Area III and is therefore not subject to any solar access restrictions.

✓ (iv) If the character of the area is identifiable, the project is made compatible by the appropriate use of color, materials, landscaping, signs, and lighting;

The surrounding area is regional retail in character, and includes a wide variety of architectural styles. The proposed building and site design are consistent with the existing character of the area. The Bank materials will be a mix of high quality stone, brick, metal panel and stucco accents to complement the character of surrounding developments and reflect vernacular building materials in the BVRC.

✓ (v) Projects are designed to a human scale and promote a safe and vibrant pedestrian experience through the location of building frontages along public streets, plazas, sidewalks and paths, and through the use of building elements, design details and landscape materials that include, without limitation, the location of entrances and windows, and the creation of transparency and activity at the pedestrian level;

As stated previously, the Bank proposal places the drive through on the west side of the building, where it has the least visibility to adjacent roadways, and will be screened by the proposed building and landscaping. The proposed building entrance faces south, and will support a lively street presence by placing the majority of glazed areas on the south and east sides of the building adjacent to 28th street. A direct connection from the 28th street

multi-modal path is provided for pedestrians, which will also serve to enhance transparency and visual interest at the pedestrian level.

✓ (vi) To the extent practical, the project provides public amenities and planned public facilities;

The proposal includes adding 14 new bicycle parking spaces as well as a new 7' multi-use path running north-south across the site consistent with the adopted BVRC Connections plan.

N/A (vii) For residential projects, the project assists the community in producing a variety of housing types, such as multifamily, townhouses and detached single family units, as well as mixed lot sizes, number of bedrooms and sizes of units;

Not applicable. There are no residential units in the existing development and no new residential units are proposed.

N/A (viii) For residential projects, noise is minimized between units, between buildings, and from either on-site or off-site external sources through spacing, landscaping, and building materials;

Not applicable. There are no residential units in the existing development and no new residential units are proposed.

✓ (ix) A lighting plan is provided which augments security, energy conservation, safety, and aesthetics;

A lighting plan meeting current city lighting standards will be required at time of building permit.

N/A (x) The project incorporates the natural environment into the design and avoids, minimizes, or mitigates impacts to natural systems;

Not applicable, as the site is already fully developed in an urban context and this does not contain any significant natural systems.

✓ (xi) Buildings minimize or mitigate energy use; support on-site renewable energy generation and/or energy management systems; construction wastes are minimized; the project mitigates urban heat island effects; and the project reasonably mitigates or minimizes water use and impacts on water quality.

The applicant will be required to meet current energy code requirements for commercial buildings, which include the 2012 International Energy Conservation Code (IECC) standard as well as the 2010 American Society of Heating, Refrigeration, and Air Conditioning Engineers (ASHRAE) 90.1 standards, with additional local amendments requiring a 30 percent increase in performance requirements. This requirement is considered aggressive and represents a significant step toward improved energy efficiency

in buildings in balance with the cost impact for new construction. As discussed as a part of the adoption process in October, 2013, the adopted codes, if supported by continued improvements in cost-efficient building and energy management technology, could achieve a "net zero" building code in the future (in which buildings, on balance, produce as much energy as they consume).

✓ (xii) Exteriors or buildings present a sense of permanence through the use of authentic materials such as stone, brick, wood, metal or similar products and building material detailing;

The Bank materials will be a mix of high quality stone, brick, metal panel and stucco to complement the character of surrounding developments and reflect vernacular building materials within the BVRC.

✓ (xiii) Cut and fill are minimized on the site, the design of buildings conforms to the natural contours of the land, and the site design minimizes erosion, slope instability, landslide, mudflow or subsidence, and minimizes the potential threat to property caused by geological hazards;

As previously mentioned, the site is already fully developed and thus does not require cut or fill. The existing grade will be largely maintained, with existing drainage patterns to be preserved and pervious area to be increased slightly.

N/A (xiv) In the urbanizing areas along the Boulder Valley Comprehensive Plan boundaries between Area II and Area III, the building and site design provide for a well-defined urban edge; and

Not applicable.

N/A (xv) In the urbanizing areas located on the major streets shown on the map in Appendix A of this title near the Boulder Valley Comprehensive Plan boundaries between Area II and Area III, the buildings and site design establish a sense of entry and arrival to the City by creating a defined urban edge and a transition between rural and urban areas.

Not applicable.

N/A (G) Solar Siting and Construction: For the purpose of ensuring the maximum potential for utilization of solar energy in the City, all applicants for residential site reviews shall place streets, lots, open spaces, and buildings so as to maximize the potential for the use of solar energy in accordance with the following solar siting criteria:

Not applicable. There are no residential units in the existing development and no new residential units are proposed.

N/A (H) Additional Criteria for Poles Above the Permitted Height: No site review application for a pole above the permitted height will be approved unless the approving agency finds all of the following:

N/A (I) Land Use Intensity Modifications:

N/A (J) Additional Criteria for Floor Area Ratio Increase for Buildings in the BR-1 District:

N/A (K) Additional Criteria for Parking Reductions: The off-street parking requirements of section 9-9-6, "Parking Standards," B.R.C. 1981, may be modified as follows:

N/A (L) Additional Criteria for Off-Site Parking: The parking required under section 9-9-6, "Parking Standards," B.R.C. 1981, may be located on a separate lot if the following conditions are met:

USE REVIEW CRITERIA

Criteria for Review: No use review application will be approved unless the approving agency finds all of the following:

(1) Consistency with Zoning and Non-Conformity: The use is consistent with the purpose of the zoning district as set forth in [Section 9-5-2\(c\)](#), "Zoning Districts Purposes," B.R.C. 1981, except in the case of a non-conforming use;

The subject property is located within the BR-1 zone district, which is defined in section 9-5-2(c)(2)(I), B.R.C. 1981, as "Business centers of the Boulder Valley, containing a wide range of retail and commercial operations, including the largest regional-scale businesses, which serve outlying residential development; and where the goals of the Boulder Urban Renewal Plan are implemented." The financial institution use is consistent with such purpose. Per section 9-6-1, "Use Standards," B.R.C. 1981, drive-thru uses are allowed if approved through the Use Review process.

(2) Rationale: The use either:

(A) Provides direct service or convenience to or reduces adverse impacts to the surrounding uses or neighborhood;

The proposed Bank of America will provide a direct service to the surrounding area by increasing the banking options for residents and visitors. The proposed drive-thru will also add to the variety of commercial services available within the BVRC, and will further help to implement the high-quality redevelopment of the BVRC as intended by the BVRC Design Guidelines.

N/A (B) Provides a compatible transition between higher intensity and lower intensity uses;

N/A (C) Is necessary to foster a specific city policy, as expressed in the Boulder Valley Comprehensive Plan, including, without limitation, historic preservation, moderate income housing, residential and non-residential mixed uses in appropriate locations, and group living arrangements for special populations; or

N/A (D) Is an existing legal non-conforming use or a change thereto that is permitted under subsection (e) of this section;

✓ (3) **Compatibility:** The location, size, design, and operating characteristics of the proposed development or change to an existing development are such that the use will be reasonably compatible with and have minimal negative impact on the use of nearby properties or for residential uses in industrial zoning districts, the proposed development reasonably mitigates the potential negative impacts from nearby properties;

The proposed project is to construct a 2,850 square foot, single story Bank of America building with a drive-thru facility. The project site is part of the larger 1955 28th Street PUD, which currently includes two large-scale retail businesses, Hazel's and Denver Mattress. The project site was originally approved in 1977 as a Wendy's drive -thru, which remained on the site until 2012 when the building was demolished and the site reconfigured as a temporary parking lot with the intention of developing it as a pad site at a later time. Given the site's history as a drive-thru use, its location within the BVRC and the high-intensity regional commercial character of the surrounding area, the proposal to add a new drive-thru banking facility with standard hours of operation and ample parking (a total of 134 parking spaces are provided across the site as part of this proposal) to the subject site will be compatible with and have minimal negative impact on the use of surrounding properties.

✓ (4) **Infrastructure:** As compared to development permitted under [Section 9-6-1](#), "Schedule of Permitted Uses of Land," B.R.C. 1981, in the zone, or as compared to the existing level of impact of a non-conforming use, the proposed development will not significantly adversely affect the infrastructure of the surrounding area, including, without limitation, water, wastewater, and storm drainage utilities and streets;

All of the infrastructure required to serve the proposed development is already existing. The proposed project will improve storm drainage on site by reducing the amount of impervious surface area.

✓ (5) **Character of Area:** The use will not change the predominant character of the surrounding area or the character established by adopted design guidelines or plans for the area; and

As mentioned above, the site is located on the west side of 28th Street within the Boulder Valley Regional Center (BVRC). The character of this area is predominantly commercial and retail

oriented, with Target and the 29th Street Shopping Center located immediately across 28th Street to the east. To the north is the Google office building (formerly Circuit City) and pad restaurant and retail shops. To the south is the Marshall's Plaza shopping center including Marshall's, Office Depot, REI and Bed Bath & Beyond.

The Bank massing has been designed to be sensitive and appropriate to its surroundings, with a 20'4" roof height which is lower than neighboring buildings. The Bank proposal places the drive-through on the west side of the building, where it has the least visibility to the adjacent roadway and allows the building to be pushed closer to the street consistent with the existing Denver Mattress building to the south. The proposed building will support a lively street presence, placing the majority of glazed areas on the south and east sides of the building adjacent to 28th street. The Bank materials will be a mix of high quality stone, brick, metal panel and stucco to complement the character of surrounding developments and reflect vernacular building materials in the Boulder area. Please see the Site Review criteria above for an analysis of the project's consistency with the adopted BVRC Design Guidelines.

N/A (6) **Conversion of Dwelling Units to Non-Residential Uses:** There shall be a presumption against approving the conversion of dwelling units in the residential zoning districts set forth in [Subsection 9-5-2\(c\)\(1\)\(a\)](#), B.R.C. 1981, to non-residential uses that are allowed pursuant to a use review, or through the change of one non-conforming use to another non-conforming use. The presumption against such a conversion may be overcome by a finding that the use to be approved serves another compelling social, human services, governmental, or recreational need in the community including, without limitation, a use for a day care center, park, religious assembly, social service use, benevolent organization use, art or craft studio space, museum, or an educational use.

Not applicable. There are no residential units in the existing development.

Section 9-6-9(c) Drive-Thru Uses:

The following criteria will apply to any drive-thru use:

(1) No drive-thru facility is allowed in any Downtown (DT) district unless the property is located directly abutting Canyon Boulevard.

Not Applicable, as the project site is not located within the downtown area.

(2) Hazardous and other adverse effects on adjacent sites and streets are avoided.

The proposed drive-thru is located to the west of the proposed building, which maximizes the building frontage along 28th Street and minimizes the visual impacts associated with automobile queues. The building's location on the northern edge of the site also minimizes impacts to surrounding uses and provides a buffer for the proposed drive-thru use, as the area immediately adjacent to the drive-thru is mainly undeveloped land serving as storm water detention for the neighboring property and bordered on the north by a parking lot. In addition, access to and from

the proposed drive-thru would be entirely contained within an existing parking area, meaning that no new traffic impacts would be generated on 28th Street.

(3) The location of any access to the drive-thru facility from an adjacent street does not impair its traffic-carrying capacity.

Not Applicable, as access to the proposed drive-thru would be taken from within the existing parking area, and no new curb cuts are proposed.

(4) Internal circulation and access to and egress from the site do not substantially impair the movement of other modes of transportation, such as bicycles and pedestrians, to and through the site.

The proposed drive-thru facility would not affect pedestrian or bicycle access to and movement within the site, as the drive-thru would be located on the west side of the building, while a new access point off the 28th Street multi-use path to the east of the building will allow bicyclists and pedestrians to access the bank without having to cross the parking area or drive-thru lanes. In addition, the new proposed 7' multi-use path would provide a safe way for bicyclists and pedestrians to get across the site between Walnut Street and the adjacent site to the north without interacting with traffic entering and exiting the bank.

(5) Clearly marked pedestrian crosswalks are provided for each walk-in customer access to the facility adjacent to the drive-thru lanes.

The existing parking area includes clearly marked pedestrian crosswalks, and the proposed drive-thru is located so as to minimize pedestrian/vehicular conflicts. Specifically, the parking for the proposed bank does not require customers to cross the drive-thru lanes. A sidewalk is provided at the main entrance of the south side of the building, which will allow customers to either access the parking area directly or utilize the nearby 28th Street multi-use path to access the parking spaces.

(6) The drive-thru use is screened from adjacent rights-of-way and properties through placement of the use, screening, landscaping, or other site design techniques.

As previously discussed, the drive-thru use would be screened from 28th Street by the proposed bank building. In addition, new landscaping is proposed on the north and west sides of the drive-thru, which would help to screen the drive-thru from the adjacent property. The portion of the adjacent property immediately to the north of the drive-thru is a landscaped area currently used for stormwater detention, which will provide an additional buffer.

(7) Environmental impacts, including, without limitation, noise, air emissions, and glare are not significant for the employees of the facility or the surrounding area.

The drive-thru is located so as to minimize environmental impacts for employees of the facility. Rather than extend around the entire building as is commonly the case with drive-thru facilities, the drive-thru is located entirely on the west side of the building. This allows the main office windows

on the east side of the building to remain unobstructed, thereby reducing associated environmental impacts for employees.

(8) Any curb cuts serving the use are not located within two hundred feet of any intersection of the rights-of-way of any two of the major streets or major arterials shown on the map of major streets.

As mentioned in the staff memorandum, the PUD is comprised of three separate parcels under common ownership. The existing curb cut serving the Hazel's and Denver Mattress retail stores is located on a separate parcel than the proposed drive-thru use, and as such is not considered to serve the drive-thru. Access to the proposed drive-thru loop would be taken from within the existing parking area.

(9) The location, size, design, and operating characteristics of the proposed facility are such that the drive-thru operation will be reasonably compatible with and have minimal negative impact on the use of nearby properties.

The proposed project is to construct a 2,850 square foot, single story Bank of America building with a drive-thru facility. The project site is part of the larger 1955 28th Street PUD, which currently includes two large-scale retail businesses, Hazel's and Denver Mattress. The project site was originally approved in 1977 as a Wendy's drive -thru, which remained on the site until 2012 when the building was demolished and the site reconfigured as a temporary parking lot with the intention of developing it as a pad site at a later time. Given the site's history as a drive-thru use, its location within the BVRC and the high-intensity regional commercial character of the surrounding area, the proposal to add a new drive-thru banking facility with standard hours of operation and ample parking (a total of 134 parking spaces are provided across the site as part of this proposal) to the subject site will be compatible with and have minimal negative impact on the use of surrounding properties.

(10) The noise generated on the site is inaudible to adjacent residential uses, measured at or inside the property line of property other than that on which the sound source is located.

Not applicable, as there are no residential uses immediately adjacent to the subject site.

(11) Nonconforming drive-thrus shall comply with the criteria of subsection 9-10-2(d), B.R.C. 1981.

Not applicable, as the proposed use is allowed through the Use Review process and is not replacing an existing non-conforming drive-thru use.



CITY OF BOULDER
Community Planning & Sustainability

1739 Broadway, Third Floor • P.O. Box 791, Boulder, CO 80306-0791
 phone 303-441-1880 • fax 303-441-3241 • web www.bouldercolorado.gov

CITY OF BOULDER
LAND USE REVIEW RESULTS AND COMMENTS

DATE OF COMMENTS: **August 25, 2014**
 CASE MANAGER: **Chandler Van Schaack**
 PROJECT NAME: **Bank of America**
 LOCATION: **1955 28TH ST**
 COORDINATES: **N03W04**
 REVIEW TYPE: **Site and Use Review**
 REVIEW NUMBER: **LUR2014-00057**
 APPLICANT: **ANDREW FAIRBAIRN**
 DESCRIPTION: **Site and Use Review to establish one new 2,850SF, single story "Bank of America" branch - including drive thru facility, drainage, lighting, and landscaping. Proposal includes modification to existing parking on adjacent Bank pad and modifications to traffic plow pattern at "Hazels" parking area.**

REQUESTED VARIATIONS FROM THE LAND USE REGULATIONS:

- **Section 9-7-1, "Form and Bulk Standards," – Building Setbacks: request to modify the rear yard setback to allow for a 12' setback where 20' is the minimum required by the BR-1 zone district standards.**

I. REVIEW FINDINGS

Overall, staff finds the proposed project to be an appropriate use given the surrounding commercial context; however, staff has identified several issues with the proposed site layout and building design which will require revisions in order to meet the intent of the Site Review criteria and Boulder Valley Regional Center Design Guidelines. These issues are discussed in detail in the comments below, and will require a revision-level resubmittal. Therefore, please revise the project plans as noted herein and submit five copies of the revised plans as well as digital copies of the plans in pdf from to the front counter of the P&DS Service Center prior to the beginning of a 3-week review track. Please note that review tracks begin on the first and third Mondays of every month.

Please contact the case manager, Chandler Van Schaack, at 303-441-3137 or vanschaackc@bouldercolorado.gov with any questions or to set up a meeting to go over these comments in further detail.

II. CITY REQUIREMENTS

Access/Circulation David Thompson, 303-441-4417

1. Please revise the site plan to: (1) provide a tabulation of the vehicle and bike parking being provided on the site and (2) show the dimensions of the parking stalls and drive aisles in order to verify compliance with the Boulder Revised Code with respect to parking and landscape standards.
2. Pursuant to section 9-9-6(g)(3) of the Boulder Revised Code, 1981, (BRC) please revise the landscape plan to show the new short-term bike parking within fifty feet of the building entrance.
3. Pursuant to the [Boulder Valley Regional Center Transportation Connections Plan](#) (BVRC TCP) and in support of the project's objective to improve multi-modal circulation and safety within the site, please revise the site plan to show a 12' wide north / south multi-use path within a 16' wide public access easement. The alignment of the multi-use path shall be consistent with the alignment shown in the BVRC TCP and minimize conflicts with automobiles.
4. Pursuant to section 9-9-6(d)(3)(A) of the BRC, please revise the site plan to remove the parking stalls opposite of the north and south drive aisles into the site.

5. Please revise the site and landscape plans to show the existing City of Boulder easements across the west side of the site adjacent to 28th Street in order to confirm the site improvements do not encroach within the easements.

Building Design Chandler Van Schaack, Case Manager

1. With the large amounts of grey stucco included in the proposal, staff finds the current building design does not adequately meet the intent of the BVRC Design Guidelines or section 9-2-14(h)(2)(K)(xii) of the Site Review criteria, which requires that “exteriors or buildings present a sense of permanence through the use of authentic materials such as stone, brick, wood, metal or similar products and building material detailing.” As discussed in the pre-submittal meeting with the applicant, stucco should be used as an accent material rather than a primary building material. Staff recommends replacing the stucco with grey brick or some other authentic material as listed above. Sample materials should also be provided with the resubmittal.
2. The project site is located within the Boulder Valley Regional Center (BVRC), and as such is subject to the BVRC Design Guidelines (Guidelines). In general, the Guidelines discourage standardized corporate architecture, and Sections 5.2.E. and 5.2.F. of the Guidelines promote the use of four-sided architecture and architectural detailing to enhance the pedestrian experience at ground-level. The current building design should be revised to make the east, west and north elevations more visually attractive and interesting. See Section 5, Building Design Guidelines, of the [BVRC Guidelines](#) for further information on methods for enhancing visual interest.

Drainage Erik Saunders, 303-441-4493

Section 7.13, “Permanent Storm Water Quality Management”, of the City of Boulder Design and Construction Standards (DCS), states that all projects and development that disturb less than an acre of and do not increase the impervious area, shall provide storm water quality facilities to the maximum extent practicable. Given the scope of the redevelopment of this project, including construction of a new structure and the reconfiguration of most of the parking lot, attention should be paid to providing additional and/or more robust water quality improvements. It appears that opportunities exist to introduce bioretention (rain gardens) or porous pavement as part of the parking lot improvements and decrease the amount of directly connected impervious area (DCIA) by discharging runoff through grass buffers or swales rather than cobble rundowns and swales. Please revise plan as necessary to incorporate drainage enhancements that utilize additional water quality strategies. Also revise the letter to include a discussion of any proposed changes.

Engineering Erik Saunders, 303-441-4493

The proposed low site walls east of the drive thru lane appear to be located within the existing access/ sidewalk easement intended for the construction and installation of the multi-use path. Per section 8-6-3 of the Boulder Revised Code (B.R.C.) 1981, no structures are permitted within an easement. Also, per section 9-9-15 B.R.C. 1981, walls must not be placed nearer than 18 inches from any public sidewalk. Please revise plans to show all easements affecting the property, remove all encroachments and relocate any structures that are less than 18 inches from the back edge of the proposed multi-use path.

Fees

Please note that 2014 development review fees include a \$131 hourly rate for reviewer services following the initial city response (these written comments). Please see the P&DS Questions and Answers brochure for more information about the hourly billing system.

Landscaping Elizabeth Lokocz, 303-441-3138

Please address the following coordination issues at the next submittal. Contact staff with any questions or concerns.

1. There is a proposed fire hydrant on the utility plan that does not appear on the landscape plan. The location heavily impacts an existing street tree, a Common Hackberry. As one of the few non-ash in this section of 28th Street, alternatives must be thoroughly evaluated. Permission for removal must be granted by the City Forester.
2. The proposed water services for the new structure do not meet the minimum ten foot separation requirements. Please adjust accordingly.
3. Please label typical and atypical parking lot island dimensions and remove anything that does not meet the minimum standards (eight feet in any dimension and 150 sq. ft. of area) from the calculations plan and landscape compliance table.
4. It is unclear to staff how the area delineated in the Calculations plan (sheet L2.0) was reached. Is it the area of disturbance? Per definition, this area does not meet the total parking lot area. Please adjust the graphic and total square footage to include the total parking lot versus the area of disturbance.
5. Add the total number of required and provided parking spaces and corresponding percentage of interior landscaping to the compliance table.
6. On the Landscape Calculations plan, there is a hatched area in the northwest corner that is misaligned with the nearby parking lot island. The site landscaping label in the legend is somewhat confusing. Is this indicating open space?

7. Staff is very supportive of the possible re-use of existing plant material. Please clarify that the plant schedule assumes full replacement in the event that the construction schedule does not support transplanting.
8. Please coordinate the proposed wall location per the comment below in Plan Documents. Is the proposed material sandstone? Does it match any existing stone on the site? Staff is supportive of the proposed wall, but it might feel more inviting at a slightly lower height (30-36 inches).
9. At the time of technical document review, please specify if any weed barrier fabric is proposed.

Legal Documents Julia Chase, City Attorney's Office, 303-441-3052

1. At the time of resubmittal, the Applicant shall provide the following:
 - a) A title commitment current within 30 days (none was provided with the initial application); and
 - b) Proof of authorization to bind.
2. Prior to signing the Development Agreement, if approved, the Applicant shall provide an updated title commitment current within 30 days of signing the agreement.

Neighborhood Comments

Staff has not received any neighborhood comments regarding the proposal.

Plan Documents Chandler Van Schaack, Case Manager

1. Please revise Sheet A00.01 to include the entire project site. Also revise the plan to include a Site & Parking Data table showing the total existing versus proposed floor area for the site (1945-1965 28th) as well as the required, existing and proposed parking, including number of standard, compact and accessible stalls. Different stall types should also be clearly labeled on the site plan.
2. On the architectural site plan (sheet A00.01), please include the proposed multi-use path alignment and adjust the proposed wall as needed. It does not appear to provide the required 18 inches of separation from the edge of path.

Site Design Chandler Van Schaack, Case Manager / Sam Assefa, Senior Urban Designer

1. There are several aspects to the current proposal which should be modified to be more consistent with the intent of the Guidelines. Specifically, staff finds the current layout of the drive-thru facility does not meet the intent of section 3.1.B. of the Guidelines, which requires buildings to be located close to the street with parking behind and/or beside the building. In addition, the current layout, which requires pedestrians to cross the proposed drive-thru aisle, does not appear to meet section 3.1.E. of the Guidelines, which discourages site design requiring pedestrians to walk across drives. It would be preferable to relocate the drive-thru aisle to the west of the proposed building and to push the building forward towards 28th Street, thereby continuing the existing building setback pattern established by the Denver Mattress building to the south, improving street presence and enhancing pedestrian access to the building. Staff has included a rendering showing a potential design option for the revised layout.

2. As discussed in comment #3 under "Access/Circulation" above, the Boulder Valley Regional Center Transportation Connections Plan (BVRC TCP) shows a direct connection running north-south through the project site. While the proposed re-alignment of the vehicular circulation through the site is an acceptable modification to the adopted connection, staff finds that the existing alignment serves as a key pedestrian and bicycle connection, and that re-aligning the pedestrian and bicycle circulation through the site would be inconsistent with the policies found in Part 3, "Pedestrian and Bicycle Circulation," of the Guidelines. Therefore, revise the site plan to include a multi-use path connecting the Walnut St. entrance to the adjacent site to the north, as noted in the "Access/ Circulation" comment referenced above.

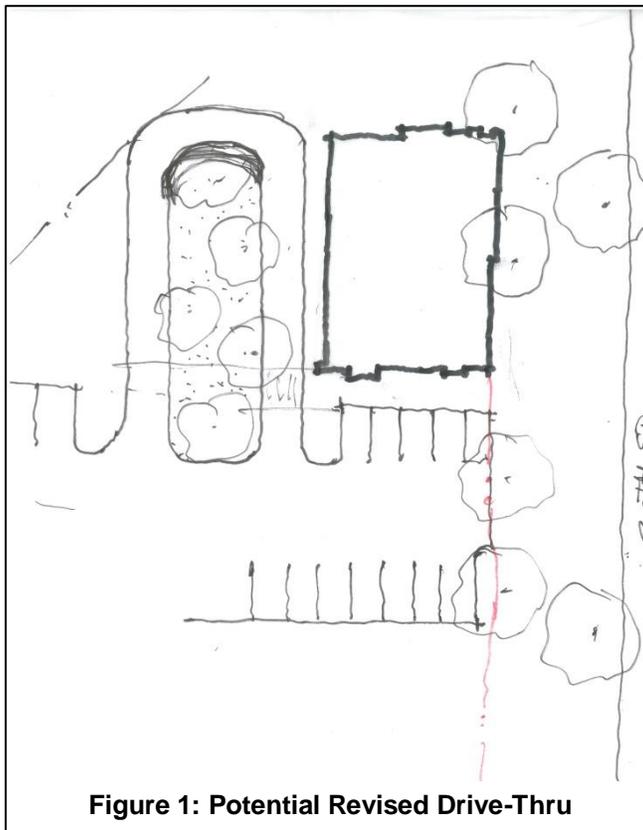


Figure 1: Potential Revised Drive-Thru

Utilities Erik Saunders, 303-441-4493

1. The proposed fire hydrant location and the 6" water line serving that hydrant do not meet the 10 foot minimum separation requirements between utilities and trees. In addition, per section 5.10(A), "Fire Hydrants", of the City of

Boulder Design and Construction Standards (DCS), hydrants must be located such that no exterior portion of any building will be over 175 feet of fire access distance from the nearest hydrant, (fire access distance is the distance from a hydrant to any external portion of any building, measured along public or private roadways or fire lanes, as would be traveled by motorized firefighting equipment). Because of this and in consideration of landscaping comments from staff (above) regarding removal of existing trees, it may be more appropriate to relocate the hydrant to the landscaping peninsula east of the drive thru exit. Please revise plans as necessary to address both concerns. Also, any relocation of the fire hydrant and service line will require the proposed utility easement to be realigned accordingly in conformance with section 5.10(A)(2) of the DCS.

2. The Utility Plan states that the existing water meter will be refitted with a ¾" meter to serve as the dedicated irrigation service. The existing water meter is required to be upgraded to current City standards and must be relocated out of the sidewalk and placed within the public right-of-way (ROW) or utility easement. Please revise plans and labeling accordingly.
3. The proposed domestic water and fire service lines are shown less than 10 feet from the existing trees. Proposed and existing utilities and trees must maintain a minimum separation of 10 feet. While staff recognizes that these trees were planted recently as part of the Hazel's Beverage World project, removal of these trees should be avoided if at all possible (see staff comments regarding landscaping above). Please revise plans as necessary.
4. The proposed 1" domestic water meter is shown to be placed on property the without benefit of a utility easement. Per section 11-1-36, "Location and Installation of Meters; Maintenance of Access to Meters", B.R.C. 1981, water meters must be placed in the ROW or easement and must not be placed in sidewalks or drives. Please revise plans accordingly.
5. The proposed 4" fire service line is shown with a configuration that places the valve in the gutter pan of 28th Street. Revise plans to show the private service line valve located in the landscape planting strip at the back of the curb.
6. The proposed sanitary sewer service is shown to connect to the existing service stub abandoned from the Wendy's demolition. However, the previous approved plans (TEC2012-00017) indicate that the continuation of that sanitary sewer service that paralleled the ROW has been removed or abandoned in place. Please verify that the sanitary service remains in place and continuous to the former point of connection at the manhole east of Mattress Firm. Revise plans as necessary.
7. The proposed area inlet east of the drive thru aisle is shown to discharge into the existing storm sewer via a new manhole connection. The manhole is shown directly beneath the future multi-use path along the west side of 28th Street and less than the 10 feet minimum required separation from the existing tree. While the lid has been shown to be offset such that the access cover is out of the pavement area, the proximity of the cover to the pavement edge and the shallow nature of the storm sewer in this area are likely to cause unforeseen conflicts or construction challenges during installation of the manhole. In addition, construction of a manhole in this location will require the removal of an existing street tree. Staff recommends a storm sewer service connection to the existing manhole, approximately 20 feet north of the proposed manhole, or the existing curb inlet. Revise plans as necessary.

III. INFORMATIONAL COMMENTS

Access/Circulation

The applicant is encouraged to provide sidewalks between the parking stalls in order to improve pedestrian circulation and minimize conflicts between automobiles and pedestrians.

Drainage, Erik Saunders 303 441-4493

1. A Final Drainage Plan and Report will be required as part of the Technical Document Review process. All plans and reports shall be in accordance with the City of Boulder *Design and Construction Standards*.
2. At time of Technical Document Review, the applicant shall submit information (geotechnical report, soil borings, etc.) regarding the groundwater conditions on the property, and all discharge points for perimeter drainage systems must be shown on the plan. The applicant is notified that any proposed groundwater discharge to the city's storm sewer system will require both a state permit and a city agreement.

Engineering, Erik Saunders 303 441-4493

No portion of any structure, including footings and eaves, may encroach into any public right-of-way or easement.

Flood Control Heidi Hansen, Floodplain Administration, 303-441-3273

There is a section of 100-year regulatory floodplain in the northwest corner by the new building. This area of floodplain has been remapped as 500-year floodplain and the new mapping is in to FEMA for adoption. If the building permits are submitted before FEMA adopts the new mapping and the building extends into the area currently mapped as 100-year floodplain, a floodplain development permit will be required and the building will have to comply with the city's floodplain regulations.

Utilities Erik Saunders 303 441-4493

1. The applicant is advised that any proposed street trees along the property frontage may conflict with existing utilities, including without limitation: gas, electric, and telecommunications, within and adjacent to the development site. It is the applicant's responsibility to resolve such conflicts with appropriate methods conforming to the Boulder Revised Code 1981, the City of Boulder Design and Construction Standards, and any private/franchise utility specifications.
2. A Final Utility Connection Plan will be required as part of the Technical Document Review process (which must be completed prior to building permit application). The Final Master Utility Plan (Utility Connection Plan) will be required to show all existing water service lines and fire lines.
3. The applicant is advised that at the time of building permit application the following requirements will apply:
 - a. The applicant will be required to provide an accurate proposed plumbing fixture count to determine if the proposed meters and services are adequate for the proposed use.
 - b. Water, wastewater and storm Plant Investment Fees and service line sizing will be evaluated.
 - c. If the existing water and/or wastewater services are required to be abandoned and upsized, all new service taps to existing mains shall be made by city crews at the developer's expense. The water service must be excavated and turned off at the corporation stop, per city standards. The sewer service must be excavated and capped at the property line, per city standards.
 - d. Since the building will be sprinklered, the approved fire line plans must accompany the fire sprinkler service line right-of-way permit application.
4. All water meters are to be placed in city R.O.W. or a public utility easement, but meters are not to be placed in driveways, sidewalks or behind fences.
5. Trees proposed to be planted shall be located at least 10 feet away from existing or future utility mains and services.

IV. NEXT STEPS

Please revise the project plans as noted herein and submit five copies of the revised plans as well as digital copies of the plans in pdf form to the front counter of the P&DS Service Center prior to the beginning of a 3-week review track. Please note that review tracks begin on the first and third Mondays of every month.

Please contact the case manager, Chandler Van Schaack, at 303-441-3137 or vanschaackc@bouldercolorado.gov with any questions or to set up a meeting to go over these comments in further detail.

V. CITY CODE CRITERIA CHECKLIST

A completed criteria checklist will be provided following review of the revised plans.



CITY OF BOULDER
Community Planning & Sustainability

1739 Broadway, Third Floor • P.O. Box 791, Boulder, CO 80306-0791
phone 303-441-1880 • fax 303-441-3241 • web www.bouldercolorado.gov

CITY OF BOULDER
LAND USE REVIEW RESULTS AND COMMENTS

DATE OF COMMENTS: **October 24, 2014**
CASE MANAGER: **Chandler Van Schaack**
PROJECT NAME: **BANK OF AMERICA**
LOCATION: **1955 28TH ST**
COORDINATES: **N03W04**
REVIEW TYPE: **Site and Use Review**
REVIEW NUMBER: **LUR2014-00057**
APPLICANT: **ANDREW FAIRBAIRN**
DESCRIPTION: **Site and Use Review to establish one new 2,850SF, single story "Bank of America" branch - including drive thru facility, drainage, lighting, and landscaping. Proposal includes modification to existing parking on adjacent Bank pad and modifications to traffic plow pattern at "Hazels" parking area.**

REQUESTED VARIATIONS FROM THE LAND USE REGULATIONS:

- **Section 9-7-1, "Form and Bulk Standards," – Building Setbacks: request to modify the side yard setback to allow for a 9' setback where 12' is the minimum required by the BR-1 zone district standards.**

I. REVIEW FINDINGS

Overall, the Applicant has addressed many of the previous review comments; however, there are still some remaining issues which will require minor corrections to the plan set before an approval can be reached. These issues are discussed in further detail in the reviewer comments below. Once the issues identified herein have been addressed, please submit three copies of the final plans as well as digital copies of the plans in pdf form directly to the Case Manager, Chandler Van Schaack (303-441-3137 or vanschaackc@bouldercolorado.gov), at your earliest convenience. Staff is happy to meet and discuss these comments at your convenience.

II. CITY REQUIREMENTS

Access/Circulation David Thompson, 303-441-4417

1. Pre previous comment, please revise the civil site plan to show the bike parking being provided on the site and revise the site plan show the existing and proposed bike parking in order to verify the bike parking.
2. Please revise the dimensions of the concrete pad for the Bank of America bike parking to measure 6' x 11' and locate the concrete pad outside of the 28th Street multi-use path public access easement.
3. Please revise the landscape plan to show the concrete pad for the proposed four inverted "u" bike racks to be installed at the front of the building in order to evaluate the impacts of the parking on the site.
4. Please revise the site plan to show how the proposed concrete path connects to the existing facilities at each end of the path.

Building Design Chandler Van Schaack, Case Manager

While overall the applicant has done an excellent job of addressing staff's previous concerns regarding architecture and materiality, staff has one remaining concern regarding the east elevation along 28th Street. Specifically, the service door on the north side of the east elevation appears out of character with the remainder of that side of the building. Staff recommends utilizing a door that matches the primary entrance and windows along the south and east sides of the building. Staff recognizes that this is an emergency access; however, a door with spandrel glass (or some other type of more aesthetically pleasing material) will compliment the high quality façade while lightening the appearance along 28th. The service doors on the north elevation are not as visible for passers-by and are therefore not a concern.

Address: 1955 28TH ST

Drainage Erik Saunders, 303 441-4493

1. At the time of Technical Document review, the proposed connection to the dual 60" piped N. Boulder Farmers and Boulder and Left Hand ditches will require ditch company approvals. Please contact Dan Lisco (303 530-4216) of N. Boulder Farmers Ditch and John Bruner (303 652-3124) of Boulder and Left hand Ditch for permission and approval requirements.
2. The water quality design spreadsheet for the proposed rain garden taken from Urban Drainage and Flood Control District (UDFCD) Urban Storm Drainage Criteria Manual Vol. 3 (*Design Procedure Form (RG)*), must be revised to include the underdrain system entries. In addition, the basin geometry section must be revised to match the geometry as indicated by the proposed raingarden grading.
3. The drainage letter narrative and supporting calculations, drainage plan and the proposed water quality facilities (rain garden) design information must be included in the Final Drainage Report submitted as part of the Technical Document review process.

Engineering Erik Saunders, 303 441-4493

1. At the time of Technical Document Review, the final engineering construction plans must include all of the required City of Boulder construction and erosion control notes. In addition, the plans must include erosion control /storm water management and all relevant construction details sheets.
2. The proposed utility easement for the fire hydrant extension onto property must be widened to 25' around the hydrant assembly.

Fees

Please note that 2014 development review fees include a \$131 hourly rate for reviewer services following the initial city response (these written comments). Please see the P&DS Questions and Answers brochure for more information about the hourly billing system.

Legal Documents Julia Chase, City Attorney's Office, 303-441-3052

1. Prior to signing the Development Agreement, if approved, the Applicant shall provide the following:
 - a) A title commitment current within 30 days (the estate or interest in the land must be for the fee interest (not leasehold interest); and
 - b) Proof of authorization (confirmation of signature block) on behalf of Andre Family Partnership, LTD.

Parking Chandler Van Schaack, Case Manager

1. It is unclear how the number of existing spaces as shown on the revised plans is currently 146; however, it should be noted that per the previous approval for this site (LUR2012-00007), a maximum of 135 parking spaces may be maintained on this site without triggering compliance with the parking lot landscaping requirements for "*Parking Lots Containing One Hundred Twenty Percent or More of The Minimum Required Parking Spaces*" as set forth in section 9-9-14(d)(5), B.R.C. 1981. In revising the number of parking spaces to not exceed 135 spaces, the applicant should also note that a maximum of 60% of the total parking spaces may be compact spaces, with the exception of 8 "universal" spaces (8'6" x 17'6") that were approved through the original PUD and maintained through LUR2012-00007. This means that for 135 spaces, a total of 81 spaces may be compact, with an additional 8 spaces that may be "universal" in size. All "universal" stalls being counted toward the total should be clearly labeled on the site plan and civil site plan accordingly.
2. The three 17' long parking stalls labeled as standard immediately south of the proposed Bank of America building do not meet minimum standard stall size requirements and should therefore not be counted as such.
3. Currently the parking calculations for Denver Mattress shown on the parking data table on the site plan and civil site plan are incorrect. Please ensure that all calculations are correct on the revised tables.

Utilities Erik Saunders, 303 441-4493

Please address the following correction comments:

1. Water pipe 4" and larger requires thrust blocking at all fittings. Add thrust blocking graphic at all fitting locations for the proposed fire hydrant and relocated fire hydrant.
2. Include all relevant standard detail drawings from the City of Boulder Design and Construction Standards including, trenching, bedding, joint restraint, thrust blocking, etc.

3. The proposed 12" PVC storm drain lateral connection to the existing 60" RCP storm/ditch pipe must be shown as a manhole connection.

III. INFORMATIONAL COMMENTS

Access/Circulation David Thompson, 303-441-4417

1. Consider revising the head-in parking along the Hazel's Building in order to provide a direct cross-walk across the drive aisle which would be beneficial to visual impaired individuals who would not realize the crosswalk is skewed.
2. Consider using colored concrete (red) and raising the 7' wide concrete path which would provide enhancements to the path by slowing down the crossing vehicles.

Engineering, Erik Saunders 303 441-4493

No portion of any structure, including footings and eaves, may encroach into any public right-of-way or easement.

Flood Control Heidi Hansen, Floodplain Administration, 303-441-3273

There is a section of 100-year regulatory floodplain in the northwest corner by the new building. This area of floodplain has been remapped as 500-year floodplain and the new mapping is in to FEMA for adoption. If the building permits are submitted before FEMA adopts the new mapping and the building extends into the area currently mapped as 100-year floodplain, a floodplain development permit will be required and the building will have comply with the city's floodplain regulations.

Utilities Erik Saunders 303 441-4493

1. The applicant is advised that any proposed street trees along the property frontage may conflict with existing utilities, including without limitation: gas, electric, and telecommunications, within and adjacent to the development site. It is the applicant's responsibility to resolve such conflicts with appropriate methods conforming to the Boulder Revised Code 1981, the City of Boulder Design and Construction Standards, and any private/franchise utility specifications.
2. A Final Utility Connection Plan will be required as part of the Technical Document Review process (which must be completed prior to building permit application). The Final Master Utility Plan (Utility Connection Plan) will be required to show all existing water service lines and fire lines.
3. The applicant is advised that at the time of building permit application the following requirements will apply:
 - a. The applicant will be required to provide an accurate proposed plumbing fixture count to determine if the proposed meters and services are adequate for the proposed use.
 - b. Water, wastewater and storm Plant Investment Fees and service line sizing will be evaluated.
 - c. If the existing water and/or wastewater services are required to be abandoned and upsized, all new service taps to existing mains shall be made by city crews at the developer's expense. The water service must be excavated and turned off at the corporation stop, per city standards. The sewer service must be excavated and capped at the property line, per city standards.
 - d. Since the building will be sprinklered, the approved fire line plans must accompany the fire sprinkler service line right-of-way permit application.
4. All water meters are to be placed in city R.O.W. or a public utility easement, but meters are not to be placed in driveways, sidewalks or behind fences.
5. Trees proposed to be planted shall be located at least 10 feet away from existing or future utility mains and services.

IV. NEXT STEPS

Once the issues identified herein have been addressed, please submit three copies of the final plans as well as digital copies of the plans in pdf form directly to the Case Manager, Chandler Van Schaack (303-441-3137 or vanschaackc@bouldercolorado.gov), at your earliest convenience.

V. CITY CODE CRITERIA CHECKLIST

Address: 1955 28TH ST

A completed checklist will be provided with the staff memorandum to planning board.

VI. CONDITIONS ON CASE

Draft conditions will be provided following review of the corrected materials.

CITY OF BOULDER
PLANNING BOARD AGENDA ITEM
MEETING DATE: January 22, 2015

AGENDA TITLE: Public hearing and consideration of a Minor Amendment to an Approved Site Review (LUR2014-00088) for a 1,950 square foot addition to an existing single-family residence partially located in the rear yard setback at 3059 6th St. The project site is zoned Residential - Low 1 (RL-1).

Applicant: COBURN DEVELOPMENT INC
Owner: KARA AND ADAM GOUCHER

REQUESTING DEPARTMENT:

Community Planning & Sustainability
David Driskell, Executive Director
Susan Richstone, Deputy Director
Charles Ferro, Development Review Manager
Sloane Walbert, Planner I

OBJECTIVE:

Define the steps for Planning Board consideration of this request:

1. Hear Applicant and Staff presentations
2. Hold Quasi-Judicial Public Hearing
3. Planning Board discussion
4. Planning Board action to approve, approve with conditions or deny

SUMMARY:

Proposal: MINOR AMENDMENT to a previously approved Site Review application (#P-92-21). Proposal to expand the existing 3,146 square foot home by 1,402 square feet of new floor area for a living area on two levels, covered porch, basement and attached garage. The use will remain a single family residence. Request also includes a setback modification for a 2'-6" setback for new portions of the building where 25' is required.

Project Name: 3059 6TH ST AMENDMENT
Location: 3059 6th St.
Size of Tract: 9,375 square feet (0.22-acres)
Zoning: Residential - Low 1 (RL-1)
Comprehensive Plan: Low Density Residential

KEY ISSUE:

Is the proposed Site Review Amendment consistent with the criteria for Minor Amendments to Approved Site Plans as set forth in section 9-2-14(l), B.R.C. 1981?

BACKGROUND:

Existing Site/Site Context

The subject property is comprised of three lots located on 6th Street, south of Evergreen Avenue in the Newlands neighborhood (see vicinity map). An alley exists on the west side of the property. The stone and frame portion of the existing home was part of the original structure constructed on the far west side (rear) of the property in 1927.



Figure 1: Vicinity Map

The property is located in the RL-1 zone district, which is defined as “single-family detached residential dwelling units at low to very low residential densities” (section 9-5-2(c)(1)(A), B.R.C. 1981). See Figure 2 below for a Zoning Map. The corresponding [Boulder Valley Comprehensive Plan \(BVCP\)](#) land use designation for the property and the surrounding neighborhood is Low Density Residential (refer to Figure 3 below).



Figure 2: Zoning Map

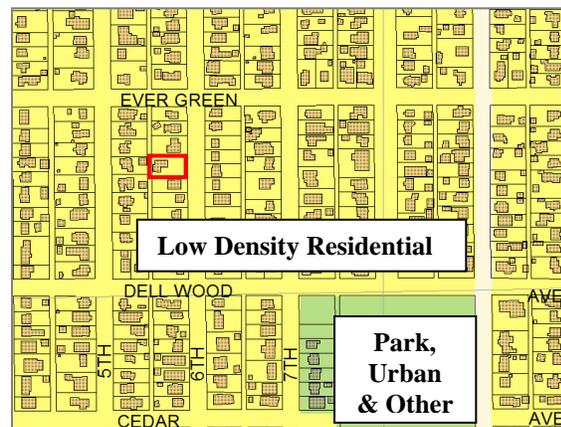


Figure 3: BVCP Land Use Designation Map

Project Description

The applicant is requesting a 793 square foot addition to the first floor and a 609 square foot addition to the second floor for a total addition of 1,402 square feet in above grade floor area to the existing 3,146 square foot single-family residence. The remodel will include raising the floor plate height at the rear of the house to expand usable floor area and to construct an attached garage on the north side of the house. The remaining floor area will be part of an addition on the front of the structure, facing 6th Street. A portion of the attached garage and second floor addition (310 square feet) will be located in the modified rear yard setback but will not extend beyond the previously approved 2'-6" setback (see figure 4 on the following page). The request also includes the addition of 540 square feet to the basement; however, this area is not included in floor area calculations since no portion of the basement wall is exposed more than 3 feet adjacent to finished grade, pursuant to section 9-8-2(e)(1)(D), B.R.C. 1981.

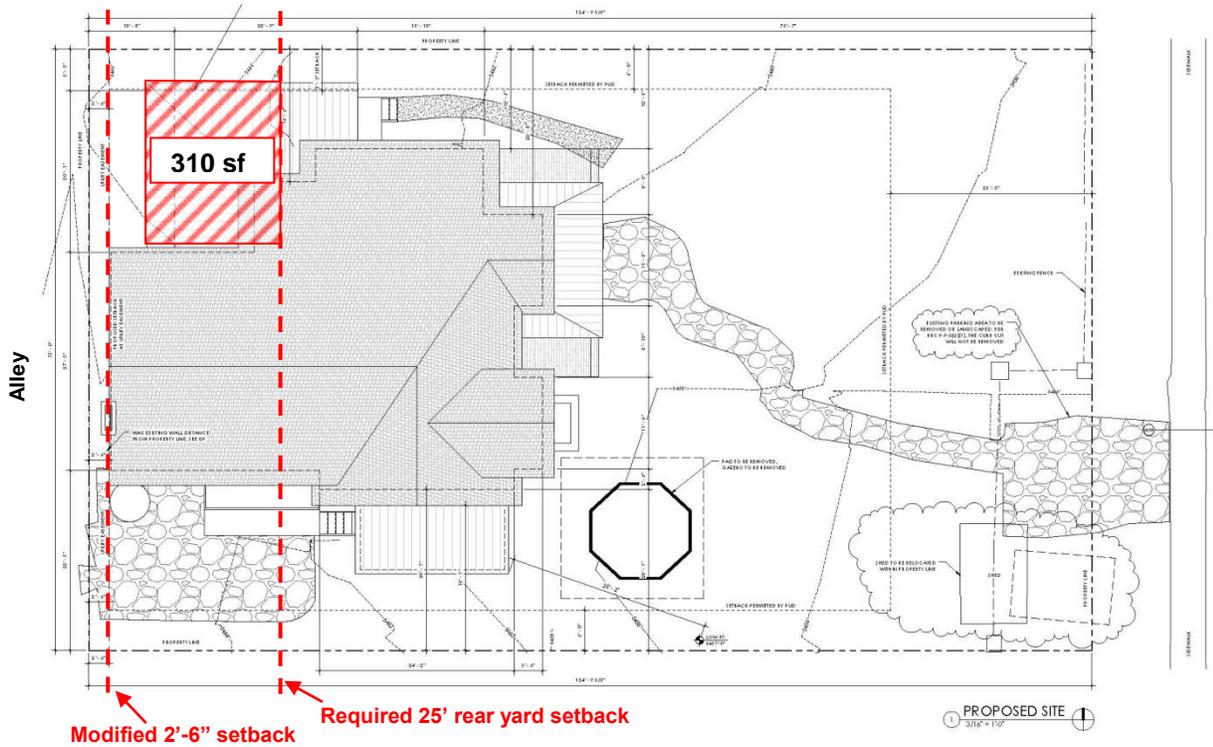


Figure 4: Proposed addition made within the rear yard setback



Figure 5: Rear Elevation (alley) showing existing elevation (left) and proposed elevation (right)

A new covered front porch addressing 6th Street is proposed. Figure 6 on the following page illustrates the existing and proposed street elevations. As part of the project, an existing gazebo structure on the property and an existing parking area located in the front yard landscape setback will be removed. An approximately 100 square foot shed currently straddles the front property line, a portion of which is located in the public right-of-way. The shed will be relocated onto the property and screened with new landscaping as a condition of this approval. See [Attachment D](#) for approved plans.

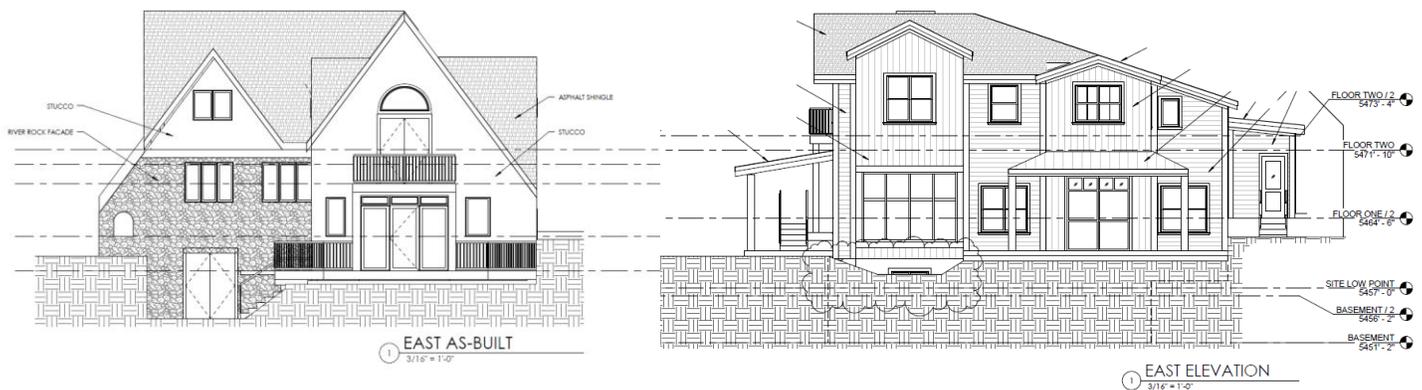


Figure 6:
Front Elevation (6th Street) showing existing elevation (left) and proposed elevation (right)

Project History

The historic home was constructed in 1927 and can be viewed from 6th Street in a photograph taken between 1942 and 1948, shown in figure 7. The building's setbacks are unique since the house was originally constructed at the far west end of the lot. Subsequently, a nonconforming review and Planned Unit Development (PUD) was approved in 1993 for an addition (#NC-92-03 and #P-92-21). The approval included a rear yard setback modification for a 2.5-foot setback, where 25 feet are required. The two-story frame section was added in front of the original front door in 1995.



Figure 7: Photograph of subject home in the background, taken between 1942 and 1948

REVIEW PROCESS:

On Nov. 12, 2014, the Landmarks Design Review Committee (LDRC) reviewed the demolition permit application for the demolition of the most recent addition and street-facing walls. The LDRC found that its demolition would not cause a significant impact or potential detriment to the historic resources of the city, as the house had been significantly altered by the 1990's addition.

On December 12, 2014, following review by the LDRC, city staff approved the Minor Amendment to the approved Site Review to allow the proposed additions (refer to [Attachment A](#) for staff disposition). Pursuant to section 9-2-14(l), B.R.C. 1981, changes to approved building location or additions to existing buildings, which exceed the limits of a Minor Modification, require a Minor Amendment to the Approved Site Plan. The Minor Amendment is a staff-level decision subject to call-up by the Planning Board or by the public within 14 days of staff's decision. The application was called up for discussion by the Planning Board on December 22, 2014.

KEY ISSUE:

Staff has identified the following key issue for the board's consideration:

Is the proposed Site Review Amendment consistent with the criteria for Minor Site Review Amendments to Approved Site Plans as set forth in section 9-2-14(l), B.R.C. 1981?

Section 9-2-14(l), "*Minor Amendments to Approved Site Plans*," B.R.C. 1981 includes the procedures and review criteria for approval of a minor amendment to an approved Site Review development. The criteria for a Minor Site Review Amendment require an evaluation of a project with only specific Site Review criteria of the B.R.C. 1981 subsections 9-2-14(h)(2) (A), (C), and (F), Open Space, Landscaping, and Building Design respectively.

Open Space and Landscaping:

In terms of open space, the building coverage on the property will increase with the addition, which effectively reduces the open space. The open space change is essentially in-filling an area on the side of the house currently occupied by patio space and an area used as a parking pad off the alley. The total open space proposed on the site is 7,092 square feet, including the front and side covered porches. The usable area of the open space, primarily in the front of the house, will not change substantially. The existing landscaping, which includes several mature trees and a stone retaining wall, will remain.

There is no minimum required open space per dwelling unit in the RL-1 zone district. The allowable intensity is determined by the maximum floor area ratio and number of dwelling units per acre. The proposed addition will not materially affect the character or quality of the open space or landscaping. The majority of the open space is oriented toward 6th Street and provides a visual relief to the density.

Building Design, Livability and Relationship to the Surrounding Area

Regarding building design, Site Review criteria (F) examines the compatibility of the proposed "*height, mass and scale in the existing character of the area, or the character established by adopted design guidelines for the area.*" The Newlands neighborhood is characterized by predominantly single-family homes ranging from modest ranch style homes to stately homes of new construction in an eclectic mix of architectural styles. The height, mass and scale of the subject home including the proposed additions are compatible with the character of the area. Although the front yard setback is larger than typical, the orientation and configuration of the home is similar to others in the neighborhood. In order to maintain the historic character of the house the applicant has proposed additions on each side of the house rather than expanding into the front yard. This configuration also allows vehicular access into a new garage from the alley. All existing stucco will be removed and replaced by vertical wood siding or painted cement lap board siding. The existing stone façade will remain as it is, with the exception of the east face, which will be enclosed by the addition. Staff finds that the proposed materials are compatible with the surrounding neighborhood and historic character of the home. In particular, the proposed wood siding and existing stone meet the site review criteria in section 9-2-14(h)(2)(F), which states that "*exteriors of buildings present a sense of permanence through the use of authentic materials such as stone, brick, wood, metal or similar products and building material detailing.*"

With regard to criterion (F)(iii) which states, "*the orientation of buildings minimizes shadows on and blocking of views from adjacent properties,*" the site is located within Solar Access Area I, that restricts shading from the structure to a degree less than that created by a solar fence twelve feet in height. The solar analysis provided demonstrates that the proposed development is in compliance with the Solar Access Ordinance. Further, the proposed additions are in compliance with side yard bulk plan regulations and the building steps down toward the neighboring properties in order to preserve views and enhance

privacy.

The proposal was found to be consistent with the criteria for Minor Amendments to Approved Site Plans found in section 9-2-14(l), B.R.C. 1981. Refer to [Attachment C](#) for staff's complete analysis of the review criteria.

PUBLIC COMMENT:

Required public notice was provided in the form of written notifications of the application to property owners within 600 feet of the subject property. In addition, a public notice sign was posted on the property. Therefore, all public notice requirements of section 9-4-3, "Public Notice Requirements," B.R.C. 1981 were met. Several phone calls and emails were received from neighbors regarding the proposed project. The majority of the correspondence was general questions regarding the proposal. The neighbor to the west across the alley expressed concern about the size of the addition and that the new roof line would block the sun they currently enjoy in the morning that comes over the existing roof line. Staff communicated that the solar analysis provided in the application indicates that the morning sun will not be impacted since the shadow cast by the roofline will not extend beyond the alley boundaries. No neighbors expressed direct opposition to the project. Refer to [Attachment B](#) for neighborhood correspondence.

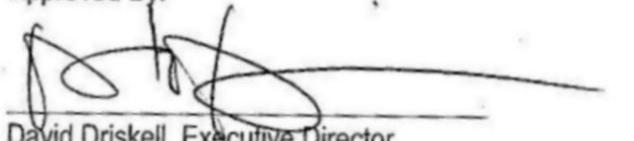
STAFF FINDINGS AND RECOMMENDATION:

Staff finds that the application for a Minor Amendment meets the criteria of section 9-2-14(l), B.R.C. 1981. Therefore, staff recommends that Planning Board approve Land Use Review # LUR2014-00088 incorporating this staff memorandum and associated review criteria as findings of fact and subject to the recommended conditions of approval.

RECOMMENDED CONDITIONS OF APPROVAL:

1. The Applicant shall ensure that the development shall be in compliance with all plans prepared by the Applicant on December 12, 2014 on file in the City of Boulder Planning Department, except to the extent that the development may be modified by the conditions of this approval.
2. The Applicant shall relocate the existing shed on the east side of the property so as to be entirely located on the property, as shown on the plans dated December 12, 2014, and shall screen the shed from the adjacent right-of-way with landscaping.
3. The Applicant shall comply with all previous conditions contained in any previous approvals, except to the extent that any previous conditions may be modified by this approval, including, but not limited to the Development Agreement recorded in the office of the Boulder County Clerk and Recorder at Reception No. 01306466 on June 22, 1993.

Approved By:



David Driskell, Executive Director
Department of Community Planning and Sustainability

ATTACHMENTS:

- A. Staff Disposition
- B. Neighborhood Correspondence
- C. Staff Analysis of Review Criteria
- D. Proposed Site Plan



CITY OF BOULDER
Community Planning & Sustainability

1739 Broadway, Third Floor • P.O. Box 791, Boulder, CO 80306-0791
phone 303-441-1880 • fax 303-441-3241 • web www.bouldercolorado.gov

CITY OF BOULDER PLANNING DEPARTMENT
NOTICE OF DISPOSITION

You are hereby advised that the following action was taken by the Planning Department based on the standards and criteria of the Land Use Regulations as set forth in Chapter 9-2, B.R.C. 1981, as applied to the proposed development.

DECISION: **Approved with conditions**
PROJECT NAME: **3059 6TH STREET AMENDMENT**
DESCRIPTION: **SITE REVIEW to amend a previously approved Site Review (#P-92-21) to expand the existing 3,146 square foot home by 1,402 square feet of new floor area for a living area on two levels and attached garage. The project also includes two covered patios and a 548 square foot addition to the below-grade basement. The use will remain a single family residence.**

LOCATION: **3059 6TH ST**
COORD: **N05W07**
LEGAL DESCRIPTION: **Lots 39, 40 and 41, Block 33, Newland Addition, City of Boulder, County of Boulder, State of Colorado**

APPLICANT: **COBURN DEVELOPMENT INC**
OWNER: **Kara and Adam Goucher**
APPLICATION: **Minor Site Review Amendment, LUR2014-00088**
ZONING: **Residential - Low 1 (RL-1)**
CASE MANAGER: **Sloane Walbert**
VESTED PROPERTY RIGHT: **NO; the owner has waived the opportunity to create such right under Section 9-2-19, B.R.C. 1981.**

APPROVED MODIFICATIONS FROM THE LAND USE REGULATIONS:

- Section 9-7-1, "Schedule of Form and Bulk Standards," B.R.C. 1981: Minimum rear yard landscaped setback of 2'6" where 25' is required.

FOR CONDITIONS OF APPROVAL, SEE THE FOLLOWING PAGES OF THIS DISPOSITION.

Approved On: 12/12/14
Date

By: 
David Driskell, Executive Director of Community Planning and Sustainability

This decision may be appealed to the Planning Board by filing an appeal letter with the Planning Department within two weeks of the decision date. If no such appeal is filed, the decision shall be deemed final fourteen days after the date above mentioned.

Appeal to Planning Board expires: 12/26/14

Final Approval Date: _____

Address: 3059 6TH ST

IN ORDER FOR A BUILDING PERMIT APPLICATION TO BE PROCESSED FOR THIS PROJECT, A SIGNED DEVELOPMENT AGREEMENT AND FINAL PLANS FOR CITY SIGNATURE MUST BE SUBMITTED TO THE PLANNING DEPARTMENT WITH DISPOSITION CONDITIONS AS APPROVED SHOWN ON THE FINAL PLANS, IF THE DEVELOPMENT AGREEMENT IS NOT SIGNED WITHIN NINETY (90) DAYS OF THE FINAL DECISION DATE, THE PLANNING DEPARTMENT APPROVAL AUTOMATICALLY EXPIRES.

Pursuant to Section 9-2-12 of the Land Use Regulations (Boulder Revised Code, 1981), the applicant must begin and substantially complete the approved development within three years from the date of final approval. Failure to "substantially complete" (as defined in Section 9-2-12) the development within three years shall cause this development approval to expire.

CONDITIONS OF APPROVAL

1. The Applicant shall ensure that the development shall be in compliance with all plans prepared by the Applicant on December 12, 2014 on file in the City of Boulder Planning Department, except to the extent that the development may be modified by the conditions of this approval.
2. The Applicant shall relocate the existing shed onto the property, as shown on the plans dated December 12, 2014, and shall screen the structure from the adjacent right-of-way with landscaping.
3. The Applicant shall comply with all previous conditions contained in any previous approvals, except to the extent that any previous conditions may be modified by this approval, including, but not limited to the Development Agreement recorded in the office of the Boulder County Clerk and Recorder at Reception No. 01306466 on June 22, 1993.

Address: 3059 6TH ST

Attachment B: Neighborhood Correspondence

From: [Peter Birkeland](#)
To: [Walbert, Sloane](#)
Subject: addition to house: 3059 6th
Date: Monday, October 13, 2014 2:35:37 PM

I would let them do additions within the current rules. However, I feel we have enough super-sized houses in the neighborhood, so I wouldn't let them have any exceptions beyond the rules.
Peter Birkeland, 3075 5th St

From: [William Malling](#)
To: [Walbert, Sloane](#)
Subject: RE: Remodel at 3059 6th st.
Date: Monday, October 20, 2014 10:18:53 AM

Hi, Walbert,

I would like to know more about the remodel at 3059 6th st. I live across the ally at 3060 5th st. The property at 3059 6th sits right on the ally and any remodel will affect our property as well.

Would like more detail about the remodel.

Please call me at 303-960-5177.

Thanks,

William Malling

From: [William Malling](#)
To: [Walbert, Sloane](#)
Subject: Re: Remodel at 3059 6th st.
Date: Tuesday, November 04, 2014 3:45:37 PM

Hi Sloane,

Thanks for the email on the purposed project at 3059 6th st. My house is directly across the ally on 5th st. My concern is that the house not be any bigger then the allowed square footage for the lot under currant code.

Also my concern is will the new roof line be any higher than the existing roof line? We currently enjoy sun in the morning that comes over the existing roof line, which is a second story roof line.

**Thanks
William Malling
303-960-5177**

From: jacksonjackson@comcast.net
To: [Malling, William](#)
Cc: [Walbert, Sloane](#)
Subject: question about garage set back in the alley from neighbor Re: Remodel at 3059 6th st.
Date: Friday, November 21, 2014 7:50:15 AM

Hi-I'm curious if the new garage will be one story or two and will it be set back from alley-how many feet?- There is a parking pad at the back of the house now.....hoping the garage will begin where the parking pad ends on the far east side (set back maybe 16 feet or so from alley at this point). thanks jacque michelle (wife of william malling and neighbor.)

December 3, 2014 – Phone conversation with neighbor to the south. Answered questions about the project.

From: jacksonjackson@comcast.net
To: [Walbert, Sloane](#)
Subject: Re: question about garage set back in the alley from neighbor Re: Remodel at 3059 6th st.
Date: Saturday, December 06, 2014 9:58:35 AM

Hi Sloane-when does construction begin? 2015 winter or spring or summer? thanks neighbor-jacque michelle

Section 9-2-14(l) Minor Amendments to Approved Site Plans:

- (1) Standards: Changes to approved building location or additions to existing buildings, which exceed the limits of a minor modification, may be considered through the minor amendment process if the following standards are met:

The application is for an addition to an existing building but exceeds the limits of a minor modification because the additions would expand the floor area by more than ten percent.

- Y (A) In a residential zone as set forth in Section 9-5-2, "Zoning Districts," B.R.C. 1981, all approved dwelling units within the development phase have been completed;

The development consists of one dwelling unit completed in 1927.

- Y (B) In residential zones, dwelling unit type is not changed;

The structure will remain a single-family dwelling unit.

- Y (C) The required open space per dwelling unit requirement of the zone is met on the lot of the detached dwelling unit to be expanded; and

There is no minimum required open space per dwelling unit in the RL-1 zone district. The allowable intensity is determined by the maximum floor area ratio and number of dwelling units per acre. Based on these standards the property is limited to 3,975 square feet of floor area.

- Y (D) The total open space per dwelling unit in the development is not reduced by more than ten percent of that required for the zone; or

There is no minimum required open space per dwelling unit in the RL-1 zone district.

- N/A (E) If the residential open space provided within the development or an approved phase of a development cannot be determined, the detached dwelling unit is not expanded by more than ten percent and there is no variation to the required setbacks for that lot;

- N/A (F) For a building in a nonresidential use module, the building coverage is not increased by more than twenty percent, the addition does not cause a reduction in required open space, and any additional required parking that is provided is substantially accommodated within the existing parking arrangement;

- Y (G) The portion of any building over the permitted height under Section 9-7-1, "Schedule of Form and Bulk Standards," B.R.C. 1981, is not increased;

The existing home is 30'-8" in height where 35' is permitted.

- Y (H) The proposed minor amendment does not require public infrastructure improvements or other off-site improvements.

All public infrastructure to serve the home is existing and no off-site improvements are required.

- (2) Amendments to the Site Review Approval Process: Applications for minor amendment shall be approved according to the procedures prescribed by this section for site review approval, except:

- √ (A) If an applicant requests approval of a minor amendment to an approved site review, the city manager will determine which properties within the development would be affected by the proposed change. The manager will provide notice pursuant to Subsection 9-4-3(b), B.R.C. 1981, of the proposed change to all property owners so determined to be affected, and to all

property owners within a radius of 600 feet of the subject property.

The development consists of only one property and written notification of the application was provided to property owners within 600 feet of the subject property. In addition, a public notice sign was posted on the property consistent with the requirements of Subsection 9-4-3(b), B.R.C. 1981.

√ (B) Only the owners of the subject property shall be required to sign the application.

√ (C) The minor amendment shall be found to comply with the review criteria of Subparagraphs (h)(2)(A), (h)(2)(C), and (h)(2)(F) of this section.

See analysis below.

Y (D) The minor amendment is found to be substantially consistent with the intent of the original approval, including conditions of approval, the intended design character, and site arrangement of the development, and specific limitations on additions or total size of the building which were required to keep the building in general proportion to others in the surrounding area or minimize visual impacts.

The proposal is substantially consistent with the intent of the original approval in 1993. The intended design character and site arrangement have not been substantially modified. No other limitations were included in the original approval. The proposed home will meet compatible development standards and will be in general proportion to other homes in the neighborhood.

√ (E) The city manager may amend, waive, or create a development agreement.

Subparagraphs (h)(2)(A), (h)(2)(C), and (h)(2)(F) of section 9-2-14:

(h) Criteria for Review: No site review application shall be approved unless the approving agency finds that:

(2) Site Design: Projects should preserve and enhance the community's unique sense of place through creative de-sign that respects historic character, relationship to the natural environment, multi-modal transportation connectivity and its physical setting. Projects should utilize site design techniques which are consistent with the purpose of site review in subsection (a) of this section and enhance the quality of the project. In determining whether this subsection is met, the approving agency will consider the following factors:

Y **(A) Open Space:** Open space, including, without limitation, parks, recreation areas and playgrounds:

The reduction in open space is essentially in-filling an area on the side of the house currently occupied by patio space and an area used as a parking pad off the alley. The total open space proposed on the site is 7,092 square feet, including the front and side covered porches.

Y (i) Useable open space is arranged to be accessible and functional and incorporates quality landscaping, a mixture of sun and shade and places to gather;

The usable area of the open space, primarily in the front of the house, will not change substantially. A large yard is located in front of the house, which is open and accessible to the inhabitants of the home.

Y (ii) Private open space is provided for each detached residential unit;

Approximately 7,092 square feet (76%) of the lot is open space. An ample amount of open space will remain on the site to serve the single-family dwelling.

- Y (iii) The project provides for the preservation of or mitigation of adverse impacts to natural features, including, without limitation, healthy long-lived trees, significant plant communities, ground and surface water, wetlands, riparian areas, drainage areas and species on the federal Endangered Species List, "Species of Special Concern in Boulder County" designated by Boulder County, or prairie dogs (*Cynomys ludovicianus*), which is a species of local concern, and their habitat;

The mature trees on the property will remain. No other natural features exist.

- Y (iv) The open space provides a relief to the density, both within the project and from surrounding development;

The open spaces provide a relief to the density since the majority of the open space is located in front of the house.

- N/A (v) Open space designed for active recreational purposes is of a size that it will be functionally useable and located in a safe and convenient proximity to the uses to which it is meant to serve;

This criterion is not applicable to single-family homes.

- N/A (vi) The open space provides a buffer to protect sensitive environmental features and natural areas; and;

Not applicable since there are no sensitive environmental features or natural areas located adjacent to the property.

- N/A (vii) If possible, open space is linked to an area- or city-wide system.

Not applicable since no open space systems are located in the vicinity.

Y **(C) Landscaping:**

The existing landscaping, which includes several mature trees and a stone retaining wall, will remain.

- Y (i) The project provides for aesthetic enhancement and a variety of plant and hard surface materials, and the selection of materials provides for a variety of colors and contrasts and the preservation or use of local native vegetation where appropriate;

The established landscaping in the front yard of the property includes coniferous and deciduous trees and large shrubs. The area also flagstone walkways and a stone wall.

- Y (ii) Landscape design attempts to avoid, minimize or mitigate impacts on and off site to important native species, healthy, long lived trees, plant communities of special concern, threatened and endangered species and habitat by integrating the existing natural environment into the project;

The proposed additions will have minor impacts on existing plants and the natural environment.

- Y (iii) The project provides significant amounts of plant material sized in excess of the landscaping requirements of sections 9-9-12, "Landscaping and Screening Standards," and 9-9-13, "Streetscape Design Standards," B.R.C. 1981; and

The mature landscaping on the property exceeds the landscaping standards for single-family homes. In addition, several large public street trees are located along 6th Street.

- Y (iv) The setbacks, yards and useable open space along public rights of way are landscaped to provide attractive streetscapes, to enhance architectural features and to contribute to the development of an attractive site plan.

The existing mature landscaping enhances the proposed design. The applicant will also be providing a landscape screen to the relocated shed. Several mature public street trees screen the property from the street.

Y (F) **Building Design, Livability and Relationship to the Existing or Proposed Surrounding Area:**

All existing stucco will be removed and replaced by vertical wood siding or painted cement lap board siding. The existing stone façade will remain as it is, with the exception of the east face, which will be enclosed by the addition. Staff finds that the proposed materials are compatible with the surrounding neighborhood and historic character of the home.

Y (i) The building height, mass, scale, orientation, architecture and configuration are compatible with the existing character of the area or the character established by adopted design guidelines or plans for the area;

The proposed home will meet compatible development standards, which are designed to preserve the single family character of existing neighborhoods, and is compatible with the Newlands neighborhood. The Newlands neighborhood is characterized by predominantly single-family homes ranging from modest ranch style homes to stately homes of new construction in an eclectic mix of architectural styles. The height, mass and scale of the subject home including the proposed additions are compatible with the character of the area. Although the front yard setback is larger than typical, the orientation and configuration of the home is similar to others in the neighborhood with a front porch facing 6th Street and access from the alley. Staff finds that the proposed materials are compatible with the surrounding neighborhood and historic character of the home.

Y (ii) The height of buildings is in general proportion to the height of existing buildings and the proposed or projected heights of approved buildings or approved plans or design guidelines for the immediate area;

The existing home is 30'-8" in height where 35' is permitted in the zone, which is compatible with the neighborhood.

Y (iii) The orientation of buildings minimizes shadows on and blocking of views from adjacent properties;

The site is located within Solar Access Area I, that restricts shading from the structure to a degree less than that created by a solar fence twelve feet in height. The solar analysis provided demonstrates that the proposed development is in compliance with the Solar Access Ordinance. Further, the proposed additions are in compliance with side yard bulk plan regulations and the building steps down toward the neighboring properties in order to preserve views and enhance privacy.

Y (iv) If the character of the area is identifiable, the project is made compatible by the appropriate use of color, materials, landscaping, signs and lighting;

The proposed design and building materials are traditional and compatible with the Newlands neighborhood.

Y (v) Projects are designed to a human scale and promote a safe and vibrant pedestrian experience through the location of building frontages along public streets, plazas, sidewalks and paths, and through the use of building elements, design details and landscape materials that include,

without limitation, the location of entrances and windows, and the creation of transparency and activity at the pedestrian level;

The proposed front porch will address 6th Street and will contribute to the pedestrian experience. Landscaping located along 6th Street also contributes to activity at the pedestrian level.

N/A (vi) To the extent practical, the project provides public amenities and planned public facilities;

N/A (vii) For residential projects, the project assists the community in producing a variety of housing types, such as multifamily, townhouses and detached single family units, as well as mixed lot sizes, number of bedrooms and sizes of units;

The project will not add any dwelling units.

Y (viii) For residential projects, noise is minimized between units, between buildings and from either on-site or off-site external sources through spacing, landscaping and building materials;

Noise will be minimized by side yard setbacks and existing landscaping.

N/A (ix) A lighting plan is provided which augments security, energy conservation, safety and aesthetics;

A lighting plan is not necessary since this is a single family home. All lighting will be required to meet the lighting code for residential zone districts.

Y (x) The project incorporates the natural environment into the design and avoids, minimizes or mitigates impacts to natural systems;

The project has preserved the existing mature landscaping on the property. No natural systems exist in relation to the property.

Y (xi) Buildings minimize or mitigate energy use; support on-site renewable energy generation and/or energy management systems; construction wastes are minimized; the project mitigates urban heat island effects; and the project reasonably mitigates or minimizes water use and impacts on water quality;

The proposed structure will be required to comply with the Green Building and Green Points Program at building permit application, including compliance with the Energy Code provisions. The final determination for the level of energy performance will be determined at the time of building permit application. However, it appears possible that the addition will exceed the thresholds of section 10-7.5-3 b)(2), B.R.C. 1981, which requires that when an addition is 50 percent or more of the conditioned floor area of the existing dwelling unit (after demolition), and when the dwelling unit will have a total conditioned area upon completion from 3,001 to 5,000 square feet in size, the entire building must be treated as new construction for the purposes of establishing energy efficiency.

Y (xii) Exteriors of buildings present a sense of permanence through the use of authentic materials such as stone, brick, wood, metal or similar products and building material detailing;

The proposed vertical wood siding and painted cement lap board siding, along with the existing stone façade, contribute to a sense of permanence.

Y (xiii) Cut and fill are minimized on the site, the design of buildings conforms to the natural contours of the land, and the site design minimizes erosion, slope instability, landslide, mudflow or subsidence, and minimizes the potential threat to property caused by geological hazards;

The proposed additions, including the area within the rear yard setback, will have less of an impact than an addition within the approved building envelope since they would require less fill and grading.

N/A (xiv) In the urbanizing areas along the Boulder Valley Comprehensive Plan boundaries between Area II and Area III, the building and site design provide for a well-defined urban edge; and

N/A (xv) In the urbanizing areas located on the major streets shown on the map in Appendix A to this title near the Boulder Valley Comprehensive Plan boundaries between Area II and Area III, the buildings and site design establish a sense of entry and arrival to the City by creating a defined urban edge and a transition between rural and urban areas.

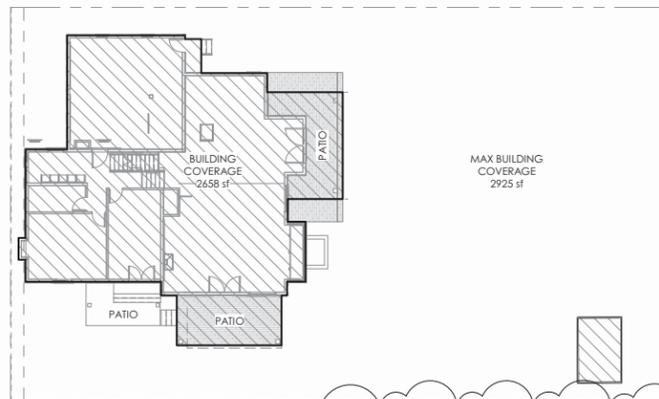
GOUCHER RESIDENCE

3059 6TH STREET
BOULDER, CO 80302

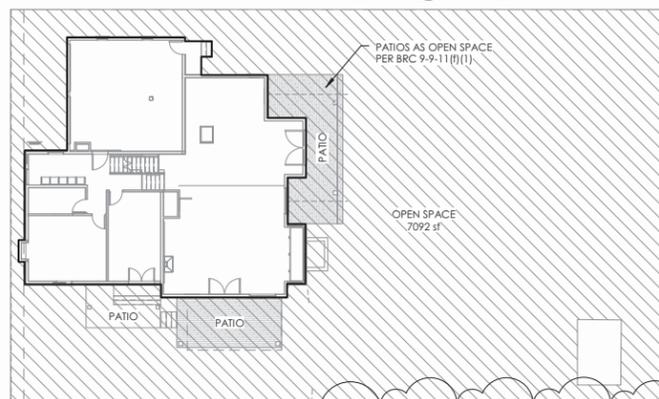


SHED	103.51 sf
SECOND FLR	1603.27 sf
FIRST FLR	2033.23 sf
BASEMENT	1025.07 sf
TOTAL	4765.08 sf

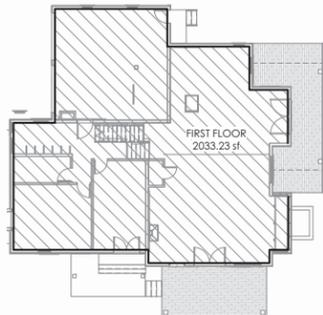
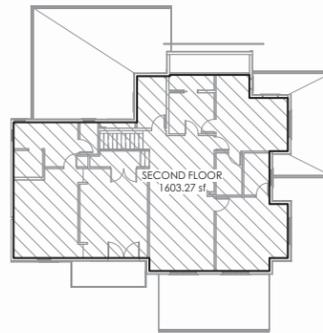
SHED	103.51 sf
SECOND FLR	1661.43 sf
FIRST FLR	2138.24 sf
TOTAL FAR	3903.18 sf
ALLOWED FAR	3975.00 sf



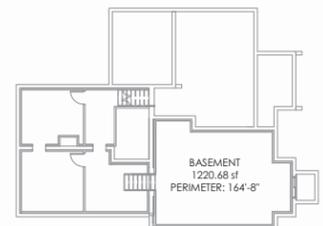
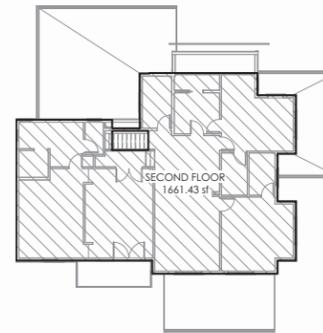
3 **BUILDING COVERAGE**
1/16" = 1'-0"



2 **OPEN SPACE**
1/16" = 1'-0"



4 **FLOOR AREA (IRC)**
1/16" = 1'-0"



NOTE: BASEMENT SQUARE FOOTAGE DOES NOT CONTRIBUTE TO FAR CALCULATIONS. NO PORTION OF WALL IS EXPOSED MORE THAN 3'-0" ADJACENT TO FINISHED GRADE. SEE BRC 9-8-2(e)(1)(D).

1 **FLOOR AREA RATIO (C.O.B.)**
1/16" = 1'-0"

SCOPE OF WORK	
REMODEL	Remodel including an addition to the ground floor area, adding an attached garage, and raising the second floor plate height in combination with the increased floor area
PROJECT INFORMATION	
EXISTING	
LEGAL DESCRIPTION	LOTS 39-40-41 BLK 33 NEWLANDS
BUILDING TYPE	SINGLE FAMILY DWELLING
ZONING	RL-1
CONSTRUCTION TYPE	5B NON RATED
OCCUPANCY CLASSIFICATION	R-3
LOT SIZE	9,375 SF
SETBACK MINIMUMS	FRONT: 25 FT, SIDE: 15' COMBINED, 5'MIN REAR: 25 FT

BUILDING CODE ANALYSIS: APPLICABLE CODES:

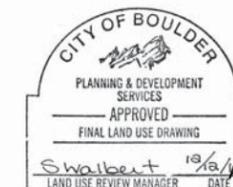
- 2012 INTERNATIONAL RESIDENTIAL CODE (IRC)
- 2012 INTERNATIONAL MECHANICAL CODE (IMC)
- 2012 INTERNATIONAL PLUMBING CODE (IPC)
- 2012 INTERNATIONAL FIRE CODE (IFC)
- 2012 INTERNATIONAL ENERGY CONSERVATION CODE (IECC)
- 2012 INTERNATIONAL FUEL GAS CODE (IFGC)
- 2011 NATIONAL ELECTRIC CODE (NEC)

PROJECT TEAM

OWNER ADAM & KARA GOUCHER 3059 6TH ST BOULDER, CO P: 303-803-8515	STRUCTURAL ENGINEER GEBAU INC. 1121 BROADWAY ST #201 BOULDER, CO P: 303-444-8545
ARCHITECT COBURN DEVELOPMENT INC. 3020 CARBON PLACE #203 BOULDER, CO P: 303-442-3351 F: 303-447-3933	CONTRACTOR COBURN DEVELOPMENT INC. 3020 CARBON PLACE #203 BOULDER, CO P: 303-442-3351 F: 303-447-3933



1 **VICINITY MAP**
NOT TO SCALE



3020 Carbon Place #203
Boulder, Colorado
p: 303-442-3351
f: 303-447-3933

GOUCHER RESIDENCE

3059 6TH ST.
BOULDER, CO 80302

DISCLAIMER:
THESE DOCUMENTS ARE PROVIDED BY COBURN DEVELOPMENT INC., FOR THE DESIGN INTENT OF THIS SPECIFIC PROJECT AND ONLY THIS PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CONSTRUCTION COORDINATION, METHODS AND MATERIALS REQUIRED FOR THE SUCCESSFUL COMPLETION OF THE PROJECT. THIS INCLUDES BUT IS NOT LIMITED TO THE QUALITY OF WORKMANSHIP AND MATERIALS REQUIRED FOR EXECUTION OF THESE DOCUMENTS AND WORK OR MATERIALS SUPPLIED BY ANY SUBCONTRACTORS. ALL WORK SHALL COMPLY WITH GOVERNING CODES AND ORDINANCES. THE CONTRACTOR SHALL REVIEW AND UNDERSTAND ALL DOCUMENTS AND SHALL NOTIFY THE ARCHITECT IMMEDIATELY OF ANY DISCREPANCIES IN THE DRAWINGS, FIELD CONDITIONS OR DIMENSIONS.

ISSUED/REVISION SCHEDULE			
DESCRIPTION	AUTHOR	CHECKED	DATE
LUR	LJ	PW	10.17.14
LUR REVISED	LJ	PW	10.31.14
LUR REVISED 2	LJ	PW	11.24.14

LUR REVISED
12.12.2014

SHEET No.

T1.0



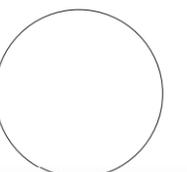
3020 Carbon Place #203
 Boulder, Colorado
 p: 303-442-3351
 f: 303-447-3933

GOUCHER RESIDENCE

3059 6TH ST.
 BOULDER, CO 80302

DISCLAIMER:
 THESE DOCUMENTS ARE PROVIDED BY COBURN DEVELOPMENT INC., FOR THE DESIGN INTENT OF THIS SPECIFIC PROJECT AND ONLY THIS PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CONSTRUCTION COORDINATION, METHODS AND MATERIALS REQUIRED FOR THE SUCCESSFUL COMPLETION OF THE PROJECT. THIS INCLUDES BUT IS NOT LIMITED TO THE QUALITY OF WORKMANSHIP AND MATERIALS REQUIRED FOR EXECUTION OF THESE DOCUMENTS AND WORK OR MATERIALS SUPPLIED BY ANY SUBCONTRACTORS. ALL WORK SHALL COMPLY WITH GOVERNING CODES AND ORDINANCES. THE CONTRACTOR SHALL REVIEW AND UNDERSTAND ALL DOCUMENTS AND SHALL NOTIFY THE ARCHITECT IMMEDIATELY OF ANY DISCREPANCIES IN THE DRAWINGS, FIELD CONDITIONS OR DIMENSIONS.

ISSUED/REVISION SCHEDULE			
DESCRIPTION	AUTHOR	CHECKED	DATE
LUR	LJ	PW	10.17.14
LUR REVISED	LJ	PW	10.31.14
LUR REVISED 2	LJ	PW	11.24.14



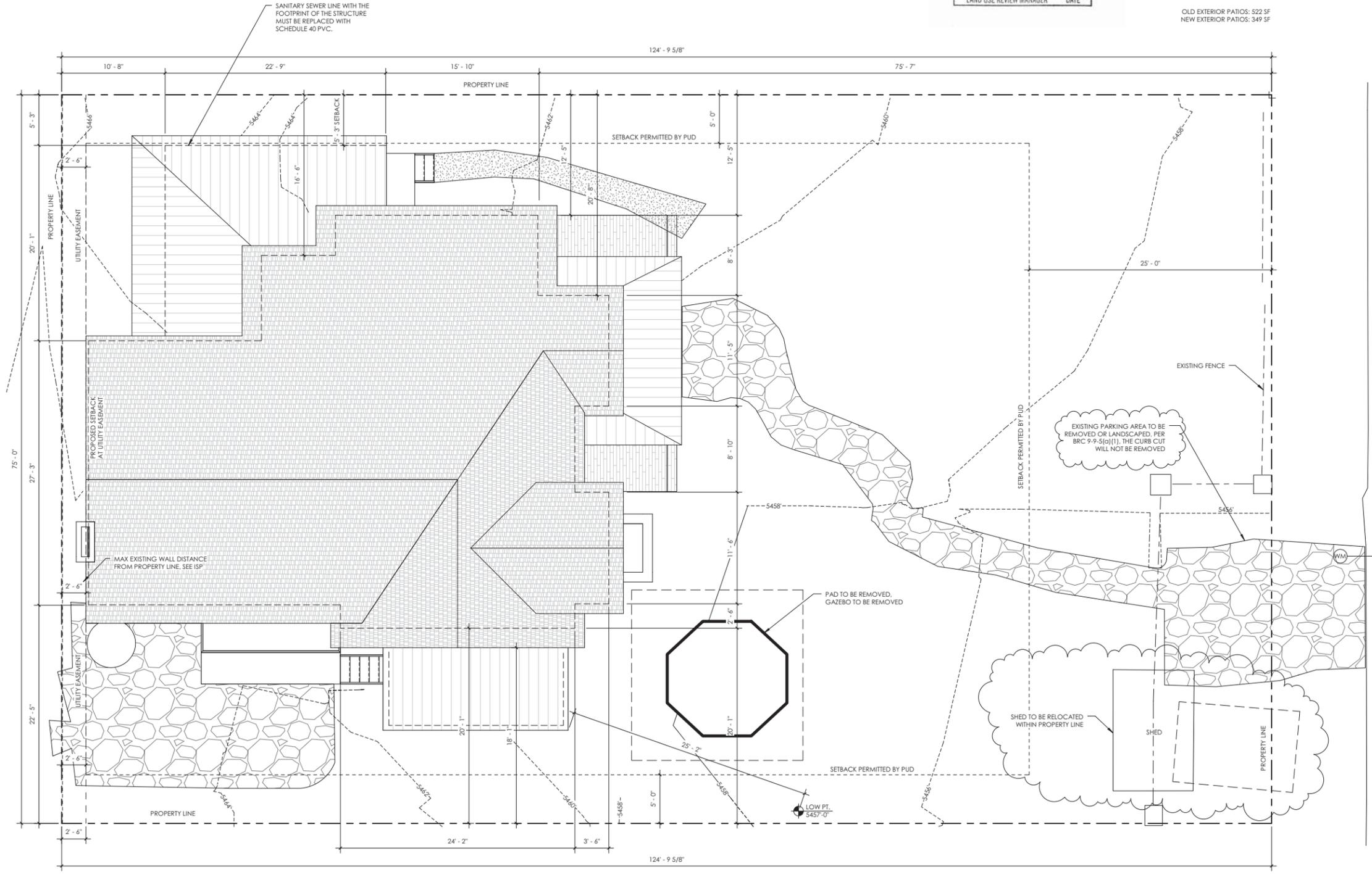
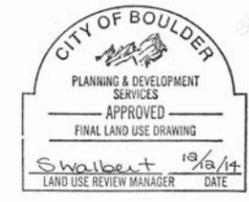
LUR REVISED
 12.12.2014

SHEET No.

T1.2

SITE PLAN

SITE: 9375 SF
 PER TABLE 9-B-2, BOULDER CODE
 RL-1, 5000 TO 10000 SF
 [9375 SF (LOT SIZE) x 0.2] = 2100
FAR: 3975 SF
 EXISTING SHED: 104 SF
 EXISTING GAZEBO: 121 SF
 EXISTING FLR 1: 1345 SF
 EXISTING FLR 2: 1116 SF
EXISTING TOTAL: 2686 SF
 EXISTING SHED: 104 SF
 PROPOSED FLR 1: 2138 SF
 PROPOSED FLR 2: 1725 SF
PROPOSED TOTAL: 3967 SF
 BASEMENT OLD: 485 SF
 BASEMENT NEW: 1233 SF
 OLD EXTERIOR PATIOS: 522 SF
 NEW EXTERIOR PATIOS: 349 SF



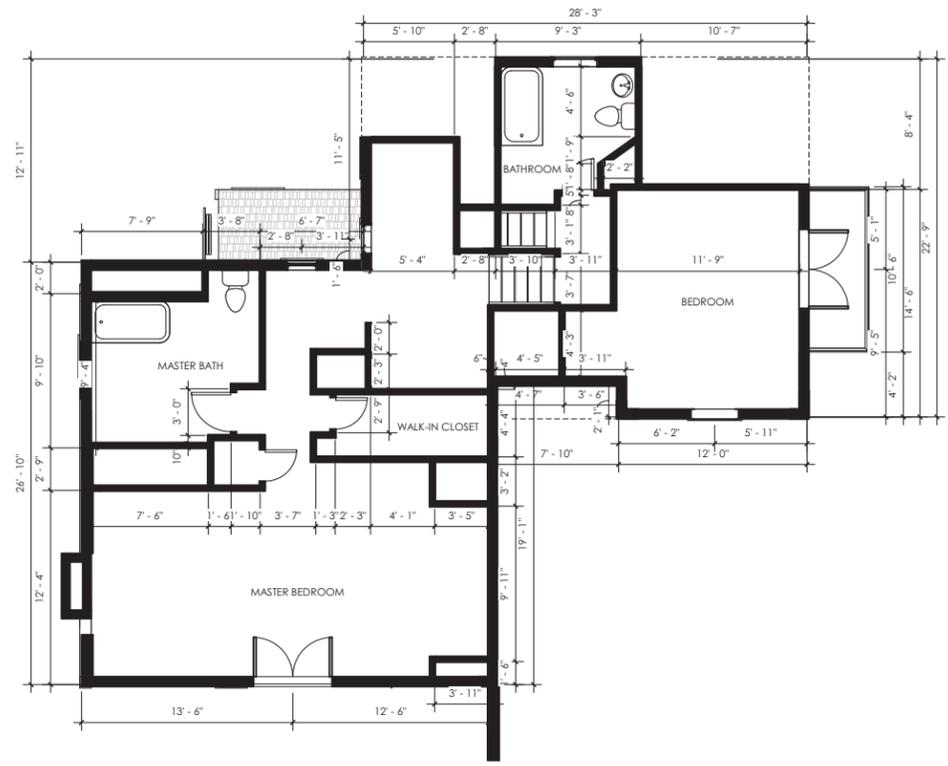
1 PROPOSED SITE
 3/16" = 1'-0"

SIDEWALK
6TH STREET
SIDEWALK

GOUCHER RESIDENCE

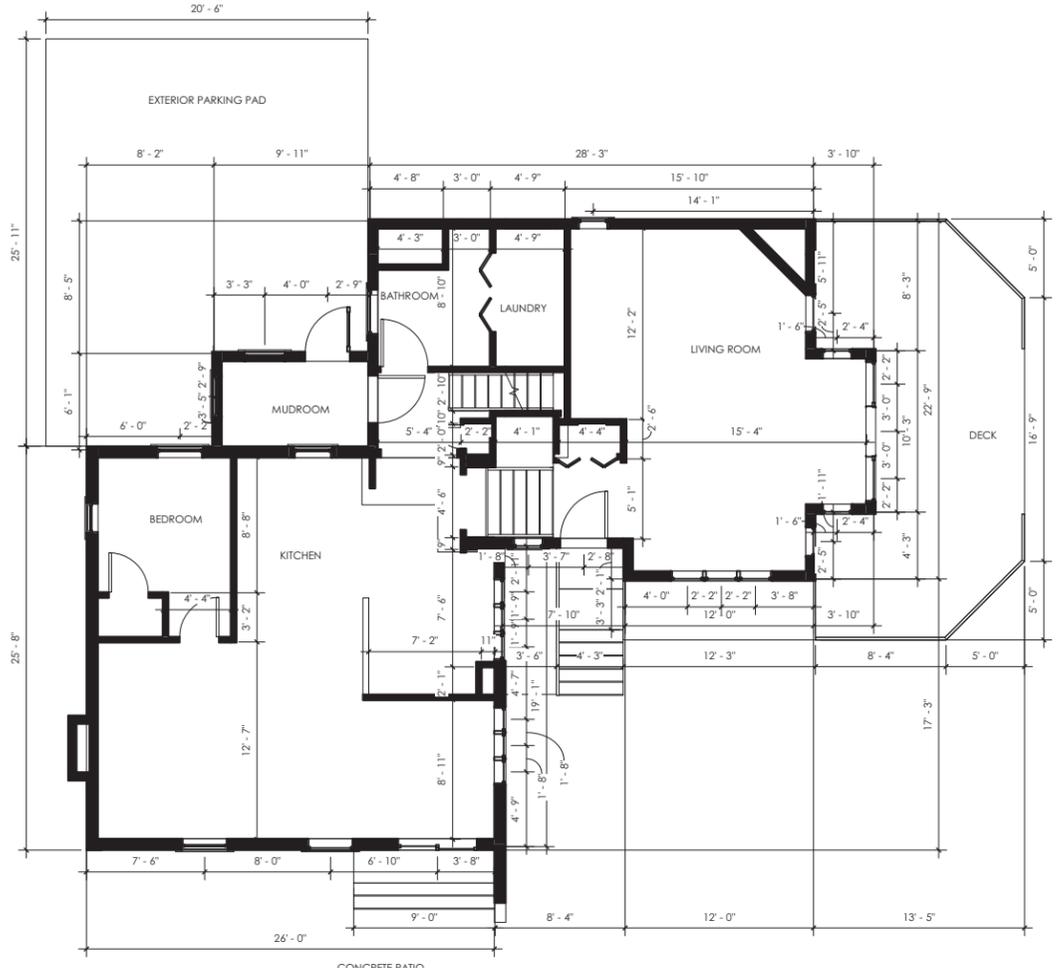
3059 6TH ST.
 BOULDER, CO 80302

DISCLAIMER:
 THESE DOCUMENTS ARE PROVIDED BY COBURN DEVELOPMENT INC., FOR THE DESIGN INTENT OF THIS SPECIFIC PROJECT AND ONLY THIS PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CONSTRUCTION COORDINATION, METHODS AND MATERIALS REQUIRED FOR THE SUCCESSFUL COMPLETION OF THE PROJECT. THIS INCLUDES BUT IS NOT LIMITED TO THE QUALITY OF WORKMANSHIP AND MATERIALS REQUIRED FOR EXECUTION OF THESE DOCUMENTS AND WORK OR MATERIALS SUPPLIED BY ANY SUBCONTRACTORS. ALL WORK SHALL COMPLY WITH GOVERNING CODES AND ORDINANCES. THE CONTRACTOR SHALL REVIEW AND UNDERSTAND ALL DOCUMENTS AND SHALL NOTIFY THE ARCHITECT IMMEDIATELY OF ANY DISCREPANCIES IN THE DRAWINGS, FIELD CONDITIONS OR DIMENSIONS.

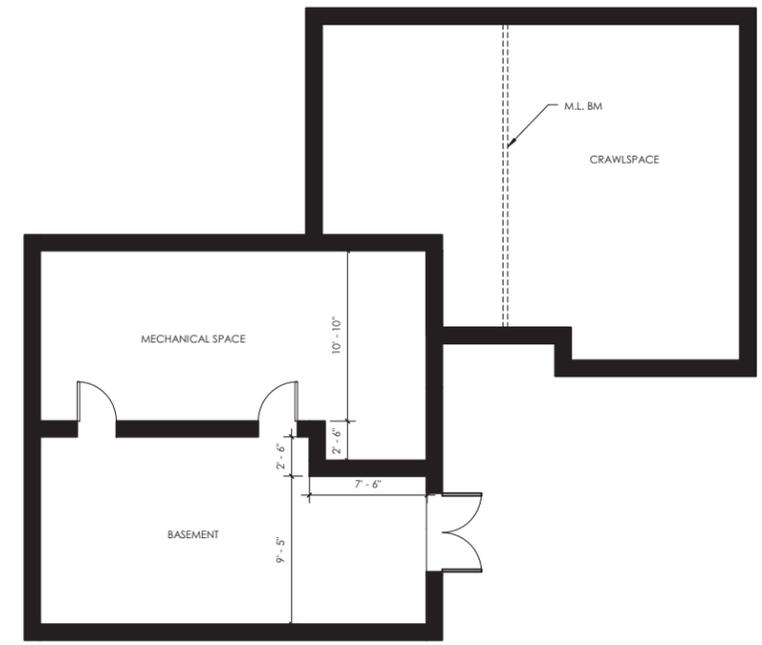


2 FLOOR 2
 3/16" = 1'-0"

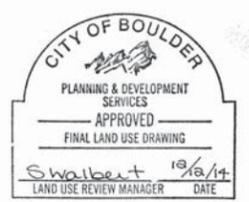
-  DEMOLISH
-  EXISTING TO REMAIN
-  NEW



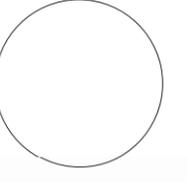
1 FLOOR 1
 3/16" = 1'-0"



3 BASEMENT
 3/16" = 1'-0"



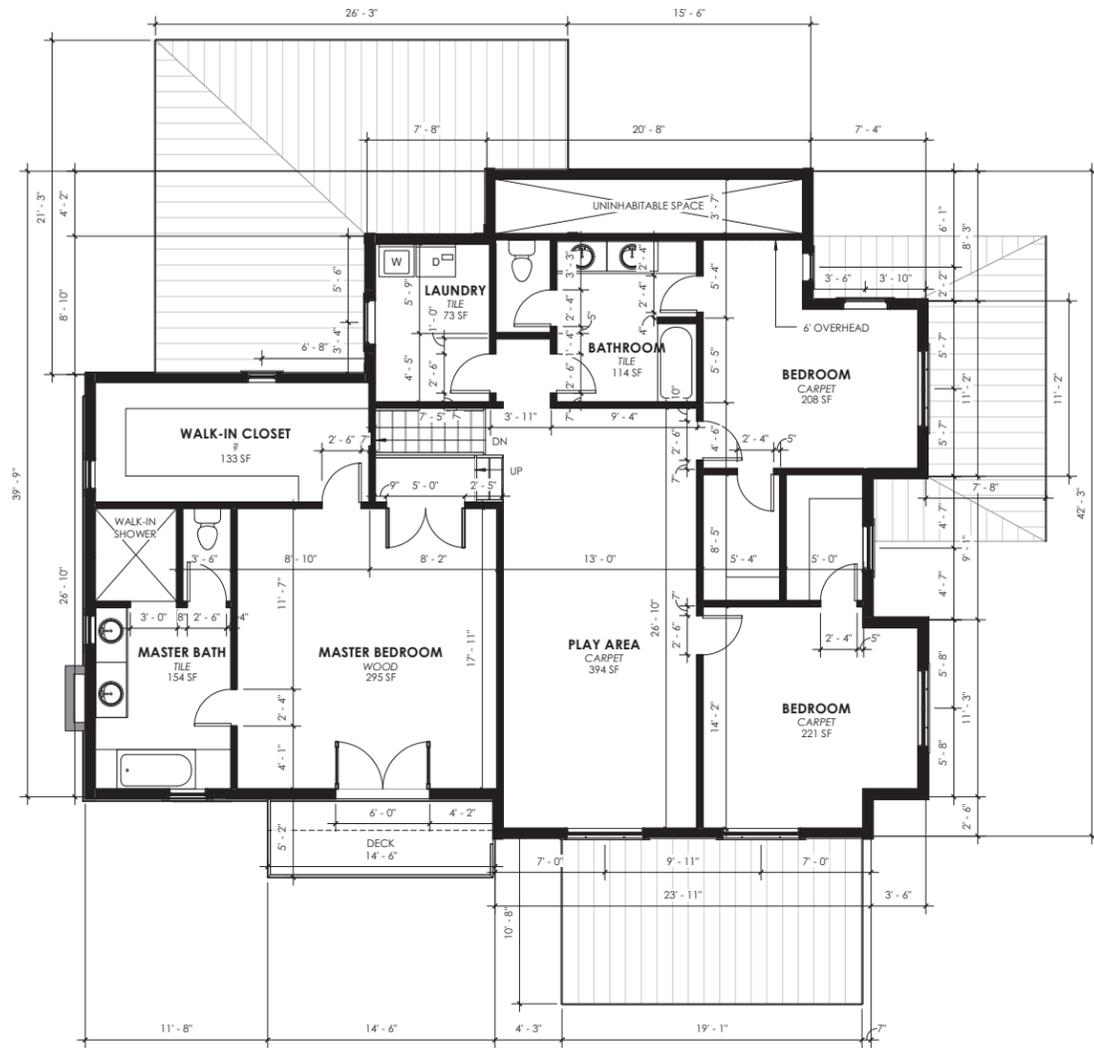
ISSUED/REVISION SCHEDULE			
DESCRIPTION	AUTHOR	CHECKED	DATE
LUR	LJ	PW	10.17.14



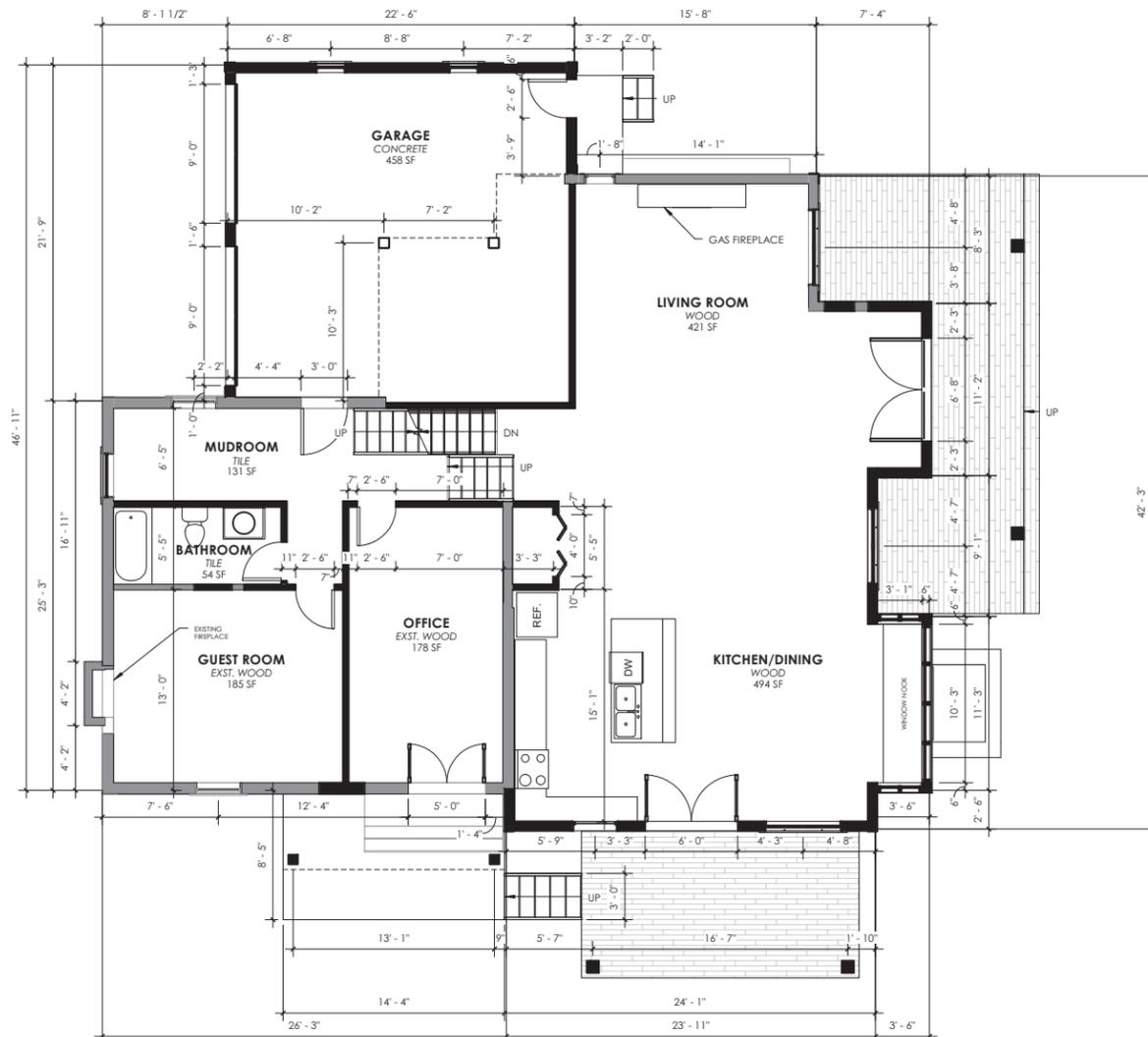
LUR REVISED
 12.12.2014

SHEET No.

A1.3

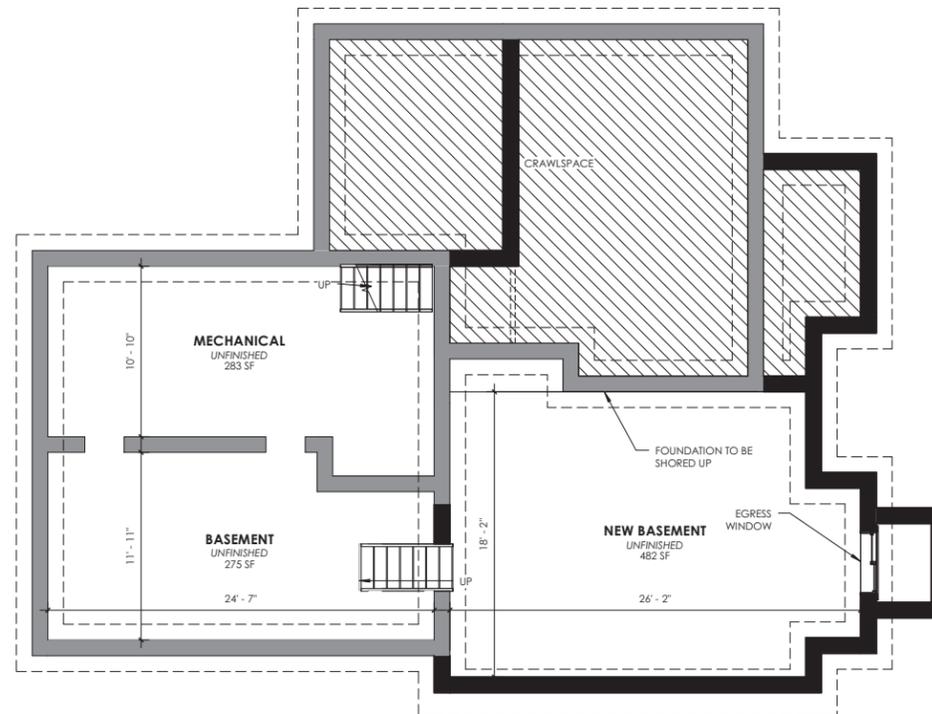


2 FLOOR 2
3/16" = 1'-0"

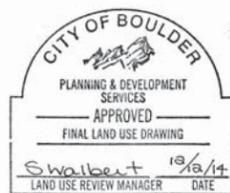


1 FLOOR 1
3/16" = 1'-0"

- DEMOLISH
- EXISTING TO REMAIN
- NEW



3 BASEMENT
3/16" = 1'-0"



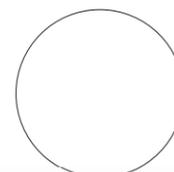
3020 Carbon Place #203
Boulder, Colorado
p: 303-442-3351
f: 303-447-3933

GOUCHER RESIDENCE

3059 6TH ST.
BOULDER, CO 80302

DISCLAIMER:
THESE DOCUMENTS ARE PROVIDED BY COBURN DEVELOPMENT INC., FOR THE DESIGN INTENT OF THIS SPECIFIC PROJECT AND ONLY THIS PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CONSTRUCTION COORDINATION, METHODS AND MATERIALS REQUIRED FOR THE SUCCESSFUL COMPLETION OF THE PROJECT. THIS INCLUDES BUT IS NOT LIMITED TO THE QUALITY OF WORKMANSHIP AND MATERIALS REQUIRED FOR EXECUTION OF THESE DOCUMENTS AND WORK OR MATERIALS SUPPLIED BY ANY SUBCONTRACTORS. ALL WORK SHALL COMPLY WITH GOVERNING CODES AND ORDINANCES. THE CONTRACTOR SHALL REVIEW AND UNDERSTAND ALL DOCUMENTS AND SHALL NOTIFY THE ARCHITECT IMMEDIATELY OF ANY DISCREPANCIES IN THE DRAWINGS, FIELD CONDITIONS OR DIMENSIONS.

ISSUED/REVISION SCHEDULE			
DESCRIPTION	AUTHOR	CHECKED	DATE
LUR	LJ	PW	10.17.14



LUR REVISED
12.12.2014

SHEET No.

A1.4



3020 Carbon Place #203
 Boulder, Colorado
 p: 303-442-3351
 f: 303-447-3933

GOUCHER RESIDENCE

3059 6TH ST.
 BOULDER, CO 80302



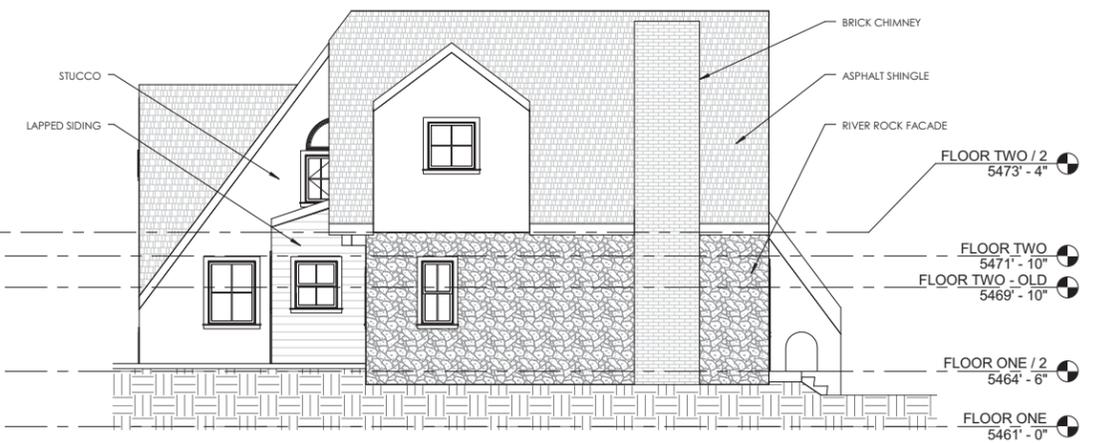
1 EAST AS-BUILT
 3/16" = 1'-0"



2 NORTH AS-BUILT
 3/16" = 1'-0"



3 SOUTH AS-BUILT
 3/16" = 1'-0"

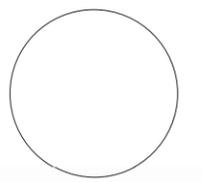


4 WEST AS-BUILT
 3/16" = 1'-0"

- FLOOR TWO / 2 5473' - 4"
- FLOOR TWO 5471' - 10"
- FLOOR TWO - OLD 5469' - 10"
- FLOOR ONE / 2 5464' - 6"
- FLOOR ONE 5461' - 0"

DISCLAIMER:
 THESE DOCUMENTS ARE PROVIDED BY COBURN DEVELOPMENT INC., FOR THE DESIGN INTENT OF THIS SPECIFIC PROJECT AND ONLY THIS PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CONSTRUCTION COORDINATION, METHODS AND MATERIALS REQUIRED FOR THE SUCCESSFUL COMPLETION OF THE PROJECT. THIS INCLUDES BUT IS NOT LIMITED TO THE QUALITY OF WORKMANSHIP AND MATERIALS REQUIRED FOR EXECUTION OF THESE DOCUMENTS AND WORK OR MATERIALS SUPPLIED BY ANY SUBCONTRACTORS. ALL WORK SHALL COMPLY WITH GOVERNING CODES AND ORDINANCES. THE CONTRACTOR SHALL REVIEW AND UNDERSTAND ALL DOCUMENTS AND SHALL NOTIFY THE ARCHITECT IMMEDIATELY OF ANY DISCREPANCIES IN THE DRAWINGS, FIELD CONDITIONS OR DIMENSIONS.

ISSUED/REVISION SCHEDULE			
DESCRIPTION	AUTHOR	CHECKED	DATE
LUR	LJ	PW	10.17.14



LUR REVISED
 12.12.2014

SHEET No.

A2.1



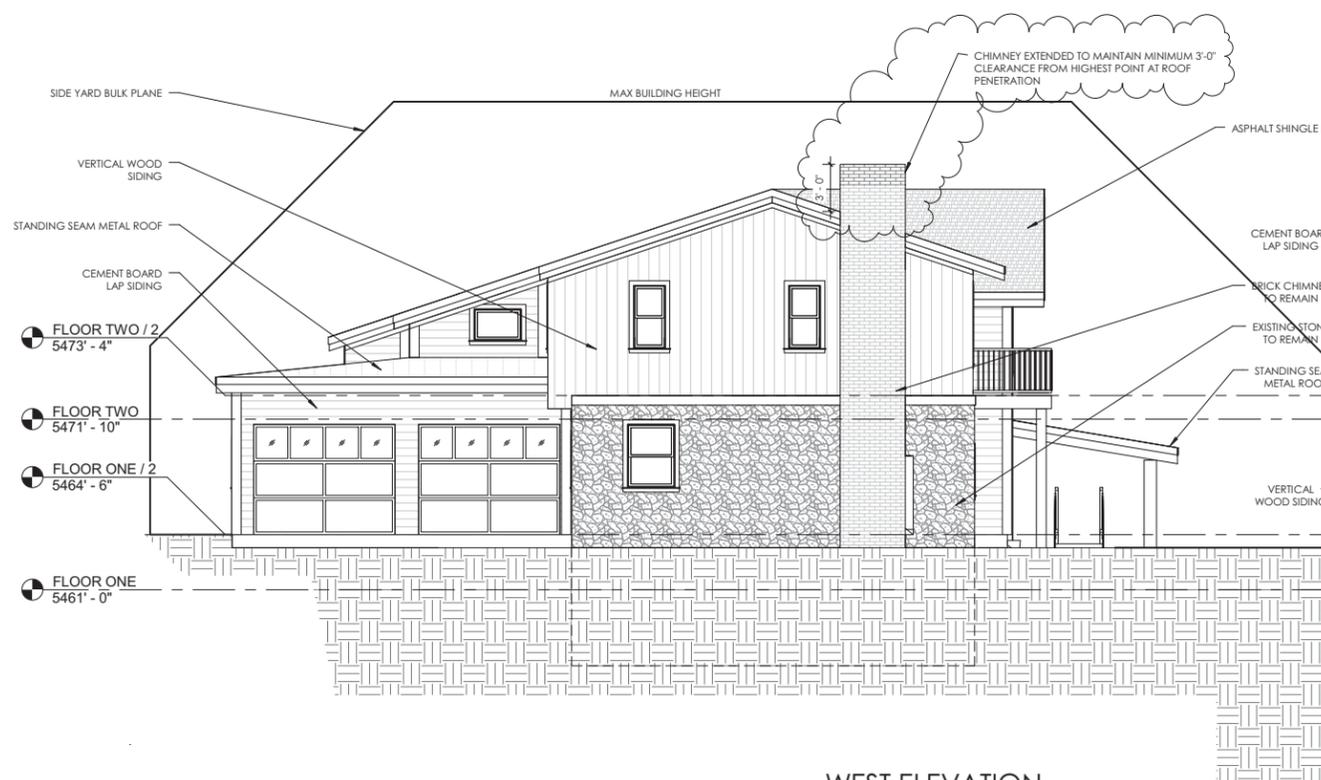
3020 Carbon Place #203
 Boulder, Colorado
 p: 303-442-3351
 f: 303-447-3933

GOUCHER RESIDENCE

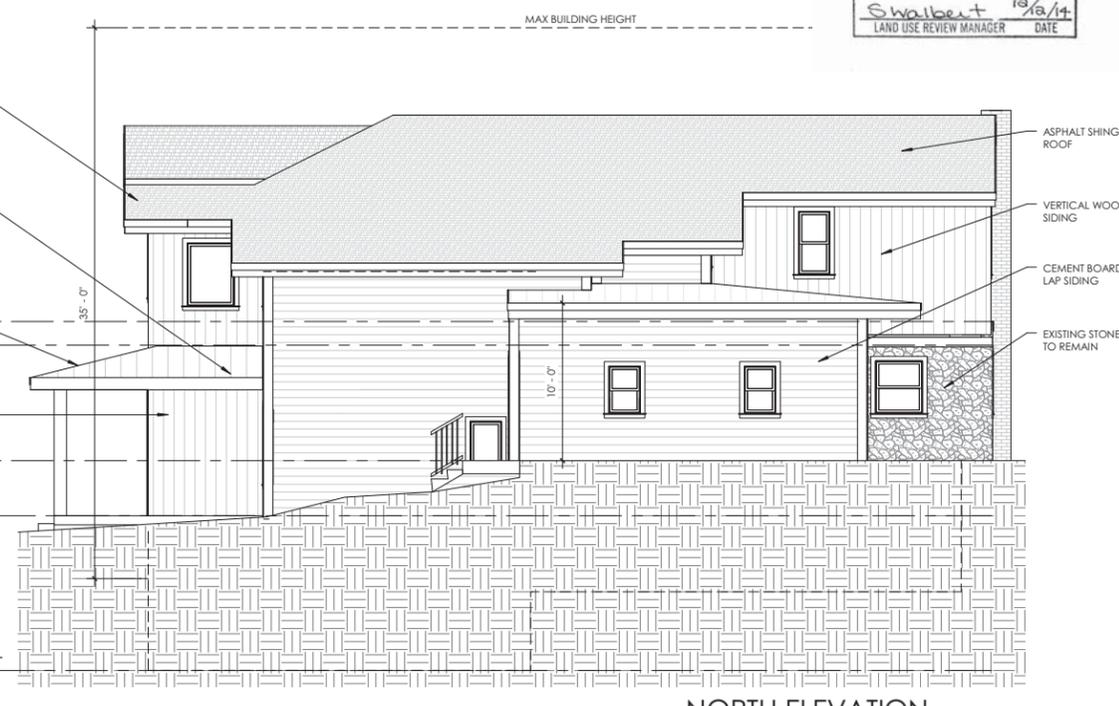
3059 6TH ST.
 BOULDER, CO 80302

DISCLAIMER:
 THESE DOCUMENTS ARE PROVIDED BY COBURN DEVELOPMENT INC., FOR THE DESIGN INTENT OF THIS SPECIFIC PROJECT AND ONLY THIS PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CONSTRUCTION COORDINATION, METHODS AND MATERIALS REQUIRED FOR THE SUCCESSFUL COMPLETION OF THE PROJECT. THIS INCLUDES BUT IS NOT LIMITED TO THE QUALITY OF WORKMANSHIP AND MATERIALS REQUIRED FOR EXECUTION OF THESE DOCUMENTS AND WORK OR MATERIALS SUPPLIED BY ANY SUBCONTRACTORS. ALL WORK SHALL COMPLY WITH GOVERNING CODES AND ORDINANCES. THE CONTRACTOR SHALL REVIEW AND UNDERSTAND ALL DOCUMENTS AND SHALL NOTIFY THE ARCHITECT IMMEDIATELY OF ANY DISCREPANCIES IN THE DRAWINGS, FIELD CONDITIONS OR DIMENSIONS.

ISSUED/REVISION SCHEDULE			
DESCRIPTION	AUTHOR	CHECKED	DATE
LUR	LJ	PW	10.17.14
LUR REVISED	LJ	PW	10.31.14
LUR REVISED 2	LJ	PW	11.24.14



4 WEST ELEVATION
 3/16" = 1'-0"



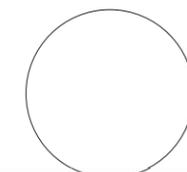
2 NORTH ELEVATION
 3/16" = 1'-0"



3 SOUTH ELEVATION
 3/16" = 1'-0"



1 EAST ELEVATION
 3/16" = 1'-0"



LUR REVISED
 12.12.2014

SHEET No.

A2.2

**CITY OF BOULDER
PLANNING BOARD AGENDA ITEM**

MEETING DATE: January 22, 2015

AGENDA TITLE:

Envision East Arapahoe Project Update and Scenarios Analysis

REQUESTING STAFF:

David Driskell, Executive Director, Community Planning & Sustainability (CP&S)

Susan Richstone, Deputy Director, CP&S

Lesli Ellis, Comprehensive Planning Manager, CP&S

Kathleen Bracke, GO Boulder Manager, Public Works Transportation

Sam Assefa, Senior Urban Designer, CP&S

Jeff Hirt, Planner II, CP&S

OBJECTIVE:

Provide an update on the Envision East Arapahoe scenarios and analysis and receive feedback from Planning Board on community input, analysis, and next steps for the project.

PURPOSE

The purpose of this packet and agenda item is to provide a briefing to the Planning Board on the status of the Envision East Arapahoe project, present refined scenarios and analysis, provide a summary of community feedback from the past several months since the last Planning Board discussion (Oct. 16, 2014), and obtain the board's feedback. Next steps in the project include addressing medical office zoning changes, continuing transportation and access planning, assessing usefulness of the planning tools piloted during this project (e.g., 3d model, photo visualizations, and Urbemis and CommunityVIZ models), determining when and how to address broader land use changes north of Arapahoe Avenue, and refining project materials in preparation for a public meeting on Feb. 4 and a City Council Study Session on Feb. 24. On January 13 City Council discussed the citywide 2015 work plan and indicated that, in light of the number of high priority planning projects proposed for 2015, it may make sense to pause the vision and land use planning aspects of the Envision East Arapahoe project.

Questions for Planning Board

Questions for Planning Board include the following:

1. Does Planning Board have any feedback regarding the revised project schedule and next steps? (See [Attachment A](#)).
2. Does the board have questions, feedback, or preferences regarding the refined scenarios or initial analysis? (See [Attachment B](#)).
3. Does the board have feedback or questions about the planning tools and their effectiveness (i.e., CommunityVIZ model, 3d plan views, and future photomorphs visualizations). (See page 8 and [Attachment C](#))
4. Does the board have feedback regarding medical office zoning or next steps? (See page 9 and [Attachment D](#)).

BACKGROUND

Study Area and Previous Discussions

In 2014, the City of Boulder launched the Envision East Arapahoe project with the community to develop an integrated land use and transportation plan for the corridor. The project was intended to respond to anticipated changes on the corridor, including the relocation of Boulder Community Health and associated medical uses and the proposal for Bus Rapid Transit (BRT) on the corridor proposed by the RTD Northwest Area Mobility Study (NAMS).

Planning Board has discussed the project several times, including project goals, a draft vision, and scenario concepts on Oct. 16, 2014. The project has been intended to result in a community-driven vision plan informed by data and to address land use, connections, and urban design. It is being coordinated with and testing ideas from other initiatives and community conversations, such as the Transportation Master Plan and action plan and Economic Sustainability Strategy.

A study area map and detailed set of base resource maps and inventory information can be found on the project webpage: www.EnvisionEastArapahoe.com under the maps and reports tabs. Memos from previous Planning Board agenda items and the last City Council Study Session packet provide additional background:

- [Oct. 28, 2014 City Council Study Session packet](#)
- [Oct. 16, 2014](#) Planning Board packet
- [July 31, 2014](#) Planning Board packet
- [Mar. 30, 2014](#) Planning Board packet

Project Progress and Timeline

Progress on the project since fall 2014 includes hosting several community events, refining and conducting analysis of scenarios with consideration of technical inputs, board feedback, community ideas, and City Council feedback during the Oct. 28 Study Session, and adding graphic representation of scenarios (both photomorphs and 3D plans).

A revised current project timeline is provided for consideration. (See **Attachment A**.) Direction for the project may be shifting, given the wide range of feedback provided over the past several months, City Council's discussion about 2015 citywide planning priorities, and related projects such as the Housing Boulder strategy development and the pending Boulder Valley Comprehensive Plan which will address citywide jobs and housing.

Even if the vision and land use planning is paused for the project, the city will move forward on near-term medical office zoning amendments and continue to assess opportunities to enhance local and regional multimodal transportation connections along and across the corridor, and continue to coordinate with the community and local and regional agency partners such as Colorado Department of Transportation (CDOT), Regional Transportation District (RTD), and Boulder County to address short term and long range transportation needs, including operations and safety issues as well as planning for future regional arterial bus rapid transit (BRT).

Staff seeks feedback from the Planning Board on how best to progress with the project given the different needs.

COMMUNITY ENGAGEMENT PROCESS AND FEEDBACK

The next few pages provide a summary of input. Attachment B provides additional detailed comments.

Recent BDAB Feedback – Jan. 14, 2015

Boulder Design Advisory Board (BDAB) discussed the urban design aspects of the project and the 3d model at their meeting on Jan. 14, 2015 and provided the following feedback.

Scenarios and 3D model:

- The land use programs for all the scenarios are timid (not visionary enough) and would not change the existing suburban character. Suggest further testing development potentials at extreme ends (i.e., very low to very high densities).
- Show the 3d SketchUp model and/or a plan view with softer edges and within the larger city context to convey how the area is connected to other parts of the city such as CU East and Boulder Junction. Show more street level views from the model and sidewalk view photo visualizations.
- Recycle Row and 63rd and Naropa should receive more attention.
- Existing conditions photos probably show too much street activity.
- Show more mixed uses.
- Slow traffic, including BRT speed. Does BRT have to have its own lane or can the lanes be multi-purpose (maybe during off hours)?

Process:

- All comments about urban design should not receive equal weight (e.g., value comments/suggestions by design/planning experts).
- Public comments are evenly divided, but it seems that the planning approach is responding more to those who do not want to see any change. Most comments seem to be from residents and not the broader community.
- Ask different questions in the public engagement process (some good examples include Lyons recovery project, successful transformations from other places with similar conditions, such as Colfax in Denver).
- Positive outcomes of the project could be a focus around hospital area and connections planning to improve the street grid (form based).

Recent TAB Feedback – Jan. 12, 2015

The Transportation Advisory Board (TAB) discussed the transportation analysis on Jan. 12, 2015 and provided the following general feedback:

1. Land use mix and projections as part of the scenarios are too timid; therefore the scenarios do not go far enough toward achieving TMP goals;
2. Importance of creating more walkable areas with any/all of the future scenarios and to help existing residents and employees, particularly in targeted areas. Be careful that these details don't get "washed out" in the corridor level analysis and helps set the stage for the future more detailed BRT station area planning.
3. Scenario C (ver. 2) projection would be most beneficial in creating a more walkable compact development pattern;
4. Improve the transportation analysis graphics, charts, and numbers, including how Walnut Street is conveyed;
5. Clearly describe and/or articulate the positive and negative impacts of current trend versus Scenario C in terms of transportation and other quality of place related issues;

6. Further analyze future land use for the west end – the area between CU East Campus and Boulder Junction. It is an omission from current scenarios.
7. Need to be concerned with creating walkable areas for both residents and employees regardless of the land use scenario as current trends will bring development to the corridor.
8. Reporting results at the corridor level tends to “wash out” more local effects that can be very beneficial, such as creating walkable areas around transit stops.

Planning Board – Oct. 16, 2014

At the Planning Board meeting on Oct. 16, the board provided the following feedback along with more detailed comments provided in [Attachment B](#):

1. Recognition of need near Boulder Community Health for medical offices and to consider options for accommodating medical office zoning changes as a near term solution.
2. Assess business needs and uses — determine why buildings in the study area are vacant.
3. Consider allowing a different mix of land use north of the corridor that is less suburban, with organic infill of the area without tall buildings. The current FAR of 0.5 may be limiting.
4. Consider more active park uses in the golf course area – more of a neighborhood amenity.
5. Measure business retention – take care not to push out local business (service commercial).
6. Residents concerns include: traffic in neighborhoods, lack of grocery stores and other amenities, and lack of comfortable walkable/bikeable options in the area.
7. Determine what the “critical mass,” or mix of uses is to foster 15-minute neighborhoods (i.e., infrastructure and services). Some questions that 55th north of Arapahoe may not support walkable housing, nor the location at South Boulder Creek. Shift to closer to 63rd Street. Avoid new nodes that would be auto-centric.
8. Preserve mature residential neighborhoods and rural character. New people or residents should be put in areas with services.
9. Tools: use visualizations and improve the graphics and legends for the scenario maps.
10. Walnut Street extension could be contentious, but if it is planned keep it close to the railroad tracks – an area already ecologically degraded.
11. Consider needs of employees and what options might entice them to live in Boulder.

Community Meeting – Oct. 27, 2014

Almost 90 people attended a community meeting at which the city presented background corridor conditions and findings, presented draft scenarios, provided information about possible transportation improvements, and sought feedback and ideas on all of the above. Community comments varied considerably – ranging from interest in seeking greater land use mix north of Arapahoe Avenue (with locally serving retail and services, medical office, and some housing) – to maintaining existing character and concern about urbanization of the corridor. A full summary of the feedback is provided in [Attachment B](#).

General Overview of Input

Meeting participants were given the opportunity to provide comments at each open house station, on written comments forms, and an online survey version of the comment form, also provided in [Attachment B](#). Several themes emerged from the comments, as participants provided a wide array of input and ideas and noted the following:

Scenarios/Land Use

- Some would like to foster additional housing and retail along the corridor, whereas other people do not want new development.
- Affordable light-industrial space for startups is important, as is some amount of service retail.
- Concern about potential scale and massing of new development.
- Pride in “Recycle Row” and its function.
- Support for promoting the arts.
- Live/work is desirable.
- Concerns and questions about the floodplain and relationship to development.

Transportation

- Arapahoe is too wide and speeds are too fast, inhibiting feelings of safety and comfort.
- Traffic congestion is a concern, and intersections don't function as well as people would like. Concerns about increasing traffic on Arapahoe as well as side streets such as Cherryvale.
- Support for enhanced and more frequent bus service.
- Make bike infrastructure on Arapahoe Avenue safer, more connected, and continuous.
- Expand the bike network and B-cycle system.
- General support for the concept of Bus Rapid Transit (BRT), but need to see more details about how it will function, look and feel, and affect traffic patterns.
- Recently added bus lanes east of 63rd are confusing – don't repeat that approach.
- Interest in expanding Ecopasses for homes and businesses in the area.

City Council Study Session – Oct. 28, 2014

Staff presented the project and heard feedback from City Council on Oct. 28, 2014. City Council commented generally and provided input on the scenarios, community engagement, housing, land use design and amenities, and transportation, saying that overall the project is heading in the right direction and that further analysis would help. Council also stated the need to address timely topics such as medical office uses near Boulder Community Health (BCH) as well as transportation safety issues. The detailed summary is provided in [Attachment B](#).

Listening Sessions – Nov. and Dec., 2014

Following the Community Meeting and Study Session in Oct. 2014 and letters and concerns about the project, the city held two neighborhood listening sessions to answer questions and primarily to record ideas, input, and concerns from neighbors and other interested parties. As with the public event on Oct. 27, staff heard a variety of input. Summaries from the two listening sessions are provided in [Attachment B](#).

SCENARIOS AND ANALYSIS

Scenario Description

Scenario planning allows the community to envision and evaluate different mixed of land use, civic features, open space, and transportation options for 20 to 30 years into future. The scenarios are intended to create hypothetical futures to support decision making around community goals such as improved connections, aesthetics, and infrastructure; reducing emissions and addressing energy goals; and adding neighborhood and civic amenities. The scenarios are designed to be dis- and re-assembled into a preferred plan.

The current East Arapahoe scenarios include:

- **Scenario A: Current Trends**
- **Scenario B: Districts**
- **Scenario C: Housing Choices**
- **Transportation Options and Bus Rapid Transit (BRT)**

Attachment C includes the overview of the updated scenarios and new diagrams and graphics as requested by the boards and community to assist with discussing choices.

Initial Analysis

Staff and consultants have updated the scenarios, graphics and description, and prepared initial analysis intended to reflect community goals and qualitatively and quantitatively measure how well scenarios perform relative to goals.

Scenario A represents existing zoning and a future based on current trends, and B and C entail modest rather than bold changes to land uses in specific locations, intensity, and overall design. Therefore, initial analysis suggests that none of the scenarios create significant benefits or impacts, and each gives a reasonable range within which to further discuss and refine choices, consider whether to test additional land use mixes (either within the parameters of current scenarios or less/more). The 3d models provide a more realistic view of what is possible within different focused areas, showing potential intensity, pattern, and mix and should aid in community conversation about the future character of the area.

Note: Additional detailed analysis will be provided at the Planning Board meeting on Jan. 22.

Transportation Analysis

Consultants used the URBIMIS transportation model and a vehicle trip distribution model to analyze transportation results. These models produce results for each of the scenarios within the Envision East Arapahoe study area and are valuable for comparing the scenarios.

From a transportation standpoint, all the scenarios (including the more intense version of Scenario C) can work with a variety of multi-modal options such as protected bike lanes, transit enhancements for bus rapid transit and local transit, and potential future street and multi-use path connections. Each could work with a potential repurposing of lanes and roadway width to accommodate arterial BRT (side or median running). See memo regarding transportation analysis sent to TAB for Jan. 12 meeting: [here](#).

Results are limited to the study area and do not yet represent the broader implications of different development patterns in the corridor. As an example, the greenhouse gas analysis performed as part of the 2014 TMP process showed that the average Boulder resident produces about 11 daily vehicle miles of travel (VMT) while the average non-resident employee produces over 28 daily VMT for just the commute trip. That same employee generally continues to use their vehicle for other trips during the day, particularly if they work in a large area of single type land use, adding to the Boulder Valley VMT. It is then likely that non-resident employees live closer to where they work in Boulder will reduce their daily VMT. Additional analysis would be necessary to gauge TMP and climate related goals.

Initial Additional Sustainability Analysis for the Three Scenarios

Scenario A – Current Trends

Overall, Scenario A achieves some but not all community goals. It marginally improves walkable, connected places as compared to current conditions. It does not achieve climate goals; however it generally supports economic vitality and open space biodiversity goals.

Additionally:

- Based on current zoning, would maintain the low intensity, light industrial focus of the area, potentially adding 4,300 new jobs in focused areas and in a suburban pattern with large setbacks and parking.
- Existing mature stable neighborhoods would not see much change.
- The scenario could support a small amount of medical office space and local retail services to support local business and residents.
- It would not allow for additional housing units or permanently affordable units in focused areas.
- Current open space and biodiversity would be protected.

Considerations for Scenario A

1. What, if any aspects of the current trends scenario should carry forward?

Scenario B – Districts

Overall Scenario B could slightly improve walkability and connected place goals. With some minor land use changes or modifications to rules to support food trucks and other employer needs it could better achieve economic vitality goals. Additionally:

- It projects 3,900 new jobs (400 fewer than Scenario A) with an additional mix of retail and medical office space, and 460 new housing units.
- Existing mature stable neighborhoods south of Arapahoe would not see much if any change.
- The scenario could provide some additional medical office space and some locally serving retail to support businesses.
- The possible Walnut Street connection could better connect east/west and increase safety response times but would require open space disposal and could have negative ecological impacts.

Considerations for Scenario B

1. What, if any aspects of Scenario B should carry forward?
2. Does B achieve an appropriate mix of uses (e.g., office, retail, and light industrial, and housing) to help implement the Economic Sustainability Strategy and support the needs of the businesses and 35,000+ employees in the area?

Scenario C – Housing Choices

Overall Scenario C could slightly improve walkability and connected places and add housing to support community workforce goals, but it could create more demand for neighborhood supportive services and infrastructure and could have create additional impacts on open space parcels. These possible impacts could be offset by additional buffering adjacent to open spaces or ecological restoration along ditches. Additionally:

- It projects 2,500 new jobs (1,800 fewer than current trend) and adds 1,300 new housing units. A version that projects 4,100 jobs and 2,360 new housing units was used for transportation analysis to better understand demands and impacts on infrastructure.
- Existing mature stable neighborhoods south of Arapahoe would not see much if any

change.

- The scenario could include approximately 260 permanently affordable units (assuming 20% of new units).
- 3 to 5 acres of new developed park land and other necessary infrastructure and services would need to be added along with housing to serve new residential.
- The scenario improves the overall jobs to housing ratio and potential walkability of new neighborhoods, however, adding more housing in parts of the area (near open space, or away from major roads) could create conflicts with existing industrial uses and may have higher impacts on biodiversity than light industrial uses.

Considerations for Scenario C

1. What, if any aspects of Scenario C should carry forward?
2. Does C achieve an appropriate mix of uses (e.g., locally serving retail light industrial, and housing), protect and support existing neighborhoods, and provide for needs of workforce housing while maintaining economic vitality?

Technical analysis remaining to be completed for East Arapahoe scenarios.

As of January 2015, the following analysis would still need to be completed as part of a preferred scenario or plan:

1. Analysis of total land use GHG output of parcels with different land use types and consideration of regional implications of land use choices (i.e., combined from buildings/land use and transportation related GHG emissions);
2. Safety response times relative to BVCP goals (to be calculated using the transportation model);
3. Determining land use mix “thresholds” to support the 15-minute walk concept (i.e., may require more research and qualitative assessment regarding great neighborhoods and critical mass; and
4. Fiscal impacts and strengths of different land use mixes and ability of development to pay its way. (Note: city may be conducting a citywide study that could be applied).
5. More detailed transportation analysis to determine details for future multimodal improvements, including coordination with local and regional agency partners.

Consideration of Planning Tools

The city hired a consultant to support scenario development and analysis using GIS-based CommunityVIZ software. Using a GIS-based tool to create scenarios by the numbers has benefits but also limitations in conveying different futures, illustrating character, and in locating uses in a sensitive infill manner. However, the model does enable more rigorous quantitative analysis and ability to adjust scenarios. The model will allow for disassembling and reassembling the scenarios and should be useful as the city embarks on citywide analysis of projected population and jobs growth for the Boulder Valley Comprehensive Plan. Currently, some but not all technical analysis is complete and some assumptions still need verification.

Additionally, the community has expressed interest in using 3d modeling tools that represent potential futures to assist with decisions about urban form. Staff worked with a consultant to take land use outputs from the GIS model and transfer them into a SketchUp model. The 3d illustrations represent sensitive infill and redevelopment. The community feedback about the 3d images can then be iterative and applied back to the GIS model to adjust scenarios and assumptions. Staff is seeking input from Planning Board about how to make the 3d model most effective for this project and potentially for use in future planning work.

MEDICAL OFFICE ZONING

One of the ongoing and immediate next steps will be to determine how to allow necessary medical office needs near Boulder Community Health.

In 2014, BCH transitioned facilities included the emergency room, new services such as surgery, imaging, laboratory services, and other inpatient services from the Broadway campus to the Foothills campus on East Arapahoe. The transition has resulted in increasing demand for medical offices in close proximity to BCH. However, only a small area near the hospital is currently zoned to allow medical office. (see [Attachment D](#)).

According to multiple health care providers who have contacted the city, there is some urgency to finding available office space closer to the Foothills location to avoid multiple daily trips across the city. The Primary Employer Study¹ noted this need, and both Planning Board and City council have provided feedback on this topic in October 2014.

Approach and Analysis

Staff is proposing to move forward with targeted Title 9 (Land Use Regulation) changes to better accommodate short term medical office uses. Staff will conduct a more in depth analysis of this issue to assess current demand for medical office near BCH, building from analysis conducted in 2013 by city staff (see link [here](#)).

The analysis will be followed by short term Title 9 changes. Any Title 9 changes will also consider and factor in the importance of existing non-medical businesses near BCH, particularly service industrial.

Staff is currently doing analysis and will be prepared to discuss pros and cons of options that would apply either in targeted locations or citywide. Targeted Title 9 options to address medical office uses may include:

- Medical office uses as conditional use based on geographic proximity to BCH (to be defined based on analysis)
- Zoning changes for areas in close proximity to BCH to a zone district more accommodating to medical office and related uses; and/or
- Adjustments to the medical office use and related use definition(s) such as “Personal Services” to ensure that all desirable types of medical uses are addressed. (e.g., Title 9 currently defines “Personal Service” uses as separate from medical office and includes treatment and therapy not typically performed by a medical doctor (e.g., physical therapy)). This approach would apply citywide.



Boulder Community Health Foothills Campus



Riverbend Office Park



Viewpoint Office Park

The Riverbend and Viewpoint Office Parks are among the only properties that are zoned to allow medical office “by right” within approximately ½ mile of BCH

¹ p. 2, 37, and 110, Study of Primary Employers in the City of Boulder (2012).

NEXT STEPS

- February 4:** Community workshop at Naropa to report on and get feedback on scenario analysis, medical office options, and transportation planning
- February 24:** City Council Study Session, including a briefing on the medical office topic (note: may be repurposed to include other planning topics)
- Ongoing** Analysis and outreach related to medical offices
- Mar (tbd)** Planning Board review of analysis and draft Title 9 changes for medical offices
- Mar/Apr (tbd)** City Council adoption of Title 9 changes related to medical offices

ATTACHMENTS

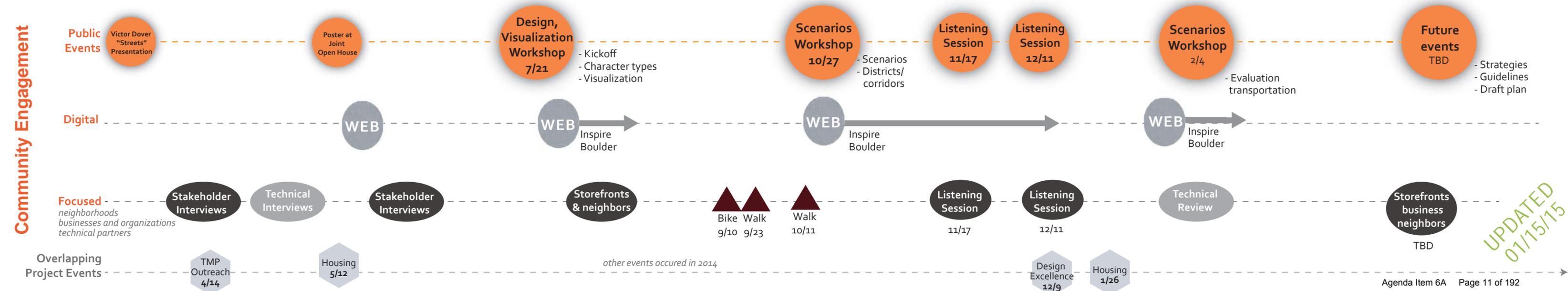
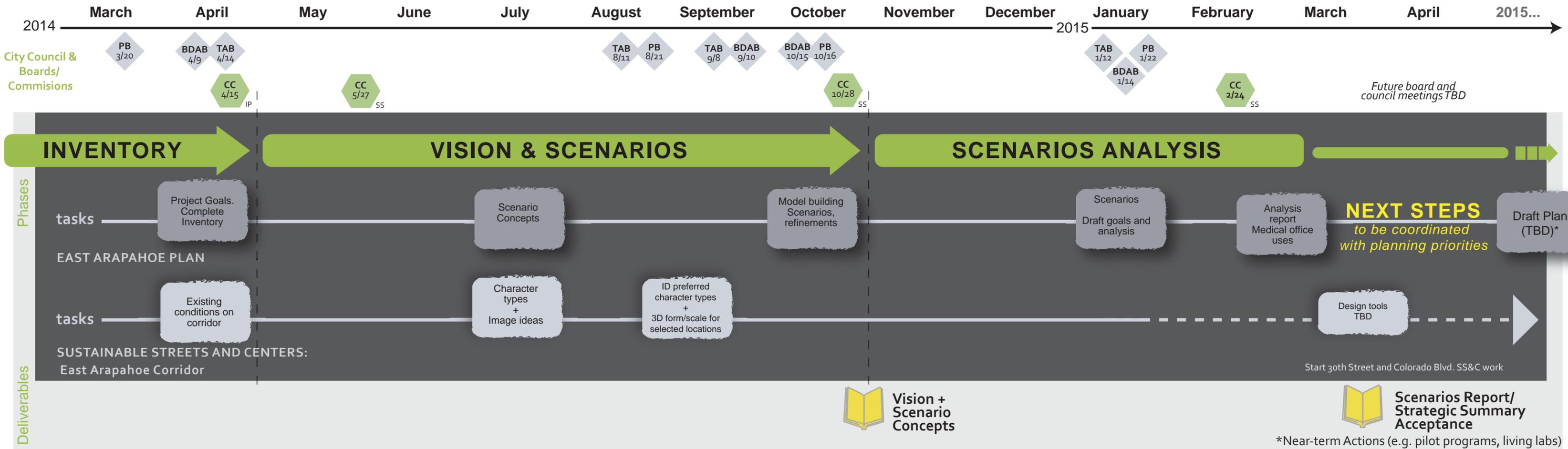
- A. Updated 2015 Timeline**
- B. Summary of Current Community Engagement – Oct. 2014 through Dec. 2014**
 - a. Oct. 16, 2014 Planning Board Summary
 - b. Oct. 27, 2014 Public Meeting Summary
 - c. Oct. 28, 2014 City Council Study Session Summary (to be approved on Jan. 20)
 - d. Online survey response (Sept. through Dec. 2014)
 - e. Nov. 17 and Dec. 11 Listening session summaries
 - f. Link to all other online summaries prior to October 2014
- C. Scenarios and Analysis:** Scenarios overview, including 3d graphics and future “photomorph” visualizations (Note: Additional analysis will follow)
- D. Medical Office Zoning Map**



Timeline

East Arapahoe Plan and Sustainable Streets and Centers

Envision East Arapahoe



UPDATED 01/15/15

**CITY OF BOULDER
PLANNING BOARD ACTION MINUTES
October 16, 2014
1777 Broadway, Council Chambers**

A permanent set of these minutes and a tape recording (maintained for a period of seven years) are retained in Central Records (telephone: 303-441-3043). Minutes and streaming audio are also available on the web at: <http://www.bouldercolorado.gov/>

PLANNING BOARD MEMBERS PRESENT:

Aaron Brockett, Chair
Bryan Bowen
Crystal Gray
John Gerstle
Leonard May
Liz Payton

PLANNING BOARD MEMBERS ABSENT:

John Putnam

STAFF PRESENT:

David Driskell, Director of Planning and Development Services
Susan Richstone, Deputy Director of Planning and Development Services
Susan Meissner, Administrative Assistant III
Lesli Ellis, Comprehensive Planning Manager
Sam Assefa, Senior Urban Designer
Charles Ferro, Development Review Manager for CP&S
Sloane Walbert, Planner I
Jonathan Woodward, Associate Planner
Jay Sugnet, Project Manager
Kathleen Bracke, GO Boulder Manager
Chris Hagelin, Senior Transportation Planner
Molly Winter, DUHMD Executive Director

1. CALL TO ORDER

Chair, **A. Brockett**, declared a quorum at 6:04 p.m. and the following business was conducted.

2. APPROVAL OF MINUTES

3. PUBLIC PARTICIPATION

Jan Morzel, 2075 Upland Avenue, spoke in support of increasing occupancy limits in senior housing, allowing for more cottage housing and ADU/OAUs and additional flexibility in rentals in general. He noted that many people support the changes, but have not come forward publically.

4. DISCUSSION OF DISPOSITIONS, PLANNING BOARD CALL-UPS/CONTINUATIONS

- a. Informational Item: Public Access easement vacations for four sidewalk easements and one road construction easement. These easements were previously dedicated to

the City of Boulder to serve the former Golden Buff Motel and EADS Newsstand site at 1725 28th Street. The property is zoned Business Regional 1 (BR-1). Case number is LUR2014-00075.

- b. Call-Up Item: Minor subdivision review, case no. LUR2014-00058, for the creation of a second residential lot at 3242 5th Street. Expires: October 16, 2014.
- c. Call-Up Item: USE REVIEW (LUR2014-00079): Request for a 2,200 square foot retail store with accessory office uses (Smart Beds Boulder). Expires on October 23, 2014.

C. Ferro, J. Woodward and S. Walbert answered questions from the board. None of the items were called up.

5. PUBLIC HEARING ITEMS

There were no public hearing items.

6. MATTERS FROM THE PLANNING BOARD, PLANNING DIRECTOR, AND CITY ATTORNEY

A. Brief Update on the Comprehensive Housing Strategy

Staff Presentation:

J. Sugnet presented the item to the board.

Board Questions:

J. Sugnet and **S. Richstone** answered questions from the board.

Board Comments:

- Explore more flexibility for building OAU's and subdivision of large lots in exchange for historical landmarking (staff is meeting with Landmarks board on 12/3 to discuss other ideas to help achieve historic preservation goals).
- Encourage the creation of smaller homes and the preservation of smaller homes. The preservation of small units may be addressed separately from the creation of small units in the toolkit.
- Cohousing normally increases affordability. Assure that this is reflected in the document.
- Improve the Comprehensive Strategy document's formatting for greater clarity.
- More clearly define the goals; this will make it easier to determine whether they have been achieved.
- Consider looking at revisions to the use tables regarding housing in commercial zones.
- Do more outreach in response to neighbor concerns that higher densities will be allowed in all residential neighborhoods. Let them know that there will be an appropriate process and that not all neighborhoods will be impacted. Note that

changes at the perimeter of neighborhoods are also impactful.

- **C. Gray** noted that there is a mixed message about where higher densities are appropriate. Involve the neighborhoods in the discussion early so everyone understands the dialogue.
- **C. Gray** requested that the CHS process address ADUs/OAUs and tiny homes. The city performed a neighbor survey on ADUs and OAUs in 2012.
- Some neighborhoods are already very organized while others are not. Staff will solicit input from neighborhood working groups and will also reach out to other, less organized neighborhoods.
- **L. May** suggested that the community engagement staff member use conventional and nonconventional means for reaching out to the group.
- Neighborhood issues must be vetted to determine which are the most important. Staff will clarify that only the top priorities can be added to the work plan; this will ensure the most robust planning effort possible.
- The City and Boulder Housing Partners (BHP) are working together to create a permanently affordable and sustainable community at Palo Parkway. It is designated medium density in the BVCP. That zoning will allow for a good range of housing types.
- Assure that BHP has an authentic discussion with the neighborhood and solicits their input.
- Assure that the city is involved in the outreach process before the ownership of the property is transferred.
- Consider the impacts of additional housing on local schools. It is important and good to have economic diversity in each school.
- Board members may send revisions to the CHS to **J. Sugnet** in the next two weeks before the next draft is released to the public on November 20.

B. Envision East Arapahoe- Scenarios

Staff Presentation:

L. Ellis presented the item

Board Questions:

L. Ellis, S. Assefa and S. Richstone answered questions from the board.

Board Comments:

- The board acknowledged that medical practices currently located near the old hospital cannot find space to relocate near the new hospital due to zoning restrictions
- What are options for accommodating medical office zoning changes in the area? The near term solution is finding spaces to lease but it seems that few longer term solutions are under consideration.
- There are many spaces for rent along 55th Street but that area is not currently zoned to allow medical offices. Consider allowing the medical uses through changes to the use table, use review, or another mechanism such as a zoning overlay.
- The exclusion of office uses from industrial zones was a conscious decision based on concerns of losing true industrial uses in those zones. This will be a topic of conversation during the Envision East Arapahoe process.
- The density along the north side of Arapahoe is very suburban. Consider allowing a different mix of land uses to encourage a more urban style.
- There is room for organic infill in this area without having a significant impact on the city. This area could accommodate a lot of uses and space without tall buildings, and take the burden off of downtown.
- Consider more active park uses in the golf course area. It currently serves a flood mitigation use but could also become more of a neighborhood amenity.
- Devise a way to measure business retention; take care to not push out local business (service commercial).
- Look for solutions for light industrial sites from other places that could be implemented or emulated here.
- Assess if there is a mismatch of business needs and uses. Determine why some buildings are not utilized or what types of spaces are needed that are not provided.
- The current 0.5 FAR limitations preclude redevelopment on some sites. They make redevelopment too expensive, promote a building typology surrounded by parking lots, and deter forward design.
- Up to 80% of employees working along the East Arapahoe corridor live outside of Boulder. As part of the housing study, the city surveyed employees to see how many would consider living in Boulder if there were appropriate workforce housing.
- Residents south of Arapahoe say the largest concerns included traffic in the neighborhood, lack of grocery stores and other amenities, and a lack of comfortable walkable/bikeable options in the area.

- Determine what must be done to foster fifteen minute neighborhoods (e.g., Infrastructure, pocket parks, libraries, and other neighborhood components require critical mass).
- 55th and Arapahoe already has the beginnings of a 15 minute neighborhood node. Add housing and retail to support that. Consider the commercial strip to the south of Arapahoe that is zoned BC as well.
- There was some skepticism that the area along 55th north of Arapahoe doesn't currently have what it takes to be a 15 minute neighborhood. The linear nature would create more auto-centric developments.
- The location at South Boulder Creek might also not be a great idea unless services are available. New housing should be part of a "critical mass" of housing with services. Shift the area of focus away from Old Tale Road and closer to 63rd Street.
- Consider how the two outlying nodes can be planned to avoid creating more auto-centric developments.
- Not everyone wants a higher density neighborhood. The more rural and mature residential neighborhoods should be preserved. New people or residents should be put in areas with services. Artists studios are a good idea.
- BDAB discussed and discouraged the idea of development of contiguous residential developments along a major arterial or BRT corridor; however, the street improvements such as landscaping could mitigate some of the detrimental factors such as noise.
- Consider allowing grocery and other uses by right in those areas.
- The train is an obvious linkage that would pull things together from a transportation standpoint.
- Consider what may happen to the Xcel power plant in the future.
- Use visualizations in key locations. Show views of continuous roadways; the stretch at 63rd is another interesting area with Naropa and the Avalon Ballroom. Street level visualizations are preferable to an aerial for giving an idea for the look and feel.
- Also a wider focus around Boulder Community Health and Ball. Consider making the medical district larger to accommodate more medical office uses around the hospital.
- Survey the medical offices at the Broadway campuses to find out how many are thinking of moving and how much space they would need.
- Improve the graphics and legends for the scenario maps.
- The proposed Walnut East street connection could be contentious. The hospital was sensitive to the open space corridor per the public's request.

- If it is to be done, consider a corridor next to the rail tracks; the area is already ecologically degraded.
- Get a more granular understanding of the employees and what options might entice them to live in Boulder. Continue existing survey but focus on employers in that area and see what the needs of the workers are.
- Consider having different height visualizations along Arapahoe per the recent conversations in the community.
- Look at opportunities for small scale connections.
- The board requested to see this again before the end of the year.

A. Update on the Access Management and Parking Strategy (AMPS)

Staff Presentation:

K. Bracke and **C. Hagelin** presented the item.

Board Comments:

- The board thanked staff for addressing the TDM issues.
- **L. May** did not think that Boulder's road system had much more capacity. He recommended that the city accommodate day trips but not commuters.
- **J. Gerstle** and **L. May** thought the city should strive for a net zero increase in automobile trips and emissions.
- The remainder of the board members agreed that an aggressive goal was preferable, but felt that net zero was too stringent. Don't make it too difficult for people to get to work; some need to drive.
- Acknowledge that some car trips are necessary for errands, getting to multiple meetings, etc.
- Assure that people feel safe walking and biking.
- The board liked the idea of a community-wide Eco Pass.
- **J. Gerstle** liked the Cambridge enforcement model and thought the escrow account funding for penalties was appealing.
- In exchange for a quantified parking reduction, consider requiring developers to put money in an escrow account for increased enforcement to quell neighborhood fears.
- Simplify neighborhood parking program renewals and provide resources for applicants to help with neighborhood parking plans.

- Consider approaches to TDM Plan enforcement with more teeth to achieve the desired mode share goals. Enforcement monitoring would be beneficial if staff has enough resources; it can be reduced over time if necessary.
- Take care to link incentives with desired outcomes. Do not incentivize the provision of more parking.
- The city needs less parking; assure that the code does not incentivize more.
- Consider basing parking requirements off of a building's square footage as opposed to occupancy.
- Parking requirements should be different for residential and commercial developments.
- Look at finer grained trip generation measurements.
- Board members generally liked the customizable approach to TDM plans for each project.
- Consider utilizing Use Reviews to trigger new TDM plan requirements.
- Attach TDM plans to properties, not to tenants.
- Consider that some businesses can increase their employee count without physically expanding their spaces. This will change the number of trips and parking.
- The board thanked staff for the focused report and clarity.

C. DEBRIEF MEETING/CALENDAR CHECK

D. ADJOURNMENT

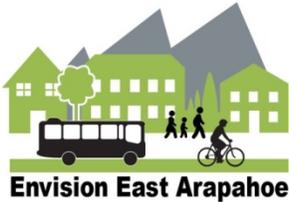
The Planning Board adjourned the meeting at 9:48 p.m.

APPROVED BY



Board Chair

12/9/19
DATE



October 27th, 2014 Workshop Summary

Date: Monday, October 27th at Naropa's Nalanda Campus, 5 -7 p.m.

Attendance: 87

Meeting Objectives:

1. Share/learn new information about the project
2. Review ideas previously generated by the community for choices for the future (e.g., future activities, character, transportation, and other infrastructure)
3. Give community opportunity for feedback regarding future choices

Format/Agenda

The meeting was designed to be collaborative with response and feedback from the community during at open house stations and on a comment form. A 25 minute informational presentation began at 5:30 p.m.

Open House Stations:

1. **Welcome and What to Expect**
Information about the project, existing conditions maps, project goals, timeline, ways to get and stay involved, and the draft vision
2. **Scenarios for the Future**
Scenarios, menu of choices, future character, and indicators
3. **Future Transportation Improvements**
Information about walk, bike, transit, street connections and possible improvements

Comment Response Summary

Meeting participants were given the opportunity to provide comments at each Open House Station as well as via a comment form drop box. Several themes emerged from the comments.

Scenarios/Land Use

- Meeting participants generally seemed to understand the scenarios and provided a wide array of input and ideas.
- Some people believe strongly in fostering additional housing and retail along the corridor, whereas other people do not want new development.
- Many participants noted that affordable light-industrial space for startups is important, as is some amount of service retail.
- People are concerned with the scale and massing of potential new development, including height, building footprints, and bulk.
- People noted pride in "Recycle Row" and it's function.
- Several noted support for promoting the arts in the area.
- Live/work was noted as desirable.

- People also noted ongoing concerns and questions about the floodplain and relationship to development.

Transportation

- Many people think that Arapahoe is too wide and that speeds are too fast. This inhibits safety and the ability of the corridor to feel safe and pleasant.
- At the same time, many people have expressed concern about traffic congestion. They think that either reducing capacity (removing lanes) or adding additional development will make traffic worse.
- Intersections don't function as well as people would like. They are either unsafe (safety issues pointed out involving all modes), in poor locations, or overly congested.
- Participants expressed support for enhanced and more frequent bus service.
- People would like to see bike infrastructure on Arapahoe made safer, more connected and continuous. The bike network and B-cycle system should be expanded.
- Generally people support the concept of Bus Rapid Transit (BRT), but many would like to see more details about how it will function or affect traffic patterns and get a clearer sense of how it would look or feel. Area residents and employees feel that the recently added bus lanes east of 63rd are confusing and don't want to see this approach repeated.
- Many would like to see expansion of Ecopasses for homes and businesses in the area.

Comment Compilation

Visualization/Map Comments

Arapahoe and Commerce

Current Trends

What do you like?

- No buildings over 35 feet

What don't you like?

- MacArthur needs a traffic light
- A 6 lane highway in the middle
- Too much parking
- Suburban Style Development
- Horrific pedestrian condition

Districts

What do you like?

- Arts and Culture, Boulder Digital Arts, Video Station, Record Store, - Let's add more!
- 3 story max – housing and commercial, no exceptions

What don't you like?

- Create dense office space with % of space for restaurants. 48th through 55th and Arapahoe.
- Reduce traffic lanes – 6 is insane
- Pedestrian access from Peloton to King Soopers center needs improvement.

Arapahoe and 55th

Current Trends

What do you like?

- Small office spaces are good for startups.
- Keep the Cherryvale neighborhood rural.
- Buffered bike lane

What don't you like?

- Speed limit of 45 is too high.
- Speed limit is fine ☺
- The “blighted” area just north of bank on 55th and Arapahoe.
- 6-8 lanes of mixed traffic.
- BCH intersections are terrible.
- No regular transport connection Arapahoe to Pearl on 55th. Need connection from Pearl to downtown.
- Gridlock already on Arapahoe. Adding more businesses will increase it.
- It's a lousy suburban eye-sore

Districts

What do you like?

- Put nice looking 2 story assisted living component
- BRT center on side
- Buffered bike lane

What don't you like?

- Why does every scenario have a buffer bike lane?
- Because bikes rock ☺
- Buffered bike lanes are ugly – like fingernails on chalk board for eyes. Temporary buffer bollards break and fall into street on Baseline – dangerous. Also get blackened and ugly. Recent study in Boulder by bicycle organization said they are hard to maneuver.
- Don't like 4-story proposals
- Don't put in so much parking, and put it in the back.
- Yes put parking in back
- Bring buildings to the sidewalk/bikeway edge.

Housing Choices

What do you like?

- Keep Cherryvale rural-residential. Designate agricultural district.
- Buffered bike lane
- Mixed-use with residential
- Enhanced crosswalks
- Give no height and setback exceptions
- You won't have people biking or walking as long as you keep building more suburbia!

What don't you like?

- No high-rise housing or commercial or retail.
- Don't like housing 4-5 stories.
- Nothing taller than 2-3 stories.
- 6 Lane highway

55th and Western

Current Trends

What do you like?

- That you asked us
- Current industrial looks nice in most places
- Boulder needs an industrial zone.

What don't you like?

- That you won't consider a new paradigm ie prosperity =<growth.
- Need quiet crossing at Railroad.
- "Yes!"

Districts

What do you like?

- Not good for housing
- Mixed use residential/business retail light industry
- Would love a market/grocer in this area
- I second that ^
- Hate the additional housing that will bring more congestion, more cars, more parking lots.
- I like 2 story housing.
- "I don't"
- "I second that"

What don't you like?

- Will need a stop light. Increase traffic time. Impossible to cross 55th on foot, bike, or a car.

- All scenarios make commuting in on Arapahoe from Erie, Lafayette, etc VERY difficult. It will not be eliminated by new housing.
- I live in The Meadows neighborhood (between 55th Cherryvale, Baseline and Arapahoe) and we have a real problem with traffic “cutting through” the residential area in order to avoid traffic lights, especially during rush hour. How will the City protect existing residential areas from being impacted by increased traffic?
- “food carts” vs. established restaurants

Housing Choices

What do you like?

- Hard to cross six lane road on foot
- All of it. Why not 3-story residential?
- More housing
- Yes please

What don't you like?

- Not a good place for housing.
- No housing

Other Comments/Stickers on Map

- When adding new underpasses keep overpass option for pedestrians and bikes for when it is dark, flood waters etc. More connections and options.
- Budget for safe crossings so train horns don't go off. I live 1 mile from trains. Been here 25 years now. Can't keep windows open at night – Instead have to have AC on – increases carbon footprint.
- If nothing else, area between East CU and TVAP/Boulder Junction has to be re-zoned to connect huge 2 areas, take advantage of transit, CU population.
- Move proposed transit super stop to 33rd (from 30th) and to 29th (from 28th) add stop at hospital.
- Pedestrian/Bike connection from 33rd to Boulder Junction.
- BRT stop at 33rd (E Campus entrance).
- This is a safe area (Residential Neighborhood SE of Foothills and Arapahoe). Please don't add more concrete here – also, we need a light at MacArthur and Arapahoe. We can't get out safely.
- Hospital grew bigger than we were told.
- Medical park (North of Ball).

- No housing here please. Clean up retail already there.
- More retail on north side of Arapahoe west of 55th.
- Better pedestrian crossing at 55th and Arapahoe = more accessibility for residents to south. Underpass?
- Grocery stores please
- Would like to see this looked at in the next Comprehensive Plan in 2015. Look at the whole town and South Boulder.
- Less pot!
- Parking/access at Wendy's at lunch is terrible... can parking lots be connected through to neighboring lots? (Enterprise Car Rental and AutoParts store).
- More retail and food choices near 55th and Arapahoe and to the north of 55th by office park.
- Events center at golf course.
- Walking paths! Nature at golf course. Open up south side.
- Mini Pearl St. Mall
- Don't change the golf course.
- Do not put residential next to a train crossing, do you know what a train sounds like?
- Try a central plaza.
- One of the things I love about East Boulder is the quality of life. More quiet, less people, less expensive. This would all change with new projects. Changing the tempo of this area code would be sad.
- Build a bike path along the rail line.
- Keep Flatirons 18-holes
- I live at **** Lodge Lane. I have attended the flood mitigation studies. I understand that open space will not participate in the most effective remedy to the flooding potential near 55th. Idea: Flatirons Golf Course is owned by the city – I believe this is true. Why not create a drainage basin at the Golf Course site which would mitigate so many problems (East Arapahoe development will only displace many acre feet of water). We need Flatiron Lake, not Flatiron Golf Course.
- Improve transit access between US 36 and East Arapahoe.
- Direct access for families to Douglas School from neighborhoods south of Arapahoe.
- Trail Connections
- Land available for arts campus. 6-8 usable acres located along transit route at Anderson Ranch/Snowmass at Valmont Butte.
- Studio Arts Campus near ballparks
- More frequent bus service to Flatiron Park!
- Connect the SBC bike path to the Eco Cycle complex on 63rd Street.
- Build low level independent living/assisted living near hospital. Some people won't drive but will use the van at the center. NO 55 ft. only 35 ft.

- B-Cycle down Arapahoe bike paths
- Keep a better entrance greenways.
- Missing bike/pedestrian path (East of Cherryvale).
- Keep industrial no to housing along creek.
- Park (Xcel Power Plant).
- Options for retail/commercial services restaurants at 63rd and Arapahoe.
- Commuters from Erie, Lafayette, etc? You are choking them and it is already bad.
- Visualize 63rd and Arapahoe.
- I like everything the way it currently is. My neighborhood is rural and I like that!
- Add more about arts
- Road diet!
- There is no way to make a pleasant place when you have a 6-lane highway running through it.
- Even with the MUP on the north side, if you are biking west to east there are so many driveways and everyone is pulling out looking the other way at traffic.
- Art district
- Call out Avalon Ballroom on maps, community resource and opportunity to expand for higher demand for dancing and restaurant adjacent to site.
- Arts and education district and link with dinner theater (Naropa and Avalon)
- Provide bike share at Avalon and at Dinner Theater
- Link dancing/arts with public health
- Participation! Exercise!
- Late night transit!
- 63rd and Arapahoe – clean up with landscaping and enforce setback requirements.
- 63rd and Arapahoe education and participatory arts (dance)
- Overall: reduce housing/jobs imbalance. Use zoning to move the area towards greater mobility, less subsidy, more energy sustainability etc. Growth pays its own way.

Scenarios Board Comments

About the Scenarios

- Think about regional connectivity to East County and Broomfield
- Recycle Row a model for State and Country
- Thousands of car commuters from the east – nothing will change this
- Not true – we can change for parking and for congestion-demand based tolls etc.

- Is the Community VIZ data and meta data available in a standard format? Or is it proprietary? That allows citizens and 3rd parties to do their own modeling and fit to their own models.

Scenario A

- Kudos to city for realizing the state of change and opportunities
- Terrible access currently to Walnut East
- Plus other medical uses. Good pairing. Jobs – where will they all live?
- Keep service retail and light industry in Boulder
- Don't count of this do improved bus operations/service leading up to BRT "BART" options are scary to suburban mentality
- "Low Level of Pedestrian and Bike activity" – This is incorrect; there are lots of folks walking and riding bikes
- We are happy, lots of places to relax
- The ball fields, bike park, golf course are not that far. Easy to bike to.
- Food trucks for current offices
- Lunch spots. Day care. Services like dry clean
- Keep service retail
- Find a way to continue to allow light industrial

Scenario B

- Walnut to Arapahoe connection is great!
- Arts integrated with other activity
- Housing yes!
- No housing!
- Public spaces will be needed to support residential infill
- Medical offices ok
- Take advantage of current industrial parking lots to redo add pocket parks, quiet areas, food trucks
- No housing, this is a rural area
- Keep 4 story limit
- Transit, easy connections from South without going through downtown
- Mobility hubs – yes! Shelter at bus stops. Crossings at bus stops
- Have pedestrian crossings as frequently as driveway cutouts.
- Continue to support "recycle row"
- Fix bus priority lane signage so SOVs don't get tickets

Scenario C

- 55th/Western/Cherryvale all need investment
- (BCH) Key employer with employees who will walk and ride the bus – need restaurants so they don't have to drive to meals
- Medical Office Example is Ugly
- Find a way to keep small businesses
- Potential “design” district where you can buy tile, carpet etc. No place to do this in Boulder! Losing tax \$\$
- I think without housing there will be an even more significant traffic problem
- More housing will bring more traffic if you keep on the trends of too much parking, not making any places people can walk and bike and change the character!
- Speed limit too high. Lower from 45.
- No more giant parking lots!
- Bury parking for new housing
- Connect pathways and mixed use to sidewalks and bus stops
- Net energy neighborhoods that are 15-minute neighborhoods.
- Lots of opportunity to walk/connect to local streets along 55th. Arapahoe needs multi-modal/pedestrian investment
- Partner with private development and require in new developments
- Medical office example is “ugly” (X2)
- Monolithic look is not good, not Boulder
- Totally logical place for this call n ride instead of fixed-route on 55th? Connections at Boulder Health/Foothills
- North/South connections needed between Boulder Junction and south and across RR tracks north of BCH
- Need protected bike-lanes on 55th
- New housing is an economic/income opportunity
- This land will become much more valuable – keep BTH in corridors and create other cultural opportunities
- Apartments, mix of size
- Preserve views of Flatirons
- Buys area for open space instead of housing
- 35' is okay
- More bike parking
- Design guidelines
- Like the idea of live/work

Transportation Comments

Community Feedback Poster

- Slow vehicle speeds on Arapahoe
- Encourage BRT, biking and walking
- Simply plowing the multi-use paths beyond 55th would be a big help for bike access year-round!
- Install buffered bike lanes/bike improvements on the south side of Arapahoe
- East Arapahoe is a perfect place to place serious bikeways, bus only lanes etc. It is way too wide for cars through much of the study area, and goes from **too** narrow to **too** wide.
- Move people not cars
- Slower travel speeds will help
- 3 mile limit for most riders. Turning cars are the major danger
- Intersection@ MacArthur and Arapahoe traffic signal requested.
- More Ecopass! Should be neighborhood based
- Multi-use paths along creek and behind golf course are good ways to access Boulder
- Continuity in Arapahoe for bikes
- More bike connected in area
- For sure! South side of Arapahoe nobody looks right to turn right going “wrong way“ down Arapahoe is dangerous.
- More B-cycle along/bicycle connections - Folsom to 65th and father east
- Consider building 15min neighborhoods
- Plow bike path east of Cherryvale! Implement Ecopass. Area Ecopass would be a backup for cyclists who can't use bike lanes in the winter
- Existing neighborhoods enhance. Peloton for example – 15-20 min neighborhood
- Hard to get to 63rd and Arapahoe from Longmont via bus right now
- Vegetation blocking sight distance on Arapahoe. Careful!
- Needs big reduction in surface parking. It's a major impediment to pedestrians and bikes
- Concerned about accidents @ Conestoga and Arapahoe.

New Options Poster

- Yes! As long as it slows SOV access on Arapahoe
- BRT is key to connections to East Boulder, Broomfield and N I-25 rail transfer
- Great addition – more transit, better; more transportation is better
- Bikes need to be part of BRT
- AB (to DIA) @Boulder Junction

Transportation Connections Poster

- Add bike station at 38th and Arapahoe
- Add B-cycle station in Flatirons Industrial Park by OZO and Upslope
- Modify existing infrastructure to connect current businesses/homes/transit
- 33rd connection upgrades to access need to consider neighborhoods
- Get rid of left turns that don't have green arrows. Too dangerous
- Add more bike parking @ RTD stops
- Love the idea of being able to walk to businesses and restaurants with 15-minute from everywhere in the study area
- Confusing lane arrangement with bus-only, bike, turn lanes from Cherryvale to Westview. Simplify!
- BVSD is a good bus station for this community (63rd/Westview)
- Add housing neighborhood

Transportation Analysis Poster

- Modify and designate for pedestrians and bikers
- More density and connectivity between existing businesses and homes
- More Ecopass access would be great
- Better signage for multi-use paths (X2)
- Ecopass for all! City of Boulder
- Stripe all driveway cut outs to alert drivers to bike/walk crossing

MindMixer Feedback Through 11/17/14

Topic Name: What's your vision for east Arapahoe?

Idea Title: I strongly prefer that any new buildings be limited to 3 stories

Idea Detail: I believe that the views of the foothills should remain as unobstructed as possible. Also that any new buildings should be set back fairly far from the sidewalk for aesthetic purposes.

Idea Author: David M C

Number of Seconds 0

Number of Points 28

Number of Comments 3

Comment 1: This is one of the ugliest streets in Boulder I don't think there is a single aspect of it that should be preserved. Deep setbacks with parking lots are the ugliest pattern of development I can imagine. | By Jim M

Comment 2: I just returned from hiking the Teller Lake trail and the view as you top the hill heading west around 70th or so is spectacular. However, as you get closer in around Cherryvale to the west there really isn't that great of a view and it is easy to see because Arapahoe is so wide. Boulders 55 foot height limit came about because that is roughly the size of mature trees and the trees are what limit the view. There is nothing wrong with nodes of

four story buildings, but O agree the cookie cutter architectural is pretty bad. | By David B
Comment 3: I agree with the set back suggestion, and would like to limit the height to 2 stories for the new vibrant East Arapahoe. The much applauded Boulder Junction with its canyons of apartments and pavement does not look like progress to me. Please do not allow this sort of compromise dictate the future appearance of our lovely town. And please do not replicate these architectural mishaps on East Arapahoe. | By Susan B

Idea Title: East Arapahoe does not need any more Storage Units

Idea Detail: It's my understanding that developers have been trying to add more housing along Arapahoe for the past 6years or so , for what ever reason it isn't happening.

It has come to my attention that the property across the street from the golf course will now be more "Storage units" because of difficulties for housing approvals.

Is this the cities idea of Envision East Arapahoe, is this what we want across the our golf course? It seems we would want people across the street, being able to use this facility that
1

seems to me under utilized.

Don't let these last large parcels of land slip away on Arapahoe to more of this type of use, they will be gone for decades.

Idea Author: craig F

Number of Seconds 0

Number of Points 24

Number of Comments 0

Idea Title: Make Arapahoe safer for bicyclists--protected bike lanes

Idea Detail: The multi-use path along Arapahoe east of Foothills often has poor visibility from the road and/or side streets. It does not feel safe traveling at bicycle speeds near intersections. Protected bike lanes similar to those on Baseline near Williams Village would make the road safer and more accessible to bicyclists.

Idea Author: Bob P

Number of Seconds 0

Number of Points 15

Number of Comments 0

Idea Title: Better connected shopping areas with fewer strip malls

Idea Detail: There are several strip malls along this corridor that are not well connected for bikes or cars and feel outdated. I think there must be another format that would serve our community better.

Idea Author: Lieschen G

Number of Seconds 0

Number of Points 14

Number of Comments 1

2

Comment 1: Strip malls were made to be torn down, rezone East Arapahoe to 5 stories and the strip malls will change "format".

| By Jim M

Idea Title: Bus service up 55th for residents who live south of golf course.

Idea Detail: Bus service is hard to use when it's more than 1/2 mile to the nearest bus line.

Idea Author: Laine G

Number of Seconds 0

Number of Points 14

Number of Comments 0

Idea Title: Make East Arapahoe a boulevard from 55th to 75th street

Idea Detail: Not that Boulder is Paris, but boulevards distinguish a city's arteries. Even more so with trees

Idea Author: Stanley G

Number of Seconds 0

Number of Points 11

Number of Comments 0

Idea Title: Lets not repeat the mistakes of Boulder Junction

Idea Detail: I do not want to see endless 4 story apartments buildings, turning Arapahoe into another "Pearl Canyon". I think the south side of Arapahoe should be left much as it is from 38th St. west to 75th. The north side from Conestoga to S. Bldr. Creek is ready for some redevelopment into mixed use, especially east of 55th St. The idea of buses running down the middle of Arapahoe similar to Denver's 16th St. seems totally unrealistic, given that there will still be auto traffic on Arapahoe. Busses crossing the auto lanes to the curb will further congest the traffic flow. Do not install useless 8 ft. wide sidewalks as was done between Folsom & 28th St., and east of 63rd St. Integrate the bike lanes into the sides of the auto lanes. Bicyclist using sidewalks do not pay attention to turning autos. In any case, I rarely see

3

pedestrians or bicyclists any where along Arapahoe.

Idea Author: Archie S

Number of Seconds 0

Number of Points 11

Number of Comments 1

Comment 1: The southeast corner of 55th and Arapahoe is the perfect spot for low impact dense multi use redevelopment. It is surrounded by the golf course, has fire station and there is already a large apartment complex. The existing uses are generally old single family homes that have been converted to businesses.

Bus rapid transit on an arterial street is not at all like the 16th St. Mall shuttles. BRT isn't slow and doesn't stop every block. The BRT busses wouldn't cross to the curb, they would stop at stations in the median and the passengers would cross the street as pedestrians. Staff has obviously not done an acceptable job of explaining this concept to the general public. | By David B

Idea Title: Build car-free housing for Boulder residents without cars.

Idea Detail: Thousands of people in Boulder do not own cars yet they are forced to pay for parking when they purchase or rent housing. Mixed use housing should be built in East Arapahoe offering the option of units without bundled parking, allowing car owners to pay for the parking they use, and allowing people without cars to avoid paying for parking they don't use.

Idea Author: Tom V

Number of Seconds 0

Number of Points 10

Number of Comments 0

Idea Title: Add cheap artists/makers warehouse rentals to Resource 2000 yard

Idea Detail: Marijuana grow operations are driving up the costs for warehouse spaces. Artists, inventors, and makers are being forced to rent workshop spaces in far away places like Denver and Golden. I think it would be nice to add a large warehouse to that empty field at the Resource 2000 yard. It could be subdivided into many smaller studio spaces to be rented out.

4

It could have a separate access so it could be used outside of Resource 2000 hours. During Resource 2000 open hours, the artists and makers could grab cheap materials! There could be a special display/purchase area in Resource 2000 highlighting the best creations. We need more spaces to create!

Idea Author: W E

Number of Seconds 0

Number of Points 9

Number of Comments 2

Comment 1: Especially for students. | By Felicia F

Comment 2: Great idea. Keep artists in Boulder. | By Felicia F

Idea Title: Build car-free housing, to Boulder residents who don't own cars.

Idea Detail: Thousands of Boulder residents do not own cars, yet they are forced to pay for mandated parking in the buildings they purchase or rent. How regressive to force everybody to pay for parking whether or not they use it. In East Arapahoe new mixed used development should have covered bike parking and any automobile parking paid for only by those who use it.

Idea Author: Tom V

Number of Seconds 0

Number of Points 8

Number of Comments 0

Idea Title: What's wrong with the way things are?

Idea Detail: If I wanted to live in town, I would. More development means more air pollution, more noise pollution, more light pollution, more traffic, more people - all things I wish to avoid living east of 55th St. Who asked the city council to make changes in the first place? These changes, not enhancements, will directly and adversely affect my life. Will it do that for any of the city council members promoting this?

5

Idea Author: Andrew J

Number of Seconds 0

Number of Points 7

Number of Comments 2

Comment 1: Ah yes. I have mine so please roll up the streets behind me and hermetically seal my sanctuary. I've lived here for 35 years back when Boulder was still building single family sprawl housing like Meadow Glen, Country Meadows, Shanahan Ridge, etc. In that time the Denver metro area has doubled in population and so has Boulder. The only constant in life is change. We need to decide how to best manage that change. | By David B

Comment 2: It need better streetscaping to welcome people to Boulder. | By Felicia F

Idea Title: East Arapahoe..A place to restore and replenish

Idea Detail: Perhaps there is also the opportunity to create a small native botanic garden as part of the Golf Course Flood water mitigation project, a sculpture garden or public art venue.. Or a walkable Labarynth? An educational Water garden that stresses the importance of conservation of our resources. Create affordable artist studio space on east Arapahoe. Perhaps Naropa could advise on a public meditation center...

The recent "calming" of traffic along Cherryvale should be kept in mind while developing ideas for this area. Clearly the residents of this residential neighborhood would like to retain the quiet peaceful nature of this once semi-rural area of Boulder County!

Idea Author: Susan B

Number of Seconds 0

Number of Points 7

Number of Comments 0

Idea Title: Add middle income housing that appeals to families

Idea Detail: But design and implement in such a way to not drive out light industry. Land uses should be integrated and diverse.

6

Idea Author: Deryn W

Number of Seconds 0

Number of Points 6

Number of Comments 0

Idea Title: Wildflowers and nature

Idea Detail: I moved to the east side because it was calm and I could view wildlife. Now I have a huge hospital across the street from me and I can't get out on Arapahoe without risking my life due to all the traffic. Please, no more traffic, no more cheap, squeezed together, compact housing. Give us dog parks, walking paths and other opportunities to connect with nature. No more cement and congestion. There are plenty of restaurants and stores in Boulder. We can go there when we need to shop or want to eat out. There's lots of new housing in town. Let's not turn into Los Angeles.

Idea Author: Kathy S

Number of Seconds 0

Number of Points 6

Number of Comments 1

Comment 1: There should be a category in the ratings for "I don't like it." I am not really neutral at all, I disagree. The hospital will continue to grow as more of us get older and have more health problems. Hospitals also are large employers of modest wage positions that could benefit greatly from affordable housing nearby, even if they chose to drive a mile or two rather than walk, bike or bus. | By David B

Idea Title: Road updates east of Cherryvale

Idea Detail: Now that the city has spent a huge amount of money re-doing the road east of Cherryvale, how about letting people use it in a more efficient fashion? Get rid of the bus lanes, it needs to be four lanes east of 63rd. The eastbound "turn only" at 63rd goes nowhere, while access to the ReSource Yard and the Ed Center is no easier than it was prior to reconstruction. MAKE THE ROAD FOUR LANES OUT TO 75TH . Choking it down to two lanes at 63rd is a ridiculous way to manage traffic.

7

Idea Author: Jeff P

Number of Seconds 0

Number of Points 5

Number of Comments 0

Idea Title: Arapahoe Ave. tunnel with paths, wildlife corridor, etc. above

Idea Detail: Put Arapahoe Ave. underground (through a tunnel) for at least a few hundred feet, preferably near 55th St., and put walkways, a bike path, a wildlife corridor, gathering places, greenery, art installations, etc. on top to serve as a connection between areas north and south of Arapahoe Ave. and to serve as an anchor for a destination spot for the area.

Idea Author: Don P

Number of Seconds 0

Number of Points 3

Number of Comments 1

Comment 1: Bad idea. This would be a senseless waste of money | By David B

Idea Title: Build sustainable, complete, bike/walk friendly areas in Boulder

Idea Detail: Increased density is very effective at preserving natural resources globally. Dwellers of dense, urban environments use less energy and resources on average than those in less dense environments. See <http://tinyurl.com/lynnces> for evidence. In a democratic society, all citizens should be able to choose to live in Boulder, not just "those who arrived first". The best kind of density is "self-sufficient" density where dwellers can meet most of their needs for commerce, entertainment, food, etc within a walkable distance.

Therefore I suggest that East Arapahoe be developed with this in mind. Specifically, create mixed-use development that allows residential, office, retail/restaurant all within very close proximity. Increased density is a positive as long as new residents don't have to drive to other areas of Boulder to fulfill their needs/wants. Zone East Arapahoe appropriately and don't compromise this with developers just to foster development. Zone it and they will come.

Idea Author: tim N

8

Number of Seconds 0

Number of Points 3

Number of Comments 0

Idea Title: Better bike and pedestrian access along Arapahoe

Idea Detail: I hate biking along Arapahoe, and avoid it despite the number of great places that have popped up recently (Wild Woods, Bru, etc.).

- Bike paths are poorly labelled
- Sidewalks only exist in some places and can be quite narrow even when they do exist
- There are a LOT of driveways with cars entering and exiting

My few experiences as a pedestrian along Arapahoe aren't much better. Businesses are quite far apart, sidewalks come and go, and bus stops aren't aligned with businesses (try getting to Avery by bus).

* I'd love to see bike routes that are clearly marked and protected from traffic.

* Make sidewalks wider and make sure they're continuous along the length of the street.

* Add additional bus stops so pedestrians don't have to walk so far to reach restaurants and other businesses.

Idea Author: Jackson F

Number of Seconds 0

Number of Points 2

Number of Comments 0

Idea Title: Rezone the whole corridor to 5 stories

Idea Detail: There isn't really a nice building on the entirety of East Arapahoe and Boulder needs to grow somewhere...Arapahoe has good connectivity to major highways and services and big building won't block anybody's views of the hills.

Idea Author: Jim M

9

Number of Seconds 0

Number of Comments 0

Idea Title: Can we bring back the Pearl Trolley

Idea Detail: Given Boulder Junction on Pearl, can we run a trolley from this East Arapahoe into downtown for all the employees who travel by transit to Boulder everyday.

Idea Author: Ryan M

Number of Seconds 0

Number of Comments 0

Idea Title: No more Supersizing Boulder! No more height exemptions!

Idea Detail: We can't go back from the ugly 55ft cement soviet block housing of Boulder Junction. Do repeat that mistake along Arapahoe.

Traffic is already maxed out on this road. Adding thousands of more jobs and residents will just throw it into complete gridlock. This "New Urbanism" trend doesn't work for Boulder. Let Denver have it.

No more 55ft Exemptions. No more setback back exemptions. No more cement paths counting as green space. We don't want another wall of tall buildings.

Listen to the citizens of Boulder, not the outside Developers!

Idea Author: Stephen H

Number of Seconds 0

Number of Comments 0

Idea Title: Go slowly. What we have works pretty well.

Idea Detail: City staff's current view of what might be good in 5 or 10 years may be correct -- or may be entirely misplaced. I don't want to see the East Arapahoe corridor micromanaged now

for the next decades. If there are slight changes, that's fine, but go slowly.

Idea Author: Brad P

10

Number of Seconds 0

Number of Comments 0

Idea Title: More commercial enterprises, e.g. small grocery, restaurants...

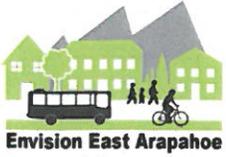
Idea Detail: East Boulder would benefit from a small grocery market (think Ideal) located near 55th and Arapahoe. This would leverage the coffee shops, small restaurants, and other shops beginning to surface in the area. The recent expansion of the Boulder Community Hospital will provide the customers necessary to support such a community. The current East Boulder neighborhood is stale and would benefit from some revitalization.

Idea Author: Mark M

Number of Seconds 0

Number of Comments 0

11



Oct. 27, 2014 Workshop – Comment Form

Envision East Arapahoe Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

<p>What should remain?</p>	<p>What should change? BUILD RESIDENTIAL HOUSING</p>
----------------------------	---

2—How would you improve the draft Vision Statement?

BUILD RESIDENTIAL HOUSING & INCREASE CAPACITY FOR VEHICLES / COMMUTERS

3—What ideas would you suggest for transportation enhancements and improving mobility?

WITH THE AMOUNT OF SERVICE JOBS IN BOULDER, VEHICLE COMMUTERS NEED BETTER ACCESS. MASS TRANSIT WILL NOT WORK FOR COMMUTER COMMUTE IN.

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the “menu” of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

<p>Like</p>	<p>Dislike and would change or add</p>
-------------	--

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

<p>Like</p>	<p>Dislike and would change or add</p>
-------------	--

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

<p>Like</p>	<p>Dislike and would change or add</p>
-------------	--

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

For the future, I would like to see:	☺ = like ☹ = neutral ☹ = dislike	Comments and Ideas:
More light industry north of Arapahoe Avenue <i>(Current Trends scenario)</i>	☺ ☹ ☹	
Affordable space for necessary services (e.g., car repair) <i>(Current Trends and Districts scenarios)</i>	☺ ☹ ☹	FISHER AUTO EXISTS
Boulder Community Health with nearby places for medical-related offices and small retail <i>(Districts scenario)</i>	☹ ☹ ☹	
A new east/west connecting street (i.e., Walnut/48 th Street) <i>(Districts scenario)</i>	☺ ☹ ☹	
Small shops, dining, offices and some apartments north of Arapahoe at 55 th , along 55 th St. <i>(Districts scenario)</i>	☹ ☹ ☹	
Improvements to sidewalks and intersections <i>(Districts and Housing Choices scenarios)</i>	☺ ☹ ☹	
Place for energy generation, recycling, eco-district <i>(Districts scenario)</i>	☹ ☹ ☹	
Small shops, dining, offices, and housing north of Arapahoe at 55 th , along 55 th St. in walk-friendly neighborhoods <i>(Housing Choices scenario)</i>	☹ ☹ ☹	
More people walking and riding bicycles – make it safe and convenient <i>(Districts and Housing Choices scenarios)</i>	☺ ☹ ☹	
Some new affordable, workforce housing north of Arapahoe Ave. <i>(Housing Choices scenario)</i>	☹ ☹ ☹	
Flatiron Golf has new trails and community gardens, in addition to golf course <i>(Housing Choices scenario)</i>	☺ ☹ ☹	
Ecological restoration along ditches and near creeks; better access to nature for current and future residents <i>(Housing Choices scenario)</i>	☺ ☹ ☹	
Arapahoe Ave. becomes a safe and attractive “boulevard” with street trees, noise buffering, safe speeds and transit	☺ ☹ ☹	

What other choices or ideas would you like to see?

2 Big Questions: ① Who will pay for all the infrastructure & ~~the services~~ services?
 ② Will levels of service for all city facilities + school district be improved, stay the same, or get worse?



Oct. 27, 2014 Workshop – Comment Form

Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?

Service businesses, now that they have been kicked out of the 30th + Pearl area

What should change?

new development should be severely limited, & all new development should pay to maintain levels of service for all city services & facilities

2—How would you improve the draft Vision Statement?

add costs & who will pay, add numbers (jobs, pop) that could be built all along the whole corridor

3—What ideas would you suggest for transportation enhancements and improving mobility?

Free E&O Passes paid by parking fees or other user fees

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the "menu" of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like

Keeps service businesses, minimal new impacts you could achieve this by down zoning, & not getting the 19,000 new jobs.

Dislike and would change or add

limit height

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like

Dislike and would change or add

I don't like building to street in these areas, destroys view, makes everything constrained

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Like

Most of the jobs are filled w/ OOT in-commuters, unless you limit job growth, the traffic congestion will just get worse & worse.

Dislike and would change or add

without limiting overall jobs all this housing has no overall benefit; it's a bandaid on ~~the~~ a serious wound.

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

☺ = like
 ☹ = neutral
 ☹ = dislike

Comments and Ideas:

For the future, I would like to see:

More light industry north of Arapahoe Avenue
 (*Current Trends scenario*)

☺ ☹

Have you heard of "down zoning"?

Affordable space for necessary services (e.g., car repair)
 (*Current Trends and Districts scenarios*)

☹ ☹

Boulder Community Health with nearby places for medical-related offices and small retail
 (*Districts scenario*)

☹ ☹

A new east/west connecting street (i.e., Walnut/48th Street)
 (*Districts scenario*)

☹ ☹

Small shops, dining, offices and some apartments north of Arapahoe at 55th, along 55th St.
 (*Districts scenario*)

☹ ☹ ☹

Improvements to sidewalks and intersections
 (*Districts and Housing Choices scenarios*)

☹ ☹

Place for energy generation, recycling, eco-district
 (*Districts scenario*)

☹ ☹

we already have it

Small shops, dining, offices, and housing north of Arapahoe at 55th, along 55th St. in walk-friendly neighborhoods
 (*Housing Choices scenario*)

☹ ☹

More people walking and riding bicycles – make it safe and convenient
 (*Districts and Housing Choices scenarios*)

☹ ☹

but totally unrelated to District & Housing

Some new affordable, workforce housing north of Arapahoe Ave.
 (*Housing Choices scenario*)

☹ ☹

That might be ok. IF you would stop job growth

Flatiron Golf has new trails and community gardens, in addition to golf course
 (*Housing Choices scenario*)

☹ ☹

Ecological restoration along ditches and near creeks; better access to nature for current and future residents
 (*Housing Choices scenario*)

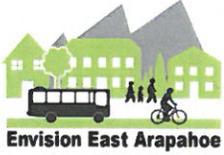
☹ ☹

Arapahoe Ave. becomes a safe and attractive "boulevard" with street trees, noise buffering, safe speeds and transit

☹ ☹

Just will create more traffic jams, UNLESS you limit job growth!

What other choices or ideas would you like to see?



Oct. 27, 2014 Workshop – Comment Form

Envision East Arapahoe Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?

most of what is there on W. Side of 55th and Flatiron Industrial Park, Levee Industrial

What should change?

2—How would you improve the draft Vision Statement?

don't try to make East Arapahoe more urban leave the area N. of Arapahoe industrial,

3—What ideas would you suggest for transportation enhancements and improving mobility?

better bike paths & safer crossings

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the "menu" of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like

pretty much of —

Dislike and would change or add

just clear up a few of the less attractive areas

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like

continued use as business area
changes to add more possible medical offices around hospital

Dislike and would change or add

don't like buildings too close to sidewalks
don't like too much height
add more green spaces

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Like

nothing —

Dislike and would change or add

do not want housing — save for business growth 50 years from now — housing will still not be affordable unless subsidized & either
--- Over way will not meet the need for the type of housing we need the most —

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

For the future, I would like to see:	😊 = like 😐 = neutral ☹️ = dislike	Comments and Ideas:
More light industry north of Arapahoe Avenue <i>(Current Trends scenario)</i>	👍 😐 ☹️	
Affordable space for necessary services (e.g., car repair) <i>(Current Trends and Districts scenarios)</i>	👍 😐 ☹️	
Boulder Community Health with nearby places for medical-related offices and small retail <i>(Districts scenario)</i>	👍 😐 ☹️	
A new east/west connecting street (i.e., Walnut/48 th Street) <i>(Districts scenario)</i>	😊 🤔 ☹️	
Small shops, dining, offices and some apartments north of Arapahoe at 55 th , along 55 th St. <i>(Districts scenario)</i>	😊 😐 🤔	
Improvements to sidewalks and intersections <i>(Districts and Housing Choices scenarios)</i>	😊 🤔 ☹️	
Place for energy generation, recycling, eco-district <i>(Districts scenario)</i>	😊 🤔 ☹️	
Small shops, dining, offices, and housing north of Arapahoe at 55 th , along 55 th St. in walk-friendly neighborhoods <i>(Housing Choices scenario)</i>	😊 😐 🤔	
More people walking and riding bicycles – make it safe and convenient <i>(Districts and Housing Choices scenarios)</i>	😊 🤔 ☹️	
Some new affordable, workforce housing north of Arapahoe Ave. <i>(Housing Choices scenario)</i>	😊 😐 🤔	
Flatiron Golf has new trails and community gardens, in addition to golf course <i>(Housing Choices scenario)</i>	😊 😐 🤔	<i>don't want current golf course to be changed</i>
Ecological restoration along ditches and near creeks; better access to nature for current and future residents <i>(Housing Choices scenario)</i>	😊 🤔 ☹️	
Arapahoe Ave. becomes a safe and attractive “boulevard” with street trees, noise buffering, safe speeds and transit	🤔 😐 ☹️	

What other choices or ideas would you like to see?



Oct. 27, 2014 Workshop – Comment Form

Envision East Arapahoe Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

<p>What should remain?</p>	<p>What should change?</p> <p>— fewer giant parking lots</p> <p>— more ped / bike connections</p> <p>— more housing; green space</p>
----------------------------	--

2—How would you improve the draft Vision Statement?

need to renew

3—What ideas would you suggest for transportation enhancements and improving mobility?

stripe all sidewalks at curbs/cutouts/driveways; signed marked paths

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the “menu” of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

<p>Like</p> <p>don't just leave as is</p>	<p>Dislike and would change or add</p>
---	--

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

<p>Like</p> <p><u>Like</u></p>	<p>Dislike and would change or add</p>
--------------------------------	--

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

<p>Like</p> <p><u>Like</u></p>	<p>Dislike and would change or add</p>
--------------------------------	--

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

😊 = like
 😐 = neutral
 😞 = dislike

For the future, I would like to see:

Comments and Ideas:

More light industry north of Arapahoe Avenue
 (*Current Trends scenario*)

😊 😐 😞

Affordable space for necessary services (e.g., car repair)
 (*Current Trends and Districts scenarios*)

😊 😐 😞

Boulder Community Health with nearby places for medical-related offices and small retail
 (*Districts scenario*)

😊 😐 😞

A new east/west connecting street (i.e., Walnut/48th Street)
 (*Districts scenario*)

😊 😐 😞

?

Small shops, dining, offices and some apartments north of Arapahoe at 55th, along 55th St.
 (*Districts scenario*)

😊 😐 😞

Improvements to sidewalks and intersections
 (*Districts and Housing Choices scenarios*)

😊 😐 😞

😊 😐 *H.V. King*

Place for energy generation, recycling, eco-district
 (*Districts scenario*)

😊 😐 😞

Small shops, dining, offices, and housing north of Arapahoe at 55th, along 55th St. in walk-friendly neighborhoods
 (*Housing Choices scenario*)

😊 😐 😞

More people walking and riding bicycles – make it safe and convenient
 (*Districts and Housing Choices scenarios*)

😊 😐 😞

Some new affordable, workforce housing north of Arapahoe Ave.
 (*Housing Choices scenario*)

😊 😐 😞

Flatiron Golf has new trails and community gardens, in addition to golf course
 (*Housing Choices scenario*)

😊 😐 😞

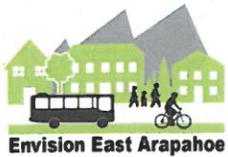
Ecological restoration along ditches and near creeks; better access to nature for current and future residents
 (*Housing Choices scenario*)

😊 😐 😞

Arapahoe Ave. becomes a safe and attractive "boulevard" with street trees, noise buffering, safe speeds and transit

😊 😐 😞

What other choices or ideas would you like to see?



Oct. 27, 2014 Workshop – Comment Form

Envision East Arapahoe Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?

This area is a business engine for Boulder. Reasonable rent for startups. Look how breweries + coffee have taken hold.

What should change?

Arapahoe is very intimidating with 6 lanes, 45 mph speed limit, long blocks. It is a major transportation corridor but not ped or bike friendly.

2—How would you improve the draft Vision Statement?

Don't understand the word "inclusive". Who is being excluded?
 In many ways ~~is~~ the area is "cheaper" than the rest of Boulder from a cost perspective.

3—What ideas would you suggest for transportation enhancements and improving mobility?

Linking area via transit to Boulder Junction would be great. Look for way to connect Pearl to Arapahoe via bike/ped path between hospital + 55th with bridge/bike path.

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the "menu" of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like

It's vibrant and driven by free enterprise/business model.

Dislike and would change or add

Transportation concepts are straight out of ~~1950~~ 1970's. There is a lot of room to improve things for peds/bikes/bus riders

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like

Great concept. 15 minute walk is nice way to connect. What would a tech worker or hospital visitor, or BUSA employee need at lunch hour?

Dislike and would change or add

Adding housing might be interesting experiment but I don't want to live way out there. I would drive into downtown.

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Like

Too Radical. Don't like

Dislike and would change or add

Doesn't make sense

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

☺ = like
 ☹ = neutral
 ☹ = dislike

For the future, I would like to see:

Comments and Ideas:

More light industry north of Arapahoe Avenue
 (*Current Trends scenario*)



Affordable space for necessary services (e.g., car repair)
 (*Current Trends and Districts scenarios*)



Boulder Community Health with nearby places for medical-related offices and small retail
 (*Districts scenario*)



A new east/west connecting street (i.e., Walnut/48th Street)
 (*Districts scenario*)



Small shops, dining, offices and some apartments north of Arapahoe at 55th, along 55th St.
 (*Districts scenario*)



Like a lot

Improvements to sidewalks and intersections
 (*Districts and Housing Choices scenarios*)



Place for energy generation, recycling, eco-district
 (*Districts scenario*)



Small shops, dining, offices, and housing north of Arapahoe at 55th, along 55th St. in walk-friendly neighborhoods
 (*Housing Choices scenario*)



More people walking and riding bicycles – make it safe and convenient
 (*Districts and Housing Choices scenarios*)



Some new affordable, workforce housing north of Arapahoe Ave.
 (*Housing Choices scenario*)



Flatiron Golf has new trails and community gardens, in addition to golf course
 (*Housing Choices scenario*)



Ecological restoration along ditches and near creeks; better access to nature for current and future residents
 (*Housing Choices scenario*)



Arapahoe Ave. becomes a safe and attractive "boulevard" with street trees, noise buffering, safe speeds and transit



What other choices or ideas would you like to see?



Oct. 27, 2014 Workshop – Comment Form

Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?

general mixed use
atmosphere

What should change?

more retail, food, grocery
and better flow to get
commuters out of town faster

2—How would you improve the draft Vision Statement?

More business centric focus

3—What ideas would you suggest for transportation enhancements and improving mobility?

more rapid connector routes for the business dense
areas such as Flatirons Park. More lanes of 7
to get out of town commuters out of the way

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the "menu" of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like

Dislike and would change or add

change is needed

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like

business focus

Dislike and would change or add

Better transport + retail
closer to businesses, not just
on arapahoe,

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Like

Dislike and would change or add

Business need places to
grow, there is plenty of
housing already

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

☺ = like
 😐 = neutral
 ☹ = dislike

For the future, I would like to see:

Comments and Ideas:

More light industry north of Arapahoe Avenue
 (*Current Trends scenario*)

☹ ☺ ☹ mire is needed

Affordable space for necessary services (e.g., car repair)
 (*Current Trends and Districts scenarios*)

☺ ☹ ☹ it would be nice but not a must have

Boulder Community Health with nearby places for medical-related offices and small retail
 (*Districts scenario*)

☹ ☺ ☹ ease of access for people

A new east/west connecting street (i.e., Walnut/48th Street)
 (*Districts scenario*)

☹ ☺ ☹ ease congestion on arapahoe

Small shops, dining, offices and some apartments north of Arapahoe at 55th, along 55th St.
 (*Districts scenario*)

☹ ☺ ☹ very much needed, especially on 55th

Improvements to sidewalks and intersections
 (*Districts and Housing Choices scenarios*)

☹ ☺ ☹

Place for energy generation, recycling, eco-district
 (*Districts scenario*)

☺ ☺ ☹ not needed, already have elsewhere & close

Small shops, dining, offices, and housing north of Arapahoe at 55th, along 55th St. in walk-friendly neighborhoods
 (*Housing Choices scenario*)

☺ ☺ ☹ housing on 55th is a bad idea, too industrial & the train

More people walking and riding bicycles – make it safe and convenient
 (*Districts and Housing Choices scenarios*)

☹ ☺ ☹ more & faster bus routes to where people work, not just up & down arapahoe

Some new affordable, workforce housing north of Arapahoe Ave.
 (*Housing Choices scenario*)

☺ ☺ ☹ Bad place for housing

Flatiron Golf has new trails and community gardens, in addition to golf course
 (*Housing Choices scenario*)

☺ ☹ ☹ could go either way

Ecological restoration along ditches and near creeks; better access to nature for current and future residents
 (*Housing Choices scenario*)

☺ ☹ ☹ why just for residents?

Arapahoe Ave. becomes a safe and attractive "boulevard" with street trees, noise buffering, safe speeds and transit

☹ ☺ ☹ as long as there is more lanes & better traffic flow

What other choices or ideas would you like to see?

more business focused



Oct. 27, 2014 Workshop – Comment Form

Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?	What should change?
Keep some industrial areas	More Restaurants.

2—How would you improve the draft Vision Statement?

Don't include 55 ft, zero setback Buildings.

3—What ideas would you suggest for transportation enhancements and improving mobility?

I bike alot, but don't like the mixed use path next to a 6 lane road. Too much cross traffic.

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the "menu" of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like	Dislike and would change or add
Seems OK.	More Restaurants.

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like	Dislike and would change or add
More Businesses = More Traffic.	Limit new office space.

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Like	Dislike and would change or add
Can add some housing without making it as ugly as Boulder Junction, Needs some styling inline with Boulder, not all square, ugly Buildings.	

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

For the future, I would like to see:	☺ = like ☹ = neutral ☹ = dislike	Comments and Ideas:
More light industry north of Arapahoe Avenue (<i>Current Trends scenario</i>)	☺ ☹ ☹	
Affordable space for necessary services (e.g., car repair) (<i>Current Trends and Districts scenarios</i>)	☺ ☹ ☹	
Boulder Community Health with nearby places for medical-related offices and small retail (<i>Districts scenario</i>)	☹ ☺ ☹	
A new east/west connecting street (i.e., Walnut/48 th Street) (<i>Districts scenario</i>)	☹ ☹ ☹	
Small shops, dining, offices and some apartments north of Arapahoe at 55 th , along 55 th St. (<i>Districts scenario</i>)	☹ ☹ ☹	Small! Not another Boulder Junction
Improvements to sidewalks and intersections (<i>Districts and Housing Choices scenarios</i>)	☹ ☹ ☹	
Place for energy generation, recycling, eco-district (<i>Districts scenario</i>)	☺ ☹ ☹	
Small shops, dining, offices, and housing north of Arapahoe at 55 th , along 55 th St. in walk-friendly neighborhoods (<i>Housing Choices scenario</i>)	☹ ☹ ☹	Small
More people walking and riding bicycles – make it safe and convenient (<i>Districts and Housing Choices scenarios</i>)	☹ ☹ ☹	Good Luck. With the traffic on Arap, it's really dangerous to bike.
Some new affordable, workforce housing north of Arapahoe Ave. (<i>Housing Choices scenario</i>)	☹ ☹ ☹	
Flatiron Golf has new trails and community gardens, in addition to golf course (<i>Housing Choices scenario</i>)	☺ ☹ ☹	Make the entire Golf course a park.
Ecological restoration along ditches and near creeks; better access to nature for current and future residents (<i>Housing Choices scenario</i>)	☺ ☹ ☹	
Arapahoe Ave. becomes a safe and attractive "boulevard" with street trees, noise buffering, safe speeds and transit	☹ ☹ ☹	It's Always going to be a major traffic jam getting to /from Boulder
What other choices or ideas would you like to see?		



Oct. 27, 2014 Workshop – Comment Form

Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?

Avalon Ballrooms - growing!
 Participatory dance!
 Evening/late night transit

What should change?

2—How would you improve the draft Vision Statement?

Growth pay own way

3—What ideas would you suggest for transportation enhancements and improving mobility?

Congestion-based pricing for commuters & parking

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the "menu" of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like

Dislike and would change or add

needs more variety
 B or C better

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like

Mixed use

Dislike and would change or add

Combine/
 trade off
 B and C

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Like

Mixed use

Dislike and would change or add

Mixed use
 reduce jobs/
 housing
 imbalance

\$ make growth,
 especially jobs
 growth pay it
 own way.

--- Over ---

more mixed use

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

For the future, I would like to see:	☺ = like 😐 = neutral ☹ = dislike	Comments and Ideas:
More light industry north of Arapahoe Avenue (<i>Current Trends scenario</i>)	☺ ☐ ☹	
Affordable space for necessary services (e.g., car repair) (<i>Current Trends and Districts scenarios</i>)	☺ ☐ ☹	
Boulder Community Health with nearby places for medical-related offices and small retail (<i>Districts scenario</i>)	☺ ☐ ☹	
A new east/west connecting street (i.e., Walnut/48 th Street) (<i>Districts scenario</i>)	☺ ☐ ☹	
Small shops, dining, offices and some apartments north of Arapahoe at 55 th , along 55 th St. (<i>Districts scenario</i>)	☺ ☐ ☹	
Improvements to sidewalks and intersections (<i>Districts and Housing Choices scenarios</i>)	☺ ☐ ☹	
Place for energy generation, recycling, eco-district (<i>Districts scenario</i>)	☺ ☐ ☹	
Small shops, dining, offices, and housing north of Arapahoe at 55 th , along 55 th St. in walk-friendly neighborhoods (<i>Housing Choices scenario</i>)	☺ ☐ ☹	
More people walking and riding bicycles – make it safe and convenient (<i>Districts and Housing Choices scenarios</i>)	☺ ☐ ☹	
Some new affordable, workforce housing north of Arapahoe Ave. (<i>Housing Choices scenario</i>)	☺ ☐ ☹	
Flatiron Golf has new trails and community gardens, in addition to golf course (<i>Housing Choices scenario</i>)	☺ ☐ ☹	
Ecological restoration along ditches and near creeks; better access to nature for current and future residents (<i>Housing Choices scenario</i>)	☺ ☐ ☹	
Arapahoe Ave. becomes a safe and attractive “boulevard” with street trees, noise buffering, safe speeds and transit	☺ ☐ ☹	

What other choices or ideas would you like to see?



Oct. 27, 2014 Workshop – Comment Form

Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?

green space

What should change?

- more shops and restaurants
- bike lanes
- more parks

2—How would you improve the draft Vision Statement?

3—What ideas would you suggest for transportation enhancements and improving mobility?

hub idea is a good one

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the "menu" of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like

Does not beautify the area
services still limited

Dislike and would change or add

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like

Dislike and would change or add

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Like

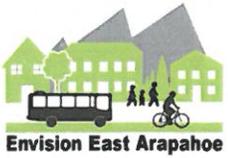
Dislike and would change or add

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

For the future, I would like to see:	= like = neutral = dislike	Comments and Ideas:
More light industry north of Arapahoe Avenue (<i>Current Trends scenario</i>)		
Affordable space for necessary services (e.g., car repair) (<i>Current Trends and Districts scenarios</i>)		
Boulder Community Health with nearby places for medical-related offices and small retail (<i>Districts scenario</i>)		
A new east/west connecting street (i.e., Walnut/48 th Street) (<i>Districts scenario</i>)		
Small shops, dining, offices and some apartments north of Arapahoe at 55 th , along 55 th St. (<i>Districts scenario</i>)		
Improvements to sidewalks and intersections (<i>Districts and Housing Choices scenarios</i>)		
Place for energy generation, recycling, eco-district (<i>Districts scenario</i>)		more it out of town
Small shops, dining, offices, and housing north of Arapahoe at 55 th , along 55 th St. in walk-friendly neighborhoods (<i>Housing Choices scenario</i>)		
More people walking and riding bicycles – make it safe and convenient (<i>Districts and Housing Choices scenarios</i>)		
Some new affordable, workforce housing north of Arapahoe Ave. (<i>Housing Choices scenario</i>)		
Flatiron Golf has new trails and community gardens, in addition to golf course (<i>Housing Choices scenario</i>)		As long as it stays green why not give access to more people
Ecological restoration along ditches and near creeks; better access to nature for current and future residents (<i>Housing Choices scenario</i>)		huge!
Arapahoe Ave. becomes a safe and attractive “boulevard” with street trees, noise buffering, safe speeds and transit		Love!
What other choices or ideas would you like to see?		

I think you should work on the BRT + some of the bike improvements that
now + come back to the land use when you are ready to zone/propose density that
will support biking/walking/transit the TMP.



Oct. 27, 2014 Workshop – Comment Form

Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain? Nothing	What should change? The suburban style development + the 6 lane hwy
--------------------------------	--

2—How would you improve the draft Vision Statement?

You are not being bold enough, If the time is not ready to do a bolder plan now, wait. Don't stick us with a lame suburban wasteland plan for the next 20yrs.

3—What ideas would you suggest for transportation enhancements and improving mobility?

Road diet
You can't make a liveable place with a 6 lane hwy down the middle

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the "menu" of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like Nothing	Dislike and would change or add Nothing Driveway, road, parking + 1 story bldgns.
-----------------	---

What do you like or dislike about the ideas presented in Scenario B: Districts?

Dislike and would change or add Not Bold enough
--

What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Dislike and would change or add Don't build more suburbia because you are afraid to bring up doing anything else,
--

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

☺ = like
 😐 = neutral
 ☹ = dislike

For the future, I would like to see:

Comments and Ideas:

More light industry north of Arapahoe Avenue
 (Current Trends scenario)

☺ ☹ ☹

There does need to be some light industrial but with less parking

Affordable space for necessary services (e.g., car repair)
 (Current Trends and Districts scenarios)

☺ ☹ ☹

Don't let this be the storage space entry to Boulder

Boulder Community Health with nearby places for medical-related offices and small retail
 (Districts scenario)

☺ ☹ ☹

yes

A new east/west connecting street (i.e., Walnut/48th Street)
 (Districts scenario)

☺ ☹ ☹

yes- Road diet

Small shops, dining, offices and some apartments north of Arapahoe at 55th, along 55th St.
 (Districts scenario)

☺ ☹ ☹

Improvements to sidewalks and intersections
 (Districts and Housing Choices scenarios)

☺ ☹ ☹

complete trail connection on N. side Ar.

Place for energy generation, recycling, eco-district
 (Districts scenario)

☺ ☹ ☹

Small shops, dining, offices, and housing north of Arapahoe at 55th, along 55th St. in walk-friendly neighborhoods
 (Housing Choices scenario)

☺ ☹ ☹

It needs to be dense enough so people have place to walk to + interesting

More people walking and riding bicycles – make it safe and convenient
 (Districts and Housing Choices scenarios)

☺ ☹ ☹

yes - same as above! you don't get biking + walking w/o

Some new affordable, workforce housing north of Arapahoe Ave.
 (Housing Choices scenario)

☺ ☹ ☹

more housing peoples scaled

Flatiron Golf has new trails and community gardens, in addition to golf course
 (Housing Choices scenario)

☺ ☹ ☹

Golf courses are bad land use places to walk + bike to

Ecological restoration along ditches and near creeks; better access to nature for current and future residents
 (Housing Choices scenario)

☺ ☹ ☹

Arapahoe Ave. becomes a safe and attractive "boulevard" with street trees, noise buffering, safe speeds and transit

☺ ☹ ☹

What other choices or ideas would you like to see?

Everyone wants walking, biking + transit but unless you are willing to propose dense, close, interesting places to walk + bike to, you won't

How did you find out about this meeting? Notice from Staff

Contact info (optional) Name: _____

Address: _____

Email: _____

Phone: _____

Get that. Because you chose not to take a bold approach it is kind of worthless - you will never get there. Either get some political will or drop it + come back later



Oct. 27, 2014 Workshop – Comment Form

Envision East Arapahoe Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?

What should change?

SENSE OF PLACE

2—How would you improve the draft Vision Statement?

DONT LIKE THE FUTURE GIVENS # 8
RETENTION WALL?

3—What ideas would you suggest for transportation enhancements and improving mobility?

BRT

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the “menu” of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like

Dislike and would change or add

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like

6

Dislike and would change or add

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Like

Dislike and would change or add

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

☺ = like
 😐 = neutral
 ☹ = dislike

Comments and Ideas:

For the future, I would like to see:

More light industry north of Arapahoe Avenue
 (*Current Trends scenario*) ☺ ☹ ☹

2024 to 2028

Affordable space for necessary services (e.g., car repair)
 (*Current Trends and Districts scenarios*) ☺ ☹ ☹

Boulder Community Health with nearby places for medical-related offices and small retail
 (*Districts scenario*) ☺ ☹ ☹

A new east/west connecting street (i.e., Walnut/48th Street)
 (*Districts scenario*) ☺ ☹ ☹

Small shops, dining, offices and some apartments north of Arapahoe at 55th, along 55th St.
 (*Districts scenario*) ☺ ☹ ☹

Improvements to sidewalks and intersections
 (*Districts and Housing Choices scenarios*) ☺ ☹ ☹

BRT

Place for energy generation, recycling, eco-district
 (*Districts scenario*) ☺ ☺ ☹

Small shops, dining, offices, and housing north of Arapahoe at 55th, along 55th St. in walk-friendly neighborhoods
 (*Housing Choices scenario*) ☺ ☺ ☹

More people walking and riding bicycles – make it safe and convenient
 (*Districts and Housing Choices scenarios*) ☺ ☹ ☹

Some new affordable, workforce housing north of Arapahoe Ave.
 (*Housing Choices scenario*) ☺ ☹ ☹

Flatiron Golf has new trails and community gardens, in addition to golf course
 (*Housing Choices scenario*) ☺ ☹ ☹

Ecological restoration along ditches and near creeks; better access to nature for current and future residents
 (*Housing Choices scenario*) ☺ ☹ ☹

2

Arapahoe Ave. becomes a safe and attractive "boulevard" with street trees, noise buffering, safe speeds and transit
 ☺ ☹ ☹

What other choices or ideas would you like to see?

BRT



Oct. 27, 2014 Workshop – Comment Form

Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

<p><i>What should remain?</i></p> <p>Affordable Retail - thrift shops light industry</p>	<p><i>What should change?</i></p> <p>less big business Remove smoke stacks from Power plant</p>
--	---

2—How would you improve the draft Vision Statement?

3—What ideas would you suggest for transportation enhancements and improving mobility?

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the “menu” of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

<p><i>Like</i></p>	<p><i>Dislike and would change or add</i></p> <p>No restaurants, no retail doesn't create a "district"</p>
--------------------	--

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

<p><i>Like</i></p> <p>really like 55th of Arapahoe district</p>	<p><i>Dislike and would change or add</i></p>
--	---

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

<p><i>Like</i></p> <p>Same as B - like 55th of Arapahoe district Thoughtful mix of retail residential</p>	<p><i>Dislike and would change or add</i></p> <p>Don't like residential north end of 55th - too far from focus of district - keep it on Arapahoe</p>
--	---

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

For the future, I would like to see:	☺ = like 😐 = neutral ☹ = dislike	Comments and Ideas:
More light industry north of Arapahoe Avenue <i>(Current Trends scenario)</i>	☺ ☐ ☹	
Affordable space for necessary services (e.g., car repair) <i>(Current Trends and Districts scenarios)</i>	☺ ☐ ☹	
Boulder Community Health with nearby places for medical-related offices and small retail <i>(Districts scenario)</i>	☺ ☐ ☹	
A new east/west connecting street (i.e., Walnut/48 th Street) <i>(Districts scenario)</i>	☐ ☺ ☹	
Small shops, dining, offices and some apartments north of Arapahoe at 55 th , along 55 th St. <i>(Districts scenario)</i>	☺ ☐ ☹	
Improvements to sidewalks and intersections <i>(Districts and Housing Choices scenarios)</i>	☺ ☐ ☹	
Place for energy generation, recycling, eco-district <i>(Districts scenario)</i>	☐ ☺ ☹	
Small shops, dining, offices, and housing north of Arapahoe at 55 th , along 55 th St. in walk-friendly neighborhoods <i>(Housing Choices scenario)</i>	☐ ☺ ☹	
More people walking and riding bicycles – make it safe and convenient <i>(Districts and Housing Choices scenarios)</i>	☺ ☐ ☹	
Some new affordable, workforce housing north of Arapahoe Ave. <i>(Housing Choices scenario)</i>	☐ ☐ ☹	
Flatiron Golf has new trails and community gardens, in addition to golf course <i>(Housing Choices scenario)</i>	☺ ☐ ☹	
Ecological restoration along ditches and near creeks; better access to nature for current and future residents <i>(Housing Choices scenario)</i>	☺ ☐ ☹	
Arapahoe Ave. becomes a safe and attractive “boulevard” with street trees, noise buffering, safe speeds and transit	☺ ☐ ☹	

What other choices or ideas would you like to see?



Oct. 27, 2014 Workshop – Comment Form

Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?

What should change?

We need more RTD ROUTES
SO THERE IS LESS TRAFFIC HEADING
IN and OUT OF TOWN

2—How would you improve the draft Vision Statement?

3—What ideas would you suggest for transportation enhancements and improving mobility?

WORK W/ RTD OR ROUTES COMING IN and OUT OF THE Boulder
JUNCTION

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the “menu” of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like

~~change B~~

Dislike and would change or add

DONT LIKE

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like

I LIKE THIS BUT NEED
MORE ROUTES AROUND
PEARL EAST and FLATIRON
PARK

Dislike and would change or add

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Like

We need housing but we
NEED ROUTES SO PEOPLE
CAN LIVE and WORK IN EAST
Boulder

Dislike and would change or add

~~use~~

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

☺ = like
 😐 = neutral
 ☹ = dislike

For the future, I would like to see:

Comments and Ideas:

More light industry north of Arapahoe Avenue
 (*Current Trends scenario*)



Affordable space for necessary services (e.g., car repair)
 (*Current Trends and Districts scenarios*)



Boulder Community Health with nearby places for medical-related offices and small retail
 (*Districts scenario*)



A new east/west connecting street (i.e., Walnut/48th Street)
 (*Districts scenario*)



Small shops, dining, offices and some apartments north of Arapahoe at 55th, along 55th St.
 (*Districts scenario*)



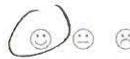
Improvements to sidewalks and intersections
 (*Districts and Housing Choices scenarios*)



Place for energy generation, recycling, eco-district
 (*Districts scenario*)



Small shops, dining, offices, and housing north of Arapahoe at 55th, along 55th St. in walk-friendly neighborhoods
 (*Housing Choices scenario*)



More people walking and riding bicycles – make it safe and convenient
 (*Districts and Housing Choices scenarios*)



Some new affordable, workforce housing north of Arapahoe Ave.
 (*Housing Choices scenario*)



Flatiron Golf has new trails and community gardens, in addition to golf course
 (*Housing Choices scenario*)



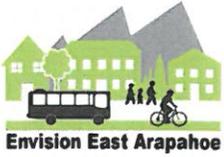
Ecological restoration along ditches and near creeks; better access to nature for current and future residents
 (*Housing Choices scenario*)



Arapahoe Ave. becomes a safe and attractive “boulevard” with street trees, noise buffering, safe speeds and transit



What other choices or ideas would you like to see?



Oct. 27, 2014 Workshop – Comment Form

Envision East Arapahoe *Please provide your ideas.*

1—What are your general comments and ideas for the future of the East Arapahoe area?

<i>What should remain?</i>	<i>What should change?</i>
----------------------------	----------------------------

2—How would you improve the draft **Vision Statement**?

3—What ideas would you suggest for **transportation enhancements and improving mobility**?

4—Do you have comments about the **future choices** (i.e., scenarios)? *(Note: The back side contains the “menu” of ideas)*

A. What do you like or dislike about the ideas presented in **Scenario A: Current Trends**?

<i>Like</i>	<i>Dislike and would change or add</i>
-------------	--

B. What do you like or dislike about the ideas presented in **Scenario B: Districts**?

<i>Like</i>	<i>Dislike and would change or add</i>
-------------	--

C. What do you like or dislike about the ideas presented in **Scenario C: Housing Choices**?

<i>Like</i>	<i>Dislike and would change or add</i>
-------------	--

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

For the future, I would like to see:	☺ = like 😐 = neutral ☹ = dislike	Comments and Ideas:
More light industry north of Arapahoe Avenue (<i>Current Trends scenario</i>)	☹ ☹ ☹	
Affordable space for necessary services (e.g., car repair) (<i>Current Trends and Districts scenarios</i>)	☹ ☹ ☹	
Boulder Community Health with nearby places for medical-related offices and small retail (<i>Districts scenario</i>)	☹ ☹ ☹	
A new east/west connecting street (i.e., Walnut/48 th Street) (<i>Districts scenario</i>)	☺ ☹ ☹	
Small shops, dining, offices and some apartments north of Arapahoe at 55 th , along 55 th St. (<i>Districts scenario</i>)	☹ ☹ ☹	
Improvements to sidewalks and intersections (<i>Districts and Housing Choices scenarios</i>)	☺ ☹ ☹	
Place for energy generation, recycling, eco-district (<i>Districts scenario</i>)	☹ ☹ ☹	
Small shops, dining, offices, and housing north of Arapahoe at 55 th , along 55 th St. in walk-friendly neighborhoods (<i>Housing Choices scenario</i>)	☺ ☹ ☹	
More people walking and riding bicycles – make it safe and convenient (<i>Districts and Housing Choices scenarios</i>)	☺ ☹ ☹	
Some new affordable, workforce housing north of Arapahoe Ave. (<i>Housing Choices scenario</i>)	☺ ☹ ☹	No buildings over 2-3 stories!!! The scenario playing out at Pearl & Arap. is a nightmare!
Flatiron Golf has new trails and community gardens, in addition to golf course (<i>Housing Choices scenario</i>)	☺ ☹ ☹	
Ecological restoration along ditches and near creeks; better access to nature for current and future residents (<i>Housing Choices scenario</i>)	☹ ☹ ☹	
Arapahoe Ave. becomes a safe and attractive “boulevard” with street trees, noise buffering, safe speeds and transit	☹ ☹ ☹	

What other choices or ideas would you like to see?



Oct. 27, 2014 Workshop – Comment Form

Envision East Arapahoe Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?	What should change?
---------------------	---------------------

2—How would you improve the draft Vision Statement?

3—What ideas would you suggest for transportation enhancements and improving mobility?

increase sensible rapid transit & public transportation
 increase bike friendly environment

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the “menu” of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like	Dislike and would change or add
------	---------------------------------

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like	Dislike and would change or add
------	---------------------------------

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Like	Dislike and would change or add no additional housing if it in any way resembles Pearl St (east of 30th)
------	---

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

For the future, I would like to see:	☺ = like 😐 = neutral ☹ = dislike	Comments and Ideas:
More light industry north of Arapahoe Avenue (<i>Current Trends scenario</i>)	☺ ☹ ☹	
Affordable space for necessary services (e.g., car repair) (<i>Current Trends and Districts scenarios</i>)	☺ ☹ ☹	
Boulder Community Health with nearby places for medical-related offices and small retail (<i>Districts scenario</i>)	☺ ☹ ☹	
A new east/west connecting street (i.e., Walnut/48 th Street) (<i>Districts scenario</i>)	☺ ☹ ☹	
Small shops, dining, offices and some apartments north of Arapahoe at 55 th , along 55 th St. (<i>Districts scenario</i>)	☺ ☹ ☹	THE HIGH DENSITY NEW CONSTRUCTION ALONG PEARL (EAST OF 30th) OPENED EYES. THIS TYPE OF DEVELOPMENT IS UNACCEPTABLE ALONG ARAP. ETC.
Improvements to sidewalks and intersections (<i>Districts and Housing Choices scenarios</i>)	☺ ☹ ☹	
Place for energy generation, recycling, eco-district (<i>Districts scenario</i>)	☺ ☹ ☹	
Small shops, dining, offices, and housing north of Arapahoe at 55 th , along 55 th St. in walk-friendly neighborhoods (<i>Housing Choices scenario</i>)	☺ ☹ ☹	
More people walking and riding bicycles – make it safe and convenient (<i>Districts and Housing Choices scenarios</i>)	☺ ☹ ☹	
Some new affordable, <u>workforce housing</u> north of Arapahoe Ave. (<i>Housing Choices scenario</i>)	☺ ☹ ☹	→ HIGH DENSITY PROBABLY THEREFORE UNACCEPTABLE
Flatiron Golf has new trails and community gardens, in addition to golf course (<i>Housing Choices scenario</i>)	☺ ☹ ☹	
Ecological restoration along ditches and near creeks; better access to nature for current and future residents (<i>Housing Choices scenario</i>)	☺ ☹ ☹	
Arapahoe Ave. becomes a safe and attractive "boulevard" with street trees, noise buffering, safe speeds and transit	☺ ☹ ☹	

What other choices or ideas would you like to see?

A comprehensive question(s) to RESIDENTS OF BOULDER, BOULDER COUNTY, LAFAYETTE, LOUISVILLE (As separate entities) to determine appetite for "growth"



Oct. 27, 2014 Workshop – Comment Form

Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

<p>What should remain?</p> <p>appreciate the proactive Comprehensive Plan for East Arapahoe development</p>	<p>What should change?</p>
---	----------------------------

2—How would you improve the draft Vision Statement?

3—What ideas would you suggest for transportation enhancements and improving mobility?

specific plans/ details regarding
parking and connectivity - cars

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the "menu" of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

<p>Like</p>	<p>Dislike and would change or add</p> <p>afraid that no plan for area would result in hodge-podge of buildings</p>
-------------	---

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

<p>Like</p> <p>like the concepts presented - good variety -</p>	<p>Dislike and would change or add</p>
---	--

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

<p>Like</p>	<p>Dislike and would change or add</p> <p>How dense will the 53 Arapahoe housing be?</p>
-------------	--

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

For the future, I would like to see:	☺ = like 😐 = neutral ☹ = dislike	Comments and Ideas:
More light industry north of Arapahoe Avenue (<i>Current Trends scenario</i>)	☑ ☹ ☹	
Affordable space for necessary services (e.g., car repair) (<i>Current Trends and Districts scenarios</i>)	☑ ☹ ☹	
Boulder Community Health with nearby places for medical-related offices and small retail (<i>Districts scenario</i>)	☑ ☹ ☹	
A new east/west connecting street (i.e., Walnut/48 th Street) (<i>Districts scenario</i>)	☑ ☹ ☹	definite need!
Small shops, dining, offices and some apartments north of Arapahoe at 55 th , along 55 th St. (<i>Districts scenario</i>)	☑ ☹ ☹	
Improvements to sidewalks and intersections (<i>Districts and Housing Choices scenarios</i>)	☑ ☹ ☹	
Place for energy generation, recycling, eco-district (<i>Districts scenario</i>)	☑ ☹ ☹	
Small shops, dining, offices, and housing north of Arapahoe at 55 th , along 55 th St. in walk-friendly neighborhoods (<i>Housing Choices scenario</i>)	☑ ☑ ☹	housing needs to be less dense than what is at Boulder Junction
More people walking and riding bicycles – make it safe and convenient (<i>Districts and Housing Choices scenarios</i>)	☹ ☑ ☹	also need planned parking for older population
Some new affordable, workforce housing north of Arapahoe Ave. (<i>Housing Choices scenario</i>)	☑ ☹ ☹	not too dense
Flatiron Golf has new trails and community gardens, in addition to golf course (<i>Housing Choices scenario</i>)	☑ ☹ ☹	
Ecological restoration along ditches and near creeks; better access to nature for current and future residents (<i>Housing Choices scenario</i>)	☑ ☹ ☹	
Arapahoe Ave. becomes a safe and attractive "boulevard" with street trees, noise buffering, safe speeds and transit	☑ ☹ ☹	

What other choices or ideas would you like to see?



Oct. 27, 2014 Workshop – Comment Form

Envision East Arapahoe Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?

Keep a height limit of 4 stories

What should change?

2—How would you improve the draft Vision Statement?

3—What ideas would you suggest for transportation enhancements and improving mobility?

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the “menu” of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like

Dislike and would change or add

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like

Mixed commercial with restaurants.

Dislike and would change or add

Don't force Eco-Pass on people that will not use it.

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Like

Dislike and would change or add

Dangerous housing next to train tracks.

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

😊 = like
 😐 = neutral
 ☹️ = dislike

Comments and Ideas:

For the future, I would like to see:

More light industry north of Arapahoe Avenue
 (Current Trends scenario)

😊 😐 ☹️

Dense office space w/ parking

Affordable space for necessary services (e.g., car repair)
 (Current Trends and Districts scenarios)

😊 😐 ☹️

Boulder Community Health with nearby places for medical-related offices and small retail
 (Districts scenario)

😊 😐 ☹️

A new east/west connecting street (i.e., Walnut/48th Street)
 (Districts scenario)

😊 😐 ☹️

Small shops, dining, offices, and some apartments north of Arapahoe at 55th, along 55th St.
 (Districts scenario)

😊 😐 ☹️

Improvements to sidewalks and intersections
 (Districts and Housing Choices scenarios)

😊 😐 ☹️

Traffic light at MacArthur.
 Improved sidewalk MacArthur to 48th on Southside Arapahoe

Place for energy generation, recycling, eco-district
 (Districts scenario)

😊 😐 ☹️

Small shops, dining, offices, and ~~business~~ north of Arapahoe at 55th, along 55th St. in walk-friendly neighborhoods
 (Housing Choices scenario)

😊 😐 ☹️

More people walking and riding bicycles – make it safe and convenient
 (Districts and Housing Choices scenarios)

😊 😐 ☹️

Some new affordable, workforce housing north of Arapahoe Ave.
 (Housing Choices scenario)

😊 😐 ☹️

Flatiron Golf has new trails and community gardens, in addition to golf course
 (Housing Choices scenario)

😊 😐 ☹️

Ecological restoration along ditches and near creeks; better access to nature for current and future residents
 (Housing Choices scenario)

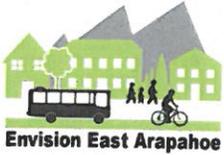
😊 😐 ☹️

Arapahoe Ave. becomes a safe and attractive "boulevard" with street trees, noise buffering, safe speeds and transit

😊 😐 ☹️

Current buses are fine, we don't need a BRT lane.

What other choices or ideas would you like to see?



Oct. 27, 2014 Workshop – Comment Form

Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?

FOCUS ON HIGH PAYING JOB
SECTOR

What should change?

BETTER TRANSIT - e.g. REGULAR
BUS SERVICE, SAFER BIKE/WALK
MORE SHOPS/SERVICES - FOOD,
DRY CLEANERS, DAY CARE
CLEAN UP LOOK OF LIGHT INDUSTRIAL

2—How would you improve the draft Vision Statement?

3—What ideas would you suggest for transportation enhancements and improving mobility?

BRT GOOD, LOW NOISE LOW POLLUTING
SLOWER CAR SPEEDS, SAFER BIKING/WALKING

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the "menu" of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like

Dislike and would change or add

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like

LOOKS GREAT W/ MORE TREES,
PEDESTRIAN FRIENDLY AREAS, PARKS

Dislike and would change or add

DON'T GO TOO FAR WITH
HEIGHT/DENSITY.

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Like

Dislike and would change or add

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

For the future, I would like to see:	= like = neutral = dislike	Comments and Ideas:
More light industry north of Arapahoe Avenue (<i>Current Trends scenario</i>)		
Affordable space for necessary services (e.g., car repair) (<i>Current Trends and Districts scenarios</i>)		
Boulder Community Health with nearby places for medical-related offices and small retail (<i>Districts scenario</i>)		
A new east/west connecting street (i.e., Walnut/48 th Street) (<i>Districts scenario</i>)		
Small shops, dining, offices and some apartments north of Arapahoe at 55 th , along 55 th St. (<i>Districts scenario</i>)		
Improvements to sidewalks and intersections (<i>Districts and Housing Choices scenarios</i>)		
Place for energy generation, recycling, eco-district (<i>Districts scenario</i>)		
Small shops, dining, offices, and housing north of Arapahoe at 55 th , along 55 th St. in walk-friendly neighborhoods (<i>Housing Choices scenario</i>)		
More people walking and riding bicycles – make it safe and convenient (<i>Districts and Housing Choices scenarios</i>)		
Some new affordable, workforce housing north of Arapahoe Ave. (<i>Housing Choices scenario</i>)		
Flatiron Golf has new trails and community gardens, in addition to golf course (<i>Housing Choices scenario</i>)		
Ecological restoration along ditches and near creeks; better access to nature for current and future residents (<i>Housing Choices scenario</i>)		
Arapahoe Ave. becomes a safe and attractive “boulevard” with street trees, noise buffering, safe speeds and transit		
<i>What other choices or ideas would you like to see?</i>		



Oct. 27, 2014 Workshop – Comment Form

Envision East Arapahoe Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?

What should change?

Glad to see the planning boards are looking at a comprehensive plan to handle growth well.

2—How would you improve the draft Vision Statement?

3—What ideas would you suggest for transportation enhancements and improving mobility?

Consider all age populations and in addition to parking spots for bikes, etc. provide adequate parking for cars.

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the "menu" of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like

Dislike and would change or add

No, it needs planning.

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like

Dislike and would change or add

Favor

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Like

Dislike and would change or add

Not sure

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

😊 = like
 😐 = neutral
 😞 = dislike

For the future, I would like to see:

Comments and Ideas:

More light industry north of Arapahoe Avenue
 (*Current Trends scenario*)

😊 😐 😞

Affordable space for necessary services (e.g., car repair)
 (*Current Trends and Districts scenarios*)

😊 😐 😞

Boulder Community Health with nearby places for medical-related offices and small retail
 (*Districts scenario*)

😊 😐 😞

A new east/west connecting street (i.e., Walnut/48th Street)
 (*Districts scenario*)

😊 😐 😞

Small shops, dining, offices and some apartments north of Arapahoe at 55th, along 55th St.
 (*Districts scenario*)

😊 😐 😞

Take a picture of the apt built on 30th Pearl and VOW, never to replicate this on Arapahoe or any place else in Boulder.

Improvements to sidewalks and intersections
 (*Districts and Housing Choices scenarios*)

😊 😐 😞

Place for energy generation, recycling, eco-district
 (*Districts scenario*)

😊 😐 😞

Small shops, dining, offices, and housing north of Arapahoe at 55th, along 55th St. in walk-friendly neighborhoods
 (*Housing Choices scenario*)

😊 😐 😞

More people walking and riding bicycles – make it safe and convenient
 (*Districts and Housing Choices scenarios*)

😊 😐 😞

Yes, but make sure you provide parking for cars.

Some new affordable, workforce housing north of Arapahoe Ave.
 (*Housing Choices scenario*)

😊 😐 😞

Flatiron Golf has new trails and community gardens, in addition to golf course
 (*Housing Choices scenario*)

😊 😐 😞

Ecological restoration along ditches and near creeks; better access to nature for current and future residents
 (*Housing Choices scenario*)

😊 😐 😞

Arapahoe Ave. becomes a safe and attractive "boulevard" with street trees, noise buffering, safe speeds and transit

😊 😐 😞

What other choices or ideas would you like to see?



Oct. 27, 2014 Workshop – Comment Form

Envision East Arapahoe Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?

The rural neighborhoods

What should change?

Nothing
All your changes bring increased congestion - IN THE FLOOD PLAIN

2—How would you improve the draft Vision Statement?

Let Boulder be rural along Arapahoe

3—What ideas would you suggest for transportation enhancements and improving mobility?

Your ideas bring lots of congestion —
leave us alone!

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the “menu” of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like

No new housing

Dislike and would change or add

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like

Dislike and would change or add

Retail - don't need it!
Recycling center already exists!
No additional housing

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Like

Dislike and would change or add

HATE THIS OPTION!
This is a rural neighborhood!!!
This options bring PEOPLE
CARS + PARKING LOTS

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

😊 = like
 😐 = neutral
 ☹️ = dislike

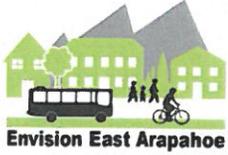
For the future, I would like to see:

Comments and Ideas:

More light industry north of Arapahoe Avenue (Current Trends scenario)	😊 😐 ☹️	Light industry is better than housing.
Affordable space for necessary services (e.g., car repair) (Current Trends and Districts scenarios)	😊 😐 ☹️	We already have car repair
Boulder Community Health with nearby places for medical-related offices and small retail (Districts scenario)	😊 ☹️ 😐	
A new east/west connecting street (i.e., Walnut/48 th Street) (Districts scenario)	😊 😐 ☹️	
Small shops, dining, offices and some apartments north of Arapahoe at 55 th , along 55 th St. (Districts scenario)	😊 ☹️ 😐	
Improvements to sidewalks and intersections (Districts and Housing Choices scenarios)	😊 😐 ☹️	IT IS FINE TO improve sidewalks, but NOT if it means OPTION C !! or B!
Place for energy generation, recycling, eco-district (Districts scenario)	😊 😐 ☹️	
Small shops, dining, offices, and housing north of Arapahoe at 55 th , along 55 th St. in walk-friendly neighborhoods (Housing Choices scenario)	😊 😐 ☹️	
More people walking and riding bicycles – make it safe and convenient (Districts and Housing Choices scenarios)	😊 😐 ☹️	We already have lots of people walking + riding bicycles
Some new affordable, workforce housing north of Arapahoe Ave. (Housing Choices scenario)	😊 😐 ☹️	
Flatiron Golf has new trails and community gardens, in addition to golf course (Housing Choices scenario)	😊 😐 ☹️	Leave the golf course alone
Ecological restoration along ditches and near creeks; better access to nature for current and future residents (Housing Choices scenario)	😊 😐 ☹️	
Arapahoe Ave. becomes a safe and attractive "boulevard" with street trees, noise buffering, safe speeds and transit	😊 😐 ☹️	This is a nice idea, but NOT if it means high rise development

What other choices or ideas would you like to see?

I live on one acre of land on Old Tale, just 3 houses from Arapahoe - I would like to see parks, but definitely NOT high rise housing on Arapahoe -



Oct. 27, 2014 Workshop – Comment Form

Envision East Arapahoe Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

<p><i>What should remain?</i> Light industry uses are good, but can actually be integrated with higher density + residential. I'd love to live in a place where people are making things.</p>	<p><i>What should change?</i> NEED MORE PEOPLE. Sticking w/ the ~4300 jobs/residents is TOTALLY USELESS, and makes this whole exercise a waste of everyone's time.</p>
---	--

2—How would you improve the draft Vision Statement?

maintaining current use intensity is incompatible w/ most of the Vision. "Socially Thriving", "Livable" "sustainable" "safe" "connected". Good luck. It's a freeway surrounded by parking craters.

3—What ideas would you suggest for transportation enhancements and improving mobility?

slow traffic on Arapahoe down! virtually impossible to cross now. Make it easier. Real BRT!

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the "menu" of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

<p><i>Like</i> basically nothing. This development pattern is why the US is fat, broke, and dependent on oil.</p>	<p><i>Dislike and would change or add</i> If we can't fix the land use intensity out here, just STOP. Get the BRT in and leave the area alone rather than waste a bunch of time to make a lousy plan. Maybe focus on the area between TVAP + East CU.</p>
---	---

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

<p><i>Like</i></p>	<p><i>Dislike and would change or add</i> a bunch of time to make a lousy plan. Maybe focus on the area between TVAP + East CU.</p>
--------------------	---

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

<p><i>Like</i> mix of uses would be better, but not much.</p>	<p><i>Dislike and would change or add</i> add 10,000 to 30,000 more jobs/people.</p>
---	--

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

😊 = like
 😐 = neutral
 ☹️ = dislike

Comments and Ideas:

For the future, I would like to see:

More light industry north of Arapahoe Avenue
 (*Current Trends scenario*)

😊 😐 ☹️ good if intensified

Affordable space for necessary services (e.g., car repair)
 (*Current Trends and Districts scenarios*)

😊 😐 ☹️ We can provide these amenities so much more space efficiently.

Boulder Community Health with nearby places for medical-related offices and small retail
 (*Districts scenario*)

😊 😐 ☹️

A new east/west connecting street (i.e., Walnut/48th Street)
 (*Districts scenario*)

😊 😐 ☹️ yes! break up the impermeable super blocks!

Small shops, dining, offices and some apartments north of Arapahoe at 55th, along 55th St.
 (*Districts scenario*)

😊 😐 ☹️

Improvements to sidewalks and intersections
 (*Districts and Housing Choices scenarios*)

😊 😐 ☹️

Place for energy generation, recycling, eco-district
 (*Districts scenario*)

😊 😐 ☹️

Small shops, dining, offices, and housing north of Arapahoe at 55th, along 55th St. in walk-friendly neighborhoods
 (*Housing Choices scenario*)

😊 😐 ☹️

NONE OF THESE MAKE SENSE W/O MUCH HIGHER LAND USE INTENSITY

More people walking and riding bicycles – make it safe and convenient
 (*Districts and Housing Choices scenarios*)

😊 😐 ☹️

Not going to happen w/o increasing intensity a bunch.

Some new affordable, workforce housing north of Arapahoe Ave.
 (*Housing Choices scenario*)

😊 😐 ☹️

how about 10,000 dwelling units?

Flatiron Golf has new trails and community gardens, in addition to golf course
 (*Housing Choices scenario*)

😊 😐 ☹️

I hate golf courses.

Ecological restoration along ditches and near creeks; better access to nature for current and future residents
 (*Housing Choices scenario*)

😊 😐 ☹️

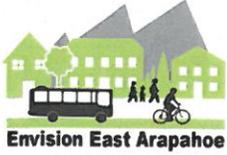
Sure. Keep the greenways green, don't build in high hazard zones. Pub.

Arapahoe Ave. becomes a safe and attractive "boulevard" with street trees, noise buffering, safe speeds and transit

😊 😐 ☹️

What's the point if we don't allow lots more people and activity to inhabit the area?

What other choices or ideas would you like to see?



Oct. 27, 2014 Workshop – Comment Form

Envision East Arapahoe *Please provide your ideas.*

1—What are your general comments and ideas for the future of the East Arapahoe area?

<i>What should remain?</i>	<i>What should change?</i>
----------------------------	----------------------------

2—How would you improve the draft **Vision Statement**?

3—What ideas would you suggest for **transportation enhancements and improving mobility**?

4—Do you have comments about the **future choices** (i.e., scenarios)? *(Note: The back side contains the “menu” of ideas)*

A. What do you like or dislike about the ideas presented in **Scenario A: Current Trends**?

<i>Like</i>	<i>Dislike and would change or add</i>
-------------	--

B. What do you like or dislike about the ideas presented in **Scenario B: Districts**?

<i>Like</i>	<i>Dislike and would change or add</i>
-------------	--

C. What do you like or dislike about the ideas presented in **Scenario C: Housing Choices**?

<i>Like</i>	<i>Dislike and would change or add</i>
-------------	--

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

For the future, I would like to see:	☺ = like 😐 = neutral ☹ = dislike	Comments and Ideas:
More light industry north of Arapahoe Avenue (<i>Current Trends scenario</i>)	☺ ☹ ☹	have to accommodate some where have been good neighbor
Affordable space for necessary services (e.g., car repair) (<i>Current Trends and Districts scenarios</i>)	☺ ☺ ☹	
Boulder Community Health with nearby places for medical-related offices and small retail (<i>Districts scenario</i>)	☹ ☺ ☹	good paying jobs environmental steward
A new east/west connecting street (i.e., Walnut/48 th Street) (<i>Districts scenario</i>)	☺ ☹ ☹	never thought about it
Small shops, dining, offices and some apartments north of Arapahoe at 55 th , along 55 th St. (<i>Districts scenario</i>)	☹ ☹ ☹ ☺ ☹	demographic wld support shopping & dining
Improvements to sidewalks and intersections (<i>Districts and Housing Choices scenarios</i>)	☹ ☺ ☹	desperately needed
Place for energy generation, recycling, eco-district (<i>Districts scenario</i>)	☹ ☺ ☹	nurture these uses
Small shops, dining, offices, and housing north of Arapahoe at 55 th , along 55 th St. in walk-friendly neighborhoods (<i>Housing Choices scenario</i>)	☹ ☺ ☹	
More people walking and riding bicycles – make it safe and convenient (<i>Districts and Housing Choices scenarios</i>)	☹ ☺ ☹	
Some new affordable, workforce housing north of Arapahoe Ave. (<i>Housing Choices scenario</i>)	☹ ☺ ☹	
Flatiron Golf has new trails and community gardens, in addition to golf course (<i>Housing Choices scenario</i>)	☹ ☺ ☹	
Ecological restoration along ditches and near creeks; better access to nature for current and future residents (<i>Housing Choices scenario</i>)	☹ ☺ ☹	
Arapahoe Ave. becomes a safe and attractive "boulevard" with street trees, noise buffering, safe speeds and transit	☺ ☹ ☹	It's still a key arterial, and str. Need to accommodate all modes

What other choices or ideas would you like to see?

what I think is missing is explaining to existing residents how the evolution of Arapahoe can positively impact them

make linkage to how families will benefit from transit/infill/densification



Oct. 27, 2014 Workshop – Comment Form

Envision East Arapahoe Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?

opportunities for startup businesses to incubate their biz plan in an affordable space, especially for businesses associated with Recycle Row

What should change?

Alternative transportation and amenities for area workforce improvements would be great

2—How would you improve the draft Vision Statement?

It's all there – what gets cut as we move forward is the key.

3—What ideas would you suggest for transportation enhancements and improving mobility?

Bike Share, finishing the connections between bike paths and walkways

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the “menu” of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like

affordable warehouse space

Dislike and would change or add

no neighborhood feel, not pretty, east county car commuting puts pressure on all modes of transport in the neighborhood

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like

better amenities for area workforce

Dislike and would change or add

any planned development necessarily increases rental rates

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Like

at some point a developer will try to put in housing, so very important to plan it.

Dislike and would change or add

would need to really prioritize affordable warehouse rent, perhaps like affordable housing program. How that works with market forces like pot grow operators I'm not sure!

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

☺ = like
 😐 = neutral
 ☹ = dislike

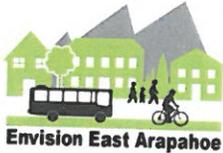
For the future, I would like to see:

Comments and Ideas:

More light industry north of Arapahoe Avenue (Current Trends scenario)	☺ ☐ ☹	more isn't necessarily as important as affordable space for new biz incubation
Affordable space for necessary services (e.g., car repair) (Current Trends and Districts scenarios)	☺ ☐ ☹	
Boulder Community Health with nearby places for medical-related offices and small retail (Districts scenario)	☐ ☺ ☹	
A new east/west connecting street (i.e., Walnut/48 th Street) (Districts scenario)	☐ ☺ ☹	not sure this is worth messing with S. Boulder Creek greenway
Small shops, dining, offices and some apartments north of Arapahoe at 55 th , along 55 th St. (Districts scenario)	☐ ☺ ☹	
Improvements to sidewalks and intersections (Districts and Housing Choices scenarios)	☺ ☐ ☹	
Place for energy generation, recycling, eco-district (Districts scenario)	☺ ☐ ☹	Prioritize the services of Recycle Row, including expansion
Small shops, dining, offices, and housing north of Arapahoe at 55 th , along 55 th St. in walk-friendly neighborhoods (Housing Choices scenario)	☐ ☐ ☹	I think this would significantly increase warehouse lease rates
More people walking and riding bicycles – make it safe and convenient (Districts and Housing Choices scenarios)	☺ ☐ ☹	
Some new affordable, workforce housing north of Arapahoe Ave. (Housing Choices scenario)	☐ ☺ ☹	If housing is added, it should prioritize housing for the area workforce
Flatiron Golf has new trails and community gardens, in addition to golf course (Housing Choices scenario)	☐ ☺ ☹	interesting idea ...
Ecological restoration along ditches and near creeks; better access to nature for current and future residents (Housing Choices scenario)	☺ ☐ ☹	
Arapahoe Ave. becomes a safe and attractive "boulevard" with street trees, noise buffering, safe speeds and transit	☺ ☐ ☹	

What other choices or ideas would you like to see?

Simple improvements such as plowing the multi-use path east of 55th and working on bike and ped connectors would help a lot.



Oct. 27, 2014 Workshop – Comment Form

Envision East Arapahoe Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?

LOW HEIGHT OF BLDGS.
NO 55' BUILDINGS.
KEEP VIEWS.

What should change?

2—How would you improve the draft Vision Statement?

3—What ideas would you suggest for transportation enhancements and improving mobility?

Have most of the transportation designs in place before all of the housing.

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the "menu" of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like

Dislike and would change or add

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like

Dislike and would change or add

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Like

Dislike and would change or add

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

☺ = like
 😐 = neutral
 ☹ = dislike

For the future, I would like to see:

Comments and Ideas:

More light industry north of Arapahoe Avenue
 (*Current Trends scenario*)

☺ ☹ ☹

Affordable space for necessary services (e.g., car repair)
 (*Current Trends and Districts scenarios*)

☺ ☹ ☹

Boulder Community Health with nearby places for medical-related offices and small retail
 (*Districts scenario*)

☺ ☹ ☹

A new east/west connecting street (i.e., Walnut/48th Street)
 (*Districts scenario*)

☺ ☹ ☹

Small shops, dining, offices and some apartments north of Arapahoe at 55th, along 55th St.
 (*Districts scenario*)

☺ ☹ ☹

SOME APARTMENTS - NO BIG SS' COMPLEXES.

Improvements to sidewalks and intersections
 (*Districts and Housing Choices scenarios*)

☺ ☹ ☹

Place for energy generation, recycling, eco-district
 (*Districts scenario*)

☺ ☹ ☹

Small shops, dining, offices, and housing north of Arapahoe at 55th, along 55th St. in walk-friendly neighborhoods
 (*Housing Choices scenario*)

☺ ☹ ☹

~~Housing~~ Housing NOT 55' HIGH. KEEP TO 35' WITH SETBACKS.

More people walking and riding bicycles – make it safe and convenient
 (*Districts and Housing Choices scenarios*)

☺ ☹ ☹

Probably won't really happen. People need cars.

Some new affordable, workforce housing north of Arapahoe Ave.
 (*Housing Choices scenario*)

☺ ☹ ☹

Developers will just build expensive places & give \$ to the city for affordable somewhere else.

Flatiron Golf has new trails and community gardens, in addition to golf course
 (*Housing Choices scenario*)

☺ ☹ ☹

Ecological restoration along ditches and near creeks; better access to nature for current and future residents
 (*Housing Choices scenario*)

☺ ☹ ☹

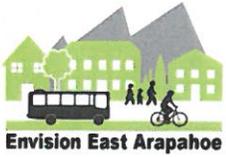
Arapahoe Ave. becomes a safe and attractive "boulevard" with street trees, noise buffering, safe speeds and transit

☺ ☹ ☹

WHAT ABOUT COMMUTERS FROM EAST OF BOULDER? THEY USE ARAPAHOE.

What other choices or ideas would you like to see?

HOLD ON THIS UNTIL THE COMPREHENSIVE PLAN IS DONE.



Oct. 27, 2014 Workshop – Comment Form

Envision East Arapahoe Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?	What should change? less pot
---------------------	---------------------------------

2—How would you improve the draft Vision Statement?

It's too broad

3—What ideas would you suggest for transportation enhancements and improving mobility?

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the "menu" of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like people can still buy houses with land that don't start @ \$1M. It's more affordable for Boulder	Dislike and would change or add
---	---------------------------------

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like my favorite option	Dislike and would change or add make it a special trip to E Blvd - not more housing
----------------------------	--

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Like	Dislike and would change or add It makes me nervous w/ existing traffic issues
------	---

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

😊 = like
 😐 = neutral
 ☹️ = dislike

For the future, I would like to see:

Comments and Ideas:

More light industry north of Arapahoe Avenue
 (Current Trends scenario)

😊 😐 ☹️

Affordable space for necessary services (e.g., car repair)
 (Current Trends and Districts scenarios)

😊 😐 ☹️

Boulder Community Health with nearby places for medical-related offices and small retail
 (Districts scenario)

😊 😐 ☹️

needs more food options

A new east/west connecting street (i.e., Walnut/48th Street)
 (Districts scenario)

😊 😐 ☹️

Small shops, dining, offices and some apartments north of Arapahoe at 55th, along 55th St.
 (Districts scenario)

😊 😐 ☹️

Improvements to sidewalks and intersections
 (Districts and Housing Choices scenarios)

😊 😐 ☹️

Place for energy generation, recycling, eco-district
 (Districts scenario)

😊 😐 ☹️

enough of those already

Small shops, dining, offices, and housing north of Arapahoe at 55th, along 55th St. in walk-friendly neighborhoods
 (Housing Choices scenario)

😊 😐 ☹️

More people walking and riding bicycles – make it safe and convenient
 (Districts and Housing Choices scenarios)

😊 😐 ☹️

Some new affordable, workforce housing north of Arapahoe Ave.
 (Housing Choices scenario)

😊 😐 ☹️

Flatiron Golf has new trails and community gardens, in addition to golf course
 (Housing Choices scenario)

😊 😐 ☹️

Ecological restoration along ditches and near creeks; better access to nature for current and future residents
 (Housing Choices scenario)

😊 😐 ☹️

flood mitigation would be my first choice

Arapahoe Ave. becomes a safe and attractive "boulevard" with street trees, noise buffering, safe speeds and transit

😊 😐 ☹️

What other choices or ideas would you like to see?

I'm concerned this whole development will snowball into another sub city & the roads can't handle it.



Oct. 27, 2014 Workshop – Comment Form

Envision East Arapahoe Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?

What should change?

hard edge for development @ east edge of city.
should be very clear when you have entered Boulder

2—How would you improve the draft Vision Statement?

3—What ideas would you suggest for transportation enhancements and improving mobility?

make any new side streets in the area narrow — like in downtown/the Hill
no 50' curb to curb side streets. reduce setbacks, make the buildings form an outdoor room

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the “menu” of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like

not much.

Dislike and would change or add

It stinks for biking

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like

Dislike and would change or add

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Like

Dislike and would change or add

More housing

--- Over ---

I support some mixed these two options

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

☺ = like
 😐 = neutral
 ☹ = dislike

For the future, I would like to see:

Comments and Ideas:

More light industry north of Arapahoe Avenue
 (Current Trends scenario) ☺ ☹ ☹ but w/ less surface P

Affordable space for necessary services (e.g., car repair)
 (Current Trends and Districts scenarios) ☺ ☹ ☹

Boulder Community Health with nearby places for medical-related offices and small retail
 (Districts scenario) ☹ ☺ ☹ but w/ less parking

A new east/west connecting street (i.e., Walnut/48th Street)
 (Districts scenario) ☺ ☹ ☹ YES YES YES

Small shops, dining, offices and some apartments north of Arapahoe at 55th, along 55th St.
 (Districts scenario) ☺ ☹ ☹

Improvements to sidewalks and intersections
 (Districts and Housing Choices scenarios) ☺ ☹ ☹ Yes Yes Yes

Place for energy generation, recycling, eco-district
 (Districts scenario) ☺ ☹ ☹

Small shops, dining, offices, and housing north of Arapahoe at 55th, along 55th St. in walk-friendly neighborhoods
 (Housing Choices scenario) ☺ ☹ ☹

More people walking and riding bicycles – make it safe and convenient
 (Districts and Housing Choices scenarios) ☺ ☹ ☹

Some new affordable, workforce housing north of Arapahoe Ave.
 (Housing Choices scenario) ☺ ☹ ☹

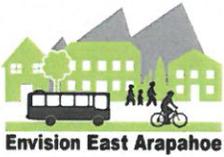
Flatiron Golf has new trails and community gardens, in addition to golf course
 (Housing Choices scenario) ☺ ☹ ☹ but get rid of Golf course

turn into Sports Fields

Ecological restoration along ditches and near creeks; better access to nature for current and future residents
 (Housing Choices scenario) ☺ ☹ ☹

Arapahoe Ave. becomes a safe and attractive "boulevard" with street trees, noise buffering, safe speeds and transit
 ☺ ☹ ☹

What other choices or ideas would you like to see?



Oct. 27, 2014 Workshop – Comment Form

Envision East Arapahoe Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?	What should change?
Do Not Bring in big buildings Keep this as a transition of low density, low height buildings	→

2—How would you improve the draft Vision Statement?

NO BRT
 NO Big Buildings

3—What ideas would you suggest for transportation enhancements and improving mobility?

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the “menu” of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like	Dislike and would change or add
I like Keeping things as is. Maybe add a few 2-3 story office Bldgs & A couple of restaurants	

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like	Dislike and would change or add
Deceptive - Planning Board member said - "Don't do visuals with 55 ft Buildings that will only upset the public	

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Like	Dislike and would change or add
Outrageous - I don't want to live in a Jetson City	

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

😊 = like
 😐 = neutral
 ☹️ = dislike

For the future, I would like to see:

Comments and Ideas:

More light industry north of Arapahoe Avenue
 (*Current Trends scenario*)

☹️ 😐 😐

Affordable space for necessary services (e.g., car repair)
 (*Current Trends and Districts scenarios*)

😊 ☹️ 😐

Boulder Community Health with nearby places for medical-related offices and small retail
 (*Districts scenario*)

☹️ 😐 😐

A new east/west connecting street (i.e., Walnut/48th Street)
 (*Districts scenario*)

😊 😐 ☹️

Small shops, dining, offices and some apartments north of Arapahoe at 55th, along 55th St.
 (*Districts scenario*)

😊 😐 ☹️

Don't lump together Dining, offices, small shops OK but no more housing
 No yellow flashing lights

Improvements to sidewalks and intersections
 (*Districts and Housing Choices scenarios*)

😊 ☹️ 😐

Place for energy generation, recycling, eco-district
 (*Districts scenario*)

😊 ☹️ 😐

Small shops, dining, offices, and housing north of Arapahoe at 55th, along 55th St. in walk-friendly neighborhoods
 (*Housing Choices scenario*)

😊 😐 😐

Don't lump

More people walking and riding bicycles – make it safe and convenient
 (*Districts and Housing Choices scenarios*)

😊 ☹️ 😐

Some new affordable, workforce housing north of Arapahoe Ave.
 (*Housing Choices scenario*)

😊 😐 ☹️

Flatiron Golf has new trails and community gardens, in addition to golf course
 (*Housing Choices scenario*)

😊 ☹️ 😐

They just put 2,000,000 into 58 in ideas - Not changing golf course

Ecological restoration along ditches and near creeks; better access to nature for current and future residents
 (*Housing Choices scenario*)

☹️ 😐 😐

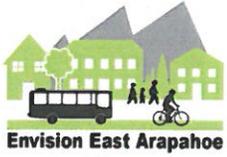
Arapahoe Ave. becomes a safe and attractive "boulevard" with street trees, noise buffering, safe speeds and transit

😊 😐 😐

NO trees Keep View of Mts. Won't be attractive with Bart down the middle

What other choices or ideas would you like to see?

Allow public to speak at these meetgs. Planning Dept Does there sell why not allow residents to speak!!



Oct. 27, 2014 Workshop – Comment Form

Envision East Arapahoe Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?

Golf Course All 18 holes -
 A water retention isn't the answer - the water flowing under Arapahoe to creek is needed

What should change?

A park and low density housing and a small number shops -
Nothing over 3 stories

2—How would you improve the draft Vision Statement?

VERY AGAINST Flatirons
 Having ANY FLOOD MITIGATION!!!

3—What ideas would you suggest for transportation enhancements and improving mobility?

Better side walks / Bike lane options

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the “menu” of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like	Dislike and would change or add
------	---------------------------------

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like	Dislike and would change or add
------	---------------------------------

No change to Flatirons golf course especially Flood Mitigation

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

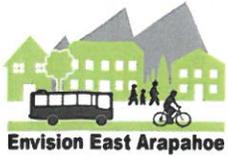
Like	Dislike and would change or add
------	---------------------------------

No Building over 3 stories

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

For the future, I would like to see:		Comments and Ideas:
	😊 = like 😐 = neutral 😞 = dislike	
More light industry north of Arapahoe Avenue (<i>Current Trends scenario</i>)	<input checked="" type="checkbox"/> 😊 <input type="checkbox"/> 😐 <input type="checkbox"/> 😞	
Affordable space for necessary services (e.g., car repair) (<i>Current Trends and Districts scenarios</i>)	<input checked="" type="checkbox"/> 😊 <input type="checkbox"/> 😐 <input type="checkbox"/> 😞	
Boulder Community Health with nearby places for medical-related offices and small retail (<i>Districts scenario</i>)	<input checked="" type="checkbox"/> 😊 <input type="checkbox"/> 😐 <input type="checkbox"/> 😞	
A new east/west connecting street (i.e., Walnut/48 th Street) (<i>Districts scenario</i>)	<input type="checkbox"/> 😊 <input type="checkbox"/> 😐 <input checked="" type="checkbox"/> 😞	
Small shops, dining, offices and some apartments north of Arapahoe at 55 th , along 55 th St. (<i>Districts scenario</i>)	<input checked="" type="checkbox"/> 😊 <input type="checkbox"/> 😐 <input type="checkbox"/> 😞	3 story limit
Improvements to sidewalks and intersections (<i>Districts and Housing Choices scenarios</i>)	<input checked="" type="checkbox"/> 😊 <input type="checkbox"/> 😐 <input type="checkbox"/> 😞	
Place for energy generation, recycling, eco-district (<i>Districts scenario</i>)	<input type="checkbox"/> 😊 <input type="checkbox"/> 😐 <input checked="" type="checkbox"/> 😞	
Small shops, dining, offices, and housing north of Arapahoe at 55 th , along 55 th St. in walk-friendly neighborhoods (<i>Housing Choices scenario</i>)	<input checked="" type="checkbox"/> 😊 <input type="checkbox"/> 😐 <input type="checkbox"/> 😞	
More people walking and riding bicycles – make it safe and convenient (<i>Districts and Housing Choices scenarios</i>)	<input checked="" type="checkbox"/> 😊 <input type="checkbox"/> 😐 <input type="checkbox"/> 😞	
Some new affordable, workforce housing north of Arapahoe Ave. (<i>Housing Choices scenario</i>)	<input type="checkbox"/> 😊 <input type="checkbox"/> 😐 <input checked="" type="checkbox"/> 😞	
Flatiron Golf has new trails and community gardens, in addition to golf course (<i>Housing Choices scenario</i>)	<input type="checkbox"/> 😊 <input type="checkbox"/> 😐 <input checked="" type="checkbox"/> 😞	
Ecological restoration along ditches and near creeks; better access to nature for current and future residents (<i>Housing Choices scenario</i>)	<input checked="" type="checkbox"/> 😊 <input type="checkbox"/> 😐 <input type="checkbox"/> 😞	
Arapahoe Ave. becomes a safe and attractive “boulevard” with street trees, noise buffering, safe speeds and transit	<input checked="" type="checkbox"/> 😊 <input type="checkbox"/> 😐 <input type="checkbox"/> 😞	
<i>What other choices or ideas would you like to see?</i>		



Oct. 27, 2014 Workshop – Comment Form

Envision East Arapahoe *Please provide your ideas.*

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?

What should change?

2—How would you improve the draft **Vision Statement**?

3—What ideas would you suggest for **transportation enhancements and improving mobility**?

4—Do you have comments about the **future choices** (i.e., scenarios)? *(Note: The back side contains the “menu” of ideas)*

A. What do you like or dislike about the ideas presented in **Scenario A: Current Trends**?

Like

Dislike and would change or add

B. What do you like or dislike about the ideas presented in **Scenario B: Districts**?

Like

Dislike and would change or add

C. What do you like or dislike about the ideas presented in **Scenario C: Housing Choices**?

Like

It allows for the community's future needs. -Timing can remain flexible & adapt to shifts.

Dislike and would change or add

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

😊 = like
 😐 = neutral
 ☹️ = dislike

For the future, I would like to see:

Comments and Ideas:

More light industry north of Arapahoe Avenue
 (*Current Trends scenario*)

😊 😐 ☹️

Affordable space for necessary services (e.g., car repair)
 (*Current Trends and Districts scenarios*)

😊 😐 ☹️

Boulder Community Health with nearby places for medical-related offices and small retail
 (*Districts scenario*)

😊 😐 ☹️ *Vitality important for community future health needs*

A new east/west connecting street (i.e., Walnut/48th Street)
 (*Districts scenario*)

😊 😐 ☹️

Small shops, dining, offices and some apartments north of Arapahoe at 55th, along 55th St.
 (*Districts scenario*)

😊 😐 ☹️

Improvements to sidewalks and intersections
 (*Districts and Housing Choices scenarios*)

😊 😐 ☹️

Place for energy generation, recycling, eco-district
 (*Districts scenario*)

😊 😐 ☹️

Small shops, dining, offices, and housing north of Arapahoe at 55th, along 55th St. in walk-friendly neighborhoods
 (*Housing Choices scenario*)

😊 😐 ☹️

More people walking and riding bicycles – make it safe and convenient
 (*Districts and Housing Choices scenarios*)

😊 😐 ☹️

Some new affordable, workforce housing north of Arapahoe Ave.
 (*Housing Choices scenario*)

😊 😐 ☹️ *Housing displaces on commuting*

Flatiron Golf has new trails and community gardens, in addition to golf course
 (*Housing Choices scenario*)

😊 😐 ☹️

Ecological restoration along ditches and near creeks; better access to nature for current and future residents
 (*Housing Choices scenario*)

😊 😐 ☹️

Arapahoe Ave. becomes a safe and attractive “boulevard” with street trees, noise buffering, safe speeds and transit

😊 😐 ☹️

What other choices or ideas would you like to see?



Oct. 27, 2014 Workshop – Comment Form

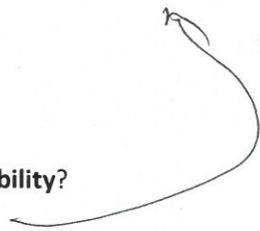
Envision East Arapahoe *Please provide your ideas.*

1—What are your general comments and ideas for the future of the East Arapahoe area?

<p><i>What should remain?</i></p>	<p><i>What should change?</i></p> <p>NEED NORTH - SOUTH CONNECTION PEDESTRIAN / BIKE CONNECTION FROM E. ARAP TO BOULDER JUNCTION (IN ADDITION TO 30TH)</p>
-----------------------------------	---

2—How would you improve the draft **Vision Statement**?

3—What ideas would you suggest for **transportation enhancements and improving mobility**?



4—Do you have comments about the **future choices** (i.e., scenarios)? *(Note: The back side contains the “menu” of ideas)*

A. What do you like or dislike about the ideas presented in **Scenario A: Current Trends**?

<p><i>Like</i></p>	<p><i>Dislike and would change or add</i></p>
--------------------	---

B. What do you like or dislike about the ideas presented in **Scenario B: Districts**?

<p><i>Like</i></p>	<p><i>Dislike and would change or add</i></p>
--------------------	---

C. What do you like or dislike about the ideas presented in **Scenario C: Housing Choices**?

<p><i>Like</i></p>	<p><i>Dislike and would change or add</i></p>
--------------------	---

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

😊 = like
 😐 = neutral
 ☹️ = dislike

For the future, I would like to see:

Comments and Ideas:

More light industry north of Arapahoe Avenue (<i>Current Trends scenario</i>)	🗑️ 😐 ☹️	
Affordable space for necessary services (e.g., car repair) (<i>Current Trends and Districts scenarios</i>)	🗑️ 😐 ☹️	
Boulder Community Health with nearby places for medical-related offices and small retail (<i>Districts scenario</i>)	😊 🗑️ ☹️	
A new east/west connecting street (i.e., Walnut/48 th Street) (<i>Districts scenario</i>)	😊 🗑️ ☹️	?
Small shops, dining, offices and some apartments north of Arapahoe at 55 th , along 55 th St. (<i>Districts scenario</i>)	🗑️ 😐 ☹️	
Improvements to sidewalks and intersections (<i>Districts and Housing Choices scenarios</i>)	🗑️ 😐 ☹️	
Place for energy generation, recycling, eco-district (<i>Districts scenario</i>)	😊 🗑️ ☹️	
Small shops, dining, offices, and housing north of Arapahoe at 55 th , along 55 th St. in walk-friendly neighborhoods (<i>Housing Choices scenario</i>)	😊 🗑️ ☹️	
More people walking and riding bicycles – make it safe and convenient (<i>Districts and Housing Choices scenarios</i>)	🗑️ 😐 ☹️	
Some new affordable, workforce housing north of Arapahoe Ave. (<i>Housing Choices scenario</i>)	🗑️ 😐 ☹️	
Flatiron Golf has new trails and community gardens, in addition to golf course (<i>Housing Choices scenario</i>)	🗑️ 😐 ☹️	
Ecological restoration along ditches and near creeks; better access to nature for current and future residents (<i>Housing Choices scenario</i>)	🗑️ 😐 ☹️	
Arapahoe Ave. becomes a safe and attractive “boulevard” with street trees, noise buffering, safe speeds and transit	🗑️ 😐 ☹️	

What other choices or ideas would you like to see?



Oct. 27, 2014 Workshop – Comment Form

Envision East Arapahoe Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?

What should change?

Less surface parking.

2—How would you improve the draft Vision Statement?

3—What ideas would you suggest for transportation enhancements and improving mobility?

Protected bike lane on Arapahoe + traffic calming / lower speed limits.

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the “menu” of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like

Dislike and would change or add

Too suburban, not dense enough.
No emphasis on walkable neighborhoods.
Too reliant on driving.

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like

Dislike and would change or add

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Like

Emphasis on 15 min neighborhoods!
Less reliance on cars would probably reduce Arapahoe traffic.
More parks!
This is the best plan. ☺

Dislike and would change or add

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

😊 = like
 😐 = neutral
 ☹️ = dislike

For the future, I would like to see:

Comments and Ideas:

More light industry north of Arapahoe Avenue (<i>Current Trends scenario</i>)	😊 ☹️	
Affordable space for necessary services (e.g., car repair) (<i>Current Trends and Districts scenarios</i>)	😊 ☹️	Would be nice to de-emphasize surface parking for new things. Area around Subaru/Acura for example is terrible.
Boulder Community Health with nearby places for medical-related offices and <u>small retail</u> (<i>Districts scenario</i>)	☹️ 😐 ☹️	
A new east/west connecting street (i.e., Walnut/48 th Street) (<i>Districts scenario</i>)	☹️ 😐 ☹️	
Small shops, dining, offices and some apartments north of Arapahoe at 55 th , along 55 th St. (<i>Districts scenario</i>)	☹️ 😐 ☹️	More housing density! More people to walk to all these wonderful things!
Improvements to sidewalks and intersections (<i>Districts and Housing Choices scenarios</i>)	☹️ 😐 ☹️	Wide sidewalks that accommodate bikes everywhere would be great if there's not going to be an on-street bike lane.
Place for energy generation, recycling, eco-district (<i>Districts scenario</i>)	☹️ 😐 ☹️	
Small shops, dining, offices, and housing north of Arapahoe at 55 th , along 55 th St. in walk-friendly neighborhoods (<i>Housing Choices scenario</i>)	☹️ 😐 ☹️	Better walkable neighborhoods ^{would fix} rather #1 thing I dislike about this area. Need to be able to walk to groceries, drug stores, etc.
More people walking and riding bicycles – make it safe and convenient (<i>Districts and Housing Choices scenarios</i>)	☹️ 😐 ☹️	
Some new affordable, workforce housing north of Arapahoe Ave. (<i>Housing Choices scenario</i>)	☹️ 😐 ☹️	I'd love more density in general. More density = more people to support walkable neighborhoods.
Flatiron Golf has new trails and community gardens, in addition to golf course (<i>Housing Choices scenario</i>)	☹️ 😐 ☹️	
Ecological restoration along ditches and near creeks; better access to nature for current and future residents (<i>Housing Choices scenario</i>)	☹️ 😐 ☹️	
Arapahoe Ave. becomes a safe and attractive "boulevard" with street trees, noise buffering, safe speeds and transit	☹️ 😐 ☹️	2 lanes + bike lane would be wonderful!

What other choices or ideas would you like to see?



Oct. 27, 2014 Workshop – Comment Form

Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?

~~EA~~

What should change?

BRT Corridor

2—How would you improve the draft Vision Statement?

3—What ideas would you suggest for transportation enhancements and improving mobility?

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the “menu” of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like

Dislike and would change or add

Inefficient use of land
poor connectivity

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like

more diverse/mixed land uses
more restaurants/retail will decrease
number of trips to 29th for lunch
this will help lessen traffic in a
congested area

Dislike and would change or add

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Like

more housing within city, reduces
N-commutes + VMT

Dislike and would change or add

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

😊 = like
 😐 = neutral
 ☹️ = dislike

Comments and Ideas:

For the future, I would like to see:

More light industry north of Arapahoe Avenue
 (*Current Trends scenario*) 😊 😐 ☹️

Affordable space for necessary services (e.g., car repair)
 (*Current Trends and Districts scenarios*) 😊 😐 ☹️

Boulder Community Health with nearby places for medical-related offices and small retail
 (*Districts scenario*) 😊 😐 ☹️

A new east/west connecting street (i.e., Walnut/48th Street)
 (*Districts scenario*) 😊 😐 ☹️

Small shops, dining, offices and some apartments north of Arapahoe at 55th, along 55th St.
 (*Districts scenario*) 😊 😐 ☹️

Improvements to sidewalks and intersections
 (*Districts and Housing Choices scenarios*) 😊 😐 ☹️

Place for energy generation, recycling, eco-district
 (*Districts scenario*) 😊 😐 ☹️

Small shops, dining, offices, and housing north of Arapahoe at 55th, along 55th St. in walk-friendly neighborhoods
 (*Housing Choices scenario*) 😊 😐 ☹️

More people walking and riding bicycles – make it safe and convenient
 (*Districts and Housing Choices scenarios*) 😊 😐 ☹️

Some new affordable, workforce housing north of Arapahoe Ave.
 (*Housing Choices scenario*) 😊 😐 ☹️

Flatiron Golf has new trails and community gardens, in addition to golf course
 (*Housing Choices scenario*) 😊 😐 ☹️ This would make for more efficient use of the land

Ecological restoration along ditches and near creeks; better access to nature for current and future residents
 (*Housing Choices scenario*) 😊 😐 ☹️

Arapahoe Ave. becomes a safe and attractive "boulevard" with street trees, noise buffering, safe speeds and transit 😊 😐 ☹️

What other choices or ideas would you like to see?



Oct. 27, 2014 Workshop – Comment Form

Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?

It should remain a major
ent. into the wk from the east
as Lafayette, Erie, Fremont, etc. are
integrated into the work.

What should change?

BCT + greater walkability
55th St. or Cypress needs
better bus service.

2—How would you improve the draft Vision Statement?

Major poles are the parking lot in back of bank on 55th + Arapahoe
(New side) Some restaurants may be coming in but it looks
so ready for improvements. That new one is slighted.

3—What ideas would you suggest for transportation enhancements and improving mobility?

1. Reduce speed limit to 40
2. Remove stop sign on 47th - Improve light s/c into BCT
3. ↑ transit + walkability

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the "menu" of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like

If Nanopa ↑ its campus +
makes it more aesthetically
appealing. That would be great but
no control over the University

Dislike and would change or add

May appear to grow but as an
approach Houston-style zoning
which is not good

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like

The concept of "districts" is
a good one

Dislike and would change or add

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Like

Diversity of housing

Dislike and would change or add

Prefer concept of "districts"
better

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

😊 = like
 😐 = neutral
 ☹️ = dislike

For the future, I would like to see:

Comments and Ideas:

More light industry north of Arapahoe Avenue
 (*Current Trends scenario*)

😊 ☹️

light industry can be very sophisticated and innovative

Affordable space for necessary services (e.g., car repair)
 (*Current Trends and Districts scenarios*)

😊 😐 ☹️

Boulder Community Health with nearby places for medical-related offices and small retail
 (*Districts scenario*)

😐 ☹️

A new east/west connecting street (i.e., Walnut/48th Street)
 (*Districts scenario*)

😐 ☹️

Small shops, dining, offices and some apartments north of Arapahoe at 55th, along 55th St.
 (*Districts scenario*)

😐 ☹️

Height levels of bldgs are critical

Improvements to sidewalks and intersections
 (*Districts and Housing Choices scenarios*)

😐 ☹️

Place for energy generation, recycling, eco-district
 (*Districts scenario*)

😐 ☹️

Small shops, dining, offices, and housing north of Arapahoe at 55th, along 55th St. in walk-friendly neighborhoods
 (*Housing Choices scenario*)

😐 ☹️

More people walking and riding bicycles – make it safe and convenient
 (*Districts and Housing Choices scenarios*)

😐 ☹️

Some new affordable, workforce housing north of Arapahoe Ave.
 (*Housing Choices scenario*)

😊 ☹️

Hopefully not in huge building up to sidewalk like Boulder Jct.

Flatiron Golf has new trails and community gardens, in addition to golf course
 (*Housing Choices scenario*)

😊 😐 ☹️

Ecological restoration along ditches and near creeks; better access to nature for current and future residents
 (*Housing Choices scenario*)

😐 ☹️

Arapahoe Ave. becomes a safe and attractive "boulevard" with street trees, noise buffering, safe speeds and transit

😊 ☹️

It should of course be safe + attractive - "blvd." may make the street less useful

What other choices or ideas would you like to see?

There are some mistakes, i.e. scale of building, scale of architecture in Boulder Jct. That it would be advisable to learn from and avoid



Oct. 27, 2014 Workshop – Comment Form

Envision East Arapahoe *Please provide your ideas.*

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?

What should change?

More arts organizations, to complement Boulder Digital Arts, video station, pro photo rental

2—How would you improve the draft Vision Statement?

3—What ideas would you suggest for transportation enhancements and improving mobility?

more pedestrian crosswalks for Ball employees, etc. to cross Arapahoe safely

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the "menu" of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like

Dislike and would change or add

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like

Dislike and would change or add

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Like

Dislike and would change or add

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

☺ = like
 😐 = neutral
 ☹ = dislike

Comments and Ideas:

For the future, I would like to see:

More light industry north of Arapahoe Avenue (<i>Current Trends scenario</i>)	☺ ☐ ☹
Affordable space for necessary services (e.g., car repair) (<i>Current Trends and Districts scenarios</i>)	☺ ☐ ☹
Boulder Community Health with nearby places for medical-related offices and small retail (<i>Districts scenario</i>)	☺ ☐ ☹
A new east/west connecting street (i.e., Walnut/48 th Street) (<i>Districts scenario</i>)	☐ ☺ ☹
Small shops, dining, offices and some apartments north of Arapahoe at 55 th , along 55 th St. (<i>Districts scenario</i>)	☺ ☐ ☹
Improvements to sidewalks and intersections (<i>Districts and Housing Choices scenarios</i>)	☺ ☐ ☹
Place for energy generation, recycling, eco-district (<i>Districts scenario</i>)	☐ ☐ ☺
Small shops, dining, offices, and housing north of Arapahoe at 55 th , along 55 th St. in walk-friendly neighborhoods (<i>Housing Choices scenario</i>)	☺ ☐ ☹
More people walking and riding bicycles – make it safe and convenient (<i>Districts and Housing Choices scenarios</i>)	☺ ☐ ☹
Some new affordable, workforce housing north of Arapahoe Ave. (<i>Housing Choices scenario</i>)	☐ ☺ ☹
Flatiron Golf has new trails and community gardens, in addition to golf course (<i>Housing Choices scenario</i>)	☺ ☐ ☹
Ecological restoration along ditches and near creeks; better access to nature for current and future residents (<i>Housing Choices scenario</i>)	☐ ☺ ☹
Arapahoe Ave. becomes a safe and attractive “boulevard” with street trees, noise buffering, safe speeds and transit	☺ ☐ ☹

What other choices or ideas would you like to see?

More arts + cultural organizations



Oct. 27, 2014 Workshop – Comment Form

Envision East Arapahoe Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?

What should change?

2—How would you improve the draft **Vision Statement**?

3—What ideas would you suggest for **transportation enhancements and improving mobility**?

4—Do you have comments about the **future choices** (i.e., scenarios)? *(Note: The back side contains the “menu” of ideas)*

A. What do you like or dislike about the ideas presented in **Scenario A: Current Trends**?

Like

Dislike and would change or add

B. What do you like or dislike about the ideas presented in **Scenario B: Districts**?

Like

Dislike and would change or add

C. What do you like or dislike about the ideas presented in **Scenario C: Housing Choices**?

Like

Dislike and would change or add

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

☺ = like
 😐 = neutral
 ☹ = dislike

For the future, I would like to see:

	☺	😊	😐	☹	☹
More light industry north of Arapahoe Avenue (Current Trends scenario)	☺	😊	😐	☹	
Affordable space for necessary services (e.g., car repair) (Current Trends and Districts scenarios)	☺	😊	😐	☹	
Boulder Community Health with nearby places for medical-related offices and small retail (Districts scenario)	☺	😊	😐	☹	
A new east/west connecting street (i.e., Walnut/48 th Street) (Districts scenario)	☺	😊	😐	☹	
Small shops, dining, offices and some apartments north of Arapahoe at 55 th , along 55 th St. (Districts scenario)	☺	😊	😐	☹	
Improvements to sidewalks and intersections (Districts and Housing Choices scenarios)	☺	😊	😐	☹	
Place for energy generation, recycling, eco-district (Districts scenario)	☺	😊	😐	☹	
Small shops, dining, offices, and housing north of Arapahoe at 55 th , along 55 th St. in walk-friendly neighborhoods (Housing Choices scenario)	☺	😊	😐	☹	
More people walking and riding bicycles – make it safe and convenient (Districts and Housing Choices scenarios)	☺	😊	😐	☹	
Some new affordable, workforce housing north of Arapahoe Ave. (Housing Choices scenario)	☺	😊	😐	☹	
Flatiron Golf has new trails and community gardens, in addition to golf course (Housing Choices scenario)	☺	😊	😐	☹	
Ecological restoration along ditches and near creeks; better access to nature for current and future residents (Housing Choices scenario)	☺	😊	😐	☹	
Arapahoe Ave. becomes a safe and attractive "boulevard" with street trees, noise buffering, safe speeds and transit	☺	😊	😐	☹	

Comments and Ideas:

Beth supports

- 1) Medical zoning as a primary use in the corridor. to Walnut - providing another access to Bchto
- 2) Connection of 48th St. to Walnut - providing another access to Bchto
- 3) More commercial + housing options in the corridor
- 4) Enhanced walking, biking, transit opportunities in the corridor.
- 5) Open to Joint Campus opportunities with Ball Corp. → truck allowed in the area near hospital.
- 6) Food in the area near hospital.

Glad to participate!

What other choices or ideas would you like to see?



Oct. 27, 2014 Workshop – Comment Form

Envision East Arapahoe *Please provide your ideas.*

1—What are your general comments and ideas for the future of the East Arapahoe area?

<p>What should remain?</p>	<p>What should change?</p> <p><i>Connections and features to make walking + cycling safer and more convenient for visitors and residents.</i></p>
----------------------------	---

2—How would you improve the draft **Vision Statement**?

2) Add a shared-use path parallel to the BNSF rail line

3—What ideas would you suggest for **transportation enhancements and improving mobility**?

1) Bicycle and limited stop express service of the JUMP would provide versatile options to travel along and beyond Arapahoe Avenue.

4—Do you have comments about the **future choices** (i.e., scenarios)? (Note: The back side contains the “menu” of ideas)

A. What do you like or dislike about the ideas presented in **Scenario A: Current Trends**?

<p>Like</p>	<p>Dislike and would change or add</p>
-------------	--

B. What do you like or dislike about the ideas presented in **Scenario B: Districts**?

<p>Like</p>	<p>Dislike and would change or add</p>
-------------	--

C. What do you like or dislike about the ideas presented in **Scenario C: Housing Choices**?

<p>Like</p>	<p>Dislike and would change or add</p>
-------------	--

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

For the future, I would like to see:	☺ = like 😐 = neutral ☹ = dislike	Comments and Ideas:
More light industry north of Arapahoe Avenue (<i>Current Trends scenario</i>)	☺ ☐ ☹	
Affordable space for necessary services (e.g., car repair) (<i>Current Trends and Districts scenarios</i>)	☺ ☐ ☹	
Boulder Community Health with nearby places for medical-related offices and small retail (<i>Districts scenario</i>)	☺ ☺ ☹	
A new east/west connecting street (i.e., Walnut/48 th Street) (<i>Districts scenario</i>)	☺ ☐ ☹	
Small shops, dining, offices and some apartments north of Arapahoe at 55 th , along 55 th St. (<i>Districts scenario</i>)	☺ ☐ ☹	
Improvements to sidewalks and intersections (<i>Districts and Housing Choices scenarios</i>)	☺ ☐ ☹	Essential!
Place for energy generation, recycling, eco-district (<i>Districts scenario</i>)	☺ ☐ ☹	
Small shops, dining, offices, and housing north of Arapahoe at 55 th , along 55 th St. in walk-friendly neighborhoods (<i>Housing Choices scenario</i>)	☺ ☐ ☹	
More people walking and riding bicycles – make it safe and convenient (<i>Districts and Housing Choices scenarios</i>)	☺ ☐ ☹	Most essential!
Some new affordable, workforce housing north of Arapahoe Ave. (<i>Housing Choices scenario</i>)	☺ ☺ ☹	
Flatiron Golf has new trails and community gardens, in addition to golf course (<i>Housing Choices scenario</i>)	☺ ☺ ☹	
Ecological restoration along ditches and near creeks; better access to nature for current and future residents (<i>Housing Choices scenario</i>)	☺ ☐ ☹	
Arapahoe Ave. becomes a safe and attractive “boulevard” with street trees, noise buffering, safe speeds and transit	☺ ☐ ☹	

What other choices or ideas would you like to see?

- A) Bike sharing installed in strategic locations for making short trips along and outside the Arapahoe corridor.
- B) Express RTD bus routes with key stops on Arapahoe.



Oct. 27, 2014 Workshop – Comment Form

Envision East Arapahoe Please provide your ideas.

1—What are your general comments and ideas for the future of the East Arapahoe area?

What should remain?

KEEP TRAFFIC LANES AS IS - DO NOT ADD LANES / EXCEPT BIKE COMMUTING LANES

What should change?

ESTABLISH A CONNECTION BTWN. ARAPAHOE AVE. and PEARL STREET - CLOSE TO BOTH.

2—How would you improve the draft Vision Statement?

MINIMIZE ADDITIONAL RESIDENTIAL UNITS... EXCEPT IMMEDIATELY ADJAZENT TO ARAPAHOE.

3—What ideas would you suggest for transportation enhancements and improving mobility?

SEE #1 ABOVE

4—Do you have comments about the future choices (i.e., scenarios)? (Note: The back side contains the "menu" of ideas)

A. What do you like or dislike about the ideas presented in Scenario A: Current Trends?

Like

Dislike and would change or add

B. What do you like or dislike about the ideas presented in Scenario B: Districts?

Like

ENTHANE 55TH NEIGHBORHOOD CENTER

Dislike and would change or add

C. What do you like or dislike about the ideas presented in Scenario C: Housing Choices?

Like

EMPLOYEE HOUSING IS GOOD!

Dislike and would change or add

--- Over ---

MENU OF IDEAS: Please tell us what you like or dislike about the ideas in the future scenarios.

😊 = like
 😐 = neutral
 😞 = dislike

For the future, I would like to see:

Comments and Ideas:

More light industry north of Arapahoe Avenue
 (*Current Trends scenario*)

😊 😐 😞

Affordable space for necessary services (e.g., car repair)
 (*Current Trends and Districts scenarios*)

😊 😐 😞

Boulder Community Health with nearby places for medical-related offices and small retail
 (*Districts scenario*)

😊 😐 😞

A new east/west connecting street (i.e., Walnut/48th Street)
 (*Districts scenario*)

😊 😐 😞

AT BCH / BAW BROTHERS

Small shops, dining, offices and some apartments north of Arapahoe at 55th, along 55th St.
 (*Districts scenario*)

😊 😐 😞

Improvements to sidewalks and intersections
 (*Districts and Housing Choices scenarios*)

😊 😐 😞

Place for energy generation, recycling, eco-district
 (*Districts scenario*)

😊 😐 😞

Small shops, dining, offices, and housing north of Arapahoe at 55th, along 55th St. in walk-friendly neighborhoods
 (*Housing Choices scenario*)

😊 😐 😞

More people walking and riding bicycles – make it safe and convenient
 (*Districts and Housing Choices scenarios*)

😊 😐 😞

Some new affordable, workforce housing north of Arapahoe Ave.
 (*Housing Choices scenario*)

😊 😐 😞

Flatiron Golf has new trails and community gardens, in addition to golf course
 (*Housing Choices scenario*)

😊 😐 😞

Ecological restoration along ditches and near creeks; better access to nature for current and future residents
 (*Housing Choices scenario*)

😊 😐 😞

Arapahoe Ave. becomes a safe and attractive “boulevard” with street trees, noise buffering, safe speeds and transit

😊 😐 😞

What other choices or ideas would you like to see?

October 28, 2014 Study Session Summary on Envision East Arapahoe (for review and approval on 1/20/15)

PRESENT

City Council: Mayor Matt Appelbaum, Mayor Pro Tem George Karakehian, Council Members Macon Cowles, Suzanne Jones, Lisa Morzel, Tim Plass, Andrew Shoemaker, Sam Weaver, and Mary Young.

Staff members: Jane S. Brautigam, City Manager; Susan Richstone, Deputy Director of Community Planning and Sustainability; Lesli Ellis, Comprehensive Planning Manager; Kathleen Bracke, GO Boulder Manager; Sam Assefa, Senior Urban Designer

STUDY SESSION SUMMARY

The Mayor introduced the topic and informed the group that the purpose of the meeting was to provide input on the scenarios and next steps for staff.

City staff provided information on the purpose and key elements of the scenarios including some of the assumptions behind the scenarios, their “menu” of choices, community engagement to date, and next steps. Staff also provided transportation background for the scenarios and opportunities along East Arapahoe. City staff is framing these opportunities in the context of the recently updated Transportation Master Plan and working to integrate planning with regional transportation opportunities such as RTD’s planned arterial Bus Rapid Transit (BRT) along the East Arapahoe/SH7 corridor.

During the presentations, council members asked questions regarding the correlation between jobs and housing and how staff is analyzing the buildout potential for the area. Staff responded that buildout is based on current zoning, with efficiency factors built in accounting for barriers like floodplain. Staff has preliminary information on scenarios and will provide a full report of the methodology and assumptions related to scenarios during the next steps of the project.

Discussion Summary

The following are the key elements from the discussion:

General Comments

- Several council members stated that this project is important, and that change will occur on this corridor whether the city plans for it or not. If we don’t plan for it, the people that live and work in the area may not be happy with the outcomes. Maintaining current trends is not a desirable option and may preclude other opportunities.
- The city has a lot of public investments and assets along the corridor (e.g., trails, golf, Sombrero Marsh, Recycle Row) – we should celebrate those, build from them, and better connect them.
- Several council members expressed that overall the project and scenarios are on the right track. Need to understand how the components tie together and need to look at targeted changes.
- This project allows for facilitating annexations. We talk about this but do not see a lot of annexations because it is expensive. There is an excellent opportunity with storage unit facilities to annex for better potential uses.
- The 1995 North Boulder Subcommunity Plan is a good example where neighbors wanted a say in the future. The community embraced the tasks and became stronger with a cohesive vision.
- Valmont Power Plant is not a part of this project, but it will have a big influence on the future of this area.
- Boulder Valley School District (BVSD) employment is an opportunity – this should be noted on the project maps.

Comments on the Scenarios

- Several council members stated that the scenarios are not quite bold enough but a good starting point.
- Several council members noted that the scenarios should be viewed as a menu of options to assemble for preferred outcomes.
- Some council members expressed support for concepts in Scenario B as a health and arts district – it addresses a lot from the primary employer study (Economic Sustainability Strategy). Other council members preferred concepts of housing presented in Scenario C.

- The East Arapahoe corridor has a lot of 1970s industrial zoning with 0.5 Floor Area Ratio (FAR). We need to understand what happens if that changes. One option is to facilitate incremental change more slowly over time. Wholesale rezoning may not be appropriate at this time.
- The indicators are important, but it is unclear how they will be quantified. This could be particularly important if we do include other amenities along with housing. (Note: Staff will present quantified indicators with the scenarios analysis.)

Community Engagement Comments

- Several council members stated that citizen engagement is an integral part of this planning effort and it is important that the community takes ownership of the project. The success of the process comes out of how well the city engages with the public. Specifically, it's important to coordinate with Naropa, Ball, and Boulder Community Health.
- The city needs to better understand which neighborhoods to involve. The residents on the south side of East Arapahoe have an interest but we need to weigh their input with others more directly affected by what happens in the industrial areas.

Housing Comments

- One council member posed the question - can we encourage development of nodes without additional housing? The Gunbarrel Town Center is a good example. The community wanted more amenities without the intensity and arrived at compromise to add additional housing.
- Several council members expressed that the corridor needs some housing, and this should be a component of the planning effort. However, there were mixed opinions on whether or not additional housing would work in this area, and what type of housing is appropriate.
- One council member posed the question - if we provided more housing options, how many existing employees working in Boulder but living outside the city might choose to move to Boulder? Staff responded that the city will use information from Housing Boulder to inform housing choices around this issue. As part of the Housing Boulder project, a variety of employers have been involved in identifying housing needs.

Land Use, Design, and Amenities Comments

- Several council members expressed that the corridor needs more amenities and a better mix of land uses to foster a place to live, work, and recreate, or a “critical mass”.
- The corridor should bring in more retail to further the goal of “20-minute” neighborhoods. Bring in neighborhood serving retail and services on the corridor, rather than attempting to make it a regional retail draw. This is important so the corridor does not compete with other areas in town.
- Several council members indicated an interest in visualizing potential changes along the corridor to inform policy choices.
- The city has very few large employers like Boulder Community Health (BCH). The East Arapahoe corridor needs places where people would want to walk to work and have an opportunity to get to the service industrial. A combination of housing with commercial strengthens the retail sector.
- In general, as the city explores different land use mixes we need to understand the corresponding amenities needed along the corridor and what makes a great neighborhood.
- Several council members expressed interest in exploring a form based code, pattern book, or overlay district – either targeted to one area or a larger area along the corridor – to achieve the urban form desired by the community.
- Throughout the process the city should keep in mind how the design of this area affects 28th and 30th streets (e.g., the Sustainable Streets and Centers initiative).

Service Industrial Comments

- Several council members noted that continued affordability of service industrial is important and should be part of this project and present across all scenarios. Industrial commercial start ups are important. These uses serve an important community purpose. Many of these businesses will not be able to afford new buildings.
- Much of East Arapahoe commercial space is really important but tired. Focus needs to stay on commercial, service industrial, and the ability for people to start small businesses.

Transportation Comments

- Several council members noted how this corridor is particularly challenging for pedestrians and bicyclists, and this project should address the challenges.

- Several council members expressed interest in the Bus Rapid Transit (BRT) concept. In particular, this presents an opportunity to show that Bus Rapid Transit (BRT) can work with a lot of local influence.
- One council member posed the question - how will we negotiate with Colorado Department of Transportation (CDOT)? We have worked on other corridors with CDOT. City staff responded that this issue is part of the analysis, and staff is working closely with partners such as CDOT and the Regional Transportation District (RTD) to determine what is possible.
- Several council members noted that next steps should focus on transportation safety issues along the corridor.

Comments on Medical Uses near Boulder Community Health

- Several council members noted that next steps should emphasize timely topics like medical office uses near Boulder Community Health. Prioritize working on those now.
- Staff should analyze and propose options to address medical uses around BCH in the short term. For example, council received a letter from a local doctor noting that patients and staff are driving several times per day after the BCH move.



Envision East Arapahoe

Survey Results

Oct. 15, 2014 to Dec. 31, 2014

envisioneatarapahoe.com

Survey (Online and Print)

Below are the results from the survey that was distributed during the public workshops and listening sessions. A link to the online version of the print survey (with identical questions) was placed on the project website and distributed via email.

Question 1

(Answered: 79, Skipped: 28)

What should remain?

Question 1: Response	
1	Golf course, office and industrial uses.
2	Anything that's there now and is contributing to the community
3	Flatirons Golf Course!!!!!!
4	Open space, trails, small residential streets.
5	Perhaps everything if that is what the current residents choose.
6	hospital; Ball; Vo-Tech; multi-use bike path
7	You need to leave things alone until a definitive purpose can be defined. Nothing developed thus far provides the opportunity for home purchase. You are changing the demographics of this community building a dense load of rental properties and enriching investor/developers. You are ruining the character of Boulder and eliminating community.
8	Existing mixed use walking/bicycle trails are adequate. No new underpasses (crossing Foothills parkway south of Valmont) are needed. There are a sufficient number now and many existing underpasses (Arapahoe and Foothills pkwy for example) are prone to frequent flooding.
9	Service industrial is important. Greenways and open spaces should be kept and enhanced - better connected
10	Keep traffic lanes as is - do not add lanes/ except bike commuting lanes
11	It should remain a major entry into the city from the east and Lafayette, Erie, etc.
12	Golf Course ALL 18 holes - a water retention isn't the answer - the water flowing under Arapahoe to creek is needed
13	Do not bring in big buildings. Keep this as a transition of low density, low height buildings.
14	Low height of bldg's. No 55' buildings. Keep views!
15	Opportunities for startup businesses to incubate their biz plan in an affordable space, especially for businesses associated with Recycle Row.
16	Light industry uses are good but can actually be integrated with higher density residential. I'd love to live in a place where people are making things.
17	The rural neighborhoods.
18	Good to see the planning boards are looking at a comprehensive plan to handle growth well.
19	Focus on high paying job sector.
20	Keep a height limit of 4 stories.
21	The golf course
22	The existing natural areas, along the creek and the drainage sloughs. And existing industrial uses are fine.
23	Golf course and the businesses that are there now.
24	Affordable retail - thrift shops, light industry
25	Nothing
26	Green Space

Question 1: Response	
27	Avalon Ballrooms - growing! Participatory dance! Evening/late night transit
28	Keep some industrial areas
29	General mixed use atmosphere
30	This area is a business engine for Boulder. Reasonable rent for start ups. Look how breweries and coffee have taken hold.
31	Service businesses, now that they have been kicked out of the 30th and Pearl area.
32	Most of what is there on W. side of 55th and Flatiron Industrial Park. Service Industrial
33	Appreciate the proactive Comprehensive Plan for East Arapahoe development.
34	The existing housing and neighborhoods
35	multi-use path, bus service, b-cycle stations
36	This is one of the main thoroughfares into Boulder from the East. It must remain so but how to make it more pedestrian and bike friendly? Restaurants and food establishments are the only vibrant area around the major intersection of 55th and Arapahoe. It's hard to get into and park at lunch time. This type of retail needs to expand along this corridor. Strip mall to the west of this area is not intensively frequented.
37	This is one of the main thoroughfares into Boulder from the East. It must remain so but how to make it more pedestrian and bike friendly? Restaurants and food establishments are the only vibrant area around the major intersection of 55th and Arapahoe. It's hard to get into and park at lunch time. This type of retail needs to expand along this corridor. Strip mall to the west of this area is not intensively frequented.
38	Light industrial sites. Boulder needs to have them somewhere.
39	Boulder Dinner Theater, appreciation of the more rural aspects of surrounding community
40	The Golf Course
41	some light industrial
42	"This entire area needs to be designated a ""Quiet Zone." The train crossing intersections have gates. That should be sufficient.
43	local business, residential neighborhoods, schools This is a strong family neighborhood!
44	Boulder Dinner Theater Low cost industrial space
45	~Quiet neighborhoods and family feel ~
46	Quality employment and living structures and places such as Foothills Hospital, Riverbend, Viewpoint, Naropa, Flatirons golf course, neighborhoods to south of Arapahoe, apartments and condos, Boulder Dinner Theater, Ball Bros, limited size and waste generation of recycling row, CU Research park, pleasing and comfortable and grounds at Jewish Community Center property, open space at and around Sombrero marsh.
47	Small businesses and services for people who work and/or already live along Arapahoe Ave east of Foothills Parkway.
48	new pubs and new dining options that have recently opened along Arapahoe, as well as some that have been there awhile, such as Ozo and Snarf's. Dated red brick buildings should get a face-lift.
49	The current mixture of small manufacturing and retail in the areas around Conestoga and east to Naropa should remain. We can't keep driving those businesses out of Boulder. We don't need high density housing east of 55th, especially if it looks like North Boulder or Pearl Street east of 30th with tall buildings built right up to the sidewalk. If the car dealers want to remain, they should have that choice. Don't drive them off with new restrictive zoning, or cause them to sell with new permissive zoning that would make it so attractive to sell to dense residential developers that they move out. If you want incubator businesses, you need to allow the ""not beautiful"" architecture to remain. Do keep setbacks from the sidewalk-- Peloton is tight but at least not on the sidewalk like Broadway north of Violet. It is not the city's job to provide shopping or restaurants. Let the marketplace do that. Most, if not all of the the flood plain/open space at the golf course should remain.
50	Everything

Question 1: Response	
51	The present configuration.
52	I like east Arapahoe the way it is, rural and quiet!!!
53	I like having space for restaurants like Basta, Fate, and Bru and love having Ozo (would like to have more coffee shops and restaurants). I don't love all of the strip malls they are housed in, but I do feel like they should get first choice of the new commercial space. I want more of a neighborhood feel in that area, as more and more homes and apartments will be built along Arapahoe. I like mixed use spaces like the Peloton and the new apartments going in downtown on Pearl Street (and near 30th and Pearl). I think the bike path needs to connect better to the area, so have a dedicated off-street bike path on Arapahoe and/or make the bike path on Pennsylvania connect all the way through to the path that goes under 55th.
54	Good mix of residential and commercial. Limit commercial on side-streets off of Arapahoe
55	It's hard to say. Love access to Boulder Creek Path and other biketrails, but they are good for recreation. I love Arapahoe Animal, Dinner Theater, sometimes go to PICA's and the liquor store, but have to drive, too far of a walk, and would not bike. More community oriented, places for kids to visit, but do not enjoy walking along Arapahoe, too much busy traffic, not really safe for little ones, noisy, dirty, car exhaust.
56	Flexibility of business development.
57	Most of the present structures should remain.
58	The service industrial uses that serve Boulder residents should remain. The peaceful winding streets with green lawns and nice office buildings should also remain.
59	Existing wetlands, waterway lands and then to expand their environmental health and well-being.
60	The hospital, breweries, and Ozo
61	Boulder's height restrictions on buildings; some light industrial; Jump bus route
62	Multiple use paths
63	Leave the single-family housing south of the street alone and focus on the corridor and areas north of the street.
64	Diversity, all old trees
65	Give consideration to existing homes in this area and property values Flatirons golf course Shopping, restaurants, gas, grocery, recycling
66	The street as it is. No median which would take away the views. No increased density which would add too much traffic to the cities streets.
67	Community feel. Rural Character. Small Farms.
68	Green Spaces.
69	Leave all current zoning in place - DO NOT BUILD ANOTHER URBAN, MIXED-USE MONSTROSITY LIKE BOULDER JUNCTION!!! Leave remaining one story business buildings as-is and let this neighborhood remain a comfortable place to live
70	everything that is currently within height guidelines
71	Local businesses, the golf course.
72	Major employers (Boulder Health, Ball). Give Peloton a chance to succeed. Existing employment in Flatiron business park located off 55th. Humane Society on 55th. Emerging public facilities (sheriff, coroner). Interior design/home products small businesses (tile, carpet, lighting). Education nodes. Recycle row. Some light manufacturing, existing lower rent office facilities to accommodate a mix of commerce and industry.
73	Access to multiuse path, low rooflines, low density of development. We chose to live in this area for its lack of congestion and relative quiet. We do not want those attributes to go away.
74	Golf Course
75	Large employers, business park, CU East, service businesses, mature trees.
76	It's fine as it is. Boulder has too many five story, highly dense buildings. Enough.

Question 1: Response	
77	Green and open space, but permit growth through increased density The rural-residential character of neighborhoods east of golf course and south of Arapahoe The two lane configuration of Cherryvale and Westview
78	There are a number of small businesses in the area from auto to breweries etc. that it would be nice to keep, but allow them to enhance their options, e.g. zoning currently restricts those microbreweries in the industrial zone from also preparing food. There are some house remodel stores - perhaps allow these types to cluster to be able to visit a variety of specialized businesses supporting remodeling/design (capco is there, thurstons kitchen used to be around, there's a flooring place near the dinner theater, etc.
79	A good mix of housing and business

Question 2

(Answered: 87, Skipped: 20)

What should change?

Question 2: Responses	
1	Upgrades to the streetscape, signalize at 57th. Low impact residential.
2	There needs to be some affordable living in the area. Not affordable per Boulder's standards, but per neighboring counties. I work in Boulder and in no way can afford to live there (unless I want a slumlord for a landlord - been there, done that.)
3	Access to the area via bike paths and sidewalks.
4	Eliminate or isolate the high speed roads. Side streets should be redesigned using bicycles as the primary design vehicle. Parking and turning areas must accommodate bicycles and pedestrians
5	I think the people who live in each area ought to have a final vote on whether or not they wish to change their neighborhood.
6	anything else, especially buildings with set-backs and parking along Arapahoe; better multi-use path on south side of Arapahoe between Parkway and 55th
7	Considering the best interests of investor/developers over the current tax payer base and community members well being.
8	Include separate bicycle lanes along Arapahoe. Extend existing sidewalks on both sides of Arapahoe to at least 63rd St, and consider extending to 75th St.
9	Overall land use pattern. More connections! Break up the super blocks and create new, quaint, memorable streets and places.
10	Establish a connection between Arapahoe Ave. and Pearl Street. Close to BCH
11	Connection and features to make walking and cycling safer and more convenient for visitors and residents.
12	More arts organizations, to complement Boulder Digital Arts, Video Station, Pro Photo Rental
13	BRT and great walk-ability. 55th or Cypress needs better bus service.
14	BRT corridor
15	Less surface parking
16	Need north-south pedestrian/bike connection from East Arapahoe to Boulder Junction (In addition to 30th)
17	A park and low density housing and a small number shops - Nothing over 3 stories.
18	Hard edge for development at east edge of city. Should be very clear when you have entered Boulder.
19	Less pot.
20	Alternative transportation and amenities for area workforce improvements would be great.

Question 2: Responses	
21	NEED MORE PEOPLE. Sticking with the ~4300 jobs/residents is TOTALLY USELESS and makes this whole exercise a waste of everyone's time.
22	Nothing. All your changes bring increased congestion - in the floodplain.
23	Better transit especially regular bus service, safer bike/walk, more shops/services - food, dry cleaners, day care, clean up look of light industrial.
24	We need more RTD routes so there is less traffic heading in and out of town.
25	The industrial areas
26	Pretty much everything else! The street, the coarse separation of land uses, the building form, the street grid (meaning there should be one). Overall, it's a terrible street currently, so much more should be thrown out than kept.
27	The area lacks a grocery store, parks and retail shops. Affordable restaurants and cafés would be nice. The one there are sometimes crowded.
28	Less big business. Remove smoke stacks from Power Plant.
29	Sense of Place
30	The suburban style development and the 6 lane highway. I think you should work on the BRT and some of the bike improvements now and come back to the land use when you are ready to zone/propose density that will support biking/walking/transit and the TMP.
31	more shops and restaurants, bike lanes, more parks
32	More restaurants.
33	More retail, food, grocery and better flow to get commuters out of town faster.
34	Arapahoe is very intimidating with 6 lanes, 45mph speed limit, long blocks. It is a major transportation corridor but not ped or bike friendly.
35	New development should be severely limited, and all new development should pay to maintain levels of service for all city services and facilities.
36	Fewer giant parking lots. More ped/bike connections. More housing; green space.
37	Build residential housing.
38	add passing lane for motor vehicles, cars
39	More small local restaurants and local markets like Alfalfas and a drugstore
40	narrow lanes, provide bus only lanes, install a centerline rail line to transit center and regional bus route hubs. increase business frontage. provide protected bike lane in addition to multi use path.
41	I would love to see an area more like the Broadway and Alpine area. It's walkable to go to market, out for a meal or coffee, etc. There's a mix of housing within this zone too. How to make the Arapahoe Ave. frontage a more appealing place for people to shop and hang out. The shopping center where Wine Merchant is located used to be dead and now it's just hopping.
42	I would love to see an area more like the Broadway and Alpine area. It's walkable to go to market, out for a meal or coffee, etc. There's a mix of housing within this zone too. How to make the Arapahoe Ave. frontage a more appealing place for people to shop and hang out. The shopping center where Wine Merchant is located used to be dead and now it's just hopping.
43	Add additional housing for residents. Not large Single Family Homes but compact, high density neighborhoods that include a mixture of SFH, retail, live-work, townhomes, and affordable units. Develop along Arapaho to create a nice looking street with buildings pulled up to the sidewalk. Make Arapahoe pedestrian friendly and make it somewhere people actually want to walk (or bike) on.
44	We need more regular bus service - not another years long construction project to put in a BART. We have enough traffic issues already, and people out here use cars. It is what it is - not going to change, so that needs to be taken into consideration and accommodated.
45	The strip malls will lead to decay; they should be replaced with mixed used housing that can accommodate many of the lower income workers who work in nearby areas, like the hospital.

Question 2: Responses	
46	Golf course should become a lake park for flood retention. Lake to be surrounded by affordable housing and mixed uses.
47	horrible auto centric & sprawl design
48	This entire area needs to be designated a "Quiet Zone." Trains should pass along this corridor quietly. Each intersection has gates.
49	Like any neighborhood, it's no fun to discuss the type of people that start migrating in when an area changes. With the expansion of the hospital, hopefully this neighborhood does not become the next destination for homeless and transients. Unfortunately we have a handful of pot shops too so hopefully that is regulated as well and doesn't expand unnecessarily as an economic drive in the area.
50	Pull down ugly smokestack...beautify the area...mitigate traffic congestion as traffick heads into and out of town from 75th
51	More housing, fewer office
52	Traffic patterns to divert onto main arteries and off of residential streets such as Cherryvale for safety and ease of travel. A clean up of run down areas and a refurbishment of those worth saving. Safer and better pedestrian and cycling paths along Arapahoe
53	Car dealerships, tacky signs for collision repair, auto sales, fast food restaurants and stores of various kinds, no expansion of self storage facilities, stretches of 28th Street type feeling, high speed vehicle traffic, push industrial uses out or to inconspicuous, already industrial areas north of Arapahoe.
54	Corrent ideas about making the Arapahoe corridor into a apartment house strip or into an area for other high density housing. This would only produce another typical U.S. strip city which is exactly what Boulder should avoid.
55	More bike paths and better connection to the rest of Boulder
56	The feel right now leans too much toward "light industrial," and is kind of ugly. I'm not voting any particular businesses out, but a nice median with trees, extended bike path or lane for safer bike travel along Arapahoe (going west-east), for a start. Any new buildings should be low, no more than 3 story. And the architecture should have some character, not just brick/cement tenement blocks as we're seeing spring up elsewhere in Boulder.
57	Better bike paths. Better flood plain management.
58	Nothing
59	Nothing. No additional commercial or housing.
60	I would love to get rid of the dated strip malls and industrial buildings. I also want to get rid of storage facilities--I think those bring down the property value of the surrounding homes. I think that developers that are trying to build business parks need to also include space for coffee shops, restaurants, open space/parks, etc to make sure their buildings are desirable to work in and live near. There needs to be a bike path and a open space/park area incorporated into the new construction.
61	Limit # of marijuana distributors; better speed control in residential side streets (i.e. Patton and Merritt); NO South turn lane onto Patton from W Bound Arapahoe; no parking on Patton Drive in front of Humane Society Thrift Store
62	Not sure if East Arapahoe has enough neighborhoods that easily access much along Arapahoe without using a car. Too much is paved already. Need more greenery, trees, soften the experience. Retain as much natural space as possible. A fresh grocer would be nice, but hard for them to compete with bigger stores. another coffee/tea shop. Please avoid chain fast food. Remember that 6 months of the year biking may not be possible in Boulder for the young and old, and those running errands or having children.
63	Safe bike access across the corridor for commuting and local trips.
64	There needs to be more office space for doctors and other health care providers so they do not have to waste time and increase traffic driving back and forth across Boulder from their current offices to the Foothills Hospital.
65	Some of the lower cost businesses adjacent to Arapahoe and adjacent to 55th Street should upgrade the landscaping and at least the front side of their buildings. Some of the bicycle and pedestrian crossings could be made better, safer.

Question 2: Responses	
66	Move businesses that are in existing wetland areas and floodways that could be better used and restored to natural areas for plants and animals and peaceful places for people to visit nature. Link these corridors so animals and people can connect without the need to cross roads when possible.
67	I would love for the industrial feel to change. As is, there are lots of empty office buildings that make the space feel abandoned. I would love for arapahoe to become more walkable with shops and restaurants and parks.
68	Add more bus lanes; widen sidewalks in areas where they are now very narrow; trim trees so walks don't stay so icy in snow; reduce speed limit west of Foothills
69	Better landscaping especially at intersection of Arapahoe and Foothills. Put power lines underground. Perhaps narrowing to 4 lanes until east of Foothills and have green way on sides or between lanes.
70	I like the idea of Scenario C with housing and a more vibrant community friendly area. Light industry is boring but important for jobs.
71	Use form-based zoning to define the look of the street, keeping all development under the 55-ft limit. Require a diversity of styles and heights, even within a development, to avoid the monolithic look of the Peleton and Boulder Junction. Build true Dutch-style cycle tracks that physically separate people driving, biking, and walking from each other. Build intersections according to Dutch standards too! Break up the superblocks north of Arapahoe. Remove zoning regulations in currently light-industrial areas and use the form-based code to govern the look. Stop micro-managing zoning and let the market work! Eliminate all parking requirements and again, let supply and demand determine how much gets built. Build a TRUE center-running BRT route connecting with Louisville, removing a lane of traffic in each direction to accomplish this.
72	Connectedness pedestrian bridge, more defined civic centers, public art or prominent landscaping; reinforce and support cultural presence of Boulder Dinner Theater and Avalon, for example more continuous programming for Avalon, provide complimentary functions and opportunities for attractive and safe hanging out spot. Examples: a roof garden, a fountain, a cafe; shallow pond for toy boats a seasonal festival, outdoor music (Thorne Institute)
73	Improve consistency of look. Get rid of abandoned dated eyesore buildings. Makeover to buildings that look old, dated, and dilapidated such as the strip of shops across the street from the Peleton where the Aquatic store is, jimmy johns, etc. Arapahoe is a main artery of Boulder where people get an impression of the city. It would look better with some trees.
74	We need more residential owner housing. Townhomes and condominiums that are 900 sq ft to 1800 sq ft. Spaces that allow for home gardening. If there are appropriate spaces in which new homes could be built this should be allowed. Reasonably priced commercial spaces should stay available for the entrepreneurs looking for this type of space. We should not build large new high rise commercial buildings because there are already too many commuters into boulder and because the best asset this town has is its views as people live, work, and drive through here.
75	Complete sidewalks on at least one side of the street. New bridge under Arapahoe at Old Tale that will not cause flood water to backup. More complete range of business services (grocery, pharmacy, restaurant, etc). Safer for pedestrians and cyclists. Bus pullouts. Train stop. Farmer's Market with convenient parking.
76	The look and feel of the giant suburban arterials. Make them memorable boulevards.
77	Improve Arapahoe Rd. for bikes and buses. The notion that its an "opportunity" to cram more business and residential units into this space is nothing other than a thinly-veiled attempt for developers to cash in without bearing any of the negative impact that will have on Boulder residents - traffic, crammed open spaces, a loss of the small, urban town feel that has drawn current residents to Boulder to begin with. Zane Selvans says, "I would like to think that we would want to urbanize some of these corridors." Well, Zane. WE DON'T!!!
78	no changes to height restrictions, one should not allow builders/developers to buy out of the current height restrictions. That is insane.
79	Need a turn light at Arapahoe and Conestoga Street - Ozo shopping center. Safety issues.

Question 2: Responses	
80	Pedestrian environment is pretty bad - need safe crossings and accommodations for bikes on street. In the short term the speed limit should be reduced from 45 MPH west of 55th with current density and mix of uses. Bus service frequency should be increased and further roadway queue jumps and bus priority locations.
81	Would like to see power plant decommissioned and all power lines buried.
82	Not the Golf Course
83	Housing options, dense walk-able neighborhoods, excellent bike and transit services, more entertainment venues, restaurants, public gathering places, intricate network of multi-use paths, accommodation for reasonable auto traffic (shared streets), inclusionary up-zoning development...
84	Your plans. There are too many people in Boulder. Why am I on a water budget if you've got water for all these people?
85	Road diet on Arapahoe! More non-motorized routes running north-south More retail and work destinations with mixed size, price residential
86	Be nice to have more neighborhood areas that have cafes, restaurants, stores, and access to transit. If the speed limit were reduced, it would make it more pedestrian and bike friendly (although what would be the impact on the commuters going out this way? Getting more public transit along the corridor would be good) and a lower speed limit would allow us to have NEV's to get into Boulder itself (currently, both arapahoe and Baseline are 40mph).
87	More transportation options including bus routes and bike stations/trails. Also more frequent options to downtown from streets other than Arapahoe. Also more focus on fast casual restaurants to serve the businesses.

Question 3

(Answered: 50, Skipped: 57)

How would you improve the draft Vision Statement?

Question 3: Responses	
1	I would like a prime focus on maintaining open sight lines. No massive Pearl Street Wall options. Projects need to provide adequate on-site parking!!
2	I don't have any suggestions, but I think it would be helpful if more people were aware of the proposal so more could get involved. I only know because I work for a business in the area.
3	The current vision element about biking and walking is extremely weak. "provides safe and welcoming places for pedestrians and bicycles" This sounds like the same segregation policy that has allowed cars to be the only design vehicle for too long. Roads belong to all people, not just those in cars. All roads must be designed with bicycles and pedestrians in mind. Car traffic is a dangerous industrial process to be calmed, separated, and restricted - or better yet, eliminated! Any city improvement project must fit into the long-term goal of making Boulder car-free!
4	This statement is written by folks who enjoy upgrades, newness and change. I would allow the people who live in these neighborhoods to draft their own vision statement.
5	East Boulder has a large residential community. The vision changes the current quality of life in the area and reduces it to a business, retail and public areas. Again the focus of this vision is tipped toward best interests of investors and ignores the quiet, peaceful and convenient aspects of the area.
6	Minimize additional residential units. Except immediately adjacent to Arapahoe.
7	Major places are the parking lot in front of bank on 55th and Arapahoe (nw side) some restaurants may be (illegible) but it looks so ready for improvements. Think now
8	VERY against Flatirons housing ANY flood mitigation.
9	No BRT. No big buildings.

Question 3: Responses	
10	It's too broad.
11	It's all there - what gets cut as we move forward is key.
12	Maintaining current use intensity is incompatible with most of the vision "socially thriving," "livable," "sustainable," "Safe," "connected." Good luck. It's a freeway surrounded by parking craters.
13	Let Boulder be rural along Arapahoe
14	It's too restrictive to try to preserve affordable service industry space. Let's let the market determine where is the best place for service (and other) industry uses. We shouldn't be presupposing BRT. Don't enshrine conclusions in the vision statement! Strictly speaking, there already are bicycle, pedestrian, and auto options. They're just not very good. The goal should be a great street for walking and biking, and we should include specific, numerical goals, e.g., for mode share along the street.
15	I don't know what the vision statement is. In light of development happening in other parts of town I think I should be worried.
16	Don't like the future gives #8 retention wall?
17	You are not being bold enough. If the time is not ready to do a bolder plan now, wait. Don't stick us with a lame suburban wasteland and plan for the next 20 years.
18	Growth pay own way.
19	Don't include 55ft, zero setback buildings.
20	More business centric focus.
21	Don't understand the word "inclusive." Who is being excluded? In many ways the area is "cheaper" than the rest of Boulder from a cost perspective.
22	Add costs and who will pay, add numbers (jobs, pop) that could be built all along the whole corridor. 2 big questions: 1 who will pay for all the infrastructure and services? 2 will levels of service for all city facilities and school districts be improved, stay the same, or get worse?
23	Need to review
24	Don't try to make East Arapahoe more urban. Leave the area N. of Arapahoe industrial
25	Build residential housing and increase capacity for vehicles/commuters
26	I think it looks great if a bit lofty.
27	NA
28	It's all the same stuff we hear in Boulder all the time. What are we really getting? We have most of what you say there now, except reliable public transit. More housing will just mean more traffic congestion and parking issues. I would also like to know you will solicit LOCAL developers if you proceed. Not folks from other states that have no stake in what happens here after they build/rent/sell.
29	East Arapaho needs a bold new design that envisions a place where moderate income people can afford to live and there are services for them in a compact urban design.
30	The entire area should be designated a "Quiet Zone."
31	Be sure to keep things low (height of buildings) and local.
32	Less jargon clearer text
33	Put more emphasis on Scenario 3, and reduce emphasis on industrial uses in Scenario 2 and rthe Current Trends
34	Require that any development adhere to a strict 55 ft height limit. Require that any buildings adhere to a 35 ft setback form all public sidewalks and bicycle trails. Downtown Boulder should be the local "attraction center". Therefor avoid all concepts that purport to develop east Arapahoe Ave.into another "vibrant shopping area".
35	I haven't seen it.
36	Statements such as: "offer space that welcomes people" should be the private sector's job. The City can do that on transportation facilities that it owns and operates, but don't force that on the private sector.
37	I'd axe it. I do not think East Arapaho should be developed at the expense of existing neighborhoods.

Question 3: Responses	
38	I have no idea what the draft Vision Statement is, and I cannot find it on the website. I like the idea of mixing the current B & C scenarios. I prefer scenario B, but know that there also needs to be some additional housing built.
39	Emphasize mix of residents - East Arapahoe has lots of families as well as multi-unit developments; maintaining this balance and NOT skewing it too far to one or the other would be a detriment to the neighborhood
40	You should repeat the Vision Statement for reference here, haven't read it in a few weeks.
41	I have to re-read it. I do not like anything related to making this area vibrant! Vibrant belongs downtown. This was once a beautiful area, and to the extent possible that should be restored. It should be a quiet peaceful area where the industrial uses provide good jobs, attract the high tech businesses, and they in turn can attract good employees.
42	Should focus less on zoning and use and more on the look--form based coding!
43	provide definitions. for example, what is a district? how do districts overlap? Do they have centers? what is the scale?
44	Speak more plainly. State pros/cons for each scenario. State funding and costs for each scenario.
45	Not sure. It is vague.
46	Set the bar beyond a standard development model such that the project will be emulated and considered innovative.
47	Stop trying to "improve" the area by developing the living tar out of it, and instead realize that is has already accomplished the goal of becoming a mixed-zoning area with terrific residential, suburban neighborhoods, several established and extremely well-liked business (Snarf's, Video Station, Pica's, etc.)...several Boulder-affordable office buildings, an accessible medical campus, and a buffer/transition area between the already overbuilt 29th street mall area (which now stretches more or less to Foothills) and the open space which we so dearly value.
48	Haven't studied it yet.
49	The draft vision statement says nothing about protecting the quality of life of existing residents of the East Arapahoe neighborhood, nor does it refer to prioritizing the wishes of the people who live in this neighborhood NOW above those of the people who want to develop the neighborhood for their own profit.
50	Way too timid. Get aggressive in bringing density to this area of the city.

Question 4

(Answered: 74, Skipped: 33)

What ideas would you suggest for transportation enhancements and improving mobility?

Question 4: Responses	
1	Be realistic about the need for auto transportation for young and old citizens. Improve the the quality of the medians. Do not hinder the free flow of traffic.
2	Please no more road construction. We've had enough of that already.

Question 4: Responses	
3	It's hard to list them all. 1. Make more bikeable roads. For instance, why is the speed limit 35 on 55th st S. of Arapahoe, when there are 25 mph speed bumps? Make the speed limit 25 for this whole stretch and make it a ""bike boulevard."" Cars can take Foothills, or go bike speed on 55th. 2. Eliminate 1 of 3 lanes on Arapahoe and reduce the speed limit to 35 mph. Expand pedestrian and bike space and reduce the number of turns available to cars - which are just conflict points with bikes and peds. Separate cars from liveable spaces with bike lanes, trees, etc... 3. Separate buses from bike traffic - buses may be the most dangerous thing on the roads to bikes. 4. Expand trail widths to allow more traffic. Post signs on trails about keeping to the right side of the trail, keeping dogs on leashes, and traveling single file. 5. Build WAY MORE overpasses and underpasses in the vicinity of foothills pkwy, Arapahoe, and any other high speed motorway. Build such over/underpasses at bus stops. 6. Rezone and otherwise encourage businesses to put their store fronts facing large sidewalks along Arapahoe with parking in rear. Make Arapahoe a street for people, not cars. 7. Build electronic toll collectors (like NW Parkway/E470) and charge drivers for the use of Arapahoe. 8. Eliminate "nature preserves" in the city. Parks should be designed for humans or eliminated, otherwise they just act as barriers that require car-based transportation to get past. 9. Eliminate unnecessary ""islands"" in streets, which narrow lanes, waste space, and force traffic into bike lanes. For instance, the islands on 55th street crowd cars into the bike lanes. The islands on Arapahoe make the street wider without providing any benefit to cars, bikes, or peds.
4	Arapahoe is congested. Any new development will only add to the congestion. You can offer public transportation, but that does not mean anybody will find a reason to use it.
5	anything that enhances bike and public transit comes at the expense of an already crowded vehicular access into Boulder and exit from Boulder, especially during rush hour
6	Many people in this area are elderly or families. While many ride bikes or walk most do not use bikes as a form a transportation. You need to define what the purpose of this vision accomplishes and who it serves. Arapahoe was just "redone" at great expense and constricted access in order to put in a bike lane and sidewalk for some unknown reason - capacity was not increased, no one rides their bikes on Arapahoe but comes down Baseline. Now you're proposing a mulligan on a tax payer "enhancement" that didn't work??!!
7	improve or provide along-side road bicycle lanes on major routes (e.g. Arapahoe, Valmont).
8	More neighborhood side streets.
9	see #1 above
10	1) Bicycle and limited stop (illegible) service of the JUMP would provide versatile options to travel along and beyond Arapahoe Avenue.
11	More pedestrian crosswalks for Ball employees, etc. to cross Arapahoe safely.
12	Reduce speed limit to 40. Remove stop light on 47th - improve light BCH. Increase transit
13	Protected bike lane on Arapahoe and traffic calming/lower speed limits.
14	Better sidewalks/bike lane options
15	Make any new side streets in the area narrow - like in downtown/the Hill. No 50' curb to curb side streets. Reduce setbacks, make the buildings form an outdoor room.
16	Have most of the transportation designs in place before all of the housing.
17	Bike share, finishing the connectors between bike paths and walkways.
18	Slow traffic on Arapahoe down! Virtually impossible to cross now. Make it easier. Real BRT!
19	Your ideas bring lots of congestion - leave us alone!
20	Consider all age populations and in addition to parking spots for bikes, etc. Provide adequate parking for CARS.
21	BRT good, low noise, low polluting, slower car speeds, safer biking/walking.
22	Increase sensible rapid transit and public transportation. Increase bike friendly environment.
23	Work with RTD on routes coming in and out of the Boulder Junction.
24	More bike paths

Question 4: Responses	
25	First and foremost, get over the notion that we have to have vast capacity for automobiles. And get beyond the notion that we're going to get a great street, with lots of people walking and biking, if it's still primarily designed to move lots of cars. That's not going to happen. It's never happened. A great street can only occur if we really stop prioritizing cars. Saying we're not prioritizing cars isn't enough. We have to really do it.
26	Better bike lanes.
27	BRT
28	Road diet. You can't make a livable place with a 6 lane hwy down the middle.
29	hub idea is a good one
30	Congestion-based pricing for commuters and parking
31	I bike a lot, but don't like the mixed use path next to a 6 lane road. Too much cross traffic.
32	More rapid connector routes for the business dense areas such as Flatirons Park. More lanes of 7 to get out of town commuters out of the way.
33	Linking area via transit to Boulder Junction would be great. Look for way to connect Pearl to Arapahoe via bike/ped path between hospital and 55th with bridge/bike path.
34	Free eco passes paid by parking fees or other user fees.
35	Stripe all sidewalks at cutouts/driveways.
36	Better bike paths and safer crossings.
37	With the amount of service jobs in Boulder, vehicle commuters need better access. Mass transit will not (illegible) for commuters coming in.
38	Specific plans/details regarding parking availability - cars.
39	add passing lane for cars
40	Improve left hand turns from the neighborhoods to the south onto Arapahoe Ave. Very long waits at the lights at 48th St and Eisenhower
41	The speed of the cars along Arapahoe and 55th are just too fast to make biking and walking fun. There needs to be more off-road access for bikers and pedestrians. However, with this being a main thoroughfare connecting to East County, have to make sure a bottleneck doesn't develop.
42	Make it difficult for cars to drive fast on Arapahoe. Include a separate bus lane with a protected lane for cyclists. Utilize the Bound line to create T.O.D. and make it easier for people to take the bus than to drive. Increase frequency and routes of Bound to make it easy to get to Pearl Street and CU. Include street trees and convenient pedestrian crossing medians and signals.
43	MORE REGULAR RTD SERVICE - not just on Arapahoe, but on Baseline (to assure the whole area is properly served). Also, if you want to encourage biking, you'll need more than 2 hooks for bikes on the front of RTD busses that are already full by the time they get to our neighborhood from Lafayette/ Louisville.
44	complete bike and ped connections. make more local roads. traffic calm arapahoe
45	This entire area should be designated a "Quiet Zone."
46	Instead of the large RTD buses coming through the neighborhood, I would use smaller, quieter buses to reduce noise and pollution.
47	Improved bus service with outside curb lane restricted to buses, bikes and turning vehicles
48	traffic calming features, improved trails and paths for pedestrians and bicyclists, enhanced shuttle bus service like Denver's 16th St. mall with less frequent service and fewer buses linking Arapahoe fro Folsom St. to 75th St.

Question 4: Responses	
49	We, as a forward looking community, need to accept the reality that more transportation is not always a benefit. Increased mobility is what has encouraged the workforce inflow glut that we now have. The Arapahoe corridor cannot be enlarged without sacrificing even more of the amenities that attracted most of us to Boulder. There will always be those who, coming from more crowded urban areas, cannot see or appreciate to openness that is so precious to many of us who settled along the east Arapahoe corridor. Somehow, we must come to realize the unended growth is not only unsightly but, even, unhealthy for us all.
50	There is already quite a bit of parking, but perhaps it could be made more accessible once new buildings are built or existing buildings are modified. It would be nice to have safe bike travel along the Arapahoe corridor. If buses ran later in the evening on weekends (and this is true throughout Boulder), that would support any bars/restaurants that might open here, as people don't have to pay for a cab or worry about driving home after drinking.
51	Better connection from 48th to Pearl. Improve Pearl/Valmont east of 55th.
52	Boulder has tried without much success to encourage public transportation. The City's ultimate response has been to widen the major traffic arteries -- thus allowing more cars and more traffic. Not a good solution for the neighborhoods bordering these city highways. On the other hand, I don't have any good solutions to offer.
53	I detest the idea of fast tracts and a train blowing its whistle 70 times a day. Put in light rail on 36.
54	A better bike/pedestrian path would be needed. I'm not sure of the current RTD schedule/route in that area so I can't speak to that, but it might make sense to have some route that goes from Table Mesa park n ride to that area of Arapahoe (maybe along 55th) and then from Arapahoe & 55th the bus would drive down Arapahoe to downtown Boulder.
55	Improved bike lane access between 44th and Cherryvale; Easier way to use public transportation to get from East Arapahoe neighborhoods E of Foothills to Table Mesa Park N Ride (would make it easier for commuters to Denver); more family friendly parks/open space, E of 44th Street
56	Please, please no bus lane down the middle of Arapahoe. Current busses are almost always empty. Timing of the Traffic light coordination and better management of cars would improve the safety for the current few pedestrians and bike commuters. The ingress and egress of traffic from the gas station on the east side of 55th is dangerous. Drivers often cut/cross between lanes on 55th. Also, bikes often use sidewalks and cross and cross with the pedestrian lights and then switch to the bike lane. It is impossible to anticipate where they are going. Also bikers come behind cars in right turn lanes, bikers are going straight, very hard to watch behind and beside when turning. And since you asked!...the right turn lane going south at 55th and Arapahoe is confusing where there are suddenly two left turn lanes, confusing to know which lane for going straight. I've seen many cars confusedly and suddenly changing lanes.
57	There is already good mobility, lots of bike paths, wide sidewalks. We don't need checkerboard narrow blocks.
58	Outlying parking structures to encourage use of busses / trains / bikes, walking, multiuser paths. Look into the cost of making an ECO-PASS available to all who live in Boulder City limits and a special Pass for all those who live in Boulder County. The current program that is limited to blocks of homes around town is very limiting for those of us who don't have the time to coordinate another group of homes. Would it really cost much to have an assessment tax on each home for ECO-Transportation? I've often times envisioned individual's and families to "Sponsor" other individual's and families for a one year period. The purpose is to help these individuals / families to use bikes-buses-walking routs around town. The sponsor would help them to choose the proper equipment (such as bikes / trailers / bags) for going to the stores, helping kids get to school, options for getting to work. The Sponsor would accompany them on their initial rides / travel until they are comfortable with going on their own. I feel a years commitment helps to teach the variations for the four seasons we experience in Boulder. Then the Sponsored individual or family would sponsor someone their second year or sooner if they are confident and really succeeding.
59	Better sidewalks and bike lanes
60	Dedicated bike lanes or widen multi-use paths on all of Arapahoe; more bus-only lanes; earlier Jump buses on weekends;

Question 4: Responses	
61	Connecting this area to Boulder with frequent bus/rail service would be terrific. We drive a lot now. Only time with bike is down the bike path to Boulder. Faster to go down Arapahoe but street is very busy and dangerous.
62	See earlier answer.
63	attractive and comfortable bus stop shelters. connect places with a zip line (?) withing a district...
64	The obvious question that everyone asks is what was the point of the construction on Arapahoe if it wasn't made into 2 lanes each way at least to 75th St. It is already a congested route. Add more bike paths to connect Arapahoe to downtown Boulder without going on streets.
65	Bike trails are always helpful wherever they can be incorporated into the city plans.
66	Train stop to Denver. Focus on pedestrians, cyclists and mass transit.
67	Separated bus and bike lanes with more landscaping. Reduce ROW for autos.
68	Improve the road with bike lanes, and bus pull-out lanes to improve traffic. No need to create a bus diamond lane. That's overkill and only increases road expenses, and forces established business and neighborhoods to deal with set back issues, etc.
69	See previous response. Work with RTD to evaluate the success of bus service on Foothills Parkway, keep healthy local bus routes such as 206. Consider non-fixed route service as a way to move people in adjacent neighborhoods to transit nodes (Via is not going to be able to do all the lifting as we age).
70	Curb cuts at all intersections to improve mobility of wheelchair-bound residents and visitors.
71	See #1
72	Stop packing this place with more apartments and putting more people on the road.
73	Increase the JUMP type service. Also, some larger rapid transit out to lafayette - can we get some of those commuters to not have to come down arapahoe, by having some large park'n'rides out by 75th. Extra bike and pedestrian access is nice, but if arapahoe stays at the fast 6 lanes (or even fast 4 lanes), how about a quieter street (or bike ways) running parallel to arapahoe, but a block or two to the north?
74	More lanes on the road to ease congestion. More bike access (both paths and b-cycle). More frequent bus routes to downtown and other hubs from streets other than Arapahoe.

Question 5

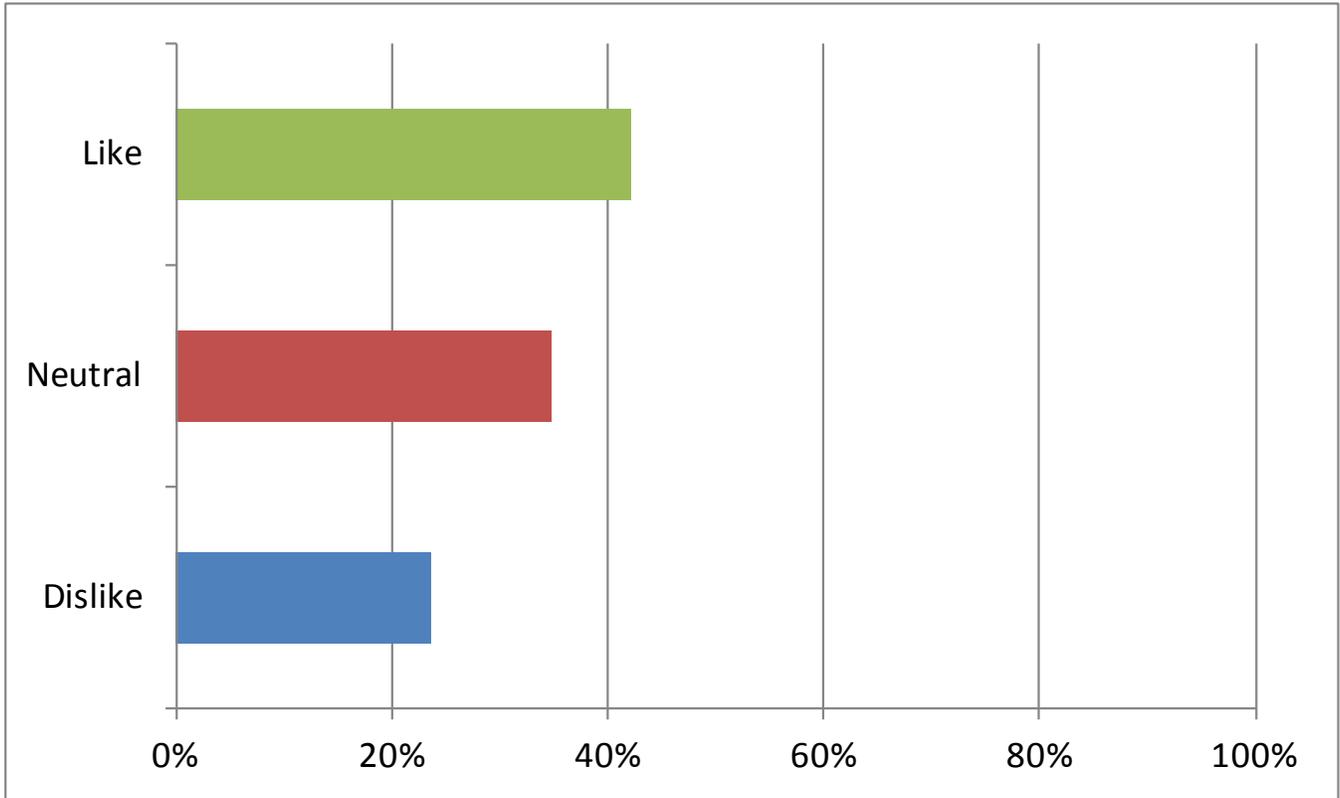
(Answered: 91, Skipped: 16)

Menu of Ideas: Please tell us what you like or dislike about the ideas in the future scenarios.

Question 5: Responses	
1	what about a connecting pedestrian bridge from CU's East Campus between 30th and Parkway? this would enhance viability of small businesses on Arapahoe
2	Most of these items are currently in place - it would seem spending tax payer dollars to "decorate" what is already in place doesn't seem cost effective. Currently 55th Street is so busy it takes current residents forever to get out onto Baseline or 55th in the mornings and evenings. Your proposals will just increase current traffic problems. Many people commuting to the industrial park on 55th come from outside Boulder - I don't see them riding bikes (despite the new bike lane on 36...)
3	A) Bike sharing installed in strategic locations for making short trips along and outside the Arapahoe corridor. B) Express RTD bus routes with key stops on Arapahoe.
4	BCH Supports - 1) Medical Zoning as a primary use in the corridor. 2) Connection of 48th St. to Walnut - providing another access to BCH. 3) More commercial and housing options in the corridor. 4) Enhanced walking, biking, transit opportunities in the corridor. 5) Open to joint campus opportunities with Ball Corp. 6) Food truck allowances in the area near hospital. Glad to participate!
5	More arts and cultural organizations
6	Do NOT change the golf course. It provides a lovely place to be, serves many seniors and should not be changed. You cannot take away every east west street that provides quick, easy access out of the city. Pearl is now congested, Arapahoe should not be.
7	Most of these sound great, though details matter a lot. E.g., what is an "improved intersection"? Is it improved to move cars through faster? Or is it narrower, safer, and easier to walk across (from one streetfront door to another across the street)?
8	BRT
9	The area drained by South Boulder Creek was the hardest hit in the flood of 2013. Careful planning will have to be made for all of this area, especially along the lowest lying areas. I would love to see ecological restoration of waterways, but I wouldn't want to see much building along these areas.
10	This is a terrible survey. Too vague, so some of my answers are probably inaccurate. For instance: I'm pro-recycling, but another facility out here? We already have EcoCycle.... And more apartments? What does that mean? Where? More people biking? SURE. But I don't want to force the issue by making driving/parking difficult. And what does "attractive boulevard" mean? If it's what I see going up in Boulder right now, NO. The way these questions are obviously guided to elicit answers that support the current "vision" statement is offensive, and results should be under question.
11	Most important is much more affordable housing.
12	There should be more new retail and services and housing in more places, not just 55th
13	The entire Corridor needs to be declared a "Quiet Zone." There are gates at the intersections for the trains. This is really a backward moving idea if noise reduction is not the first priority.
14	Any new building: #1, #3, #5, #8, #10 should remain in the city restrictions of height regulation and not go above (i.e.) east Pearl construction
15	Anything that is done to bring more living space and more people to the area is a problem.

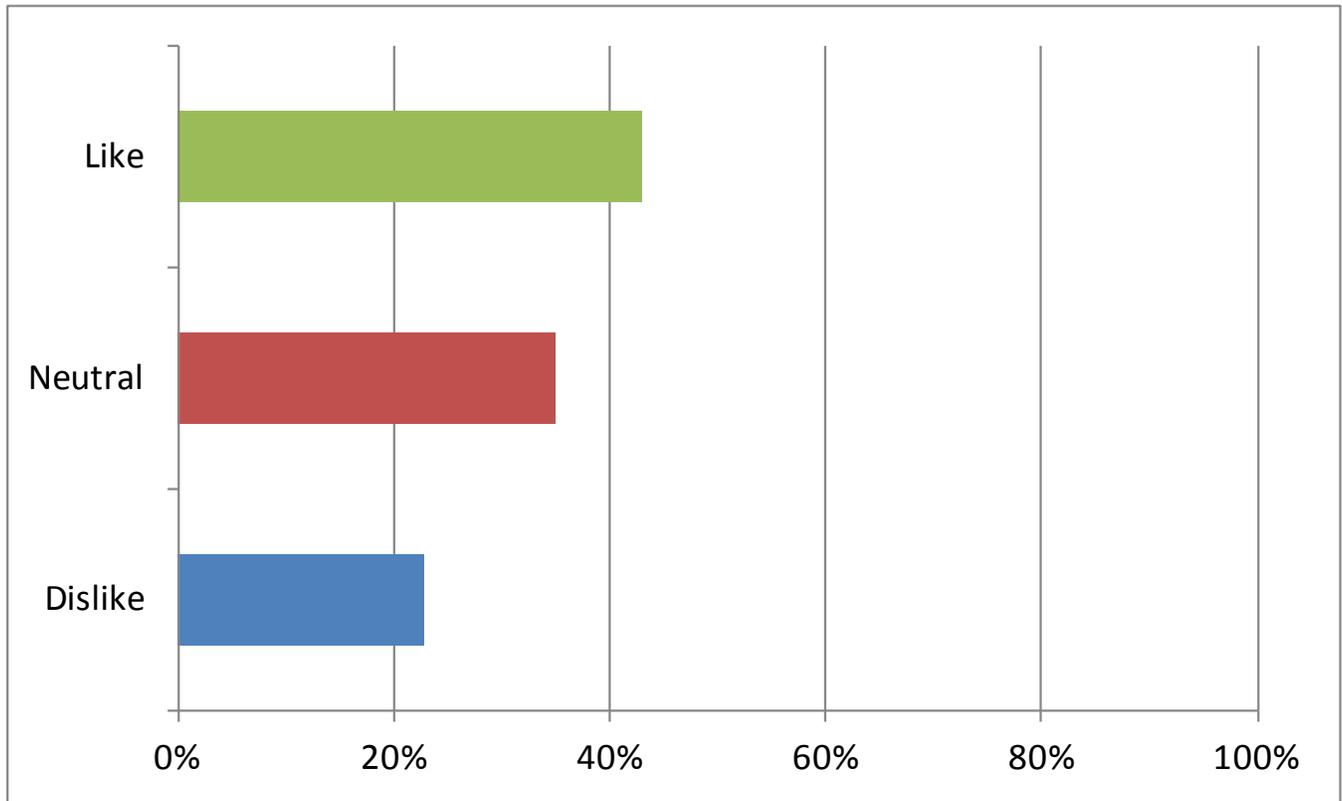
Question 5: Responses	
16	From scenario B I really like the 55th/Arapahoe idea (Retail, Services, Some housing, Arts and Culture, Mobility Hub) and the 55th Street North (Retail to serve industrial park). From scenario C I really like the 15-minute Walk (Enhanced pedestrian safety and connections), 55th/Arapahoe (New housing in select locations, Dinner theater and other businesses become part of an art center, Mobility hub, Shops and restaurants--this sounds very similar to scenario B), and the housing choices. I think having mixed use housing instead of just industrial parks would be a nice way to bring a community to that area while still allowing businesses to have space. It would need to be done well so that the housing remains desirable for the people living there.
17	Concerned about additional housing. Can the current streets support more traffic? Really don't want to see more lanes on Arapahoe.
18	I have marked "dislike" for a number of these ideas (5,8, 10) because I do not trust the City of Boulder to implement them well. I worry about additional big, ugly, and overly tall apartment buildings similar to those which have now destroyed the environment in the area around Pearl and 30th. If we are worried about car trips generated by the residents of these behemoths, let's just build a convenient bike path from "Pearl City" to the hospital and other businesses on Arapahoe. Many people, you must know, do not want to live extremely close to their places of work because they prefer variety in their lives. In addition, many of those who start out in apartments, will eventually want to move to their own houses. What will happen to housing prices when these people want to leave their apartments to secure more living space?
19	Workforce housing of the type we need, for families, will not happen, and this is no longer an appropriate location, if it ever was.
20	Your page only allows survey respondents to select one item they like, one they find neutral, and one they dislike. I hope this isn't an intentional way to limit responses and narrow the scope of feedback that the "Envision East Arapahoe" people don't want to hear.
21	Construct the remaining cross-section of Pearl Parkway to increase east-west roadway choices.
22	Why am I not able to select more options above?? I DISLIKE all options that expand development for housing, industry, energy generation, recycling, medical or retail. I only like the ideas for ecological restoration and better nature access.
23	I hate city council and planning and will do everything in my power to vote current city council out. You're all a bunch of greedy assholes.
24	More frequent bus routes. Better traffic flow on Arapahoe.

1) More light industry north of Arapahoe Avenue (Current Trends scenario)



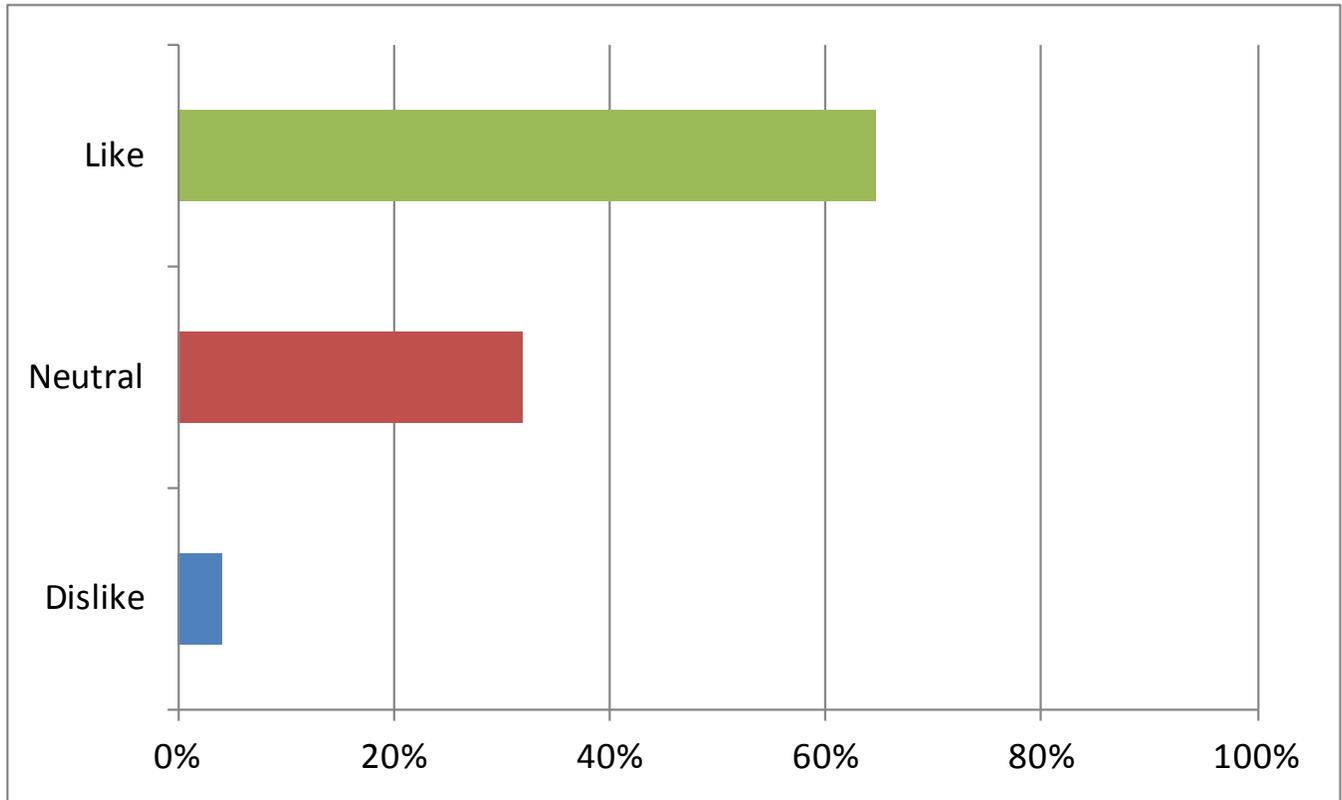
	Like	Neutral	Dislike	Total
	41.98%	34.57%	23.46%	
	34	28	19	81
Comments				
1	But with less surface parking.			
2	More isn't necessarily as important as affordable space for new biz incubation.			
3	Have to accommodate somewhere have been good neighbors.			
4	Good if intensified			
5	Light industry is better than HOUSING.			
6	Dense office space w/parking.			
7	There does need to be some light industrial but with less parking.			
8	More is needed.			
9	Have you heard of downzoning?			

2) Affordable space for necessary services (e.g., car repair) (Current Trends and Districts scenarios)



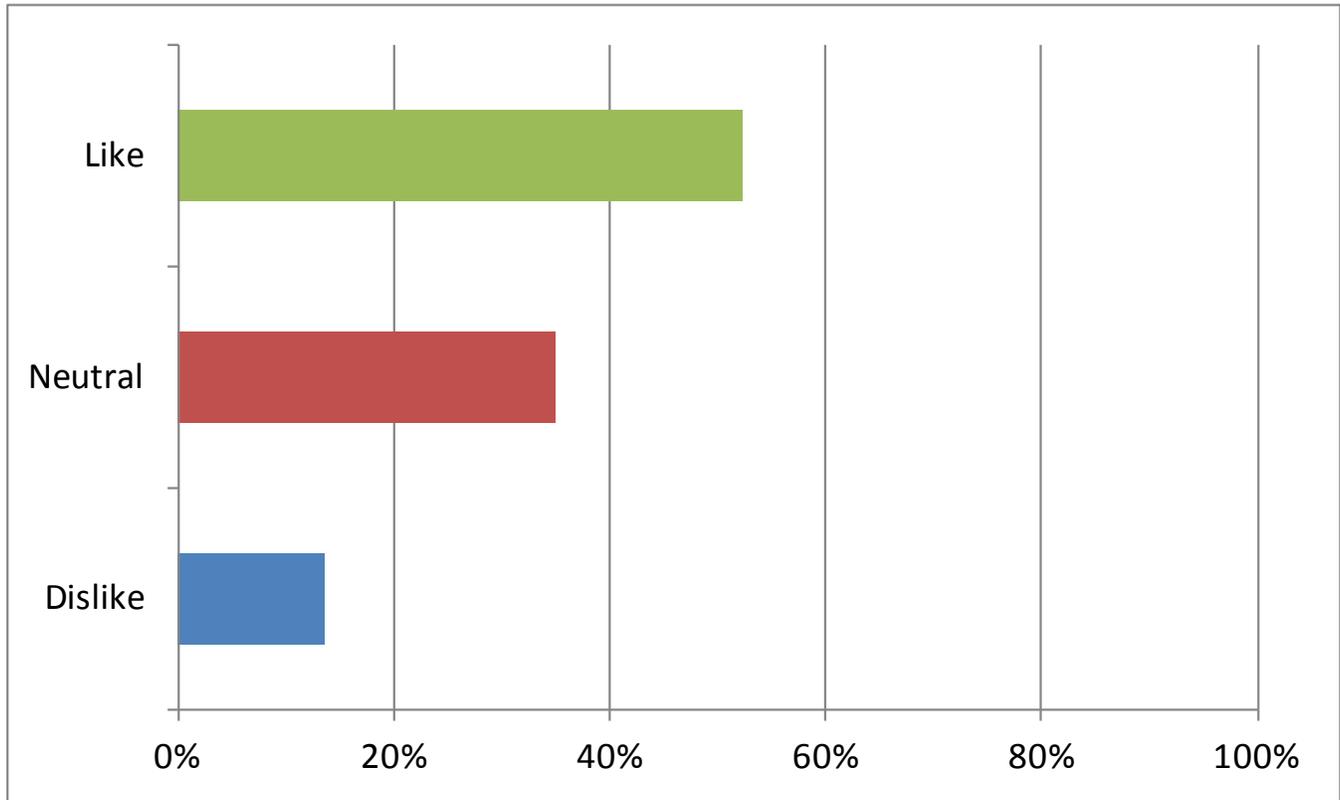
	Like	Neutral	Dislike	Total
	41.98%	34.57%	23.46%	
	34	28	19	81
Comments				
1	Would be nice to de-emphasize surface parking for these things. Area around subaru/acrua for example is terrible.			
2	We can provide these amenities much more space efficiently.			
3	We already have car repair.			
4	Don't let this be the parking storage space entry to Boulder.			
5	It would be nice but not a must have.			
6	Fisher Auto Exists			

3) Boulder Community Health with nearby places for medical-related offices and small retail (Districts scenario)



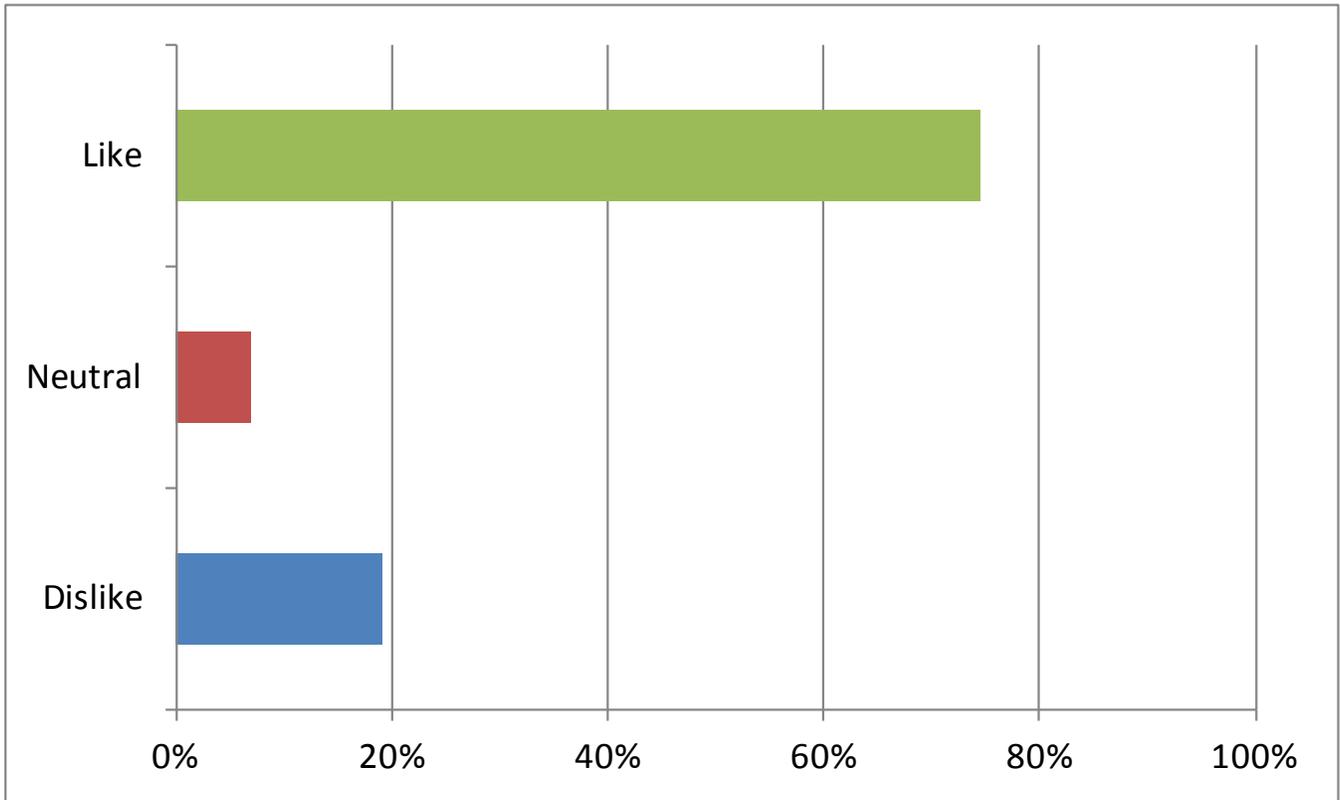
Like	Neutral	Dislike	Total
64.47%	31.58%	3.95%	
49	24	3	76
Comments			
1	But with less surface parking.		
2	Needs more food options.		
3	Good paying jobs and environmental stewards.		
4	Yes.		
5	Ease of access for people.		

4) A new east/west connecting street (i.e., Walnut/48th Street) (Districts scenario)



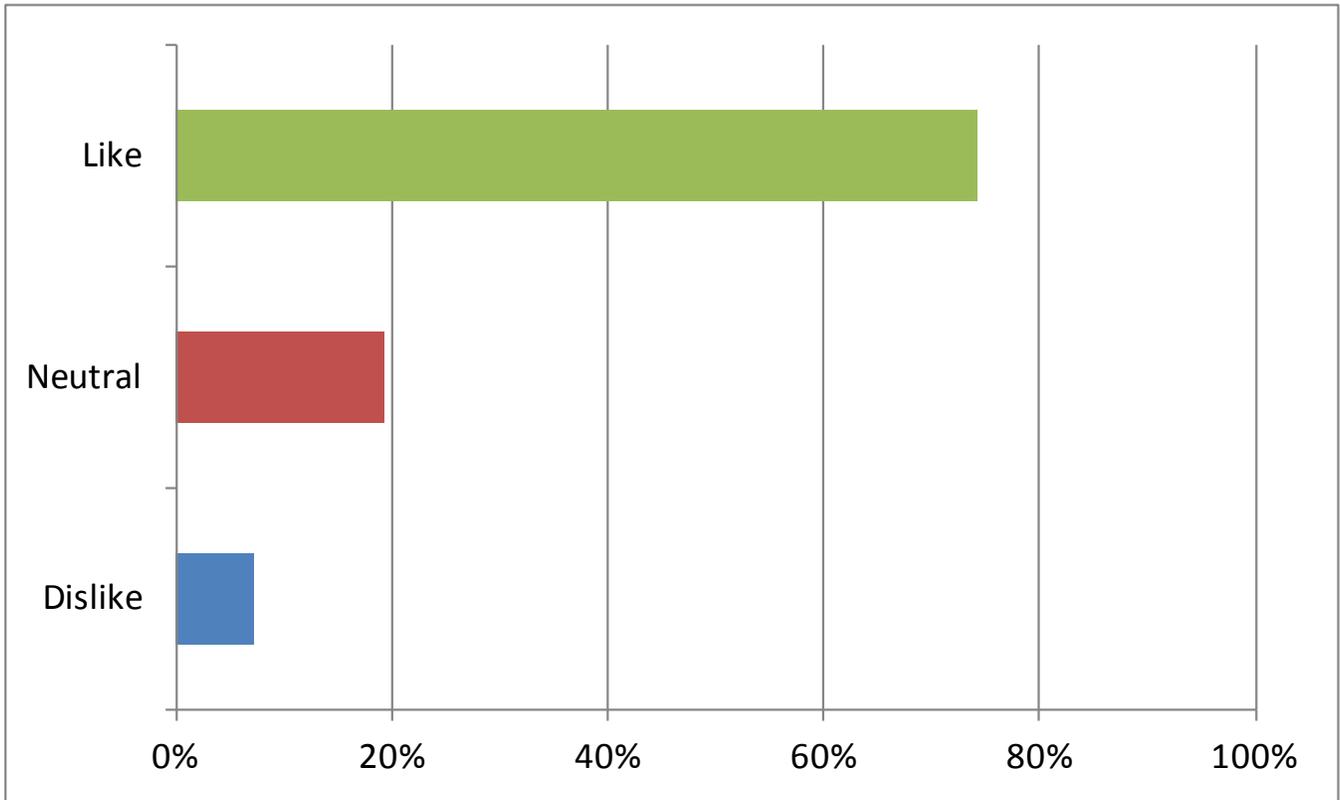
Like	Neutral	Dislike	Total
52.00%	34.67%	13.33%	
39	26	10	75
Comments			
1	No more car-centered streets! Walnut is a calm, dead end street with planned trail connection. It is an ideal candidate for and east-west bicycle boulevard, not another dangerous car street! Walnut near downtown is already close to being a bicycle boulevard, with 25 mph speed limit and car dead-end at Folsom. This design should continue as far east as possible, with more bike-friendly improvements!		
2	YES YES YES		
3	Not sure this is worth messing with S. Boulder Creek Greenway.		
4	Never thought about that.		
5	Yes! break up the impermeable super blocks!		
6	Yes - Road Diet		
7	Ease congestion on Arapahoe.		
8	Definite need!		

5) Small shops, dining, offices and some apartments north of Arapahoe at 55th, along 55th St. (Districts scenario)



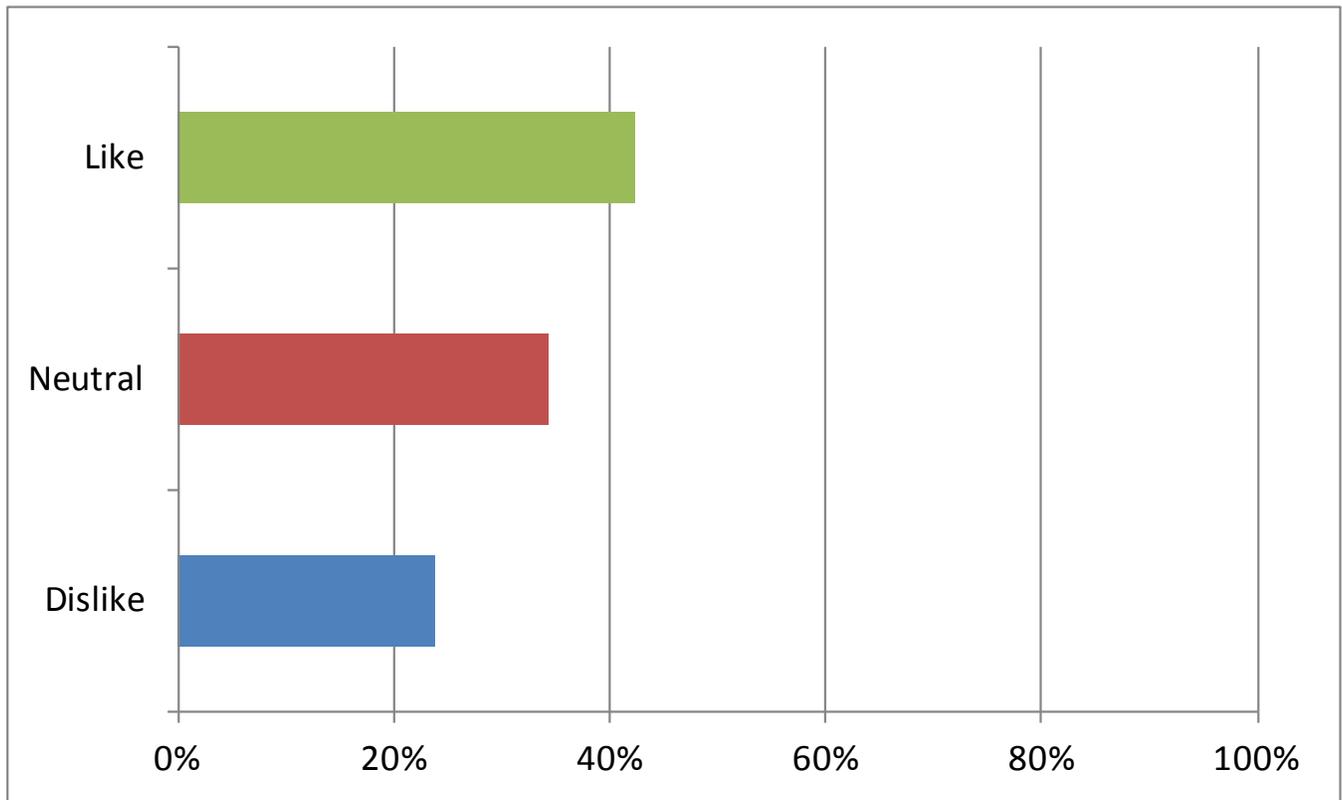
	Like	Neutral	Dislike	Total
	74.32%	6.76%	18.92%	
	55	5	14	74
Comments				
1	more housing density! more people to walk to all these wonderful things!			
2	Don't lump together dining, offices, small shops ok but no more housing. No flashing yellow lights.			
3	Some apartments - no big 55' complexes.			
4	Demographic would support dining and shopping.			
5	None of these make sense without much higher land use intensity.			
6	Take a picture of the apt built on 30th and Pearl and VOW never to replicate this on Arapahoe or any place else in Boulder.			
7	The high density new construction along Pearl (East of 30th) opened eyes. This type of development is unacceptable along Arapahoe.			
8	Small! not another Boulder Junction.			
9	Very much needed especially on 55th.			
10	Like a lot			

6) Improvements to sidewalks and intersections (Districts and Housing Choices scenarios)



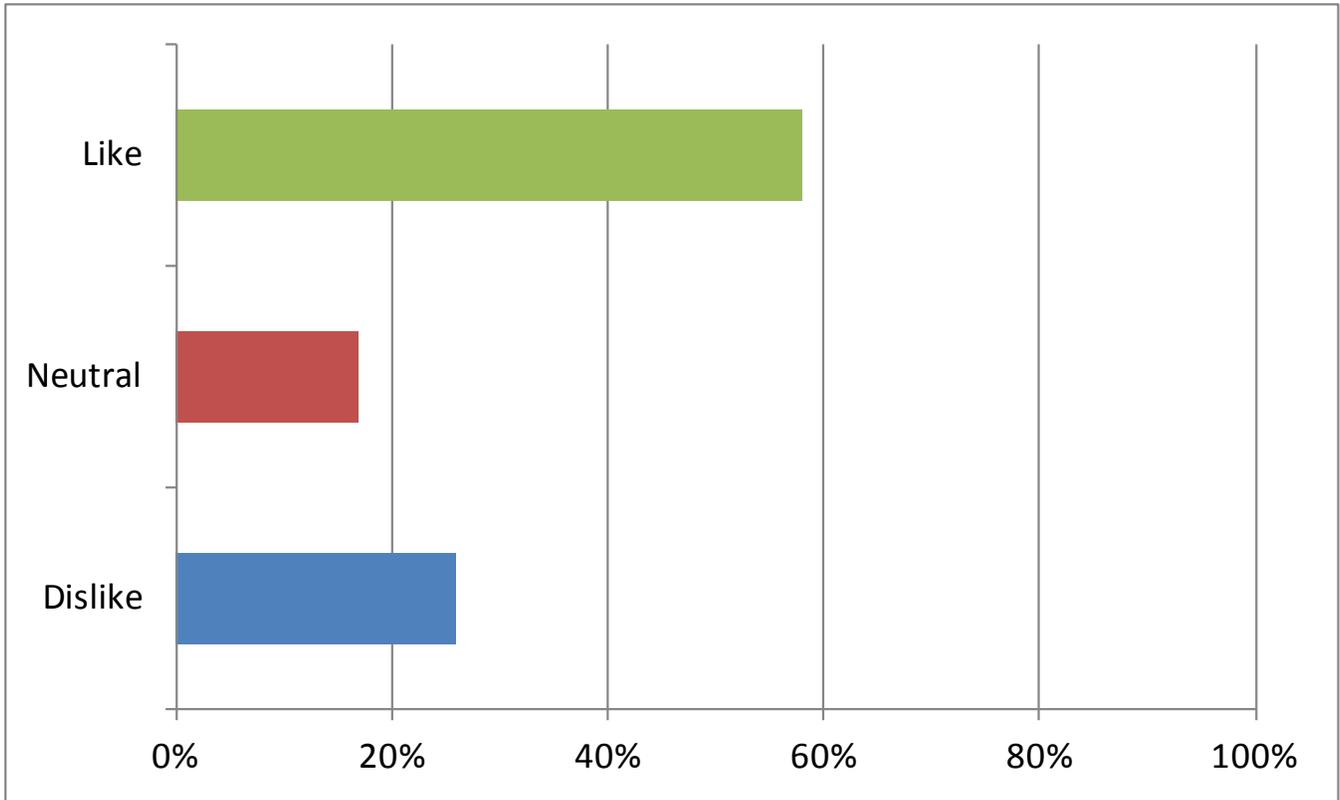
	Like	Neutral	Dislike	Total
	73.97%	19.18%	6.85%	
	54	14	5	73
Comments				
1	Essential!			
2	Wide sidewalks that accommodate bikes everywhere would be great if there's not going to be an on-street bike lane.			
3	YES YES YES			
4	Desperately needed.			
5	None of these make sense without much higher land use intensity.			
6	It is fine to improve sidewalks but not if it means option C!! or B!!			
7	Traffic light at MacArthur. Improved sidewalk MacArthur to 48th on South Side of Arapahoe.			
8	Complete trail connection N. side of Arapahoe.			

7) Place for energy generation, recycling, eco-district (Districts scenario)



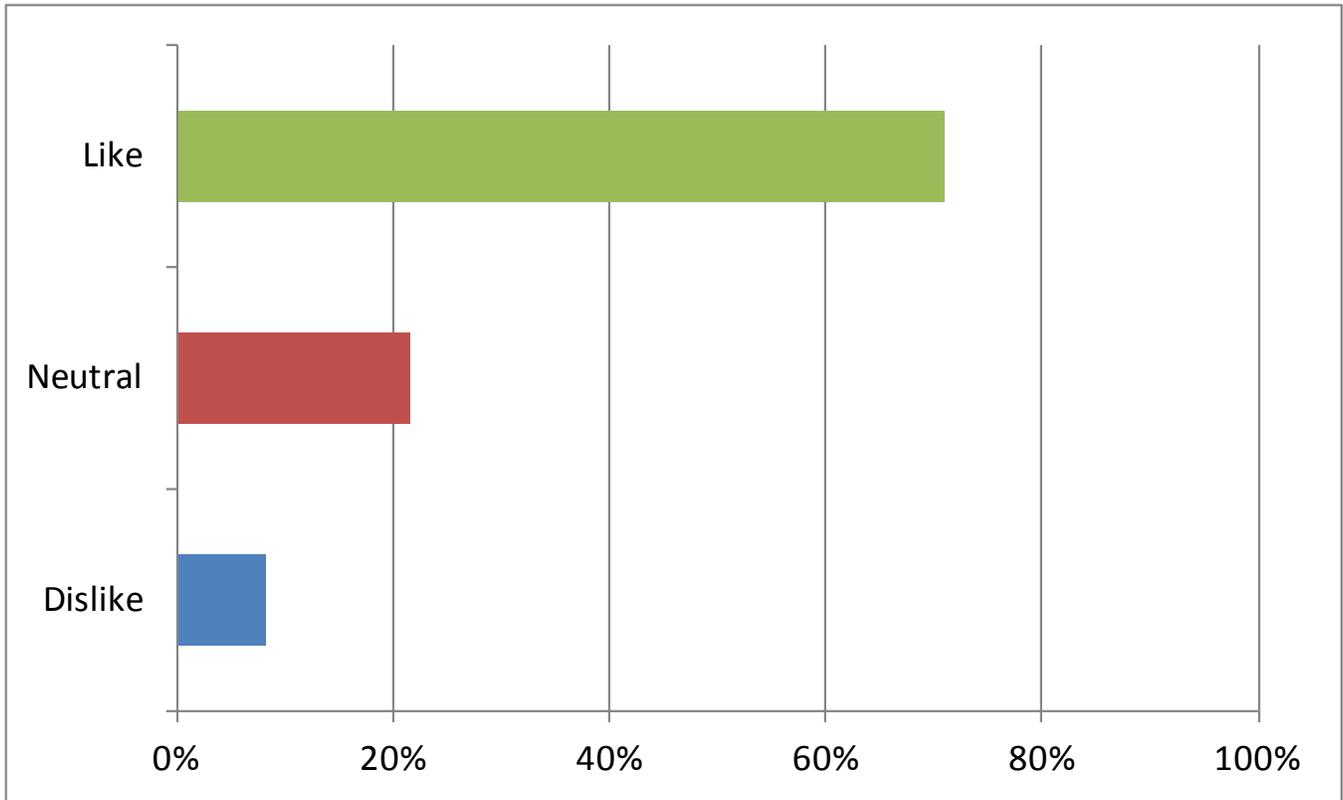
	Like	Neutral	Dislike	Total
	42.11%	34.21%	23.68%	
	32	26	18	76
Comments				
1	Enough of those already.			
2	Prioritize the services of Recycle Row including expansion.			
3	Nurture these uses.			
4	None of these make sense without much higher land use intensity.			
5	Move it out of town			
6	not needed, already have elsewhere and close.			
7	We already have it			

8) Small shops, dining, offices, and housing north of Arapahoe at 55th, along 55th St. in walk-friendly neighborhoods (Housing Choices scenario)



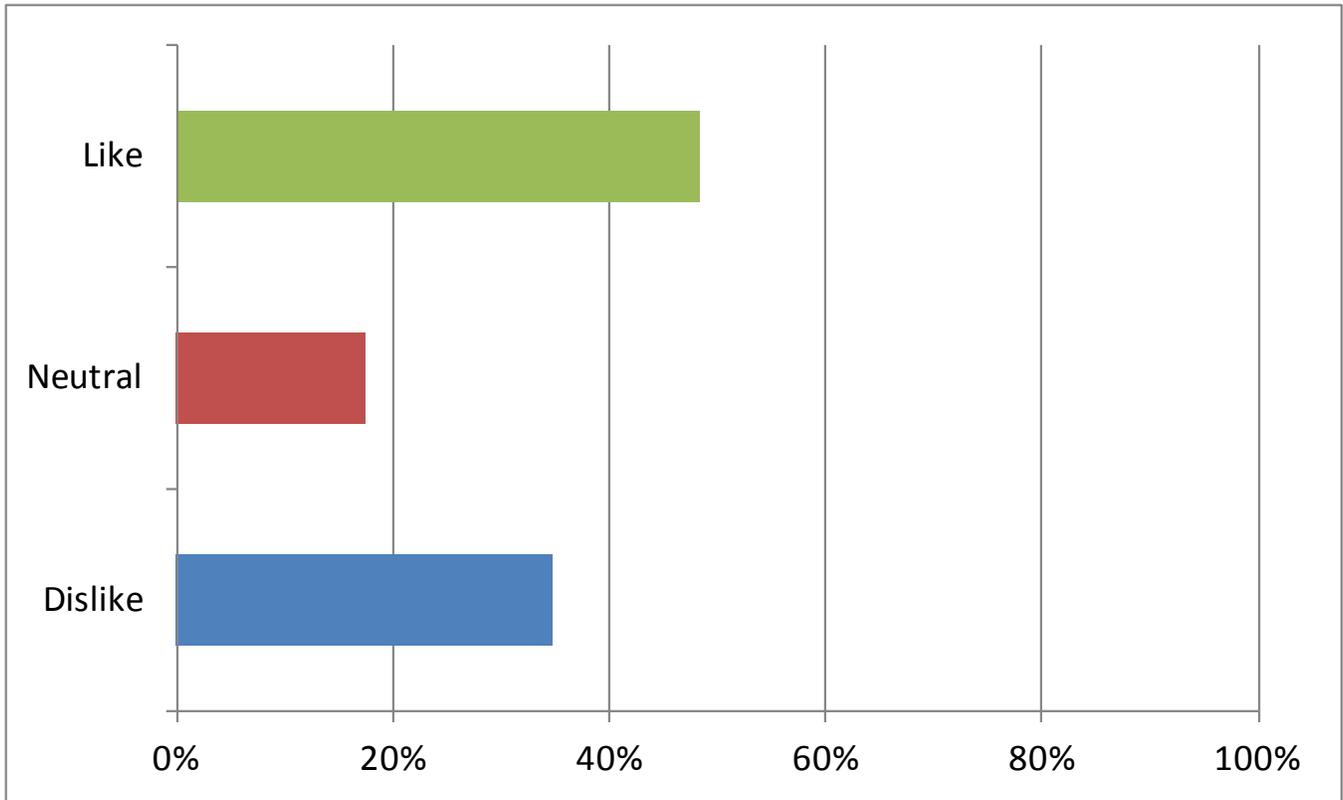
Like	Neutral	Dislike	Total
57.69%	16.67%	25.64%	78
45	13	20	
Comments			
1	Better walkable neighborhoods would fix #1 thing I dislike about this area. Need to be able to walk to groceries, stores, etc.		
2	Housing not 55' high. Keep to 35' with setbacks.		
3	I think this would significantly increase warehouse lease rates.		
4	None of these make sense without much higher land use intensity.		
5	It needs to be dense enough so people have place to walk to and interesting.		
6	Small.		
7	housing on 55th is a bad idea, too industrial and the train.		
8	housing needs to be less dense than what is at Boulder Junction		

9) More people walking and riding bicycles – make it safe and convenient (Districts and Housing Choices scenarios)



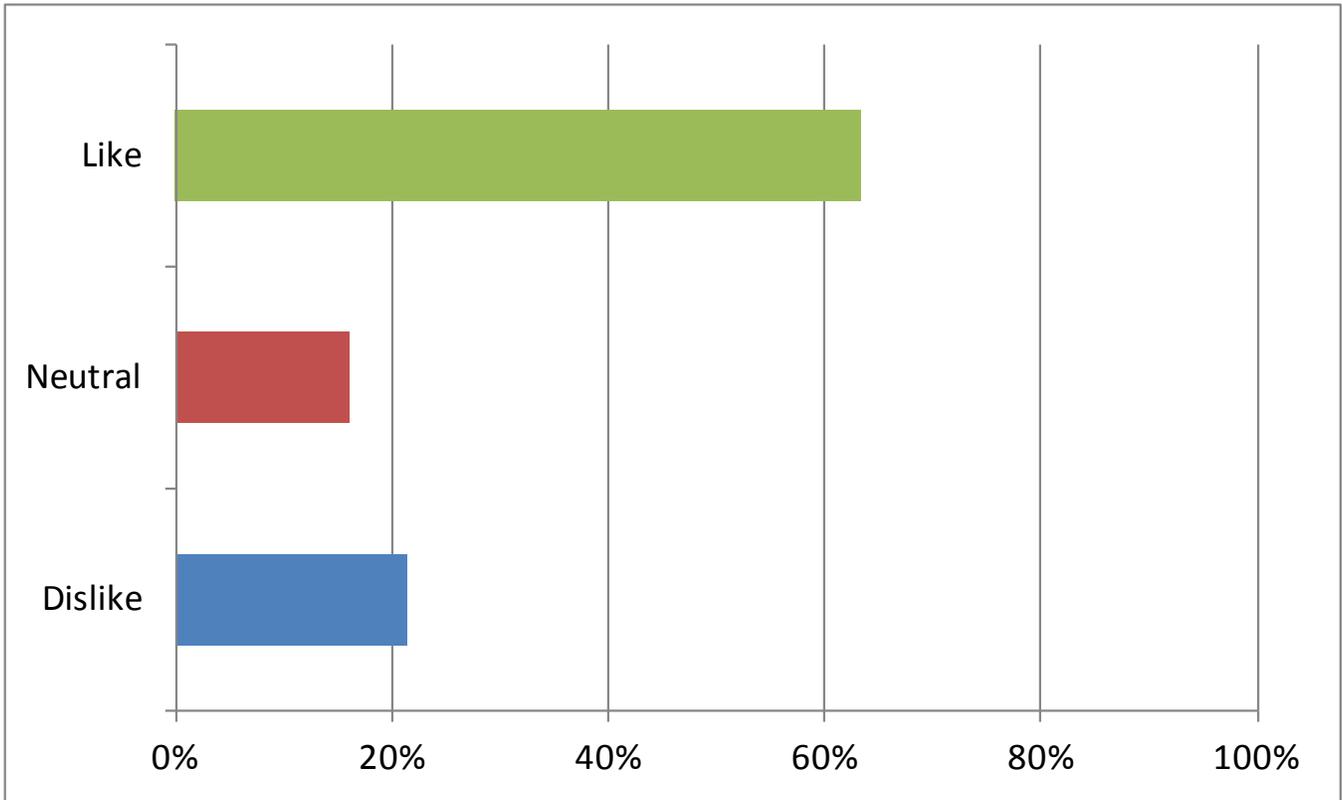
	Like	Neutral	Dislike	Total
	70.67%	21.33%	8.00%	
	53	16	6	75
Comments				
1	Most essential!			
2	Probably won't happen. People need cars.			
3	Not going to happen without increasing intensity a bunch.			
4	We already have lots of people walking and riding bicycles.			
5	Yes, but make sure you provide parking for cars.			
6	Yes - same as above, you don't get biking and walking w/o more housing.			
7	Good luck. With traffic on Arapahoe, its really dangerous to bike.			
8	more and faster bus routes to where people work, not just up and down Arapahoe.			
9	But totally unrelated to districts and housing.			
10	Also need planned parking for older population			

10) Some new affordable, workforce housing north of Arapahoe Ave. (Housing Choices scenario)



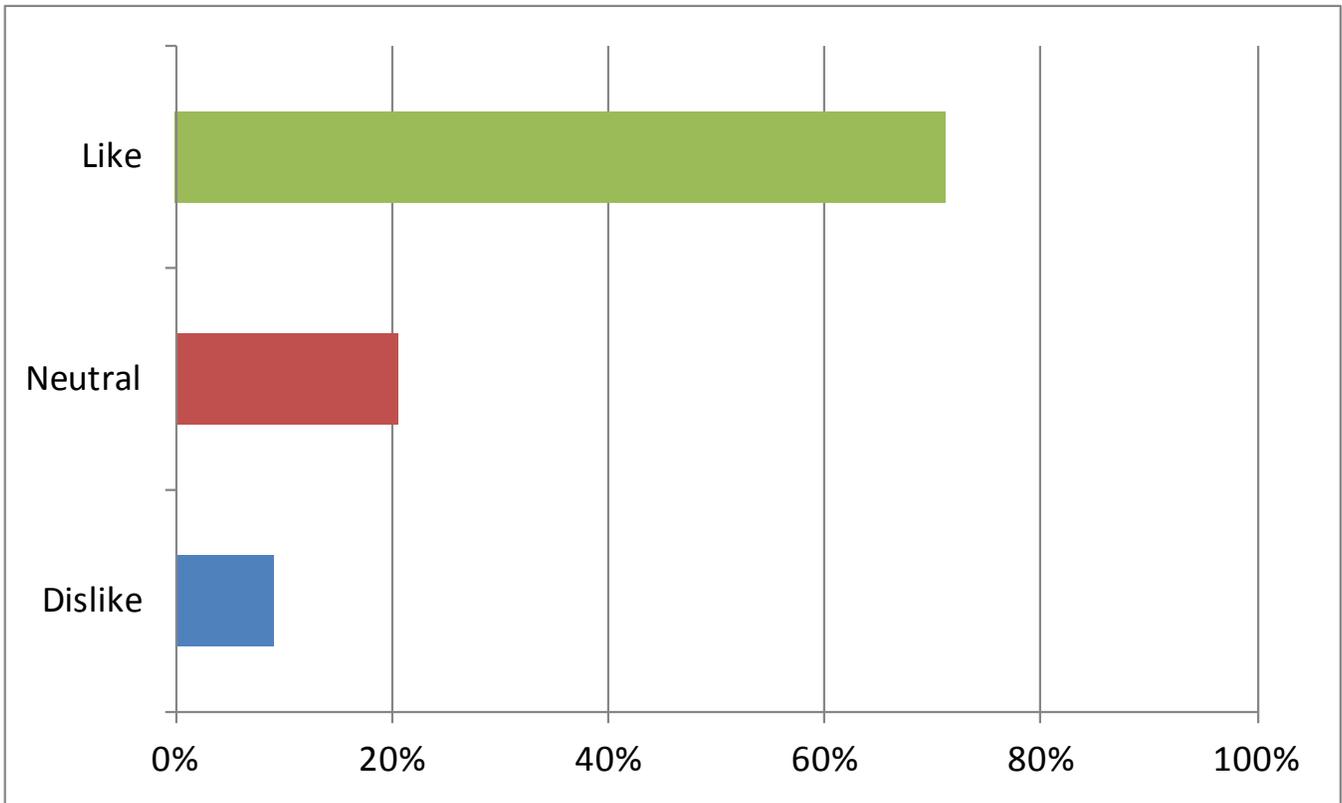
	Like	Neutral	Dislike	Total
	48.15%	17.28%	34.57%	
	39	14	28	81
Comments				
1	I'd love more density in general. More density - more people to support walkable neighborhoods.			
2	Developers will just build expensive places and give money to the city for affordable somewhere else.			
3	If housing is added, it should prioritize housing for the area workforce.			
4	How about 10,000 dwelling units?			
5	High density probably, therefore unacceptable. A comprehensive question to the residents of Boulder, Boulder County, Lafayette, Louisville (as separate entities) to determine appetite for "growth."			
6	NO buildings over 2-3 stories!!! The scenario playing out at Pearl and Arap. is a nightmare!			
7	Bad place for housing.			
8	That might be ok. If you would STOP job growth.			
9	Not too dense			

11) Flatiron Golf has new trails and community gardens, in addition to golf course (Housing Choices scenario)



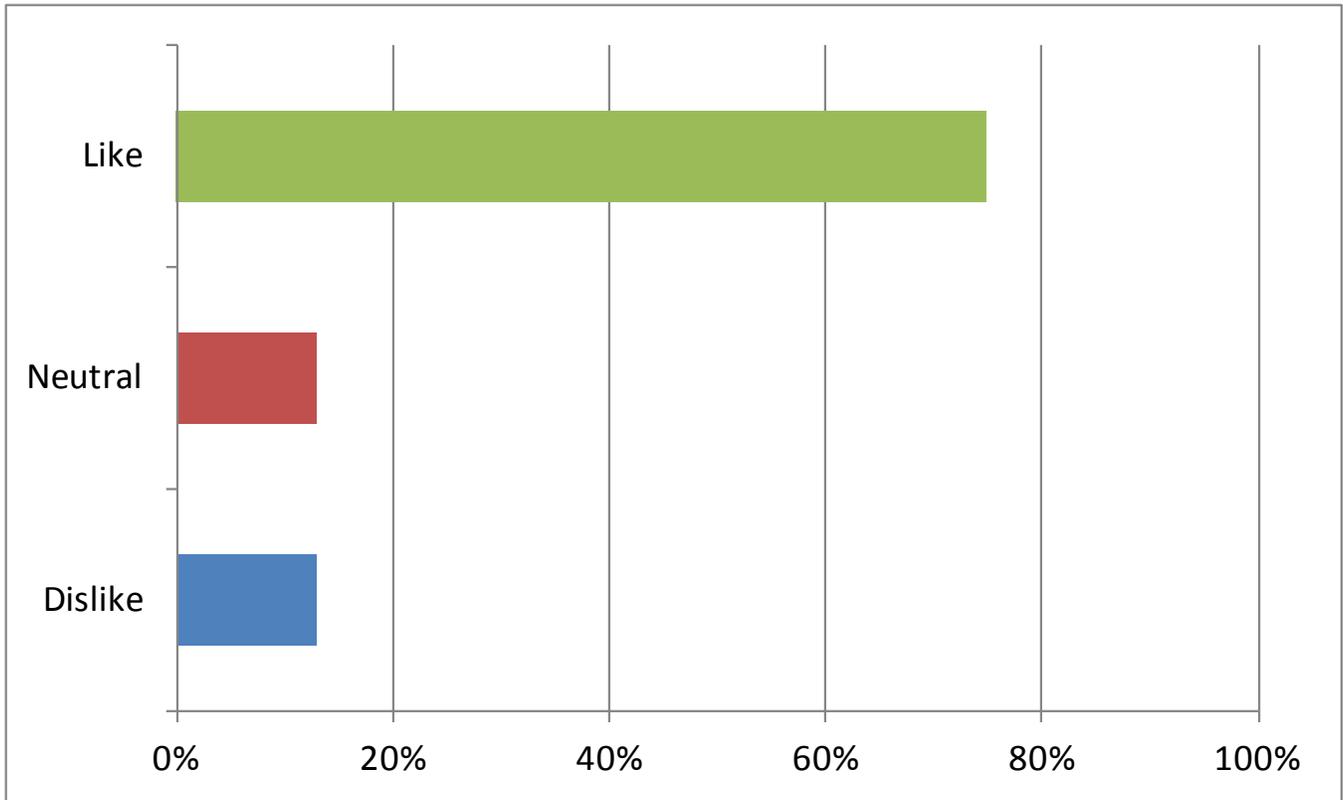
	Like	Neutral	Dislike	Total
	63.16%	15.79%	21.05%	
	48	12	16	76
Comments				
1	This would make for more efficient use of land.			
2	They just put 2,000,000 into sprinklers - not changing golf course.			
3	But get rid of golf course - turn into sports fields.			
4	Interesting idea. Simple improvements such as plowing the multi-use path east of 55th and working on bike and ped connections would help a lot.			
5	I hate golf courses.			
6	Leave the golf course alone.			
7	Golf courses are bad land use.			
8	as long as it stays green why not give access to more people?			
9	Make the entire golf course a park.			
10	Could go eitherway.			
11	Don't want current golf course to be changed.			

12) Ecological restoration along ditches and near creeks; better access to nature for current and future residents (Housing Choices scenario)



Like	Neutral	Dislike	Total
70.89%	20.25%	8.86%	79
56	16	7	
Comments			
1	Flood mitigation would be my first choice. I'm concerned this whole development will snowball into another sub city and the (illegible) can't handle it.		
2	Sure. keep the greenways green. Don't build in high hazard zones. puh.		
3	What's the point if we don't allow lots more people and activity in the area?		
4	Huge!		
5	Why just for residents?		

13) Arapahoe Ave. becomes a safe and attractive “boulevard” with street trees, noise buffering, safe speeds and transit



	Like	Neutral	Dislike	Total
	74.68%	12.66%	12.66%	
	59	10	10	79
Comments				
1	2 lanes + bike lane would be wonderful!			
2	NO trees. Keep view of MTS. Won't be attractive with BART down the middle.			
3	What about commuters from east of Boulder? They use Arapahoe. Hold on this until the comprehensive plan is done.			
4	It's still a key arterial and SH. Need to accomodate all modes. What I think is missing is explaining to existing residents how the evolution of Arapahoe can positively impact them. Make linkage to how families will benefit from transit/infill/densification.			
5	This is a nice idea but NOT if it means high rise development. I live on one acre of land on Old tale, just 3 houses from Arapahoe - I would like to see parks, but definitely not high rise housing on Arapahoe.			
6	Current buses are fine. We don't need BRT lane.			
7	LOVE!			
8	It's always going to be a major traffic jam getting to/from Boulder.			
9	As long as there is more lanes and better traffic flow.			
10	Just will create more traffic jams, unless you limit job growth!			

Question 6

(Answered: 41, Skipped: 66)

Scenario A: Current Trends -- What do you like about the ideas presented?

Question 6: Responses	
1	Providing more retail and service options.
2	not much, other than I won't have to deal with additional traffic congestion if nothing changes
3	East boulder is an employment hub. Don't disrupt this, jobs are important!
4	I understand the hospital's request to change zoning for their medical personnel. I think the people who live in that neighborhood ought to have final vote about whether or not to allow such a change.
5	I like the focus on pedestrians; right now, it is not the sort of street where pedestrians would choose to linger or, even, use. Likewise I'm comfortable riding my bike from the Boulder Creek path to the hospital, but would hesitate venturing further afield. I know that serious bike commuters feel safer on Base Line than on Arapahoe, but it means going a mile out of their way.
6	I don't like your ideas because they lack a clear purpose and many of their elements are not feasible/realistic assumptions.
7	If Naropa increases its campus and makes it more aesthetically appealing, that would be great.
8	I like keeping things as is. Maybe add a few 2-3 story office buildings, a couple of restaurants.
9	not much
10	People can still buy houses with land that don't start at \$1Mil. It's more "affordable" for Boulder.
11	Affordable warehouse space
12	Basically nothing. This development pattern is why the US is fat, broke, and dependent on oil.
13	No new housing
14	No, it needs planning
15	It doesn't make things worse.
16	?
17	Nothing
18	Does not beautify the area. services stay limited.
19	Seems ok
20	It's vibrant and driven by free enterprise/business model.
21	Keeps service businesses, minimize new impacts. You could achieve this by downzoning, and not getting the 19,000 new jobs.
22	Don't just leave as is.
23	Pretty much ok.
24	I think some businesses will need to stay in this area, as affordable locations to relocate within Boulder are minimal. Boulder already has a reputation of not being business friendly, so it's important to balance this aspect too. I think there can be some creative thinking on how to incorporate some of the more light industrial businesses within this plan.
25	Some more amenities, introducing more green space to new/remodled potential industrial/housing areas. Also, the idea of safer biking. It's hard out here.
26	not much
27	Noise buffering design in keeping with a sense of charm for the neighborhood
28	We need low cost places for startups and services like auto repair
29	No new housing on the north side of Arapahoe and south of Boulder Creek (between Foothills Parkway, city limits. Affordable service industrial and places for storage units
30	The affordable light industrial is important and will be driven out if the City is not careful.

Question 6: Responses	
31	Nothing that brings more noise and people.
32	NOTHING. This is the worst scenario and I do not like it.
33	Wish I could figure out how to get back to see the model referred to
34	You have not really given us enough information to enable us to know what the area would look like if current trends continue. The one current trend that is observable is the self storage warehouses. There is apparently a great need for them or they would not be so profitable. They are not a problem in that they do not generated much traffic, or crime, or other social problems. They seem to be well kept and reasonable landscaped.
35	As for new housing areas: I'd like to see homes with "food" gardens and plants that support the natural wildlife of the area. Limit areas of "grass" that requires mowing and weed killers. I'd also like to see community parks / areas where kids can be kids and build forts etc., play in water. How about natural walk ways vs. always having cement sidewalks?
36	need to be coordinated with the future "givens"
37	Convenience and access to some light industry and services.
38	Great plan
39	Not a big increase in density or rising rooflines
40	Boulder need to keep/increase it's industrial sector
41	Keeping it attractive to small business.

Question 7

(Answered: 44, Skipped: 63)

Scenario A: Current Trends -- What do you dislike and would change or add?

Question 7: Responses	
1	No high density housing!
2	More transit solutions for in-commuters. Get workers out of their cars! Bus rapid transit is needed, but will require solutions from the places commuters start from - they all are going to the same place, they just need a common starting point.Keep people out of their cars by promoting restaurants, shops, services, etc... near the employment centers.
3	I dislike any ideas that are not welcomed by the people who live in the affected neighborhoods. In my neighborhood, none of my neighbors are happy with this project.
4	Be careful not to create a wall of tall, fancy buildings at the expense of affordable spaces for places like Independent Motors. A Peleton fortress definitely holds no appeal.
5	I would go back to the drawing board on your dense urban design. Most of us were not attracted to the area because it was dense - we moved here from dense areas with many amenities. We moved here for the small town quirkiness and community of Boulder - your "vision" ends that character.
6	May appear organic but can approach houston-ztyle zoning which is not good.
7	Inefficient use of land. Poor connectivity.
8	Too suburban, not dense enough. No emphasis on walkable neighborhoods. Too reliant on driving.
9	it stinks for biking
10	No neighborhood feel, not pretty, east county car community puts pressure on all modes of transport in the neighborhood.
11	If we can't fix the land use intensity out here, just STOP. Get the BRT in and leave the area alone rather than waste a bunch of time to make a lousy plan. May focus on the area between TVAP and East CU.

Question 7: Responses	
12	Don't go too far with height/density.
13	Don't Like
14	No change to golf course. No neighborhood. Sops. It is nice being a residential area
15	It also doesn't really make things better.
16	?
17	no restaurants, no retail, doesn't create a "district"
18	Driveway, road, parking and 1 story buildings.
19	Needs more variety. B or C Better
20	More residential
21	Change is needed.
22	Transportation concepts are straight out of the 1970s. There is a lot of room to improve things for peds/bikes/bus riders.
23	Limit height.
24	Just clean up a few of the less attractive areas.
25	Afraid that no plan for area would result in hodgepodge of buildings.
26	The way to businesses are set up, it makes the corridor sterile and unattractive as a place to go to shop or hang out.
27	LESS DENSITY. The density experiment isn't working. More housing doesn't mean more affordable housing, unless the affordable bit is SPECIFIED. Just allowing developers to plunk the affordable "folks" in the ugly buildings further from transportation, or to buy out, isn't OK. We need to change some State law to address that, I know, but it should be done. We will need more setbacks for more lanes/turn-lanes to truly address traffic congestion, and we MUST start lobbying RTD for more reliable, frequent bus service NOW (and succeeding in that effort!)
28	it is a waste of expensive land to keep this suburban design.
29	The entire project needs to address noise pollution.
30	possibility of big buildings
31	Too much employment
32	Unplanned patterns of development with large parking lots.
33	A new bridge/road to connect to Pearl near 48th would help.
34	We do not need more noise or congestion.
35	Honestly I would scrap this whole idea and do a combo of scenario B & C.
36	Merely fix the landscaping and facades of some of the older buildings and parking lots facing the street. Don't redevelop to higher intensities.
37	it is not either vibrant or pedestrian friendly right now. lack of human scale, services and amenities
38	Dislike that there are abandoned buildings and/or old, dated buildings. Many need a makeover.
39	nothing
40	The Peloton is less than 2 miles away and is not fully occupied. We don't need more condos/apartment buildings.
41	Ecological restoration, better connections between multiuse paths
42	Everything. We don't need more apartments in Boulder.
43	Too car-dominated, make Arapahoe way more of a complete street ... plus non-motorized routes to and from
44	Remains a scattered industrial zone that's not really serving local neighborhoods (beyond the current services). No real transit and not pleasant for biking or walking. It's an isolated area - more suburbia than town.

Question 8

(Answered: 41, Skipped: 66)

Scenario B: Districts -- What do you like about the ideas presented?

Question 8: Responses	
1	Overall, I would support the goals
2	more places to eat
3	Let the neighborhoods decide whether they want any changes.
4	Nothing.
5	Enhance 55th Neighborhood center
6	The concept of "districts" is a good one.
7	More diverse mixed land uses, more restaurants/retail will decrease number of drives for lunch. This will help lessen traffic in a congested area.
8	Deceptive - planning board member said - "Don't do visuals with 55 ft buildings that will only upset the public"
9	I support some mix of B and C
10	My favorite option
11	Better amenities for area workforce.
12	looks great with more trees, pedestrian friendly areas, parks
13	Mixed commercial with restaurants.
14	I like this but need more routes around Pearl East and Flatiron Park.
15	It adds the new Walnut/48th St, and aspires to some other modest changes.
16	.?
17	Really like 55th and Arapahoe district.
18	6
19	Mixed use
20	More business = More traffic
21	Business focus.
22	Great concept. 15 minute walk is nice way to connect. What would a tech worker or hospital visitor, or BVSD employee need at lunch hour?
23	Like
24	Continued use as business area. Changes to add more possible medical offices around hospital.
25	Like the concepts presented - good variety.
26	I think this is a very attractive plan. I like how the various aspects are integrated to make the corridor more attractive.
27	I missed A-B-C - sorry - will have to look again. My previous comments were my overall impression from how I understand the project so far.
28	it's ok. not bold enough
29	Adds a night time population to support things like restaurants
30	Increased professional office space associated with Boulder Community Health center. Improvements to sidewalks, intersections, so people can walk safely and conveniently. Affordable service industrial along Arapahoe at the east end
31	Pocket parks and plazas should be the decision of the developer land owner--unless the City wants to buy the land.
32	Nothing.

Question 8: Responses	
33	Love the 55th/Arapahoe ideas (Retail, Services, Some housing, Arts and Culture, Mobility Hub). I also like the north 55th idea of having retail to serve the industrial parks.
34	The fact that this area would be preserved for income producing businesses.
35	boulevards with retail and restaurants.
36	ecodistricts and enhanced streetscape - sound good.
37	Retail, shops, restaurants, improved intersections, some recycling services, improved overall look of Arapahoe such as trees, noise buffering.
38	Poor excuse to let developers begin having at it and making money by cramming more into an already too dense Boulder, and by insisting that every square inch of space be developed to max "potential" which I translate as "max ROI on <given developer's> investment". Developer's ROI is not the same as return to the community, and at this point in Boulder's history I'd argue it's actually the antithesis
39	I like the key features presented. I would be interested in hearing more specifics about housing in this scenario - density, target income.
40	mprovements to sidewalks and intersections
41	This begins to think about neighborhoods.

Question 9

(Answered: 32, Skipped: 75)

Scenario B: Districts -- What do you dislike and would change or add?

Question 9: Responses	
1	Recycling was forced on the Count yresidents and is not a desired use on Arapahoe.
2	no more industrial
3	Too much of one thing in one place - is this the same zoning trap the separates housing from shops and restaurants and leads to car traffic?
4	I think this project has moved far too quickly, and is rapidly moving out of logical thinking. Why can't the neighborhoods decide for themselves?
5	Start over.
6	no change to Flatirons Golf Course especially flood mitigation.
7	Make it a special trip to East Boulder - not more housing
8	Any planned development necessarily increases rental rates.
9	Retail - don't need it! Recycling center already exists! No additional housing
10	Don't go too far with height/density.
11	Don't force eco-pass on people that will not use it.
12	It's still much too timid.
13	.?
14	Not bold enough
15	Combine/trade off B and C.
16	Limit new office space.
17	Better transport and retail closer to businesses, not just on Arapahoe.
18	Adding housing might be interesting experiment but I don't want to live way out there. I would drive into downtown.
19	I don't like building to street in these areas, destroy views, makes everything constrained.

Question 9: Responses	
20	Don't like buildings too close to sidewalks. Don't like too much height. Add more green spaces.
21	I don't know if there are too many types of districts. Traffic backup to get into the hospital area going east and Arapahoe is really bad now. Adding more offices, at least on that side of the street, could make the problem worse.
22	make it more bold and visionary
23	Too much emphasis on taking away traffic lanes on Arapahoe
24	Adding mixed retail, dining, office along 55th Street would severely damage the current residential character of 55th St south of Arapahoe. Adding an east/west connecting street (Walnut /48th St.) would exacerbate the current traffic problem; such an addition would be a detriment to the character of Arapahoe rather than an improvement.
25	We don't need an arts and entertainment district focus.
26	The roads cannot take anymore congestion.
27	Wouldn't want the services mentioned in the 55th/Arapahoe to be things like car repair, storage, etc... Those types of businesses bring down property value and it would make the new housing going into that area less desirable which would drive down prices and then people would care less about their properties.
28	what is the intensity of new development? what would be the phasing for the development? are you planning on pilot projects?
29	Dislike affordable housing especially any buildings over the 55' height limit.
30	Put the plan in the shredder.
31	I like this scenario.
32	Increase in density, more traffic, addition of more housing, growth of Recycle Row. It seems a near-necessity in this plan that higher buildings will be added - MAJOR negative.

Question 10

(Answered: 47, Skipped: 60)

Scenario C: Housing Choices -- What do you like about the ideas presented?

Question 10: Responses	
1	Convenient retail and service. Roadway beautification.
2	affordable, closer housing
3	Walking streets with mixed shops and high density residential - this could be exciting!
4	I can't think of a thing.
5	affordable housing near transit centers makes sense
6	Nothing - another ugly, common development for rich college students or investors doesn't build community.
7	Good idea. Affordable housing in Boulder will never again be single family... we need a LOT more apartments and townhome style residential units.
8	Employee housing is good!
9	Diversity and housing
10	More housing within city, reduces in-commuters and VMT
11	Emphasis on 15 min neighborhoods! Less reliance on cars would probably reduce Arapahoe traffic! More parks! This is the best plan. :-)
12	It plans for the community's future needs - timing can remain flexible and adapt to shifts.
13	Outrageous - I don't want to live in a Jetson City.

Question 10: Responses	
14	I support some mix of B and C
15	At some point a developer will try to put in housing, so very important to plan for it.
16	Mix of uses would be better, but not much
17	Not sure
18	We need housing but we need routes so people can live and work in East Boulder.
19	No affordable housing
20	It's getting better. It adds much-needed housing.
21	??
22	Same as B - Like 55th and Arapahoe district. Thoughtful mix of retail residential
23	Mixed use
24	Can add some housing without making it as ugly as Boulder Junction. Needs some styling inline with Boulder, not all square, ugly buildings.
25	Too radical. Don't like
26	Most of the jobs are filled w/oout in-commuters. Unless you limit job growth, traffic congestion will just get worse and worse.
27	Like
28	Nothing
29	I like a mix of housing and it's crucial to expand the city's affordable housing stock.
30	See previous.
31	not enough
32	Adding some dense housing close to jobs and reducing the land available for more job growth
33	Added trails and amenities to the Flatirons Golf Course. Gateway beatification at the east end of city would be an attractive addition
34	Boulevard with buffering.
35	Do not put anymore housing on east Arapahoe, the road cannot take anymore congestion.
36	This may be my favorite scenario. I like the 55th/Arapahoe ideas with New housing in select locations, the Dinner theater and other businesses become part of an art center, Mobility hub, Shops and restaurants and the ideas for the North 55th Street (Live-work mixed with offices, Retail to support the people living there and the businesses). I also like the idea of the greenway enhancements to the South Boulder Creek area.
37	Nothing.
38	good mix
39	all look good
40	is it representative of New Urbanism? this seems to pay more attention to relationship of buildings to the street.
41	Bike paths, trails
42	A horrible idea to allow developers to cash in, under a thinly-veiled guise of "affordable housing" and "worker housing". If this is really the case, then why does Boulder allow developers, like those downtown, to provide "cash in lieu" of affordable units when it's all said and done. That is nothing other than crass political positioning to try and force through their development projects and enrich developers at the expense of the community.
43	I need more detail to weigh in on this. I do think redevelopment at 55th is a good idea. I think this is a good opportunity to provide options for people living in East Boulder County and are "daytimers" - time to phase the term in-commuters out of our vocabulary and move on to embrace the people that we employ and benefit from the sales tax they contribute that funds the amenities valued. I think Live/work units and additional moderate/workforce housing would be good additions.
44	Boulevard with trees/buffering

Question 10: Responses	
45	Best of the three but still way too timid. Waste of staff resources.
46	Everything. We do not need more apartments in Boulder. We don't need to use taxpayer money to support more people coming to Boulder on our dime.
47	I like this scenario the most

Question 11

(Answered: 45, Skipped: 62)

Scenario C: Housing Choices -- What do you dislike and would change or add?

Question 11: Responses	
1	Workforce housing= high density. Roads will become overcrowded, parking will be far short of needs.
2	slower speeds sound like more traffic issues
3	More housing will only generate more cars, more parking, more traffic unless things are developed as exclusively NON CAR. DO NOT BUILD PARKING. DO NOT ADD TRAFFIC OR TURN LANES. MAKE ROOM FOR PEOPLE, NOT CARS.
4	Leave "flatiron golf" out of your plan. You don't even know what it is called, let alone the history of the site and its status as a recreational site, do some research.
5	Arapahoe is already over crowded. No high density housing. No high density housing anyplace along this corridor. Save those ideas for the center of town.
6	would people really find housing along 55th appealing? traffic is pretty awful for people who live on 55th south of Arapahoe, despite mitigation gestures
7	Demand for the Peleton sure was a great success - why are we continuing with a plan that no one liked.
8	high-density does not necessarily mean Texas-Doughnut style monoliths, even if that's what developers want to build. Look at Europe or places like Colonial America for examples of high-density development with much more human-scaled buildings forms.
9	Prefer concept of "districts" better.
10	No buildings over 3 stories.
11	More Housing
12	It makes me nervous with existing traffic issues.
13	Would need to really prioritize affordable warehouse rent, perhaps like affordable housing program. how that works with market forces like pot grow operations I'm not sure!
14	add 10,000 to 30,000 more jobs/people.
15	HATE THIS OPTION! This is a rural neighborhood!! This option brings PEOPLE CARS AND PARKING LOTS
16	Don't go too far with height/density.
17	Dangerous housing next to train tracks.
18	No additional housing if it in any way resembles Pearl St. (East of 30th)
19	No affordable housing
20	This should also include the Walnut/48th St. connection, as well as a network of many more connections (at least for pedestrians). It should also radically calm Arapahoe and 55th Streets, so that all the new residents can really walk or bike places. We don't want to just add more people in a place where they'll still have to drive in order to feel safe and comfortable getting around.
21	?
22	don't like residential north end of 55th. Too far from focus of district. Keep it on Arapahoe.

Question 11: Responses	
23	Don't build more suburbia because you are afraid to bring up doing anything else.
24	mixed use reduce jobs/housing imbalance. Make growth especially jobs growth PAY ITS OWN WAY.
25	Businesses need places to grow. There is plenty of housing already.
26	Doesn't make sense
27	Without limiting overall jobs all this housing has no overall benefit; its a bandaid on a serious wound.
28	Do not want housing - save for business growth 50 years from now - housing will still not be affordable unless subsidized and either way will not meet the need for the type of housing we need the most.
29	How dense will the 55th and Arapahoe housing be?
30	N/A
31	if we don't build housing for middle income folks in this area, there will be no place for them and Boulder will be just a bunch of rich people, a small % in affordable housing and everyone else driving in from far-flung places. If the city squanders this opportunity in one of the few places left to provide middle income housing, the middle will be lost and no middle income people makes for a lousy community.
32	The general sameness of the housing choices and the fact they all seem to be rental
33	Adding high density housing to the east Arapahoe corridor. Such development would only exacerbate the congestion that is driving the current planning. How can housing along this major transportation route be an improvement if the problem is already too much traffic? Adding retail, dining and housing will only contribute to the diminished attractiveness and use of downtown Boulder; hardly a benefit to downtown vitality.
34	Drop the net zero energy neighborhoods.
35	I do not want to see more housing in an area that seriously flooded nor do i want that many more cars to add to the already congested Arapaho.
36	No moreover housing!
37	I'm a little worried about adding too much housing in the South Boulder Creek area, so would have to see that in more detail. Wouldn't want it to be too developed, but if it was a good mix of housing, park, open space, and commuter trails I think it would be nice. Maybe have a bit of retail & restaurants in there as well.
38	I do not like the Housing Choices scenario even if I like some of the things in it. The terminology re workforce housing is both vague and misleading. There is no possibility of market rate family housing at a low enough density that families will actually live there will be developed. We don't really need more high density apartments, even for entry level high tech workers. This area would need its own set of parks and other amenities, and the small amount of desirable housing that could be created does not justify the City's investment in this infrastructure. There are flood issues in most of this area, and elevating the land will only add to flooding woes upstream. There is only so much need for this mixed use, live/work thing, and it seems like everything else being built in Boulder, it just brings more of the young singles who need a place to live until they establish a career, move up the career ladder, start a family and then move to family suitable housing. It does nothing but increase the population of Boulder without providing housing for mid-level employees. It uses up the remaining industrial land that we will need to keep Boulder economically viable in another 50 years.
39	I like homes that are built on what we are learning today about energy efficiency. Building codes may need to change. Biomass interiors with "insulation" on the exterior... like we and animals have. Active and passive solar gains. Heat pumps vs. heating from fossil fuels. Food gardens vs. lawns. Natural path surfaces vs. cement / asphalt. Community parks where the kids can build forts and play in water. Natural vegetation that supports the native animals and animal migrations. Build neighborhoods that keep the cars on the "outskirts" vs. in individual garages. Hand carts can be used to move supplies around.
40	just need to maintain balance. what is an anchor retail? I would like to maintain the scale of the neighborhood.
41	Dislike affordable housing especially any buildings over the 55' height limit.
42	Put the thing in the shredder, pour some gas on it, and light a match.
43	The Peloton is less than 2 miles away and is not fully occupied. We don't need more apartments/condos.

Question 11: Responses	
44	Addition of a lot of housing, which will increase traffic and noise and will irrevocably change the neighborhood. Those of us who live here now, LIKE IT THIS WAY. We chose to live here because we don't want to live somewhere developed. We do not want a lot of change!
45	Best of the three but still way too timid. Waste of staff resources.

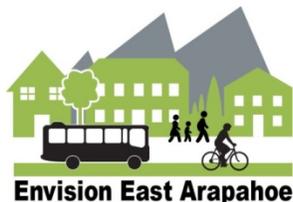
Question 12

(Answered: 54, Skipped: 53)

How did you find out about this workshop?

Question 12: Responses	
1	Newspaper
2	my boss emailed it to me
3	A listserve
4	did not attend
5	Nothing
6	Through a neighbor
7	Postcard
8	Email List
9	Postcard
10	Email
11	Judy
12	Friend
13	Email
14	Newspapers, Neighbors
15	TAB
16	Neighbor
17	Neighborhood web site
18	Post card mail
19	Friend
20	Good - informative
21	Neighbor
22	Email
23	Email
24	Everyone wants walking, biking, and transit but unless you are willing to propose dense, close interesting places to walk and bike to, you won't get that. Because you chose not to take a bold approach it is kind of worthless - you will never get there. Either get some political will or drop it and come back later.
25	Online
26	City
27	Friend
28	on email.
29	Neighbor
30	Website
31	Better Boulder

Question 12: Responses	
32	Email
33	Concerned neighbors.
34	email
35	It was e-mailed to me.
36	city employee
37	I'm on email list
38	Notice in the Boulder Daily Camera. Also,e-mail contact from a friend
39	Friend
40	City website. Sorry, I don't trust the City enough to give you my contact information. You'll just have to hack Home Depot if you want it.
41	Neighborhood organization.
42	Live near the area & my HOA sent out an email about the project.
43	A neighbor informed me.
44	web site, email
45	E-mail
46	email
47	email
48	Subscribed to the mailing list
49	Originally from the City's website, then I signed for the project updates via email.
50	News
51	Neighbors and newspaper invitations to open house and previous walk audits.
52	Neighborhood newsgroup
53	I pay attention.
54	email



Nov. 17, 2014 Listening Session Summary

Date: Monday, Nov. 17 at Naropa's Nalanda Campus, 5 -7 p.m.

Attendance: 23

Meeting Objectives:

- For city staff to listen, answer questions, and provide information to community members about all aspects of the Envision East Arapahoe project. **Format/Agenda**

Format/Agenda

1. Welcome, quick description of format (informal) – 5 minute overview
2. Staff introductions
3. Project purpose, origin, and basic facts
4. Full group Q&A
5. Breakout group conversations

Full Group Q&A

- Why didn't the city coordinate with CDOT work on East Arapahoe?
- RTD Fast Track Station at 63rd?
- Cherryvale traffic plans?
- Why not collaborate with county on the planning?
- What's the plan for the power plant?
- Sewer status at 55th and Arapahoe?
- Can zoning change include "no 5 story buildings"?

Table Conversations

- Great neighborhood as it is
 - More traffic, construction etc problematic
 - Roads currently for commuting
 - Like empty retail – limit traffic
 - North-south (55th humps should be taken out)
 - Transportation – infrastructure not sufficient
- Excited about potential and ability to age in place
 - New restaurants, retail, hospital (jobs+)
 - 30th and Pearl – learn from it

- Long term resident seen city grow
 - Why do we need restaurants here? Plenty downtown
 - In commuters – traffic on Cherryvale
 - Against mass transit/widening of Cherryvale
 - Light industrial should expand – strong economy, low traffic
 - Keep high buildings out – preserve view corridor
 - 90’ setback/front yards impacted on Cherryvale
- Rural Character +
 - Seen other communities change
 - Boulder will grow – must manage growth
 - Project creating growth or managing?
 - Up and in instead of out (traffic concerns)
 - Don’t let Cherryvale intersect with Arapahoe
 - Industry/business good, but doesn’t identify as own neighborhood
 - Supports transportation and land use planning
 - County should be present
- History – development of Boulder
 - Traffic has increased tremendously
 - Highway 7 – main route to Denver
 - Arapahoe and Foothills main mistake was not to put in an overpass
 - Stop putting up obstructions to traffic flow
 - 1971 growth management
 - Jobs/housing imbalance
 - Need moratorium to stop growth in Boulder (like Uni Hill)
- Concerned about shutting door now
 - Stopping growth has implications – it will still change, we should manage growth
 - Each person has own story of when they moved to Boulder
 - Concerned about no middle class
- Want more amenities near 55th and Arapahoe
 - Walkable, close proximity
 - Disappointed by lack of amenities
 - Height/density not required for business growth
 - Area is affordable now
- Timing is appropriate
 - Currently car-centric
 - More housing
 - Different character areas should be kept
- Concerned with big buildings, zoning exemptions and traffic
- Rush hour traffic – add people problematic (in commuters)
- Build for 20,000 in commuters – take into account families
- Not everyone should get to live in Boulder

- SE corner of 55th and Baseline – stucco, affordable housing – nice
 - Small housing appropriate
 - Table Mesa Asst Living (46th) – nice looking (not like Pearl Pkwy and Peloton)
 - Urban Open Space – a great amenity
 - Build at Valmont
- BRT – have an image of 40 people in one bus
- Don't call them incommuters, instead "daytimers" (teachers, firemen, etc)
- Change is inevitable
- Crime map – creeping east, but mostly not here – low density
- Flood – dam at RR tracks
- High ground water
- 25yr flood in Sept. 2013
- Population increase for each scenario (not just housing units)
 - People will always drive
 - Building housing for incommuters will not solve problems
- Consider resources that will be needed for community center and senior center.
- Neighborhood centers
 - Not pleasant to walk
 - Ex. Louisville Downtown; dinner theater is an amenity, neighborhood community resource but needs refurbishing to be more welcoming
 - Parks, shopping with public art
 - Theater is a dead space for pedestrians; could it be integrated into surrounding development?
- Concerns with BRT not being where jobs are – transit ideas not well formed yet.
 - Boulder Community Health – there are many jobs; looking for more transportation options to/from BCH and want more destinations to go nearby
 - Interest in Boulder B-cycle; especially to link to transit/BRT
 - Could link transit to jobs and other destinations
 - Need B-cycle at job sites too
- Concern with safety biking on East Arapahoe and same concerns on 55th – will not work for most cyclists.
 - Desire for underpasses
 - Confusion for cyclists and pedestrians. People making unsafe decisions on where to walk and bike. Need better facilities.
- Coordination needed with Recycle Row and improve access to this area. Zero waste plan.
 - Establish Recycle Row as a "place"
- Do not want a trade off – if want better facilities, then have to accept more development and traffic and higher buildings. Shouldn't be a choice between the two
 - Not want to destroy neighborhoods to have better facilities
 - Like it now and anything new will increase traffic
 - Prefer neighborhoods stay the same and have better facilities

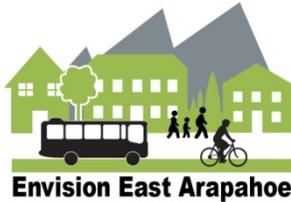
- Do not make it look like 30th and Pearl, looks like a concrete jungle – no green, canyons of buildings
 - Do not want to block views of mtns along East Arapahoe
- Want to know what amenities that businesses/employees want
- Not to bring in new people but serve people who are already there
- Lots of light industrial now – need more mixed use for area
- Want to keep area and neighborhood rural, doesn't need to be more welcoming
 - Do not want more construction on East Arapahoe – cause more headaches
 - Do not go over 3 stories
- Embrace open space and community gardens
- Separate zoning causes more traffic – if more mixed use, then could drive less
- Arapahoe is barrier so what is on north doesn't feel like part of neighborhood and want more places to walk to south of Arapahoe so don't need to cross.
 - Safety concerns getting on and off of Arapahoe, sight distance is blocked by landscaping, poles @McArthur and Arapahoe, drivers not stopping for pedestrians and bikes @FH and Arapahoe
- BCH and Ball don't block views because of setback
- BCH needs more medical office in area
 - Business Park would like to be able to offer medical office
 - Business park would welcome more retail and better access/circulation by walk, bike, and transit
- More housing/affordable housing
- If there were more amenities and Hop type bus within area to serve local businesses, then there would be less traffic/fewer cars on road
- Experience from Lakewood, hospitals do bring desire for more uses such as apartments, hotels, doctor offices => lots of change will happen and can be good
 - People who work in Boulder/corridor would like to live here and would desire higher density housing
- Need to keep in mind length of EEA corridor and could have different uses in different parts of corridor
- Would like to see less traffic
 - Could reduce traffic by more public transit
- Care less about what is in the building; care more about what they look like
- Like that no street lights and no sidewalks
 - Keep rural feel on east end of corridor
- Why does the corridor extend to 75th?
- Idea for multimodal station on east edge of corridor to intercept people before come into corridor and provide options to bus/bike in

How Could We Engage Better?

- Neighborhood town hall – email group
- January is too soon
- FTE neighborhood liaison
- come into existing neighborhood
- Next door – Google
- CU – Partner; grad students and instructors
- Employees
- HOAs
- Would like info cards on project to hand out to neighbors
- Need to get word out earlier
- Emails come out from County and City = joint emails
- Want county representation with mailings and meetings
- Some think public process works and some don't
- More transparency to build trust
- Promote design excellence event
- Different events focus on different pieces of corridor
- Daytime meetings for “daytime residents”

Comment Forms

- Connect East Boulder – Flatiron Park east and west via public transportation, e.g. Jump/Hop
- Invite amenities, (e.g., food to East Boulder accessible via public transportation)
- Encourage the development of retail use to provide amenities to the 55th street corridor
- Rezoning allowing more professional services (e.g. medical services as an amenity to the hundreds of employees working in IG zoning)
- Find a balance with change
- Need complete streets
- Retail/restaurants to support east county
- Live/work – even if folks drive, they don't have to drive as far
- Stop demonizing “In-commuters” they are important members of our community that work hard in our schools, grocery stores, medical services. Let's call them “daytimers”
- One bus carries 40 people – how many less cars can there be?
- Where is the county? You state the city and county are working closely together – I doubt that is true
- I have lived in this area almost 40 years – I like it just the way it is – why mess with it?
- I live on 1 acre of land. I like the rural feel. If I wanted all sorts of shopping, I would live someplace else
- I feel this project has already been imagined – now, the city is going through the motions of making it happen whether we want it or not.



Dec. 11, 2014 Listening Session Summary

Date: Thursday, Dec. 11 at Twenty-Ninth Street Community Room, 5 -7 p.m.

Attendance: 33

Meeting Objectives:

- For city staff to listen, answer questions, and provide information to community members about all aspects of the Envision East Arapahoe project.

Format/Agenda

1. Welcome, quick description of format (informal) – 5 minute overview
2. Staff introductions
3. Project purpose, origin, and basic facts
4. Full group Q&A
5. Breakout group conversations

Full Group Q&A

- Bike path connection shown on the connections plan – what is the status of these connections
- Project timeline
- Council meeting in Feb. – it's purpose
- Is the plan already set; has council already picked a preferred option?
- BRT and RTD's plans for regional transportation

Table Conversations

Table 1

- Flatiron Park employees would like more retail options and more transportation options along the corridor
- Zoning should attract small businesses/be more affordable than downtown.
- Need more robust public transportation to reduce the need to commute.
- Relaxation of zoning in Flatiron Park to allow mixed use for employees to walk to.
- There have been attempts to allow other uses and it was the economics that impacted success.
- There's a demand that's not allowed by zoning.

- Black Belly restaurant has been a success and the housing population is already dense enough to support it.
- Last-mile connections would be good. B-cycle is great.
- Would like to see more housing in the area.
- More biking - needs to be safer and more accessible.
- Bike path on the golf course side.
- Better last-mile connection North-South from transit stops (i.e. Pearl East and Flatiron Park).
- Would like to see housing in the corridor and transportation options to support residents and employees.
- Similar to North Broadway - walkability and infill development.
- Housing north of Arapahoe is necessary but south of Arapahoe is already dense.
- 55th and Arapahoe (SE) needs redevelopment - there's potential there.
- Mixed use housing/town houses is a great idea.
- Restaurants/retail to walk to north of Arapahoe
 - Need pedestrian infrastructure.
 - Trouble crossing (to go North) at 55th and Arapahoe.
- Need a better pedestrian experience.
- Traffic at the hospital - need better traffic control there.
- Would like to see city create a complete pedestrian experience - not just pieces.
- Industrial services (i.e. car service shop) is nice to have walking distance from home but wouldn't mind seeing auto dealerships leave.
- Signal priority for buses.
- Easier for people to commute to the corridor through transit options.
- Flood concerns along Arapahoe.

Table 2

- A lot of traffic on Arapahoe
- Worst fear is a canyon of big buildings.
 - Blocks views of mountains.
 - Do not like buildings too close to the road.
 - Tree lined boulevard a plus.
- Sidewalks on Arapahoe are very bad - especially on the south - not continuous.
- Underground utilities desired - utility poles
- Sewers in the area need to be addressed.
- Do not take car lanes away for bikes.
- Boulder Chamber supports the idea of 15-20 minute neighborhoods.
- Traffic in Wendys/Ozo/Liquor Store parking lot is quite heavy - not easy to walk to, both crossing Arapahoe and walking in parking lot.
- MacArthur left turn onto Arapahoe is difficult and dangerous.

- Train whistles are very loud and disturbing with windows open at night.
- Independent living facility would be desirable in the area.
- Some people's physical abilities restrict them to cars only.
- Two lanes in and out of Boulder on Arapahoe creates a bottleneck for commuters leaving town.
- Concerned about BRT on Arapahoe
 - ROW may not be wide enough. Where will it go?
 - RTD's plan still seems very unclear.
 - Park-n-Ride near 75th desirable.
 - RTD should not create more bottlenecks - find a way to get commuters all the way in to town.
- New CDOT improvements east of 63rd did not make things better.
- Area is lacking restaurants.
 - disagreement, there are already restaurants.
- Partner with area landowners to improve landscape, look and feel.
- Current buildings house small businesses - what will happen to them?
 - Could totally change the feel of the area.
- Concern about large housing north of Arapahoe near railroad tracks.
- Golf course? Is housing planned here?
 - Set this aside entirely for parks and recreation.
- Do not like "affordable housing" that is not truly affordable
 - Upper middle/high end, not for families.
 - No net gain for the community.
- East Arapahoe has low crime - worried what could happen with a lot of new development
- New buildings unlikely to go between existing ones - likely to replace existing buildings.
- Naropa would like to plan for the future with minimal impact - would like students to be able to walk during the day.
- Crossing Arapahoe has become very difficult.
- The more people who ride bikes = the less who will drive cars.
- You cannot currently bike on Arapahoe - many gaps in the bike network.

Table 3

- 2 year project at Arapahoe and 63rd created a bottleneck.
- Plan to improve or alleviate the gridlock?
- Concerned about traffic because Arapahoe is still congested and will remain so even under the no change scenario.
- Transportation is an issue but you can't build your way out of it
- NW Mobility study?
- CDOT study state route 7? What happens around 287?
- Process? Who makes the decision? Living document?

- Response: Council decides but they take community feedback; more of a vision plan, more flexible because this is not within our control, land use changes will likely be more incremental may need to revisit plan if trends change.
- Concerned over conversation about widening of Arapahoe – would be against every city and county plan.
- Does the BRT have dedicated lanes?
- Hard to discuss because corridor is so different from Folsom to 75th.
- Concerned about connectivity for bikes along the whole corridor.
- Arapahoe is difficult for cyclists – high speed traffic.
- Xcel energy plant closing at some point?
- Can this be broken into bite size pieces? Transportation all together makes sense, but land use is unique to areas.
- Volume on Cherryvale is huge – make local traffic only? Would be more pedestrian bike friendly; like no streetlights/no sidewalks – gives the street a rural character.
- Cut off to through traffic?
- Flatirons Park – everyone arrives in AM, leaves at noon, comes back, leaves again at 5; more restaurants or transit within park would be good but zoning doesn't allow.
- B-cycles in Flatiron Park?
- Concerned about height of buildings
- Restaurants are good.
- Flatiron Park Deli is the one restaurant in the park.
- Closest grocery store? Safeway on Foothills, King Soopers at 30th
- Concerned about transportation demand and induced demand from new development; county and city should think about induced demand.
- Concerned about light pollution. 2018 – all light fixtures must be replaced by this time – includes car lots; development done right might improve quality of life in existing neighborhoods and for employment.
- CU East connection along 33rd very circuitous, not walkable. Should have more direct routes.
- Should have/enable development at urban/human scale.
- Need to find a way to make smaller parcels.
- If Boulder Junction parcels were smaller, taller, we'd have less of a fight.
- How much can we extract from developers?
- Smaller units. More affordable. No more impact fees.
- Will transit village be served by transit? RTD connection.
- Arapahoe and Foothills intersection. Any plans to change?
- This meeting is more productive.
- Council at meetings.
- Thinking in decades might seem big but that is the legacy we are building on. Plan for the long term. Eg. 2040 train. Open space. Not like Colorado Springs.
- Cohousing artists currently part of Louisville.



Topic Name: What's your vision for east Arapahoe?

Idea Title: I strongly prefer that any new buildings be limited to 3 stories

Idea Detail: I believe that the views of the foothills should remain as unobstructed as possible. Also that any new buildings should be set back fairly far from the sidewalk for aesthetic purposes.

Idea Author: David M C

Number of Seconds 0

Number of Points 33

Number of Comments 3

Comment 1: This is one of the ugliest streets in Boulder I don't think there is a single aspect of it that should be preserved. Deep setbacks with parking lots are the ugliest pattern of development I can imagine. | By Jim M

Comment 2: I just returned from hiking the Teller Lake trail and the view as you top the hill heading west around 70th or so is spectacular. However, as you get closer in around Cherryvale to the west there really isn't that great of a view and it is easy to see because Arapahoe is so wide. Boulders 55 foot height limit came about because that is roughly the size of mature trees and the trees are what limit the view. There is nothing wrong with nodes of four story buildings, but O agree the cookie cutter architectural is pretty bad. | By David B

Comment 3: I agree with the set back suggestion, and would like to limit the height to 2 stories for the new vibrant East Arapahoe. The much applauded Boulder Junction with its canyons of apartments and pavement does not look like progress to me. Please do not allow this sort of compromise dictate the future appearance of our lovely town. And please do not replicate these architectural mishaps on East Arapahoe. | By Susan B

Idea Title: East Arapahoe does not need any more Storage Units

Idea Detail: It's my understanding that developers have been trying to add more housing along Arapahoe for the past 6years or so , for what ever reason it isn't happening.

It has come to my attention that the property across the street from the golf course will now be more "Storage units" because of difficulties for housing approvals.

Is this the cities idea of Envision East Arapahoe, is this what we want across the our golf course? It seems we would want people across the street, being able to use this facility that



seems to me under utilized.

Don't let these last large parcels of land slip away on Arapahoe to more of this type of use, they will be gone for decades.

Idea Author: craig F

Number of Seconds 0

Number of Points 29

Number of Comments 0

Idea Title: Make Arapahoe safer for bicyclists--protected bike lanes

Idea Detail: The multi-use path along Arapahoe east of Foothills often has poor visibility from the road and/or side streets. It does not feel safe traveling at bicycle speeds near intersections. Protected bike lanes similar to those on Baseline near Williams Village would make the road safer and more accessible to bicyclists.

Idea Author: Bob P

Number of Seconds 0

Number of Points 26

Number of Comments 1

Comment 1: When cyclists are forced on the sidewalk it is very difficult for them to see vehicles and for the vehicles to see them. Whenever I bike down the north side of East Arapahoe I assume I'm going to be hit at every intersection, not because anyone is doing anything wrong, simply because those of us on two and four wheels can't see each other very well. Multi-use paths are fantastic but have no business being placed along a road with turning cars.

I know of someone who was east bound turning left on to 48th but had to stop mid intersection because there was a cyclist on the multi use path legitimately crossing 48th st. The driver was hit by an oncoming car and issued a ticket. The ticket was later rescinded because the police admitted there was "no way the driver could have seen the cyclist." The Arapahoe multi-use path is not safe for anyone. | By Zach S

Idea Title: Better connected shopping areas with fewer strip malls



Idea Detail: There are several strip malls along this corridor that are not well connected for bikes or cars and feel outdated. I think there must be another format that would serve our community better.

Idea Author: Lieschen G

Number of Seconds 0

Number of Points 20

Number of Comments 1

Comment 1: Strip malls were made to be torn down, rezone East Arapahoe to 5 stories and the strip malls will change "format".

| By Jim M

Idea Title: Build car-free housing for Boulder residents without cars.

Idea Detail: Thousands of people in Boulder do not own cars yet they are forced to pay for parking when they purchase or rent housing. Mixed use housing should be built in East Arapahoe offering the option of units without bundled parking, allowing car owners to pay for the parking they use, and allowing people without cars to avoid paying for parking they don't use.

Idea Author: Tom V

Number of Seconds 0

Number of Points 15

Number of Comments 0

Idea Title: Bus service up 55th for residents who live south of golf course.

Idea Detail: Bus service is hard to use when it's more than 1/2 mile to the nearest bus line.

Idea Author: Laine G

Number of Seconds 0

Number of Points 14



Number of Comments 0

Idea Title: Make East Arapahoe a boulevard from 55th to 75th street

Idea Detail: Not that Boulder is Paris, but boulevards distinguish a city's arteries. Even more so with trees

Idea Author: Stanley G

Number of Seconds 0

Number of Points 13

Number of Comments 0

Idea Title: Build car-free housing, to Boulder residents who don't own cars.

Idea Detail: Thousands of Boulder residents do not own cars, yet they are forced to pay for mandated parking in the buildings they purchase or rent. How regressive to force everybody to pay for parking whether or not they use it. In East Arapahoe new mixed used development should have covered bike parking and any automobile parking paid for only by those who use it.

Idea Author: Tom V

Number of Seconds 0

Number of Points 13

Number of Comments 0

Idea Title: What's wrong with the way things are?

Idea Detail: If I wanted to live in town, I would. More development means more air pollution, more noise pollution, more light pollution, more traffic, more people - all things I wish to avoid living east of 55th St. Who asked the city council to make changes in the first place? These changes, not enhancements, will directly and adversely affect my life. Will it do that for any of the city council members promoting this?

Idea Author: Andrew J



Number of Seconds 0

Number of Points 12

Number of Comments 3

Comment 1: I agree with Andrew's original post. There's nothing wrong with our neighborhood. Please stop trying to "improve" it. | By Rachel B

Comment 2: Ah yes. I have mine so please roll up the streets behind me and hermetically seal my sanctuary. I've lived here for 35 years back when Boulder was still building single family sprawl housing like Meadow Glen, Country Meadows, Shanahan Ridge, etc. In that time the Denver metro area has doubled in population and so has Boulder. The only constant in life is change. We need to decide how to best manage that change. | By David B

Comment 3: It need better streetscaping to welcome people to Boulder. | By Felicia F

Idea Title: Lets not repeat the mistakes of Boulder Junction

Idea Detail: I do not want to see endless 4 story apartments buildings, turning Arapahoe into another "Pearl Canyon". I think the south side of Arapahoe should be left much as it is from 38th St. west to 75th. The north side from Conestoga to S. Bldr. Creek is ready for some re-development into mixed use, especially east of 55th St. The idea of buses running down the middle of Arapahoe similar to Denver's 16th St. seems totally unrealistic, given that there will still be auto traffic on Arapahoe. Busses crossing the auto lanes to the curb will further congest the traffic flow. Do not install useless 8 ft. wide sidewalks as was done between Folsom & 28th St., and east of 63rd St. Integrate the bike lanes into the sides of the auto lanes. Bicyclist using sidewalks do not pay attention to turning autos. In any case, I rarely see pedestrians or bicyclists any where along Arapahoe.

Idea Author: Archie S

Number of Seconds 0

Number of Points 12

Number of Comments 2

Comment 1: I largely agree with the original post. I don't feel that staff hasn't done a good job explaining the bus concept; I think many of us simply don't see the need for expanded bus



traffic, which would not be necessary unless high-density housing was added to the area, which many of us do not support. | By Rachel B

Comment 2: The southeast corner of 55th and Arapahoe is the perfect spot for low impact dense multi use redevelopment. It is surrounded by the golf course, has fire station and there is already a large apartment complex. The existing uses are generally old single family homes that have been converted to businesses.

Bus rapid transit on an arterial street is not at all like the 16th St. Mall shuttles. BRT isn't slow and doesn't stop every block. The BRT busses wouldn't cross to the curb, they would stop at stations in the median and the passengers would cross the street as pedestrians. Staff has obviously not done an acceptable job of explaining this concept to the general public. | By David B

Idea Title: East Arapahoe..A place to restore and replenish

Idea Detail: Perhaps there is also the opportunity to create a small native botanic garden as part of the Golf Course Flood water mitigation project, a sculpture garden or public art venue.. Or a walkable Labarynth? An educational Water garden that stresses the importance of conservation of our resources. Create affordable artist studio space on east Arapahoe. Perhaps Naropa could advise on a public meditation center... The recent "calming" of traffic along Cherryvale should be kept in mind while developing ideas for this area. Clearly the residents of this residential neighborhood would like to retain the quiet peaceful nature of this once semi-rural area of Boulder County!

Idea Author: Susan B

Number of Seconds 0

Number of Points 12

Number of Comments 0

Idea Title: Wildflowers and nature

Idea Detail: I moved to the east side because it was calm and I could view wildlife. Now I have a huge hospital across the street from me and I can't get out on Arapahoe without risking my life due to all the traffic. Please, no more traffic, no more cheap, squeezed together, compact housing. Give us dog parks, walking paths and other opportunities to connect with nature. No more cement and congestion. There are plenty of restaurants and stores in Boulder. We can go there when we need to shop or want to eat out. There's lots of new housing in town. Let's not turn into Los Angeles.



Idea Author: Kathy S

Number of Seconds 0

Number of Points 12

Number of Comments 1

Comment 1: There should be a category in the ratings for "I don't like it." I am not really neutral at all, I disagree. The hospital will continue to grow as more of us get older and have more health problems. Hospitals also are large employers of modest wage positions that could benefit greatly from affordable housing nearby, even if they chose to drive a mile or two rather than walk, bike or bus. | By David B

Idea Title: Add middle income housing that appeals to families

Idea Detail: But design and implement in such a way to not drive out light industry. Land uses should be integrated and diverse.

Idea Author: Deryn W

Number of Seconds 0

Number of Points 11

Number of Comments 0

Idea Title: Add cheap artists/makers warehouse rentals to Resource 2000 yard

Idea Detail: Marijuana grow operations are driving up the costs for warehouse spaces. Artists, inventors, and makers are being forced to rent workshop spaces in far away places like Denver and Golden. I think it would be nice to add a large warehouse to that empty field at the Resource 2000 yard. It could be subdivided into many smaller studio spaces to be rented out. It could have a separate access so it could be used outside of Resource 2000 hours. During Resource 2000 open hours, the artists and makers could grab cheap materials! There could be a special display/purchase area in Resource 2000 highlighting the best creations. We need more spaces to create!

Idea Author: W E



Number of Seconds 0

Number of Points 11

Number of Comments 2

Comment 1: Especially for students. | By Felicia F

Comment 2: Great idea. Keep artists in Boulder. | By Felicia F

Idea Title: Arapahoe Ave. tunnel with paths, wildlife corridor, etc. above

Idea Detail: Put Arapahoe Ave. underground (through a tunnel) for at least a few hundred feet, preferably near 55th St., and put walkways, a bike path, a wildlife corridor, gathering places, greenery, art installations, etc. on top to serve as a connection between areas north and south of Arapahoe Ave. and to serve as an anchor for a destination spot for the area.

Idea Author: Don P

Number of Seconds 0

Number of Points 10

Number of Comments 1

Comment 1: Bad idea. This would be a senseless waste of money | By David B

Idea Title: Respect the wishes of the area residents

Idea Detail: May residents of the small neighborhoods east of 55th, Old Tale Road, the Reserve and Simmons Drive, among others, are happy with our peaceful part of town and have NO desire for a 'vibrant' new landscape that includes more development, traffic, housing or other major changes. Our light industrial neighbors are largely good neighbors and we do not want to see them displaced. We are concerned about flood preparation and mitigation, but do not wish to see the golf course torn down. Please don't force YOUR vision on those of us who already live in the area. Thank you.

Idea Author: Rachel B

Number of Seconds 0



Number of Points 8

Number of Comments 0

Idea Title: Better bike and pedestrian access along Arapahoe

Idea Detail: I hate biking along Arapahoe, and avoid it despite the number of great places that have popped up recently (Wild Woods, Bru, etc.).

- Bike paths are poorly labelled
- Sidewalks only exist in some places and can be quite narrow even when they do exist
- There are a LOT of driveways with cars entering and exiting

My few experiences as a pedestrian along Arapahoe aren't much better. Businesses are quite far apart, sidewalks come and go, and bus stops aren't aligned with businesses (try getting to Avery by bus).

- * I'd love to see bike routes that are clearly marked and protected from traffic.
- * Make sidewalks wider and make sure they're continuous along the length of the street.
- * Add additional bus stops so pedestrians don't have to walk so far to reach restaurants and other businesses.

Idea Author: Jackson F

Number of Seconds 0

Number of Points 8

Number of Comments 0

Idea Title: I would like to see some public art!

Idea Detail: It seems like in all of these Envision plans around Boulder/Louisville a creative component is missing. If there is anything that can inspire and engage the public it is art. I believe we can create installations that involve the community and bring a level of curiosity and pride to the area.

Idea Author: Dawn D

Number of Seconds 0



Number of Points 8

Number of Comments 0

Idea Title: Can we bring back the Pearl Trolley

Idea Detail: Given Boulder Junction on Pearl, can we run a trolley from this East Arapahoe into downtown for all the employees who travel by transit to Boulder everyday.

Idea Author: Ryan M

Number of Seconds 0

Number of Points 8

Number of Comments 0

Idea Title: Go slowly. What we have works pretty well.

Idea Detail: City staff's current view of what might be good in 5 or 10 years may be correct -- or may be entirely misplaced. I don't want to see the East Arapahoe corridor micromanaged now for the next decades. If there are slight changes, that's fine, but go slowly.

Idea Author: Brad P

Number of Seconds 0

Number of Points 8

Number of Comments 0

Idea Title: Build sustainable, complete, bike/walk friendly areas in Boulder

Idea Detail: Increased density is very effective at preserving natural resources globally. Dwellers of dense, urban environments use less energy and resources on average than those in less dense environments. See <http://tinyurl.com/lynnces> for evidence. In a democratic society, all citizens should be able to choose to live in Boulder, not just "those who arrived first". The best kind of density is "self-sufficient" density where dwellers can meet most of their needs for commerce, entertainment, food, etc within a walkable distance.

Therefore I suggest that East Arapahoe be developed with this in mind. Specifically, create



mixed-use development that allows residential, office, retail/restaurant all within very close proximity. Increased density is a positive as long as new residents don't have to drive to other areas of Boulder to fulfill their needs/wants. Zone East Arapahoe appropriately and don't compromise this with developers just to foster development. Zone it and they will come.

Idea Author: tim N

Number of Seconds 0

Number of Points 8

Number of Comments 1

Comment 1: I respectfully disagree. The current residents, many of whom chose the quiet area east of Arapahoe specifically because it was not a dense, urban environment are not just 'those who arrived first' - they are the lifeblood of the current community. If we don't want high-density, we should not have it forced on us simply to please a bunch of people who don't live here...and to make the developers rich. | By Rachel B

Idea Title: Road updates east of Cherryvale

Idea Detail: Now that the city has spent a huge amount of money re-doing the road east of Cherryvale, how about letting people use it in a more efficient fashion? Get rid of the bus lanes, it needs to be four lanes east of 63rd. The eastbound "turn only" at 63rd goes nowhere, while access to the ReSource Yard and the Ed Center is no easier than it was prior to reconstruction. MAKE THE ROAD FOUR LANES OUT TO 75TH . Choking it down to two lanes at 63rd is a ridiculous way to manage traffic.

Idea Author: Jeff P

Number of Seconds 0

Number of Points 7

Number of Comments 0

Idea Title: More commercial enterprises, e.g. small grocery, restaurants...

Idea Detail: East Boulder would benefit from a small grocery market (think Ideal) located near 55th and Arapahoe. This would leverage the coffee shops, small restaurants, and other shops beginning to surface in the area. The recent expansion of the Boulder Community Hospital will



provide the customers necessary to support such a community. The current East Boulder neighborhood is stale and would benefit from some revitalization.

Idea Author: Mark M

Number of Seconds 0

Number of Points 7

Number of Comments 0

Idea Title: Rezone the whole corridor to 5 stories

Idea Detail: There isn't really a nice building on the entirety of East Arapahoe and Boulder needs to grow somewhere...Arapahoe has good connectivity to major highways and services and big building won't block anybodies views of the hills.

Idea Author: Jim M

Number of Seconds 0

Number of Points 4

Number of Comments 0

Idea Title: No more Supersizing Boulder! No more height exemptions!

Idea Detail: We can't go back from the ugly 55ft cement soviet block housing of Boulder Junction. Do repeat that mistake along Arapahoe.

Traffic is already maxed out on this road. Adding thousands of more jobs and residents will just throw it into complete gridlock. This "New Urbanism" trend doesn't work for Boulder. Let Denver have it.

No more 55ft Exemptions. No more setback back exemptions. No more cement paths counting as green space. We don't want another wall of tall buildings.

Listen to the citizens of Boulder, not the outside Developers!

Idea Author: Stephen H

Number of Seconds 0



Number of Points 4

Number of Comments 0

Idea Title: We already are plenty connected, resilient and vibrant!

Idea Detail: Our neighborhood is wonderful the way it is. What we don't need is high-density housing and business out here. We chose to live here because it isn't like downtown Boulder. Please don't build more of the same -- more 5-story apartments and "mixed-use" developments. That would be sadly counter to the uniquely quiet, rural feeling of this neighborhood.

Idea Author: Leah B

Number of Seconds 0

Number of Points 4

Number of Comments 0

Idea Title: Easy public transport route to the Table Mesa PnR

Idea Detail: its pretty difficult to get to the TM PnR to take the DIA bus. Also increasing the frequency of the JUMP would help

Idea Author: Karen C

Number of Seconds 0

Number of Points 3

Number of Comments 0

About the Scenarios

What are Scenarios?

Scenarios are a **starting point for community discussion about future choices and tradeoffs**. Each scenario contains a menu of choices that can be isolated or mixed. Scenario planning is a way to:

- Envision and evaluate different possible choices, or “futures;”
- Reflect community ideas given to date,
- Illustrate a range of choices such as adding retail services, public spaces, new housing, improved connections and multi-modal transportation, and new infrastructure,
- Include “puzzle pieces” or choices that can be removed from and/or placed into a different scenario.

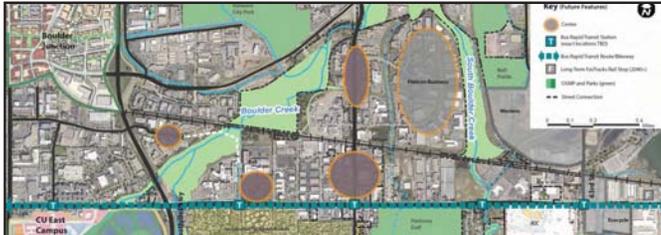


A: Current Trends - (what if zoning stays as is?)



The scenarios are thematic in nature, and study potential ideas at several geographic locations that are not in the floodplain.

1. Boulder Community Health
2. Walnut East
3. 55th and Arapahoe
4. 55th St. North
5. Flatiron Industrial Park
6. East of South Boulder Creek
7. Recycle Row
8. Proposed FasTracks Station



B: Districts



C: Housing Choices



Visualization of Potential Futures

Arapahoe Avenue at 55th Street



Today

Possible Futures



- Features**
- Enhanced Crossings and Pedestrian Refuge
 - Transit Stop
 - Street Trees and Sidewalk Enhancements
 - B-Cycle
 - BRT in Dedicated Lane



- Features**
- Buffered Bike Lanes
 - Enhanced Crossings and Pedestrian Refuge
 - Transit Stop
 - Street Trees and Sidewalk Enhancements
 - B-Cycle
 - BRT in Dedicated Lane



- Features**
- Pedestrian Refuge
 - BRT Lane
 - Enhanced Crossings and Sidewalks
 - Potential Future Buildings
 - Street Trees and Sidewalk Enhancements
 - B-Cycle
 - On-Street Parking

55th Street at Western Avenue



Today



- Features**
- Buffered Bike Lane
 - Enhanced Sidewalks and Crossing
 - Pedestrian Refuge
 - Street Trees and Landscaping



- Features**
- On-Street Parking
 - Buffered Bike Lane
 - Foodcarts
 - Adaptive Reuse of Existing Building
 - Pedestrian Refuge
 - Street Trees and Landscaping
 - Enhanced Sidewalks and Crossing



- Features**
- On-Street Parking
 - Buffered Bike Lane
 - Foodcarts
 - Potential Future Buildings
 - Adaptive Reuse of Existing Building
 - Pedestrian Refuge
 - Street Trees and Landscaping
 - Enhanced Sidewalks and Crossing

Arapahoe Avenue at Commerce Street



Today



- Features**
- Landscaped Median
 - Pedestrian Refuge
 - BRT in Dedicated Lane



- Features**
- Sidewalk and Streetscape Enhancements
 - Pedestrian Refuge
 - BRT in Dedicated Lane
 - Landscaped Median
 - Potential Future Buildings

Commerce Street



Today



- Features**
- Bike Lanes
 - Sidewalk Enhancements, Seating, Bike Racks



- Features**
- Wide, Comfortable Sidewalks
 - Street Trees
 - Bike Lanes
 - Pedestrian-Scale Street Lights
 - Seating, Bike Racks



A. Current Trends Scenario

Continues with **predominantly light industrial uses** with little change to infrastructure.

What are Key Features?

1. Light industry, low rise, suburban patterns of development with surface parking lots
2. Affordable service industrial, and places for storage units
3. Quiet suburban neighborhoods to the south.
4. A few places to eat or shop
5. People generally drive for daily needs
6. Separate from other parts of the city
7. Improved transit (BRT)



Walnut East

- Office Park




Boulder Community Health/Ball

- Office
- Surface Parking



55th/Arapahoe

- Some Retail
- Light Industry
- Mobility Hub




55th Street North:

- Light Industry




Recycle Row

- Trucks and Industry
- Recycle Center




Future FasTracks (Long Term)

- Little to No Change to this site in near term

Visualization of Potential Futures at Key Locations



A. Current Trends: 3D Aerial View



Key:
Retail Office
Health Housing
Education Light Industrial



B. Districts Scenario

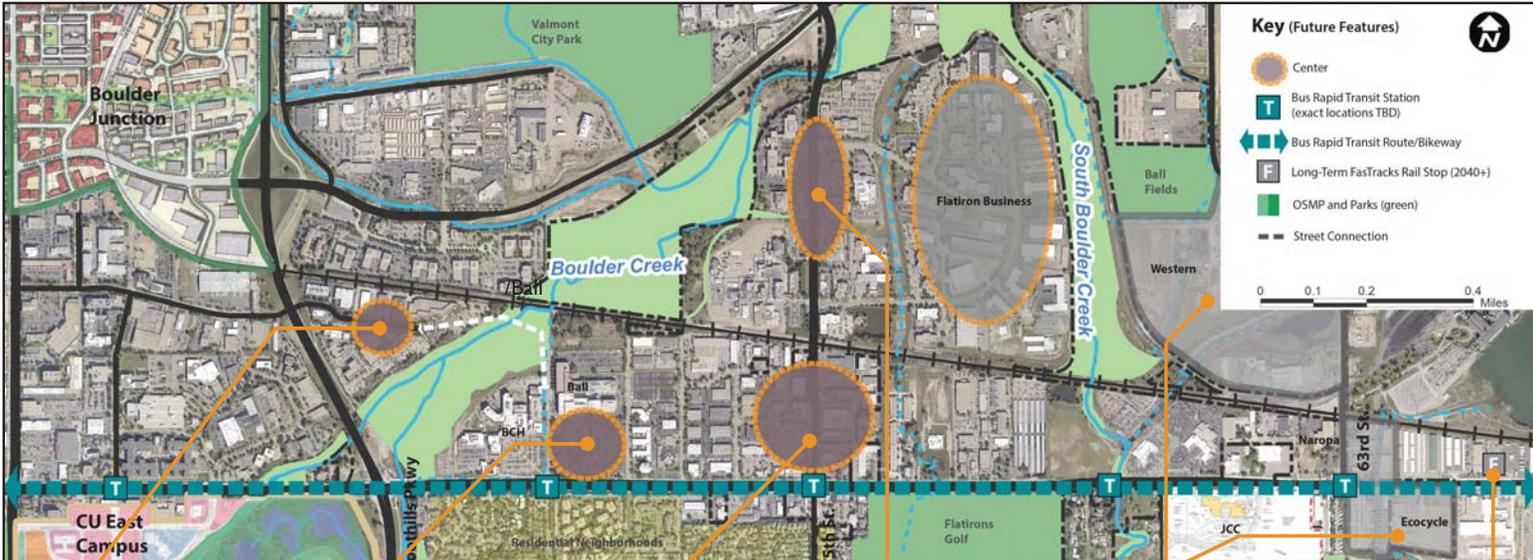
Becomes a place where **existing organizations, industry, and business thrive**, arts and entertainment are a destination, and neighborhood residents can access their daily needs. Has high level of street improvements at Arapahoe Ave. intersections, possible new road connections and net zero energy districts.

What Are Key Features?

1. Health district around Boulder Community Health – medical-related office spaces
2. Arts and entertainment near 55th/Arapahoe – Dinner theater
3. Mixed retail, dining, office at 55th/ Arapahoe and along 55th Street
4. Improvements to sidewalks and intersections so people can walk safely and conveniently
5. Public spaces for people to recreate and relax (pocket parks, plazas, interior streets)
6. East/west connecting street (Walnut /48th St.)
7. Affordable service industrial along Arapahoe at the east end
8. More activity on the street as it is easier to travel by foot, bike, transit
9. Recycle Row more of a destination, location for energy generation, net zero (earth and sun power energy replaces fossil fuels)
10. Improved eastern gateway



Most of the area will continue according to existing trends.



Walnut East

- Street Connection
- Medical related offices expand

Medical Offices

Boulder Community Health/Ball

- Medical office, Retail
- Shared parking & amenities

Medical Offices

BCH/Ball Shared Amenities

Office/Retail

55th/Arapahoe

- Retail, Services
- Some housing
- Arts and culture
- Mobility hub

Walkable Retail

Mobility Hub (Car/Bike Share)

55th Street North

- Office
- Retail to serve industrial park

Manufacturing/Retail

Manufacturing/Retail

Retail Reuse of Existing Structures

Recycle Row

- Ecodistrict
- Energy Generation
- Trucks and Industry
- Gateway

Recycling Center

Ecodistrict

Future FasTracks (Long Term)

- Future Gateway
- Mixed-use
- Edge parking

Visualization of Potential Futures at Key Locations



B. Districts: 3D Aerial View



Key:

- Retail
- Office
- Health
- Housing
- Education
- Light Industrial



C. Housing Choices Scenario

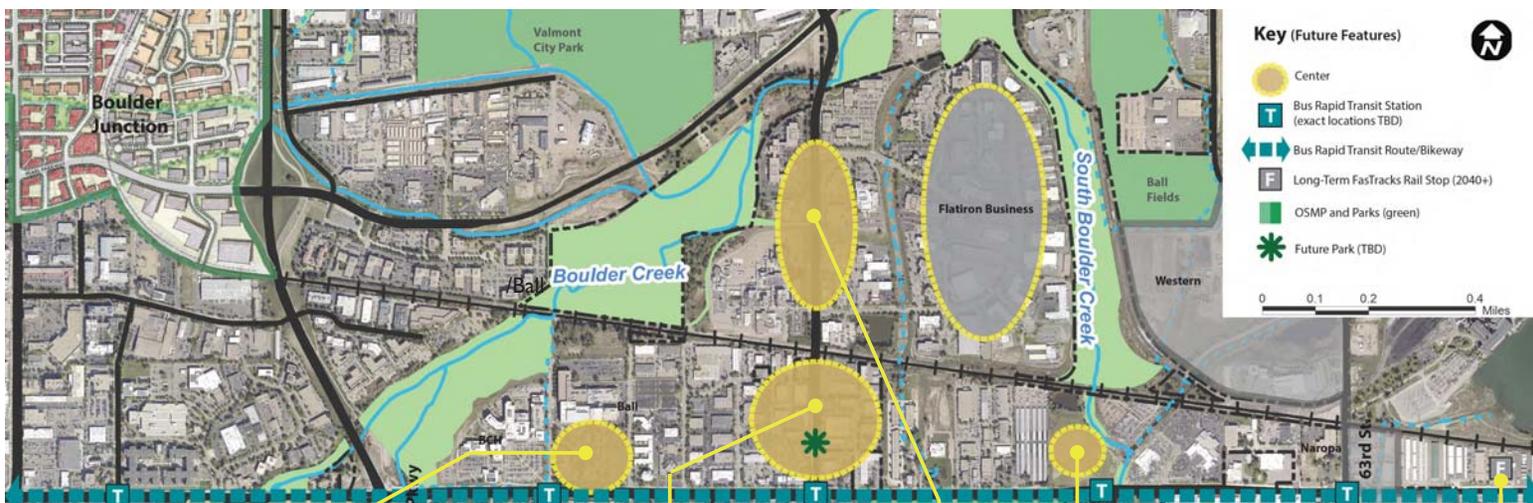
Becomes a place with **new workforce and affordable housing** in centers north of Arapahoe Ave., plus dining, shopping, arts and entertainment are within easy walking distance. Includes highest level of street improvements, beautification, and ecological restoration and connections to open space.

What Are Key Features?

1. Some housing within a short (15-minute or less) walking distance from shops, dining, everyday needs and work (e.g., mixed retail, dining, housing, and some offices)
2. Affordable workforce housing at 55th and Arapahoe and near South Boulder Creek
3. Housing intertwined with natural systems, with rain and snow melt feeding trees, landscapes, gardens, and ecological restoration (renewable energy replaces fossil fuels)
4. Golf course adds trails and community gardens
5. More public spaces and parks for residents
6. Boulevard with street trees, noise buffering, slower speeds (safe and friendly)
7. Multiple ways for people to travel
8. "Gateway" beautification east end of city
9. City services in neighborhoods (e.g., parks access to nature)



Most of the area will continue according to existing trends.



15-Minute Walk

- Enhanced pedestrian safety and connections around transit

Boulder Community Health/ Ball

- Office, Retail
- Shared parking & amenities

55th/Arapahoe

- New housing in select locations
- Dinner theater and other businesses become part of an art center
- Mobility hub
- Shops and restaurants

55th Street North:

- Live-work mixed with offices
- Retail

South Boulder Creek

- Housing
- Greenway Enhancements

Future FasTracks (Long Term)

- Mixed-use
- Edge parking

Visualization of Potential Futures at Key Locations



C. Housing Choices: 3D Aerial View



Key:

- Retail
- Office
- Health
- Housing
- Education
- Light Industrial





About the Scenarios

Jan. 15, 2015

The Envision East Arapahoe project gives the community a chance to address current needs and describe what type of future is desirable.

Scenario planning is a way to:

- ✓ envision and evaluate different possible choices, or “futures,”
- ✓ reflect community ideas given to date,
- ✓ illustrate a range of choices such as adding retail services, public spaces, new housing, improved connections and multi-modal transportation, and new infrastructure,
- ✓ include “puzzle pieces” or choices that can be removed from and/or placed into a different scenario.

Scenarios do not forecast the future or replace traditional planning, but they encourage conversation and support informed decision-making.

Future Scenarios

This packet describes three scenarios with a range of choices for the kind of places the community might like East Arapahoe to be in 20 to 30 years.

These scenarios are a starting point for discussion about choices and tradeoffs and represent focused choices in different locations that can be dis assembled. The three are:

- A—Current Trends**
- B—Districts**
- C—Housing Choices**

Future “Givens”

East Arapahoe Corridor has some known or expected future elements. Some of these “givens” are listed below.

1. **Boulder Community Health** will expand its functions and nearby medical-related offices.
2. **Ball Aerospace** will continue to occupy manufacturing and office space west of 55th Street and north of Arapahoe Avenue.
3. **CU East Campus** will expand for research, teaching facilities, and some housing; bicycle and pedestrian connections across Boulder Creek will be added.
4. **Naropa’s Nalanda Campus** will remain and expand.
5. **Established residential** neighborhoods south of Arapahoe Avenue will remain.
6. **Recycle Row**, the recycling and reuse district will continue as an important center of recycling and waste reduction.
7. **Open space parcels** will remain and greenways will become better connected. Sombrero Marsh will be buffered from trails and recreation.
8. **Flatirons Golf Course** will continue for golf and aid in flood mitigation.
9. **Planning for Regional Bus Rapid Transit (BRT)** on SH7, as adopted by RTD and described in the Northwest Area Mobility Study report.
10. **Connectivity for pedestrian, bicycles and transit** will be improved according to the city’s recently adopted Transportation Master Plan (TMP).
11. **No new development** will occur in the High Hazard and Conveyance Zones. New flood mapping will be revisited in a few years after mitigation.
12. **Urban services** will be provided according to criteria in the Boulder Valley Comprehensive plan (BVCP) (e.g., public water, public sewer, stormwater, fire and police protection, and multimodal transportation).

Possible Futures

“What if” scenarios (that have interchangeable parts) are listed below. Graphics are in separate pages that follow.



A—Current Trends

This scenario represents the future if the East Arapahoe area north of the street maintains its current course with existing employers, light industry, storage units, and older buildings. Little new potential for offices or retail, or new residential units could occur. The street and trail system would remain as it is today. Neighborhoods to the south would remain unchanged in this and all scenarios.

B—Districts

This scenario supports a future where people and businesses continue to innovate and create, with some new services and retail along Arapahoe Avenue and 55th Street where employees of existing businesses can shop and eat. New medical-related offices would be allowed (e.g., in East Walnut area connected by possible street to 48th Street and along Arapahoe) to support Boulder Community Health at Foothills Campus. New development in certain locations would be closer to the street, businesses campuses set back from the street in other parts of the corridor. A small amount of new housing along with new public spaces, pocket parks, energy generation, and “zero energy” use ecodistricts would be included. Programs such as EcoPasses for businesses, managed parking at transit hubs, and enhanced multi-modal connections and streetscape could be explored.

C—Housing Choices

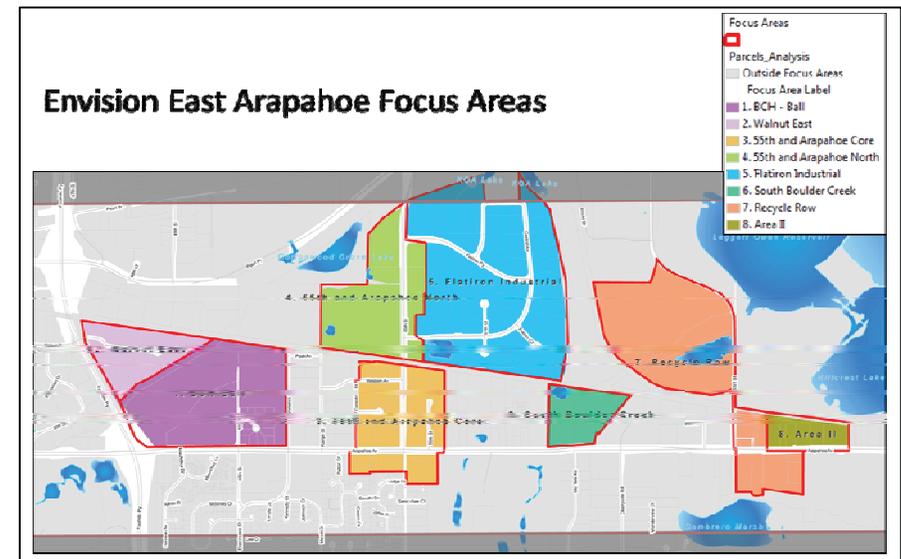
This scenario supports a future with housing where people can live near where they work and/or walk to shopping and dining establishments. Housing would be located near transit “hubs” in centers. A mix of housing types (e.g., flats, townhomes, units where people can live and work, and high-quality attached housing with private outdoor space) and existing and new neighborhood commercial, office and industrial uses could be carefully integrated, including a possible new small grocery or other anchor retail. To support the quality of existing and new neighborhoods, safer crossings and ecological restoration could occur, such as along Dry Creek Ditch or at the golf course, improving access to nature, plus adding new pocket parks and trails north of Arapahoe Ave. and other neighborhood amenities. Programs such as EcoPasses for neighborhoods, and highly improved landscape/streetscape and noise reduction could occur along Arapahoe Ave.

Geographic Focus

The scenarios consider potential land use mixes and ideas for focused locations as follows:

- Boulder Community Health/Ball Aerospace
- Walnut East
- 55th and Arapahoe Ave.
- Along 55th Street north of Arapahoe Ave.
- Flatiron Business Park
- Vacant site on Arapahoe Ave. just east of South Boulder Creek

Infrastructure or gateway character is considered for Recycle Row (north and south of Arapahoe), and the proposed Fast Tracks Station at the city’s eastern city limits in Area II.



Source: Placeways

Transportation Options

Different possibilities for new arterial Bus Rapid Transit (BRT) – buses operating down the center of the road and/or in the outside curb lanes – are included in all scenarios, with different levels of landscaping and other complete street improvements. BRT would operate in existing lanes and would not require the widening of Arapahoe Avenue. Also, Arapahoe Avenue would become a more “complete street” to accommodate safe and convenient bicycling, walking, transit, and vehicles. It would be landscaped with trees and other features. 55th Street is proposed for improvements to enhance bicycle and pedestrian travel and street trees and landscaping.

A separate connections map will be developed following the vision plan for the corridor as a whole. Some additional follow-up meetings to address transportation connections would be beneficial and allow staff to work with the community and property owners to identify specific connections.

Examples of Artrial Bus Rapid Transit



Transfort Max Fort Collins and Geary Corridor BRT, San Francisco



Future visualization example at Commerce and Arapahoe with BRT

Scenario A: Current Trends

Who will live and work there over the next 20 years?

East Arapahoe has many thriving small and large businesses, organizations, and employees and customers who plan to remain. Boulder Community Hospital, Ball Aerospace, Naropa, CU East Campus, Fisher Auto, Western Disposal and “recycle row” businesses offer services and a wide range of jobs. Existing affordable industrial services generally could be preserved. Existing arts and culture would continue (e.g., Dinner Theater, Avalon).

What would change? Where?

- The area will continue to be predominantly industrial, low rise buildings with surrounding parking lots and service areas. New industrial jobs are projected according to current zoning.
- Current zoning generally restricts retail, restaurants, and medical offices in the industrial zone district.
- Arapahoe Avenue and areas north are not particularly active or safe for bicycles and pedestrians because of heavy vehicular traffic, potential conflicts along the streets, and lack of connecting sidewalks and trails.
- North of Arapahoe has few public spaces.

How might it look?

- The streets and area would look similar to today – mainly older buildings set back from the street with parking in front and few public spaces.
- Some stretches of the corridor would retain a lot of mature trees and landscaping.

How does history carry forward?

The development of the area began toward the end of the 19th century with the establishment of farms, auto-oriented businesses, and restaurants. The largest period of development occurred after the area was annexed into the city, with the construction of residential neighborhoods to the south and industrial and commercial buildings constructed along Arapahoe Avenue. While few physical remnants of the area’s past remain, there are opportunities to interpret the area’s past through signage, art, and plaques. Buildings over 50 years old are reviewed by the Historic Preservation program to determine potential eligibility for landmark designation. This applies in all scenarios.

What is the natural environment like?

- Open space covers one third of the study area. It is an area along Boulder Creek that is rich in biodiversity, and maintaining the integrity and size of existing open space is important.
- Golf course continues as a recreational amenity.

How do people get to and from the area?

Generally, East Arapahoe is not currently nor will it be particularly walk or bicycle-friendly for employees or residents. It may continue to be rather disconnected with limited new infrastructure or trails.

- Arterial BRT stations would be built generally every ½ mile and include amenities.
- Existing bus stops would see improvements such as new bus pads, shelters, lighting, and landscaping. Local transit bus (Jump, etc) stops every two blocks.
- In the long-term, a FasTracks station could be built between 63rd Street and the eastern city limits.
- Connections will be shown on a separate map with details of pedestrian and bicycle facilities that will be addressed in a future community-developed connections plan.
- Some improvements would occur at intersections.
- No changes to Transportation Demand Management (TDM) programs or parking.



55th Street and Arapahoe Ave. today

Scenario B: Districts

Who will live and work there over the next 20 years?

As with Scenario A, many of the existing businesses, and organizations and people who currently work in the area would likely stay. Existing and future service industrial would be preserved in existing business parks away from major streets. Existing neighborhoods would be generally unchanged.

- The area continues as a business-friendly “start-up” environment where small businesses thrive.
- Locally serving business and retail (e.g., daycare, restaurants, and food service) replace some of the future industrial jobs in select locations.
- More food trucks are present, and other daytime amenities such as plazas and trail connections are provided north of Arapahoe Ave.
- A small amount of new housing would allow people to live near where they work and near transit.
- Renewable energy generation, recycling, and eco district opportunities are explored (e.g., Recycle Row, and possibly in business parks).
- New daytime arts and culture venues such as street art are encouraged.
- Area reflects the diversity of people working nearby.
- It includes a possible combination of a satellite parking/energy generation, or solar/transportation maintenance facility.
- Includes a “gateway” at the eastern boundary of Boulder signifying arrival to the city (e.g., landscaping, signage, welcome).

What changes? Where?

- **BCH/Ball – Riverbend Park:** Might include a transit hub, small retail, and parking district (with car/bike share).
- **Walnut East:** Explore street extension from 48th Street to Walnut to provide an alternative route in the Foothills Parkway area. Makes medical-related offices, and possibly housing just north of Boulder Creek more viable.
- **55th/Arapahoe:** New retail and office and mobility hub occur between Conestoga and 55th Street. The Dinner Theater and other businesses are presumed to stay. New development will depend on property owner participation and some property assemblage.
- **55th Street North:** Additional offices and services.
- **Recycle Row:** Continues to be an important community service, destination, and gateways with possibilities for new recycling, renewable energy.
- **Future FasTracks:** Over the long term, the area around the future station will convert to a mix of uses and satellite parking.

How might it look?

- Picture new retail, services, and offices near 55th and Arapahoe and along 55th Street generally consisting of two to three stories. Over time, the mix will become more vertical, but initially small infill projects would occur.
- Areas in between the centers would be lower intensity, with buildings set back from the street with and attractive landscaping.
- As part of the street reconstruction for BRT, East Arapahoe would have more landscaping, safer crossings and islands for pedestrians, and safe bicycle lanes. The mature tree canopy over time will provide shade and help buffer road noise.

What is the natural environment like?

- Existing open space connects people in business areas with nature.
- Explore additional pocket parks – places to sit, eat, and relax for employees.
- Flatirons Golf Course may become a higher-quality golf experience with natural and park land space and with flood mitigation.
- Possible local energy generation in form of local turbine(s), solar, and high-performing energy building overlay.

How do people get to and from the area?

The area would have more options for people to travel to/from and within, with transit and other improvements mainly designed to support the businesses and employees. Streetscape, landscape, and amenities are part of arterial BRT stations.

- Arterial BRT stations generally every ½ mile and mobility hub (with bike and car share) at 55th Street.
- Local bus stops (JUMP) every 2 blocks. Improved access and bus stops.
- In the long-term, a FasTracks station could be built between 63rd Street and the eastern city limits.
- A new street from 48th Street near BCH to Walnut is being explored. A new street north from CU East Campus, across Boulder Creek to Boulder Junction (33rd Street) is in CU East Plan and the TMP.
- The details of bicycle and pedestrian connections will be addressed in a future connections plan. Improvements at intersections, built towards a low-stress bike network and improved bikeway is suggested on Arapahoe Avenue.
- Satellite and managed parking at mobility hubs (TBD). EcoPass for businesses.

Scenario C: Housing Choices

Who will live and work there over the next 20 years?

As with the first two scenarios, many of the existing business, organizations, and people who currently work and live in the area will likely stay. Existing neighborhoods would be generally unchanged.

- People are able to live and work in the same area and easily walk to transit, an enhanced bicycle and pedestrian network, and shops.
- The area includes some new housing for the workforce – mainly adults rather than families with children because of the industrial environment and lack of schools north of Arapahoe Avenue.
- Walkable neighborhoods would be in designated places near services, transit, and bicycle and pedestrian network.
- Housing could enable residents to live close to work and house artists and innovators (e.g., live-work). Some net zero energy housing.
- Affordable housing is aimed to increase income diversity.
- Retail, including a possible small grocery store may be included where people can shop for daily needs.
- Interior streets away from Arapahoe Ave. would be conducive to people who walk and bicycle in the area safely and conveniently.
- The area could include additional arts venues such as that would likely have more appeal for weekend or evening visitors and residents.
- Includes designated community garden/local food production areas, community-scale grocery store, library, pocket parks, and public space.
- It includes public spaces for people that are free or low-cost, accessible, and designed for all people.

What changes? Where?

- **55th/Arapahoe:** There would be some new housing in select locations along Arapahoe Avenue. The Dinner Theater and other businesses are presumed to stay and become part of an art center. A mobility hub would be part of the site as well as shops and restaurants.
- **55th Street North:** Additional live-work housing mixed with some offices. Additional housing where appropriate along the road or near greenspace (e.g., Dry Creek ditch west of Flatiron Park).
- **South Boulder Creek Site:** Mixed housing project (some attached apartment housing, townhomes).
- **Future FasTracks:** This site could accommodate a long term mix of uses and satellite parking.
- **Other areas:** Continue with current trends until further planning completed.

How might it look?

- Some new housing near 55th and Arapahoe and along 55th Street, with up to three to four stories near the street and with townhomes tucked behind.
- Areas in between centers would be lower intensity, buildings set back.
- Additional tree-lined boulevards to buffer noise and make the area more attractive to residents to walk, use transit, and bike.

What is the natural environment like?

- Maintain existing open space and ecological areas. Additional restoration to intertwine natural areas through new neighborhoods so people can access nearby nature (e.g., Dry Creek ditch area and near South Boulder Creek).
- Site planning provides visual and physical access to nature and open space and uses rain and snow melt runoff to irrigate trees and landscaping.
- The golf course continues for golf but could become more multi-purpose, including a multi-use path, gardens. Becomes more attractive from the street and is designed for flood mitigation.
- More attention to nighttime lighting and security to minimize impacts on existing neighborhoods.
- Local energy generation (sun and earth powered energy).
- Emphasize view protection, noise mitigation, landscaping.

How do people get to and from the area?

The area would have more options for people to travel to/from and within, with transit, bicycling and walking and other improvements designed primarily to support existing neighborhoods and future 15-minute districts and employees. Streetscape, landscape, and amenities are important at arterial BRT station areas.

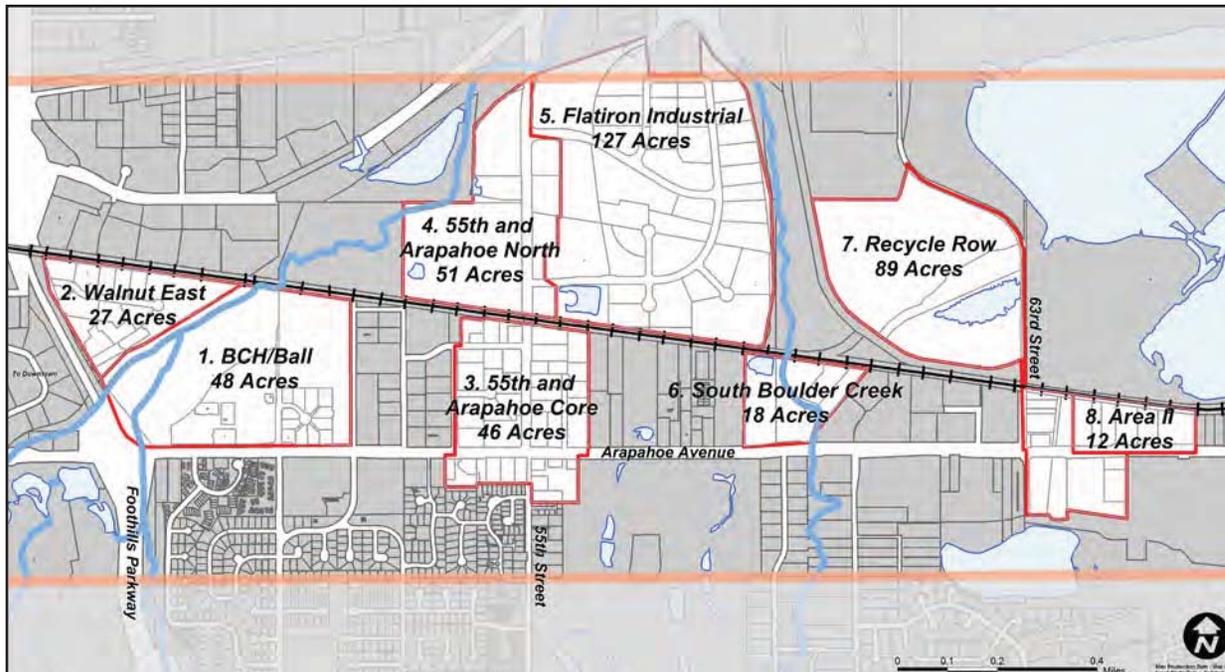
- Arterial BRT stations generally every ½ mile and a mobility hub (with bike and car share and parking). Stops every ¼ mile west of Foothills Parkway. Local bus stops (JUMP) every 2 blocks and improved access and bus stops and crossings.
- In the long-term, a FasTracks station could be built near 63rd Street/city limits.
- CU East Plan and TMP suggest a new street, north from CU East Campus, across Boulder Creek to Boulder Junction (33rd Street).
- More emphasis on crosswalks to the south side of Arapahoe Avenue.
- Improvements at intersections, built towards a low-stress bike network and improved bikeway is suggested on Arapahoe Avenue.
- Implement managed parking at mobility hubs (TBD) and other key areas. EcoPass for neighborhoods.

Envision East Arapahoe Focus Areas

Draft: 1/13/2015

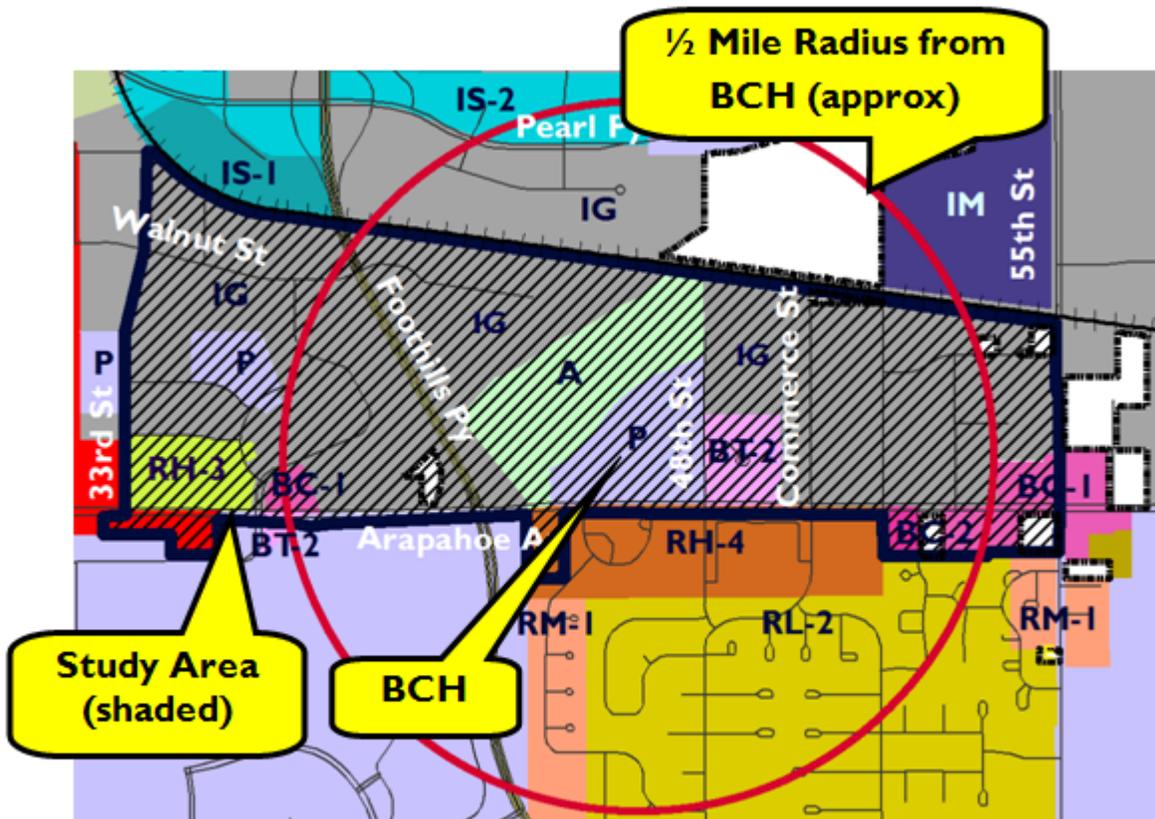
Projected Buildout for Scenarios

	Baseline ¹	A Current Trends		B Districts		C Housing Choices ²	
	Existing	New	Total	New	Total	New	Total
Employment³							
Job Units	11,100	4,300	15,500	3,900	15,000	2,500	13,500
Square Feet	4,278,000	1,656,800	5,934,800	1,485,400	5,763,400	981,100	5,259,100
Residential							
Dwelling Units	70	0	70	460	540	1,300	1,370
Residents ⁴	160	0	160	1,000	1,160	2,810	2,970
Square Feet ⁵	70,000	0	70,000	460,000	540,000	1,300,000	1,370,000



- 1 Baseline was estimated from best available data, including a combination of Boulder County assessor files for property information, sales tax point data, City of Boulder dwelling unit GIS file and US Census Bureau data. Baseline was calculated from data compiled in July and August, 2014.
- 2 In Scenario C, a higher range of potential new job units (4,100) and housing units (2,360) were tested as part of the transportation analysis.
- 3 Job Units and Employment Square Feet are both sourced from Community Viz GIS model, and rounded to the nearest 100. On average, this means there are approx. 385 square feet per employee (350-420 depending on job type), varying due to the employment mix of each scenario.
- 4 Population estimates are based on an average occupancy of 2.16 persons per unit.
- 5 Square foot estimate are based on an average of 1,000 square feet per dwelling unit.

ATTACHMENT D - MEDICAL OFFICE BACKGROUND



Use Table – Medical Related Uses Around Boulder Community Health

Land Use	Zoning District					
	Residential High 4 (RH-4)	Business Transitional 2 BT-2	Business Community BC-1, BC-2	Industrial General IG	Industrial Manufacturing IM	Public P
Hospitals	*	*	*	*	*	A
Medical or dental clinic or office	U	A	A	*	*	U
Medical and dental laboratory	*	A	A	A	*	*
Professional Offices	U	A	A	*	*	*
Personal Services	A	A	A	*	*	*

A = Allowed; * = Prohibited; U = Requires Use Review