



**CITY OF BOULDER
PLANNING BOARD MEETING AGENDA**

DATE: April 3, 2014

TIME: 5 p.m.

PLACE: Council Chambers, 1777 Broadway

1. CALL TO ORDER

2. APPROVAL OF MINUTES

3. PUBLIC PARTICIPATION

4. DISCUSSION OF DISPOSITIONS, PLANNING BOARD CALL-UPS/CONTINUATIONS

A. [Call up: 2245 Pine Street](#). Expires April 4, 2014

5. PUBLIC HEARING ITEMS

A. [Public hearing and consideration of a Use Review application, no. LUR2012-00101, for expansion of the Family Learning Center located at 3164 34th St. in the Residential Medium-One \(RM-1\) zone district.](#) The proposal includes a new, 2,427 square foot classroom addition as well as site, landscaping and parking lot improvements. Applicant/Owner: The Family Learning Center

B. [Public hearing and consideration of a Use Review application, no. LUR2013-00065, for approval of an 11-space parking lot as a second principal use in addition to a proposed 3-unit residential townhome development at 2360 Grove St. in the RH-2 zone district \(see LUR2013-00051 for associated Site Review\).](#)

Applicant: Peter Stewart

Owner: Grove 3, LLC

C. [Public hearing to consider application #LUR2014-00003 to amend the previous Site and Use Review approval \(case #LUR2008-00083\) for the Washington Village project, located at 1215 Cedar Avenue, which allowed 33 dwelling units and 2,950 square feet of office/commercial space on the ground floor of a new building along Broadway and community facilities on the 3-acre site. The following modifications within the RH-2 \(Residential High – 2\) zoning district are requested:](#)

- Addition of three dwelling units bringing the site total to 36 dwelling units (30 dwelling on the RH-2 side);
- Additional floor area within the Broadway Building and North Building totaling 5,059 square feet (1,152 square feet would be above grade with the remaining space in basements and storage).
- Reconsideration of the previously approved Use Review to change a condition of approval to reduce parking by one parking space in light of a proposed 300 square feet reduction in the commercial space; and
- A parking reduction of 11 percent.

Applicant: Adrian Sopher, Sopher Architects

Property Owner: Washington School Development Company, LLC

D. [Public hearing and consideration of Site and Use Review applications for 2200 Broadway, referred to as the Trinity Commons, consistent with Ordinance no. 7516, to redevelop the existing surface parking lot as 24 attached residential units, 16 of which are planned as permanently affordable senior housing, along with office space for the Trinity Lutheran Church, and other non-profit organizations, along with partially below grade parking. The parking will be shared with other off-site users through a Use Review management plan. The application includes a condition of approval for landmarking the existing, historic portion of the Trinity Lutheran Church. The project is reviewed under case no. LUR2014-00048 and LUR2013-00014.](#)

Applicant: Hartronft Associates

Property Owners: Trinity Evangelical Lutheran Church of Boulder, Colorado

6. MATTERS FROM THE PLANNING BOARD, PLANNING DIRECTOR, AND CITY ATTORNEY

7. DEBRIEF MEETING/CALENDAR CHECK

8. ADJOURNMENT

**CITY OF BOULDER PLANNING BOARD
MEETING GUIDELINES**

CALL TO ORDER

The Board must have a quorum (four members present) before the meeting can be called to order.

AGENDA

The Board may rearrange the order of the Agenda or delete items for good cause. The Board may not add items requiring public notice.

PUBLIC PARTICIPATION

The public is welcome to address the Board (3 minutes* maximum per speaker) during the Public Participation portion of the meeting regarding any item not scheduled for a public hearing. The only items scheduled for a public hearing are those listed under the category PUBLIC HEARING ITEMS on the Agenda. Any exhibits introduced into the record at this time must be provided in quantities of ten (10) to the Board Secretary for distribution to the Board and admission into the record.

DISCUSSION AND STUDY SESSION ITEMS

Discussion and study session items do not require motions of approval or recommendation.

PUBLIC HEARING ITEMS

A Public Hearing item requires a motion and a vote. The general format for hearing of an action item is as follows:

1. Presentations

- a. Staff presentation (5 minutes maximum*)
- b. Applicant presentation (15 minute maximum*). Any exhibits introduced into the record at this time must be provided in quantities of ten (10) to the Board Secretary for distribution to the Board and admission into the record.
- c. Planning Board questioning of staff or applicant for information only.

2. Public Hearing

Each speaker will be allowed an oral presentation (3 minutes maximum*). All speakers wishing to pool their time must be present, and time allotted will be determined by the Chair. No pooled time presentation will be permitted to exceed ten minutes total.

- Time remaining is presented by a Green blinking light that means one minute remains, a Yellow light means 30 seconds remain, and a Red light and beep means time has expired.
- Speakers should introduce themselves, giving name and address. If officially representing a group, homeowners' association, etc., please state that for the record as well.
- Speakers are requested not to repeat items addressed by previous speakers other than to express points of agreement or disagreement. Refrain from reading long documents, and summarize comments wherever possible. Long documents may be submitted and will become a part of the official record.
- Speakers should address the Land Use Regulation criteria and, if possible, reference the rules that the Board uses to decide a case.
- Any exhibits introduced into the record at the hearing must be provided in quantities of ten (10) to the Secretary for distribution to the Board and admission into the record.
- Citizens can send a letter to the Planning staff at 1739 Broadway, Boulder, CO 80302, two weeks before the Planning Board meeting, to be included in the Board packet. Correspondence received after this time will be distributed at the Board meeting.

3. Board Action

- d. Board motion. Motions may take any number of forms. With regard to a specific development proposal, the motion generally is to either approve the project (with or without conditions), to deny it, or to continue the matter to a date certain (generally in order to obtain additional information).
- e. Board discussion. This is undertaken entirely by members of the Board. The applicant, members of the public or city staff participate only if called upon by the Chair.
- f. Board action (the vote). An affirmative vote of at least four members of the Board is required to pass a motion approving any action. If the vote taken results in either a tie, a vote of three to two, or a vote of three to one in favor of approval, the applicant shall be automatically allowed a rehearing upon requesting the same in writing within seven days.

MATTERS FROM THE PLANNING BOARD, DIRECTOR, AND CITY ATTORNEY

Any Planning Board member, the Planning Director, or the City Attorney may introduce before the Board matters which are not included in the formal agenda.

ADJOURNMENT

The Board's goal is that regular meetings adjourn by 10:30 p.m. and that study sessions adjourn by 10:00 p.m. Agenda items will not be commenced after 10:00 p.m. except by majority vote of Board members present.

*The Chair may lengthen or shorten the time allotted as appropriate. If the allotted time is exceeded, the Chair may request that the speaker conclude his or her comments.

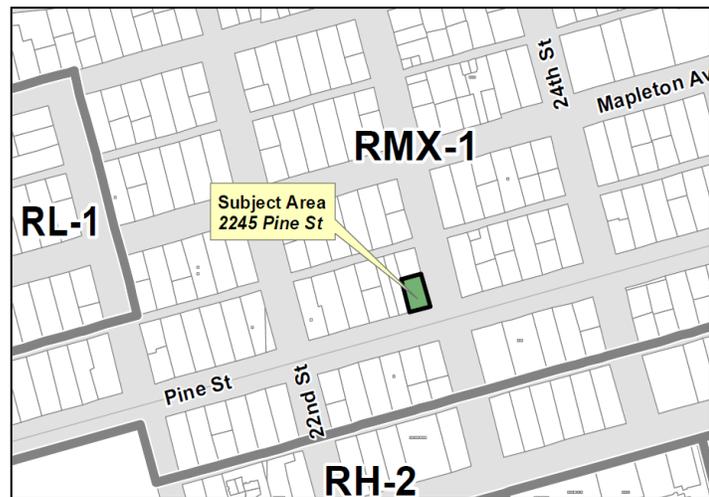
MEMORANDUM

TO: Planning Board

FROM: Elaine McLaughlin, Case Manager

DATE: March 21, 2014

SUBJECT: **Call Up Item: 2245 Pine Street.** Approval of an administrative Site Review for a minor amendment application for an addition to an existing single family residence within a two lot Planned Unit Development, within the Residential Mixed – 1 (RMX-1) zoning district.



Background: The existing single family residence is part of a two lot Planned Unit Development from the 1980s (case no. P-80-13). As such any additions exceeding 10 percent of the overall floor area are subject to an amendment process. The proposed first and second floor additions exceed the maximum 10 percent addition permitted through a Minor Modification, and therefore the proposal is subject to a Minor Amendment per Land Use Code section 9-2-14(l), B.R.C. 1981 referenced at the following weblink [here](#). The original home was built in 1883, an image of the home is shown in an 1897 photo of the house viewed from Pine Street in Figure 1. The project originally was review as a demolition permit application through historic preservation. Subsequently, the applicant applied for individual landmark designation and landmark alteration certificate review for the rear addition to the house. A landmark alteration certificate was issued by the Landmarks design review committee on Dec. 19, 2013. The front, Pine Street, elevation will not change but the change will occur visibly from the 23rd Street elevation.



Figure 1:
Photograph of front (Pine Street) home in 1897

Proposed Project: The applicant is requesting an 81 square foot addition to the rear of the first floor, and a 532 square foot addition at the rear of the property to the second floor for a total addition of 613 square feet to the existing 1,390 square foot single family residence. A new porch on the first floor at the rear of the property, facing 23rd Street is also proposed. Figure 2 illustrates the existing and proposed 23rd Street, side elevation. As part of the Minor Amendment, the applicant is also requesting a modification to the side yard setback adjacent to a street (23rd Street) of 9'-10" where 12'-5" is permitted by right.

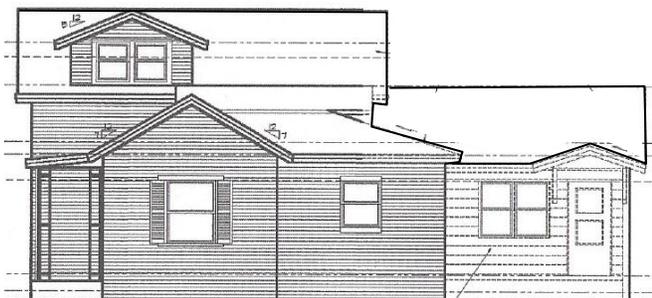


Figure 2:
Side Elevation (23rd Street) showing Existing elevation (left) and Proposed elevation (right)

Project Analysis: The criteria for a Minor Amendment requires an evaluation of a project with only specific Site Review criteria of the B.R.C. 1981 subsections 9-2-14(h)(2) (A), (C), and (F), Open Space, Landscaping, and Building Design respectively.

In terms of open space and landscaping, the first floor addition of 81 square feet effectively reduces the open space by the equivalent amount. Therefore the total open space proposed on the site is 2,416 square feet, not including the front and proposed side porches, well in excess of the 600 square feet per dwelling unit required for RMX-1. The open space change is essentially in-filling an area on the side of the house currently occupied by the porch, and then building a new, slightly smaller porch to access the area. The existing landscaping, that includes mature trees, shrubs and gardens will remain. The usable area of the open space, essentially on the side and in the front will not change substantially. Infilling the existing porch area, already occupied as building coverage, will not materially affect the character or quality of the open space and landscaping.

Regarding building design, Site Review criteria (F) looks at the compatibility of the proposed “*height, mass and scale in the existing character of the area, or the character established by adopted design guidelines for the area.*” In that regard, the proposed improvements were evaluated against the historic preservation guidelines by the Landmarks Design Review Committee (LRDC) who issued a Landmarks Alterations Certificate on Dec. 19, 2013. The LRDC considers the requested setback modification appropriate and that development in conforming locations would have an adverse effect on the landmark (the application for Landmarking the house is currently pending).

With regard to criterion (F)(iii) which states, “*the orientation of buildings minimizes shadows on and blocking of views from adjacent properties,*” the site is located within Solar Access Area II, that requires a principal building meet or be less than the height of a hypothetical 25 foot solar fence. Given that the maximum building height proposed is 21 feet, Land Use Code subsection(9-9-17(d)(2) exempts properties that are built to within the height of the 25-foot hypothetical solar fence for Solar Access area II,

“(2) Height: Unless prohibited by another section of this title, nothing in this section prevents a structure in SA Area III from being erected up to a height of thirty-five feet if located within the allowed building envelope. However, unless an exception is granted pursuant to subsection (f) of this section, no such structure may exceed thirty-five feet in height if any such excess height would cause the structure to violate, or to increase the degree of violation of, the basic solar access protection provided for any lot in SA Area I or SA Area II.

(A) Nothing in this section shall be deemed to prevent the principal building on a lot in SA Area I or II from being erected within the building envelope up to the height of the solar fence in the area in which the structure is located.”

Public Comment: Required public notice was provided in the form of written notifications of the application for Site Review to property owners within 600 feet of the subject property. In addition, a public notice sign was posted on the property. Therefore, all public notice requirements of section 9-4-3, “Public Notice Requirements,” B.R.C. 1981 were met. No comment letters or phone calls were received regarding the proposed project.

Conclusion: Staff finds that the application for a Minor Amendment meets the criteria of section 9-2-14(l), B.R.C. 1981. The proposal was *approved* by staff on **March 21, 2014** and the decision may be called up before Planning Board on or before **April 4 2014**. There is one Planning Board hearings scheduled during the required 14 day call-up period on **April 3, 2014**. Questions about the project or decision should be directed to the Case Manager, Elaine McLaughlin at (303) 441-4130 or at the following email: mclaughline@bouldercolorado.gov.



CITY OF BOULDER
Community Planning & Sustainability

1739 Broadway, Third Floor • P.O. Box 791, Boulder, CO 80306-0791
phone 303-441-1880 • fax 303-441-3241 • web www.bouldercolorado.gov

CITY OF BOULDER PLANNING DEPARTMENT
NOTICE OF DISPOSITION

You are hereby advised that the following action was taken by the Planning Department based on the standards and criteria of the Land Use Regulations as set forth in Chapter 9-2, B.R.C. 1981, as applied to the proposed development.

DECISION:	Approved with conditions
PROJECT NAME:	2245 PINE ADDITION
DESCRIPTION:	Minor amendment to PUD# P-80-13 to expand the existing 1,409 square foot single family house by 81 sf on main floor and 532 sf on upper floor, for a total floor area of 2,022 square feet.
LOCATION:	2245 PINE ST
COOR:	N04W05
LEGAL DESCRIPTION:	Refer to Exhibit A attached
APPLICANT:	Kegan John Paisley and Suzanna Thatcher Paisley
OWNER:	Bruce Douglass
APPLICATION:	Minor Site Review Amendment, LUR2014-00010
ZONING:	RMX-1
CASE MANAGER:	Elaine McLaughlin
VESTED PROPERTY RIGHT:	NO; the owner has waived the opportunity to create such right under Section 9-2-19, B.R.C. 1981.

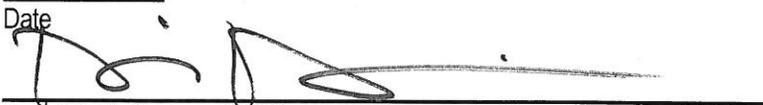
FOR CONDITIONS OF APPROVAL, SEE THE FOLLOWING PAGES OF THIS DISPOSITION.

APPROVED MODIFICATIONS FROM THE LAND USE REGULATIONS:

Sec 9-7, "Form and Bulk Standards": minimum side yard setback adjacent to a street (9'-10" when 12'-5" is permitted by right).

Approved on: March 21, 2014
Date

By:



David Driskell, Executive Director of Community Planning and Sustainability

This decision may be appealed to the Planning Board by filing an appeal letter with the Planning Department within two weeks of the decision date. If no such appeal is filed, the decision shall be deemed final fourteen days after the date above mentioned.

Appeal to Planning Board expires: April 4, 2014

Address: 2245 PINE ST

IN ORDER FOR A BUILDING PERMIT APPLICATION TO BE PROCESSED FOR THIS PROJECT, A SIGNED DEVELOPMENT AGREEMENT AND FINAL PLANS FOR CITY SIGNATURE MUST BE SUBMITTED TO THE PLANNING DEPARTMENT WITH DISPOSITION CONDITIONS AS APPROVED SHOWN ON THE FINAL PLANS, IF THE DEVELOPMENT AGREEMENT IS NOT SIGNED WITHIN NINETY (90) DAYS OF THE FINAL DECISION DATE, THE PLANNING DEPARTMENT APPROVAL AUTOMATICALLY EXPIRES.

Pursuant to Section 9-2-12 of the Land Use Regulations (Boulder Revised Code, 1981), the applicant must begin and substantially complete the approved development within three years from the date of final approval. Failure to "substantially complete" (as defined in Section 9-2-12) the development within three years shall cause this development approval to expire.

CONDITIONS OF APPROVAL

1. The Applicant shall ensure that the development **shall be in compliance with all approved plans** dated February 19, 2014 on file in the City of Boulder Planning Department, except to the extent that the development may be modified by the conditions of this approval.
2. The Applicant shall **comply with all previous conditions** contained in any previous approvals, except to the extent that any previous conditions may be modified by this approval, including, but not limited to, the following: Planned Unit Development #P-80-13.

Address: 2245 PINE ST

Exhibit A: Legal Description

PARCEL I:

ALL THAT PORTION OF THE EAST 48.00 FEET OF LOT 12, BLOCK 139, EAST BOULDER, DESCRIBED AS FOLLOWS:
BEGINNING AT THE SOUTHEASTERLY CORNER OF SAID LOT 12; THENCE N15°00'25"W, 77.90 FEET ALONG THE EASTERLY LINE OF SAID LOT 12; THENCE S75°00'15"W, 48.00 FEET; THENCE S 15°00'25" E, 77.91 FEET TO THE SOUTHERLY LINE OF SAID LOT 12; THENCE N75°00'00"E, 48.00 FEET ALONG THE SOUTHERLY LINE OF SAID LOT 12 TO THE POINT OF BEGINNING,
COUNTY OF BOULDER, STATE OF COLORADO.

PARCEL II:

AN EASEMENT OVER A STRIP OF LAND 10.00 FEET IN WIDTH FOR UTILITY PURPOSES, DESCRIBED AS FOLLOWS:
COMMENCING AT THE SOUTHEASTERLY CORNER OF SAID LOT 12; THENCE N15°00'25"W, 77.90 FEET ALONG THE EASTERLY LINE OF SAID LOT 12 TO THE TRUE POINT OF BEGINNING; THENCE CONTINUING N15°00'25"E, 62.20 FEET ALONG THE EASTERLY LINE OF SAID LOT 12 TO THE NORTHEASTERLY CORNER THEREOF; THENCE S75°00'15"W, 10.00 FEET ALONG THE NORTHERLY LINE OF SAID LOT 12; THENCE S15°00'25"E, 62.20 FEET TO A POINT THAT BEARS S75°00'15"W FROM THE TRUE POINT OF BEGINNING; THENCE N75°00'15"E, 10.00 FEET TO THE TRUE POINT OF BEGINNING,
COUNTY OF BOULDER, STATE OF COLORADO.

PARCEL III:

AN EASEMENT OVER A PARCEL OF LAND FOR PARKING PURPOSES DESCRIBED AS FOLLOWS COMMENCING AT THE SOUTHEASTERLY CORNER OF SAID LOT 12, BLOCK 139, EAST BOULDER; THENCE N15°00'25"W, 121.10 FEET ALONG THE EASTERLY LINE OF SAID LOT 12 TO THE TRUE POINT OF BEGINNING; THENCE CONTINUING N15°00'25"W, 19.00 FEET TO THE NORTHEASTERLY CORNER OF SAID LOT 12; THENCE S75°00'15"W, 20.00 FEET ALONG THE NORTHERLY LINE OF SAID LOT 12; THENCE S15°00'25"E, 19.00 FEET TO A POINT THAT BEARS S75°00'15"W FROM THE TRUE POINT OF BEGINNING; THENCE N75°00'15"E, 20.00 FEET TO THE TRUE POINT OF BEGINNING,
COUNTY OF BOULDER, STATE OF COLORADO.

Address: 2245 PINE ST

PAISLEY RESIDENCE

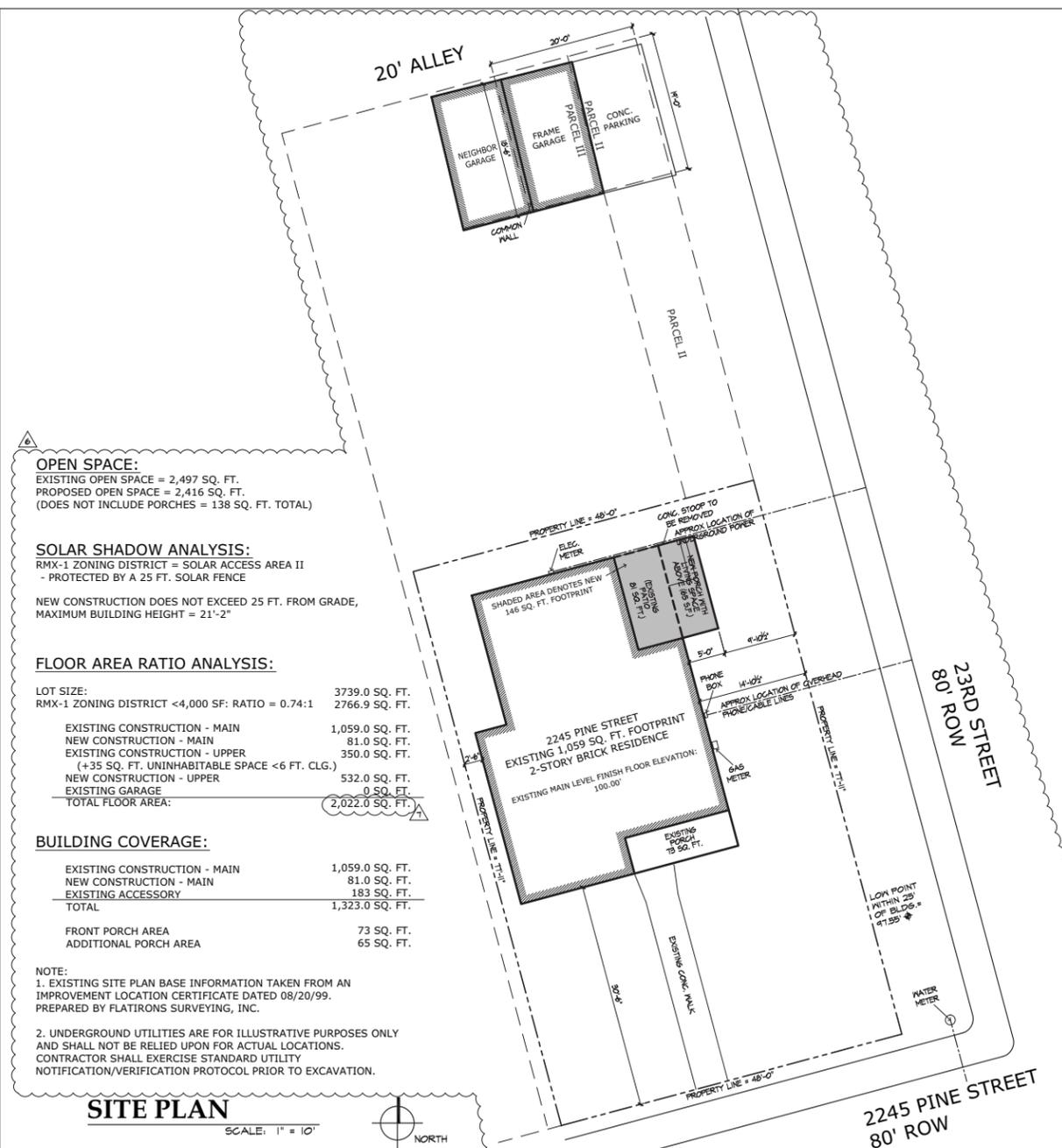
SQUARE FOOTAGE

	AREA (SQUARE FEET)
MAIN FLOOR - EXISTING = 1,040 SQ. FT. + 14 SQ. FT. MECH. SPACE	1,054 S.F.
MAIN FLOOR - NEW	81 S.F.
UPPER FLOOR - EXISTING (+ 35 SQ. FT. UNINHABITABLE SPACE @ 6 FT. CLG.)	350 S.F.
UPPER FLOOR - NEW	592 S.F.
TOTAL ABOVE GRADE FINISHED AREA	2,022 S.F.
TOTAL BELOW GRADE (BASEMENT)	N/A
TOTAL GARAGE AREA	N/A
EXISTING FRONT PORCH	73 S.F.
NEW COV. SIDE PORCH	65 S.F.
TOTAL OUTDOOR AREA	138 S.F.

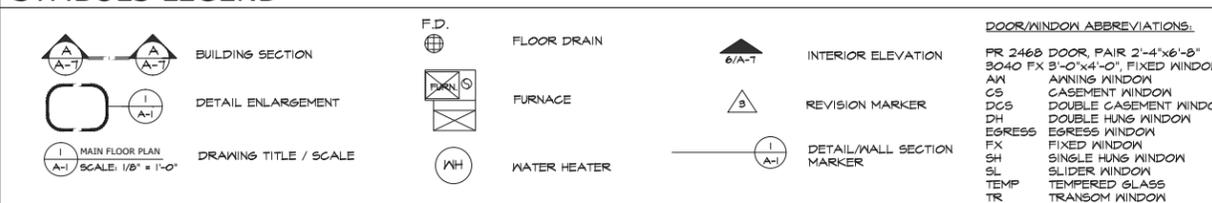
FINISHED AND UNFINISHED SQUARE FOOTAGE CALCULATIONS FOR THIS HOUSE ARE BASED ON PLAN DIMENSIONS ONLY AND MAY VARY FROM THE FINISHED SQUARE FOOTAGE OF THE HOUSE AS BUILT.

LIST OF ABBREVIATIONS

AT	MISC	MISCELLANEOUS
AND	MR	MOISTURE RESISTANT
ABV	ABOVE	
AFF	ABOVE FINISH FLOOR	NIC
ACT	ACOUSTICAL CEILING TILE	NO
ADJ	ADJACENT	
AL	ALUMINUM	OC
APPROX	APPROXIMATE	OPNS
ARCH	ARCHITECTURAL	OPT
BR&	BEARING	OD
BLN	BELOW	OH
BD	BOARD	FT
BD	BOTTOM OF...	FTD
BUR	BUILT UP ROOF	FLAM
		PL
		PLYND
		PT
CANT	CANTILEVER	REF
CPT	CARPET	REFR
CIP	CAST IN PLACE	REFR
CLG	CEILING	RCP
CL	CENTER LINE	REQD
CT	CERAMIC TILE	RAG
CLO	CLOSET	REV
COL	COLUMN	R
CONC	CONCRETE	RD
CONT	CONTINUOUS	RO
		SIM
		SC
		SF
		SGIN
		SS
		STD
		STL
		STOR
		STRUC
		SAS
		SAT
		TEL
		TEMP
		TFH
		T&G
		TO
		TOC
		TC
		TOF
		TOM
		TOS
		TOW
		TB
		TR
		T
		TS
		TYP
		UNO
		ULF
		VTR
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		VEST
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		2 SHELVES



SYMBOLS LEGEND



SHEET INDEX

ARCHITECTURAL

- COVER COVER SHEET - SITE PLAN
- A-1.0 ARCHITECTURAL FOUNDATION PLAN
- A-2.0 MAIN LEVEL FLOOR PLAN
- A-3.0 UPPER LEVEL FLOOR PLAN
- A-4.0 ROOF PLAN
- A-5.0 EXTERIOR ELEVATIONS - EXISTING
- A-5.1 EXTERIOR ELEVATIONS - NEW
- A-6.0 BUILDING SECTIONS

ELECTRICAL

- E-1.0 LOWER, MAIN & UPPER LEVEL ELECTRICAL PLANS

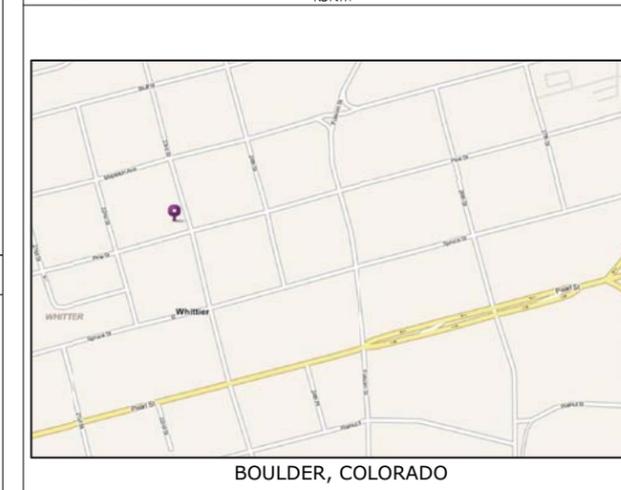
PROJECT TEAM

ARCHITECT:
OLSON ARCHITECTURE, INC.
240 SECOND AVENUE
PO BOX 1024
NINOT, CO 80544-1024
PH: 303.652.2668
FAX: 303.652.2717
EMAIL: aolson@olsonarchitecture.com

STRUCTURAL ENGINEER:
PARKS ENGINEERING, INC.
8065 S. NIAGARA WAY
CENTENNIAL, CO 80112
PH: 720.942.1101
EMAIL: robertpark31@yahoo.com

BUILDER:
PORCHFRONT HOMES
6604 BIRD CLIFF WAY
NINOT, CO 80503
PH: 303.442.8453
EMAIL: tcoance@porchfronthomes.com

VICINITY MAP



OWNERSHIP OF DOCUMENTS AND DESIGNS:
ALL ARCHITECTURAL DOCUMENTS AND DESIGNS DEVELOPED BY OSNOSIS ARCHITECTURE, INC. REMAIN OSNOSIS ARCHITECTURE, INC. PROPERTY AND ARE TO BE USED ON THIS PROJECT ONLY UNDER THE TERMS OF THIS LETTER OF AGREEMENT. THE CLIENT SHALL NOT, WITHOUT WRITTEN APPROVAL OF OSNOSIS ARCHITECTURE, INC., REUSE THE DOCUMENTS OR DESIGNS PROVIDED UNDER THIS LETTER OF AGREEMENT ON OTHER PROJECTS OR ASSIGN, SELL, OR TRANSFER THE SAME TO OTHERS.

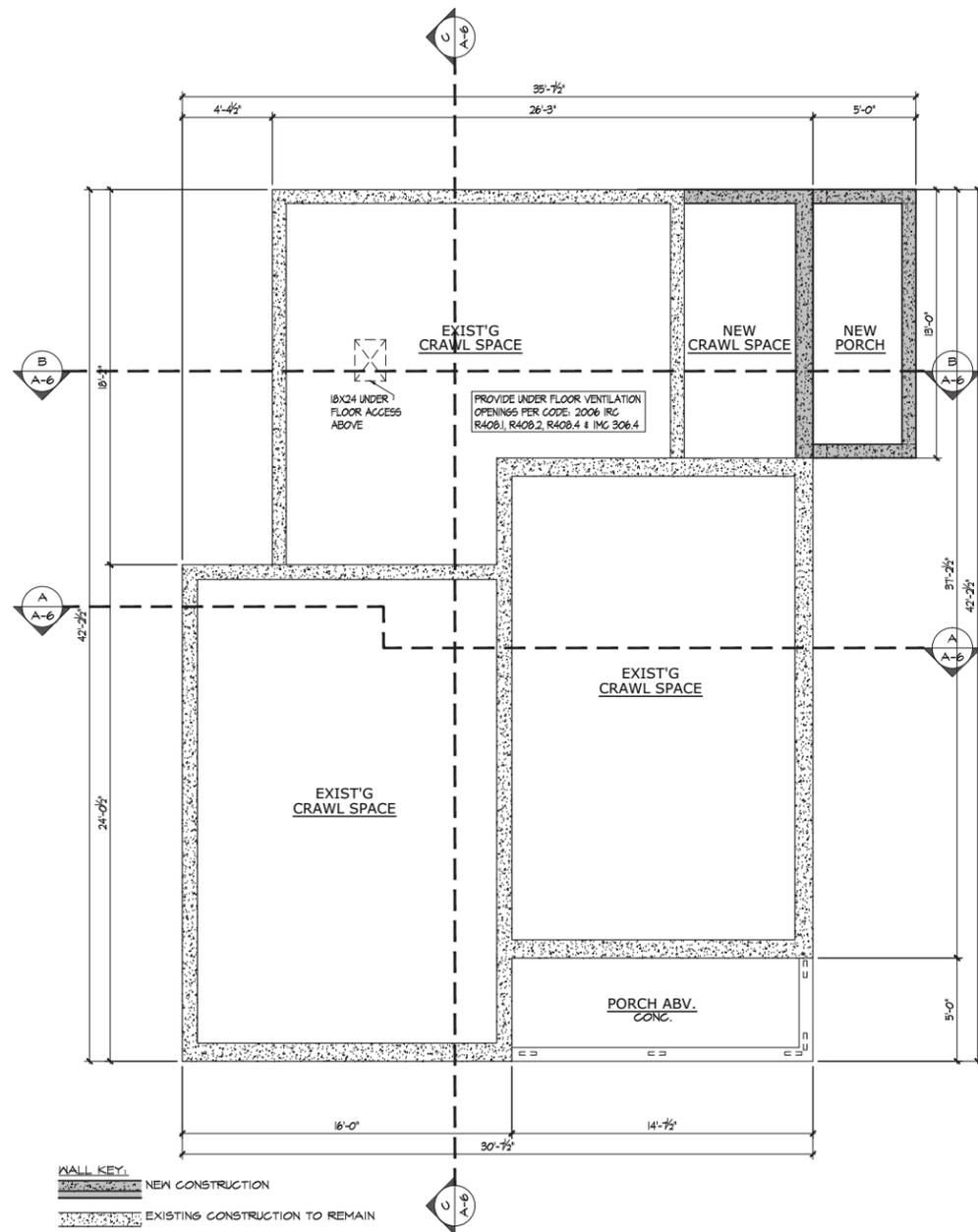
DUTY OF COOPERATION:
RELEASE OF THESE DOCUMENTS ANTICIPATES FURTHER COOPERATION BETWEEN THE OWNER, THE CONTRACTOR AND THE ARCHITECT. ALTHOUGH THE ARCHITECT AND HIS CONSULTANTS HAVE PERFORMED THEIR SERVICES WITH DUE CARE AND DILIGENCE, THEY CANNOT GUARANTEE PERFECTION. ANY VARIATION OR DISCREPANCY DISCLOSED SHALL BE REPORTED IMMEDIATELY TO THE ARCHITECT. FAILURE TO COOPERATE BY A SIMPLE NOTICE TO THE ARCHITECT SHALL RELIEVE THE ARCHITECT FROM RESPONSIBILITY OF ALL CONSEQUENCES. CHANGES MADE FROM THE PLANS WITHOUT THE CONSENT OF THE ARCHITECT ARE UNAUTHORIZED AND SHALL RELIEVE THE ARCHITECT OF RESPONSIBILITY FOR ALL CONSEQUENCES ARISING OUT OF SUCH CHANGES.

PAISLEY RESIDENCE
2245 PINE STREET
BOULDER, COLORADO

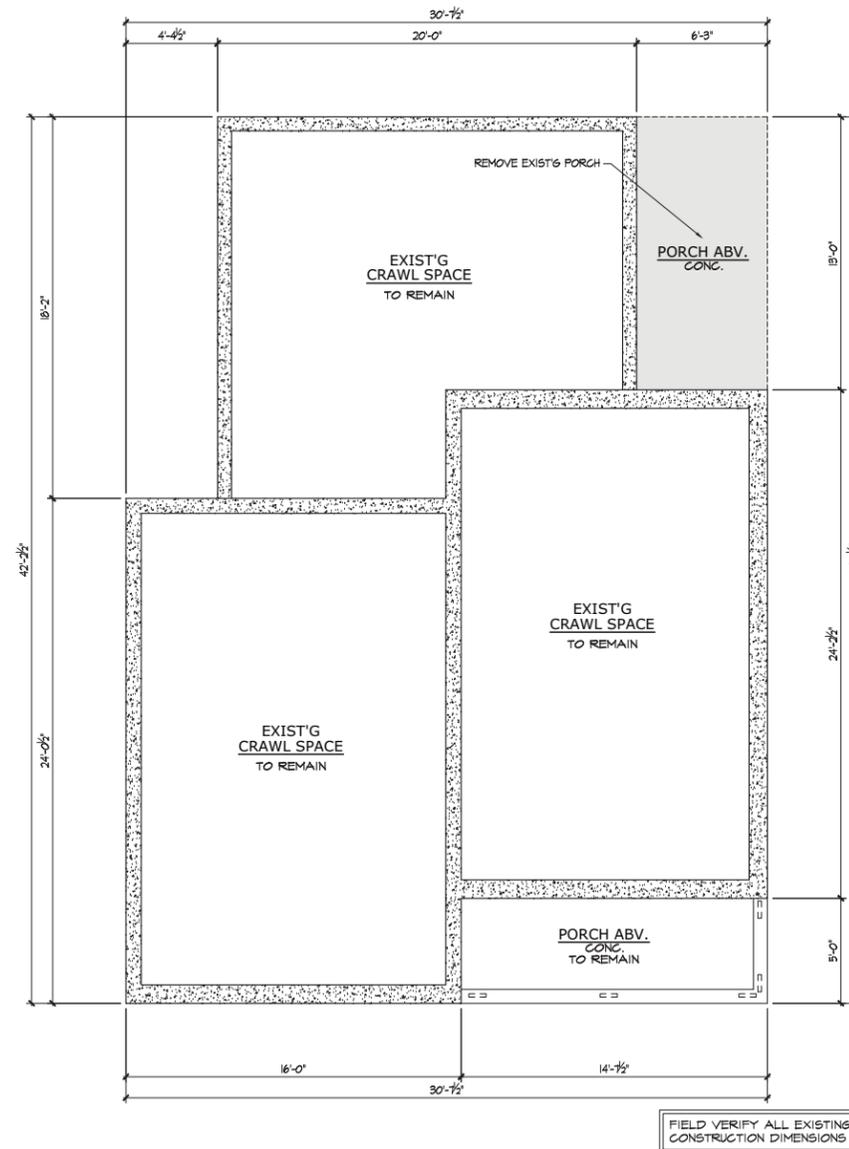
osmosis art and architecture
290 SECOND AVE., P.O. BOX 1024 | NINOT, CO 80544-1024
PHONE (303) 652-2668 | FAX (303) 652-2717
www.osmosisarchitecture.com

ORIGINAL ISSUE:	DATE:	
PERMIT SET	06/19/13	
CURRENT/REVISION ISSUE:	DATE:	
MINOR AMENDMENT REV.	03/17/14	
NO.	REVISION TITLE:	DATE:
JOB #:	12012	
DRAWN:	COVER	
CHECKED:	AEP	

COVER



2 PAISLEY RES. - NEW FOUNDATION
SCALE: 1/4" = 1'-0"



1 PAISLEY RES. - EXISTING FOUNDATION/DEMOLITION
SCALE: 1/4" = 1'-0"

GENERAL DEMOLITION NOTES:

1. THESE DEMOLITION PLANS ARE SOLELY A GRAPHIC REPRESENTATION OF THE EXISTING CONDITIONS AND THE EXTENT OF DEMOLITION TO BE PERFORMED. ALL CONDITIONS SHOWN ON THESE PLANS ARE TO BE FIELD VERIFIED AND COORDINATED WITH PLANS AND DETAILS INCLUDED UNDER, BUT NOT LIMITED TO "NEW CONSTRUCTION" DOCUMENTS.
2. CONTRACTOR IS TO REFER TO STRUCTURAL PLANS PRIOR TO DEMOLITION FOR PROPER PROCEDURE AND INFORMATION IN ORDER TO DEMOLISH IN AN ACCEPTABLE AND SAFE MANNER.
3. FOR AREAS SHOWN GRAPHICALLY ON THESE PLANS AS "TO REMAIN", CONTRACTOR IS TO PROVIDE ADEQUATE PROTECTION MEASURES IN ORDER TO MINIMIZE DAMAGE AS MUCH AS POSSIBLE.
4. ITEMS OF SALVAGEABLE VALUE ARE TO BE STORED IN A SAFE, DRY AREA TO BE DETERMINED BY THE OWNER AND CONTRACTOR.
5. PROVIDE AND MAINTAIN (AS REASONABLY POSSIBLE) WEATHER PROTECTION AT EXTERIOR OPENINGS TO FULLY PROTECT THE INTERIOR PREMISES AGAINST DAMAGE FROM THE ELEMENTS UNTIL SUCH OPENINGS ARE ENCLOSED BY NEW CONSTRUCTION.
6. MAINTAIN JOB SITE FREE OF TRASH, DEBRIS, AND ACCUMULATION OF WASTE MATERIALS ON A DAILY BASIS.

OWNERSHIP OF DOCUMENTS AND DESIGN:
ALL ARCHITECTURAL DOCUMENTS AND DESIGN DEVELOPED BY OSMOSIS ARCHITECTURE, INC. REMAIN OSMOSIS ARCHITECTURE, INC. PROPERTY AND ARE TO BE USED ON THIS PROJECT ONLY UNDER THE TERMS OF THIS LETTER OF AGREEMENT. THE CLIENT SHALL NOT, WITHOUT WRITTEN APPROVAL OF OSMOSIS ARCHITECTURE, INC., REUSE THE DOCUMENTS OR DESIGN PROVIDED UNDER THIS LETTER OF AGREEMENT ON OTHER PROJECTS OR ASSIGN, SELL, OR TRANSFER THE SAME TO OTHERS.

DUTY OF CARE/PROFESSIONAL LIABILITY:
RELIEF OF THESE DOCUMENTS ANTICIPATES FURTHER COOPERATION BETWEEN THE OWNER, THE CONTRACTOR AND THE ARCHITECT. ALTHOUGH THE ARCHITECT AND HIS CONSULTANTS HAVE PERFORMED THEIR SERVICES WITH DUE CARE AND DILIGENCE, THEY CANNOT GUARANTEE PERFECTION. ANY NEGLIGENCE OR MISFEASANCE DISCLOSED SHALL BE REPORTED IMMEDIATELY TO THE ARCHITECT. FAILURE TO COOPERATE BY A SIMPLE NOTICE TO THE ARCHITECT SHALL RELIEVE THE ARCHITECT FROM RESPONSIBILITY OF ALL CONSEQUENCES. CHANGES MADE FROM THE PLANS WITHOUT THE CONSENT OF THE ARCHITECT ARE UNAUTHORIZED AND SHALL RELIEVE THE ARCHITECT OF RESPONSIBILITY FOR ALL CONSEQUENCES ARISING OUT OF SUCH CHANGES.

PAISLEY RESIDENCE
2245 PINE STREET
BOULDER, COLORADO

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osmosis
art and architecture

290 SECOND AVE., 1 FLOOR, BOX 1024 | NWOT, CO. 80554-1124
PHONE (303) 632-2668 | FAX (303) 632-2717
www.osmosisarchitecture.com

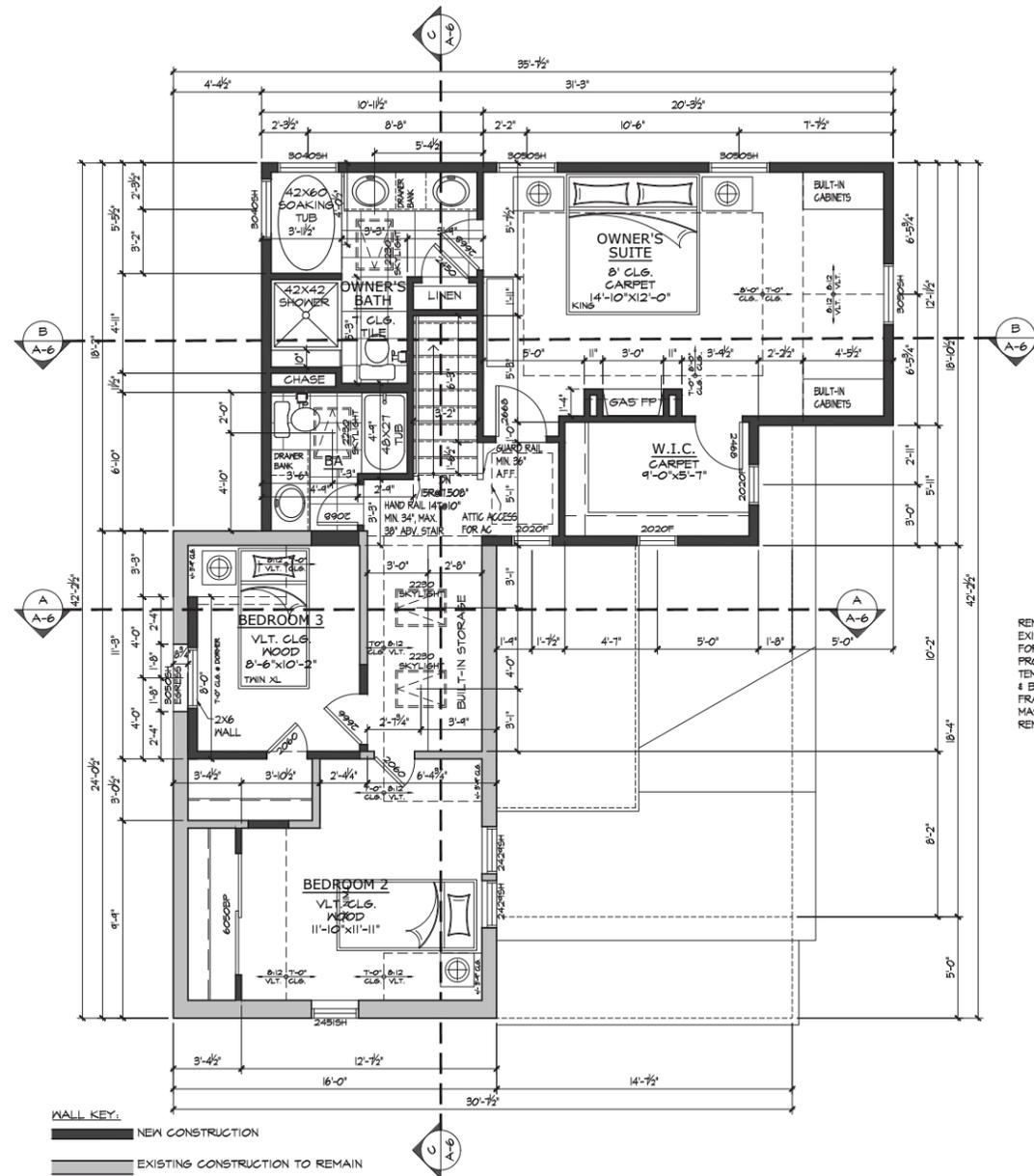
ORIGINAL ISSUE:	DATE:
PERMIT SET	06/19/13

CURRENT/REVISION ISSUE:	DATE:
MINOR AMENDMENT REV.	03/17/14

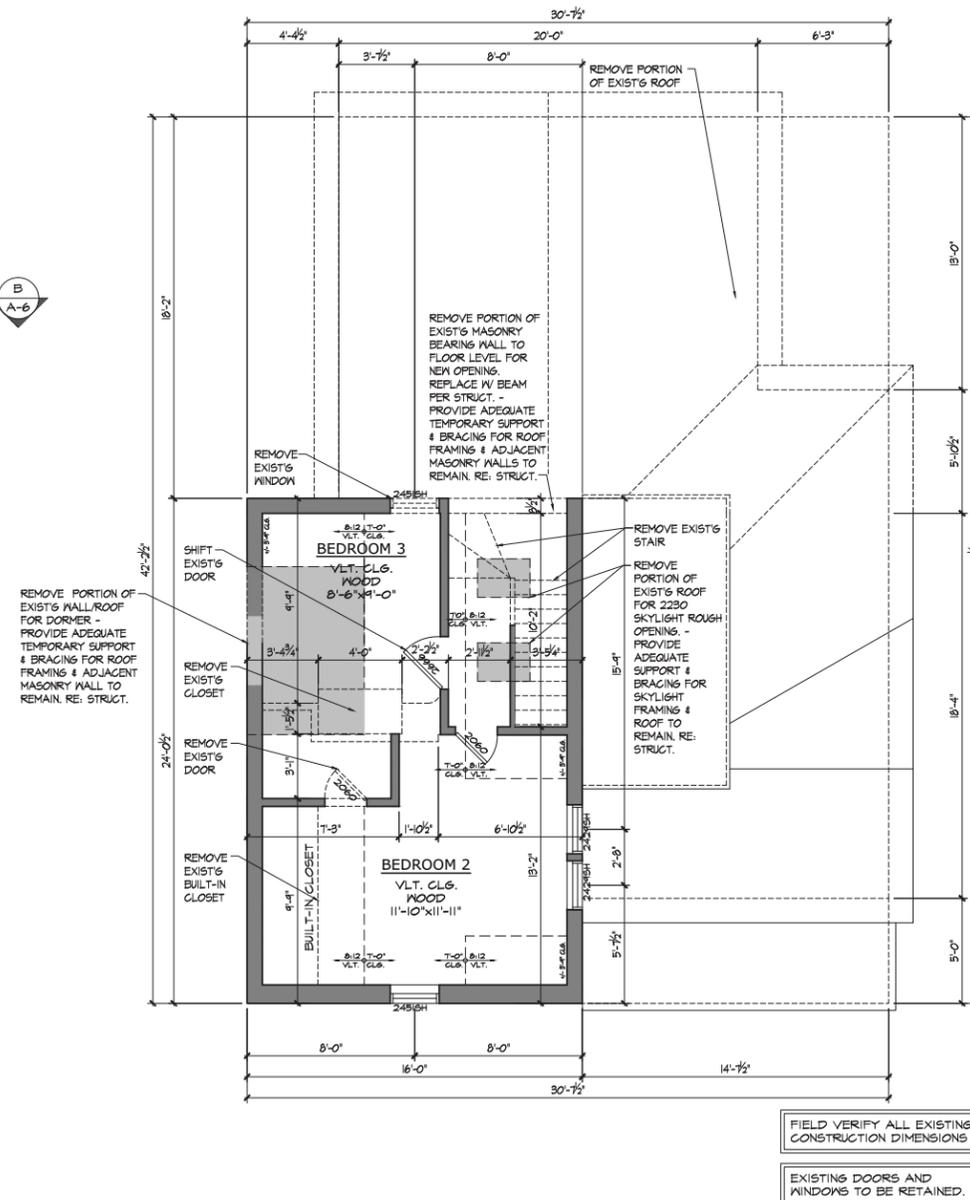
NO.	REVISION TITLE:	DATE:

JOB #:	12012
DRAWN:	AEP/JJ
CHECKED:	AEP

A-1.0



2 PAISLEY RES. - NEW UPPER LEVEL
 A-3.0 EXISTING = 350 SQ. FT. + 35 SQ. FT. UNINHABITABLE SPACE (6 FT. CLG. HT.) = 385 SQ. FT.
 RENOVATION = 120 SQ. FT., ADDITION = 532 SQ. FT. SCALE: 1/4" = 1'-0"
 NEW UPPER LEVEL = 417 SQ. FT.



1 PAISLEY RES. - EXISTING UPPER LEVEL/DEMOLITION
 A-3.0 EXISTING = 333 SQ. FT. + 52 SQ. FT. UNINHABITABLE SPACE (6 FT. CLG. HT.) = 385 SQ. FT. SCALE: 1/4" = 1'-0"

GENERAL DEMOLITION NOTES:

1. THESE DEMOLITION PLANS ARE SOLELY A GRAPHIC REPRESENTATION OF THE EXISTING CONDITIONS AND THE EXTENT OF DEMOLITION TO BE PERFORMED. ALL CONDITIONS SHOWN ON THESE PLANS ARE TO BE FIELD VERIFIED AND COORDINATED WITH PLANS AND DETAILS INCLUDED UNDER, BUT NOT LIMITED TO "NEW CONSTRUCTION" DOCUMENTS.
2. CONTRACTOR IS TO REFER TO STRUCTURAL PLANS PRIOR TO DEMOLITION FOR PROPER PROCEDURE AND INFORMATION IN ORDER TO DEMOLISH IN AN ACCEPTABLE AND SAFE MANNER.
3. FOR AREAS SHOWN GRAPHICALLY ON THESE PLANS AS "TO REMAIN", CONTRACTOR IS TO PROVIDE ADEQUATE PROTECTION MEASURES IN ORDER TO MINIMIZE DAMAGE AS MUCH AS POSSIBLE.
4. ITEMS OF SALVAGEABLE VALUE ARE TO BE STORED IN A SAFE, DRY AREA TO BE DETERMINED BY THE OWNER AND CONTRACTOR.
5. PROVIDE AND MAINTAIN (AS REASONABLY POSSIBLE) WEATHER PROTECTION AT EXTERIOR OPENINGS TO FULLY PROTECT THE INTERIOR PREMISES AGAINST DAMAGE FROM THE ELEMENTS UNTIL SUCH OPENINGS ARE ENCLOSED BY NEW CONSTRUCTION.
6. MAINTAIN JOB SITE FREE OF TRASH, DEBRIS, AND ACCUMULATION OF WASTE MATERIALS ON A DAILY BASIS.

OWNERSHIP OF DOCUMENTS AND DESIGN:
 ALL ARCHITECTURAL DOCUMENTS AND DESIGN DEVELOPED BY OSMOSIS ARCHITECTURE, INC. REMAIN OSMOSIS ARCHITECTURE, INC. PROPERTY AND ARE TO BE USED ON THIS PROJECT ONLY UNDER THE TERMS OF THIS LETTER OF AGREEMENT. THE CLIENT SHALL NOT, WITHOUT WRITTEN APPROVAL OF OSMOSIS ARCHITECTURE, INC., REUSE THE DOCUMENTS OR DESIGN PROVIDED UNDER THIS LETTER OF AGREEMENT ON OTHER PROJECTS OR ASSIGN, SELL, OR TRANSFER THE SAME TO OTHERS.
DUTY OF CARE/CONSENT:
 RELIEF OF THESE DOCUMENTS ANTICIPATES FURTHER COOPERATION BETWEEN THE OWNER, THE CONTRACTOR AND THE ARCHITECT. ALTHOUGH THE ARCHITECT AND HIS CONSULTANTS HAVE PERFORMED THEIR SERVICES WITH DUE CARE AND DILIGENCE, THEY CANNOT GUARANTEE PERFORMANCE. ANY AVOIDANCE OR DEFERMENT OF SUCH PERFORMANCE SHALL BE REPORTED IMMEDIATELY TO THE ARCHITECT. FAILURE TO COOPERATE BY A SIMPLE NOTICE TO THE ARCHITECT SHALL RELIEVE THE ARCHITECT FROM RESPONSIBILITY OF ALL CONSEQUENCES. CHANGES MADE FROM THE PLANS WITHOUT THE CONSENT OF THE ARCHITECT ARE UNAUTHORIZED AND SHALL RELIEVE THE ARCHITECT OF RESPONSIBILITY FOR ALL CONSEQUENCES ARISING OUT OF SUCH CHANGES.

PAISLEY RESIDENCE
 2245 PINE STREET
 BOULDER, COLORADO

osmosis art and architecture
 290 SECOND AVE., 1100, BOX 1024 | NWOT, CO. 80554-1124
 PHONE (303) 652-2668 | FAX (303) 652-2717
 www.osmosisarchitecture.com

ORIGINAL ISSUE:	DATE:	
PERMIT SET	06/19/13	
CURRENT/REVISION ISSUE:	DATE:	
MINOR AMENDMENT REV.	03/17/14	
NO.	REVISION TITLE:	DATE:
JOB #:	12012	
DRAWN:		
CHECKED:	AEP	

A-3.0

**CITY OF BOULDER
PLANNING BOARD AGENDA ITEM
MEETING DATE: April 3, 2013**

AGENDA TITLE: Public hearing and consideration of a **Use Review** application, no. LUR2012-00101, for expansion of the Family Learning Center located at 3164 34th St. in the Residential Medium-One (RM-1) zone district. The proposal includes a new, 2,427 square foot classroom addition as well as site, landscaping and parking lot improvements.

Applicant/Owner: The Family Learning Center

REQUESTING DEPARTMENT:

Community Planning & Sustainability

David Driskell, Executive Director

Susan Richstone, Deputy Director

Charles Ferro, Land Use Review Manager

Chandler Van Schaack, Planner I

OBJECTIVE:

1. Hear Staff and Applicant presentations
2. Hold Public Hearing
3. Planning Board discussion
4. Planning Board action to approve, approve with conditions, or deny

Proposal: Expansion of the Family Learning Center at 3164 34th St. in the RM-1 zone district. The proposal includes a new, 2,427 square foot classroom addition as well as site, landscaping and parking lot improvements.

Project Name: Family Learning Center

Location: 3164 34th St.

Size of Tract: 80,610 s.f. (1.85 acres)

Zoning: Residential Medium - 1 (RM-1)

Comprehensive Plan: Medium Density Residential

PROJECT DESCRIPTION:

This proposal is to construct a 2,427 square foot classroom addition to the existing Family Learning Center daycare center and adult educational facility at 3164 34th St. in the RM-1 (Residential- Medium 1) zone district. The purpose of the proposed expansion is to better support existing services and operations of the facility and to expand the services available to existing clients. In addition to increasing the space available for existing daycare, educational and counseling services, the proposed addition will provide room for multicultural and bilingual exercise and dance classes, cultural and art programs, clothing donations, food share programs and nutritional cooking classes. No increase in employees or clients is proposed as part of this project. In addition to the new classroom addition, which will be constructed off of the south side of the existing 5,137 square foot building over a portion of what is currently a playground area, the proposal also includes removal of an existing shed, minor site improvements and reconfiguration of the existing parking lot to provide 19 parking spaces where currently there are 14 parking spaces (refer to [Attachment A](#) for Applicant's Proposed Plans). Please refer to **Figure 1** below for a vicinity map.

BACKGROUND:

Existing Use / Area Context.

The 1.85-acre subject site is located at the terminus of 34th Street, just northwest of the intersection of Valmont Rd. and Foothills Parkway in the RM-1 (Residential-Medium 1) zone district, which is defined in section 9-5-2(c)(1)(C), B.R.C. 1981, as “Medium density residential areas which have been or are to be primarily used for attached residential development, where each unit generally has direct access to ground level, and where complementary uses may be permitted under certain conditions.”

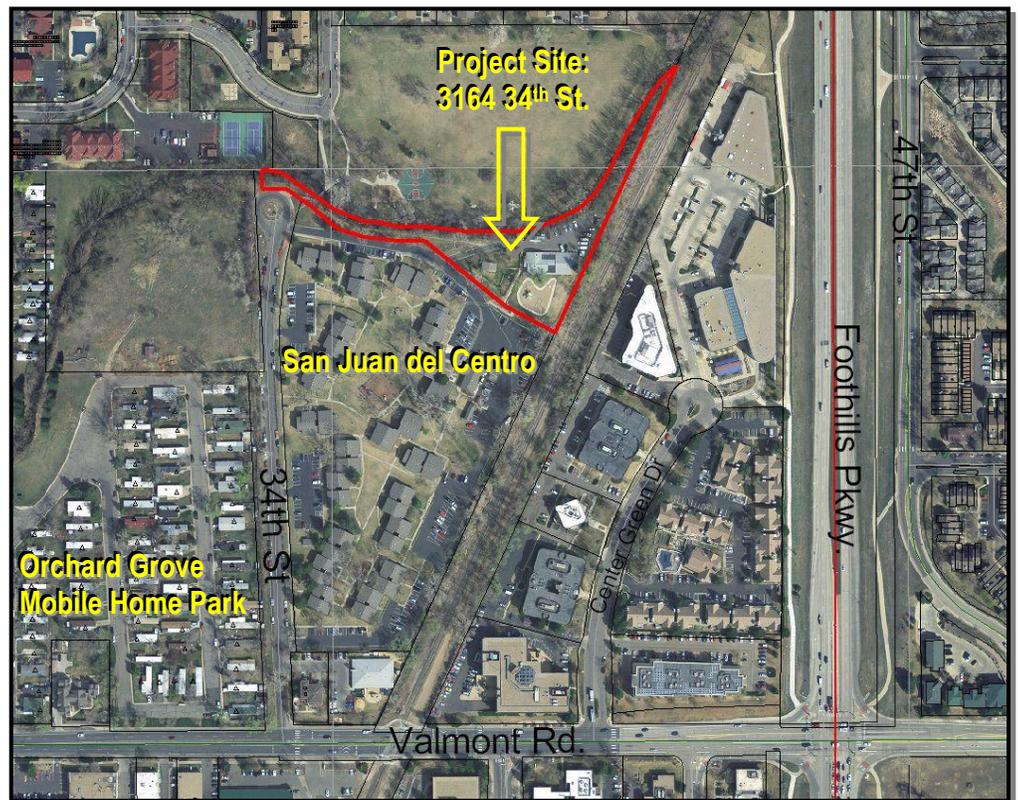


Figure 1: Vicinity Map

The Family Learning Center is a private, non-profit organization that provides comprehensive services to low-income, minority and non-English speaking children and families within Boulder County. The existing facility has been in its current location north of the San Juan del Centro affordable housing apartment complex since 1981. In 1993, planning board approved a Use Review to allow for the Family Learning Center to construct a 1,864 square foot building addition to increase service offerings and to add new services for existing clients. Since 1993, the Family Learning Center has continued to provide a variety of services, including daycare and early childhood development programs, after-school and summer educational programs for elementary, middle and high school students, adult and family educational and support services and assistance with various community outreach programs.

Currently, the primary uses occurring at the Family Learning Center are daycare services, after-school programs for elementary and middle-school children, and evening tutoring services. Each weekday, on average, the Family Learning Center provides daycare services for 60 Pre-K students (two separate groups of 30 students) and after school care for 45 elementary and middle school students, with tutoring on Wednesday nights for up to 30 students of all ages. The hours of operation for each program are staggered so that there are never more than two groups occupying the facility at the same time and pick-up and drop-off times are spread throughout the day. Based on a Parking Study provided by the applicant (see [Attachment B](#)), roughly half of all students walk, bike or take one of three facility shuttle vans to and from the site, with the other half of pick-ups and drop-offs occurring by car. Per the previous approval as well as information included with this application, at least 50 percent of the facility users live in the adjacent San Juan del Centro development, with roughly 80 percent of the total users living within walking distance of the facility. There are 18 total employees with varying shifts and work schedules so that on average, there are no more than three employee vehicles on-site at a time.

The area surrounding the subject site is predominantly residential in character. As mentioned above, the site is immediately adjacent to San Juan del Centro, a 150-unit affordable housing apartment complex constructed in 1971 that serves primarily low- to very-low income households. To the west of the subject site across 34th Street is the Orchard Grove Mobile Home Park, and to the north of the site across the White Rock ditch and Howard Hueston Park are the high-density residential developments of Northgate and Meadow Wood.

PROCESS

Based on staff's review of the information provided by the applicant, the Family Learning Center is comprised of two principal uses, including a "daycare center with more than 50 children" and an "adult education facility with less than 20,000 square feet of floor area." Both uses are permitted in the RM-1 zone district pursuant to the Use Review process pursuant to section 9-6-1, "Use Standards," B.R.C. 1981. In addition, all daycare uses must meet the Conditional Use Review standards found in section 9-6-6(a), B.R.C. 1981.

As discussed above, there is an existing Use Review approval for the existing use. Pursuant to section 9-2-15(h), B.R.C. 1981, a request to expand or modify any approved Use Review use requires a new Use Review application. Pursuant to section 9-2-15, "Use Review," B.R.C. 1981, the city manager shall review and submit a recommendation to the Planning Board for any application for a Use Review of a nonresidential use in a residential zone.

ANALYSIS OF USE REVIEW CRITERIA

Applications for Use Review are reviewed for consistency with the criteria set forth in subsection 9-2-15(e), "Criteria for Review," B.R.C. 1981.

1. Is the proposed use consistent with the Use Review criteria set forth in subsection 9-2-15(e), "Criteria for Review," B.R.C. 1981?

Criteria for Review: No use review application will be approved unless the approving agency finds all of the following:

✓ **(1) Consistency with Zoning and Non-Conformity:** The use is consistent with the purpose of the zoning district as set forth in section 9-5-2(c), "Zoning Districts Purposes," B.R.C. 1981, except in the case of a non-conforming use;

The project site is located within the Residential Medium-1 (RM-1) zone district, which is defined in section 9-5-2(c)(1)(C), B.R.C. 1981, as "Medium density residential areas which have been or are to be primarily used for attached residential development, where each unit generally has direct access to ground level, and where complementary uses may be permitted under certain conditions."

Based on information provided by the applicant, the Family Learning Center is comprised of two principal uses, including a daycare center with more than 50 children and an adult education facility with less than 20,000 square feet of floor area. Both uses are permitted pursuant to the Use Review process pursuant to section 9-6-1, "Use Standards," B.R.C. 1981.

Previous approval was granted for a daycare and adult education facility (SR-93-7).

✓ **(2) Rationale:** The use either:

✓ **(A) Provides direct service or convenience to or reduces adverse impacts to the surrounding uses or neighborhood;**

The Family Learning Center is a private, non-profit organization which provides comprehensive services to low-income, minority and non-English speaking children and families within Boulder County. The existing facility has been in its current location north of the San Juan del Centro affordable housing apartment complex since 1981, and directly serves many low-income residents of that development on a daily basis. Since 1993, when the facility underwent an expansion, the Family Learning Center has continued to provide a variety of services, including daycare and early childhood development programs,

after-school and summer educational programs for elementary, middle and high school students, adult and family educational and support services and assistance with various community outreach programs. The purpose of the proposed expansion is to better support existing services and operations of the facility and to expand the services available to existing clients. In addition to increasing the space available for existing daycare, educational and counseling services, the proposed addition will provide room for multicultural and bilingual medical screenings and health services, exercise and dance classes, cultural and art programs, clothing donations, food share programs and nutritional cooking classes. As indicated in the previous approval, approximately 50 percent of the facility's clients are residents of the nearby San Juan del Centro development, and approximately 80 percent of clients live within walking distance of the facility.

N/A (B) Provides a compatible transition between higher intensity and lower intensity uses;

Not applicable.

N/A (C) Is necessary to foster a specific city policy, as expressed in the Boulder Valley Comprehensive Plan, including, without limitation, historic preservation, moderate income housing, residential and non-residential mixed uses in appropriate locations, and group living arrangements for special populations; or

Not applicable.

N/A (D) Is an existing legal non-conforming use or a change thereto that is permitted under subsection (e) of this section;

Not applicable.

✓ (3) Compatibility: The location, size, design, and operating characteristics of the proposed development or change to an existing development are such that the use will be reasonably compatible with and have minimal negative impact on the use of nearby properties or for residential uses in industrial zoning districts, the proposed development reasonably mitigates the potential negative impacts from nearby properties;

The existing facility was donated to the Family Learning Center by the City of Boulder in 1981. In 1993, planning board approved a Use Review to allow for the Family Learning Center to expand the facility to increase service offerings and to add new services for existing clients. Since 1993, the Family Learning Center has continued to provide a variety of services to low-income, minority and non-English speaking children and families within Boulder County, including daycare and early childhood development programs, after-school and summer educational programs for elementary, middle and high school students, adult and family educational and support services and assistance with various community outreach programs. With the Family Learning Center having been adjacent to the 150-unit San Juan del Centro since 1981 when the facility was founded, approximately 50 percent of Family Learning Center users have been and continue to be San Juan del Centro residents. In that regard, the Family Learning Center is not only compatible with the surrounding residential uses, but a key component of the services available to nearby low- and very low-income residents.

The applicant is proposing to expand the existing Family Learning Center (FLC) facility by 2,427 square feet in order to better support existing services and operations and to allow for additional services for the existing clients. The proposal includes replacing a portion of the existing playground with a new addition on the south side of the building as well as site and parking lot improvements. There are no proposed

changes to the existing number of employees or customers at this time. The existing principal uses include daycare services, after school care for elementary and middle school students and evening tutoring classes for students.

Currently, the FLC provides daycare services for 30 Pre-K students, after school care for 45 elementary and middle school students and tutoring for 30 students of all ages. There are 18 total employees with varying shifts and work schedules, with three employee vehicles and three facility shuttle vans on-site at any given time. The chart below outlines the existing hours of operation and operating characteristics for the different educational services:

Service	Days	# of Students	Arrival Time	Departure Time	Drop-Off Mode	Pick-Up Mode
Daycare	Weekdays	30	7:50-8:15 a.m. (group 1); 12:30-12:40 p.m. (Group 2)	11:30-11:40 a.m. (group 1); 3:50-4:00 p.m. (Group 2)	50% car 50% van or walk	50% car 50% van or walk
After School Care	Weekdays	45	3:00 – 4:00 p.m.	5:30-6:00 p.m.	Mostly van, bus or walk	45% car (20 total) 55% walk or bus
Tutoring	Wednesdays	30	6:00 p.m.	8:00 p.m.	50% car 50% walk or bus	50% car 50% walk or bus

With regards to the existing and future parking impacts of the proposed expansion, as discussed above there are no changes to the number of employees or clients proposed as part of the expansion; therefore, it is unlikely that the proposed expansion will create any additional parking impacts. The existing use has demonstrated over the last 30 years that it has no negative impacts on the surrounding residential uses; however, in order to better accommodate their employees and serve their existing customers, the FLC is proposing to reconfigure the existing parking lot to provide 19 parking spaces where currently there are only 14 parking spaces. In addition, in terms of existing off-site parking, the San Juan del Centro development currently provides 297 parking spaces where 285 are required by the land use regulations, and there is also on-street parking available along 34th Street just west of the site. An informal parking arrangement has existed between the Family Learning Center and the neighboring San Juan Del Centro apartment complex since the use began in 1981. Under this arrangement, the FLC is allowed to use parking spaces in the San Juan Del Centro parking area for overflow parking as needed, and San Juan Del Centro residents are allowed to use FLC parking spaces for overflow parking on weekends and during the evenings as needed.

Including the 19 proposed on-site parking spaces, the available on-street parking on 34th Street and the parking arrangement with San Juan del Centro, the parking study (see [Attachment B](#)) provided by the applicant indicates that there are 53 total available parking spaces in the immediate vicinity of the FLC. With the different user groups arriving and departing from the FLC site at successive times throughout the day and approximately 50% of the students in each activity group using shuttle vans or walking from the bus stop or the adjacent San Juan Del Centro development, the proposed parking configuration will continue to adequately serve the parking demand generated by the use and will not impact the surrounding residential properties.

- ✓ (4) **Infrastructure:** As compared to development permitted under section 9-6-1, "*Schedule of Permitted Uses of Land*," B.R.C. 1981, in the zone, or as compared to the existing level of impact of a non-conforming use, the proposed development will not significantly adversely affect the infrastructure of the surrounding area, including, without limitation, water, wastewater, and storm drainage utilities and streets;

The infrastructure required to serve the facility is existing.

- ✓ (5) **Character of Area:** The use will not change the predominant character of the surrounding area; and

The character of the area is predominantly attached residential with the permanently affordable housing apartment development, San Juan del Centro located immediately adjacent to the project site. The Orchard Grove Mobile Home Park is also located in proximity to the site, although it is not adjacent and is separated by a fence running the length of the property. The site is bordered on the north by the White Rock Ditch, with the Howard Hueston Park lying further to the north. Surrounding development is comprised of one and two stories and includes residential and daycare uses.

The development proposal to expand the Family Learning Center is generally consistent with the character of the area. The site's relative seclusion at the end of 34th Street and adjacent to a ditch and a city park makes it so that the site is not really visible to anyone other than residents of immediately adjacent properties. Considering that the existing use has served residents of the adjacent development for over 30 years, the proposal to expand the existing building and increase parking while maintaining the same level of service and overall intensity of the use will not change the predominant character of the surrounding area.

- N/A (6) Conversion of Dwelling Units to Non-Residential Uses:** There shall be a presumption against approving the conversion of dwelling units in the residential zoning districts set forth in subsection 9-5-2(c)(1)(a), B.R.C. 1981, to non-residential uses that are allowed pursuant to a use review, or through the change of one non-conforming use to another non-conforming use. The presumption against such a conversion may be overcome by a finding that the use to be approved serves another compelling social, human services, governmental, or recreational need in the community including, without limitation, a use for a day care center, park, religious assembly, social service use, benevolent organization use, art or craft studio space, museum, or an educational use.

Not Applicable, as there are no existing residential uses associated with the Family Learning Center.

PUBLIC COMMENT AND PROCESS

Required public notice was given in the form of written notification mailed to all property owners within 600 feet of the subject property and a sign posted on the property for at least 10 days. All notice requirements of section 9-4-3, "*Public Notice Requirements*," B.R.C. 1981 have been met. Staff did not receive any public comments regarding the proposal.

STAFF FINDINGS AND RECOMMENDATION

Staff recommends that the Planning Board approve the Use Review application LUR2012-00101, adopting the staff memorandum as findings of fact and subject to the recommended conditions of approval.

RECOMMENDED CONDITIONS OF APPROVAL:

1. The Applicant shall ensure that the **development shall be in compliance with all approved plans** dated

February 14, 2014 on file in the City of Boulder Planning Department, except to the extent that the development may be modified by the conditions of this approval. Further, the Applicant shall ensure that the approved use is operated in compliance with the following restrictions:

- a. The hours of operation for the approved use shall be limited to: 7:30 a.m. to 8:00 p.m., Monday through Saturday, and 11:00 a.m. to 4:00 p.m. Sundays.
 - b. Size of the approved use shall be limited to 7,564 square feet.
 - c. The Applicant shall have an arrangement for and maintain ten off-site parking spaces within 500 feet of the property. The off-site parking spaces may be shared with other uses provided the parking needs of the approved use are adequately accommodated. The Applicant shall provide evidence of an arrangement for the required off-site parking spaces promptly upon request by the City Manager, and any such arrangement is subject to review and approval by the City Manager.
2. The Applicant **shall not expand or modify the approved use**, except pursuant to subsection 9-2-15(h), B.R.C. 1981.
 3. The Applicant shall **comply with all previous conditions** contained in any previous approvals, except to the extent that any previous conditions may be modified by this approval, including, but not limited to, the following:
 - a. The Development Agreement recorded at Reception No. 01326689 on August 17, 1993 in the records of the Boulder County Clerk and Recorder.

ATTACHMENTS:

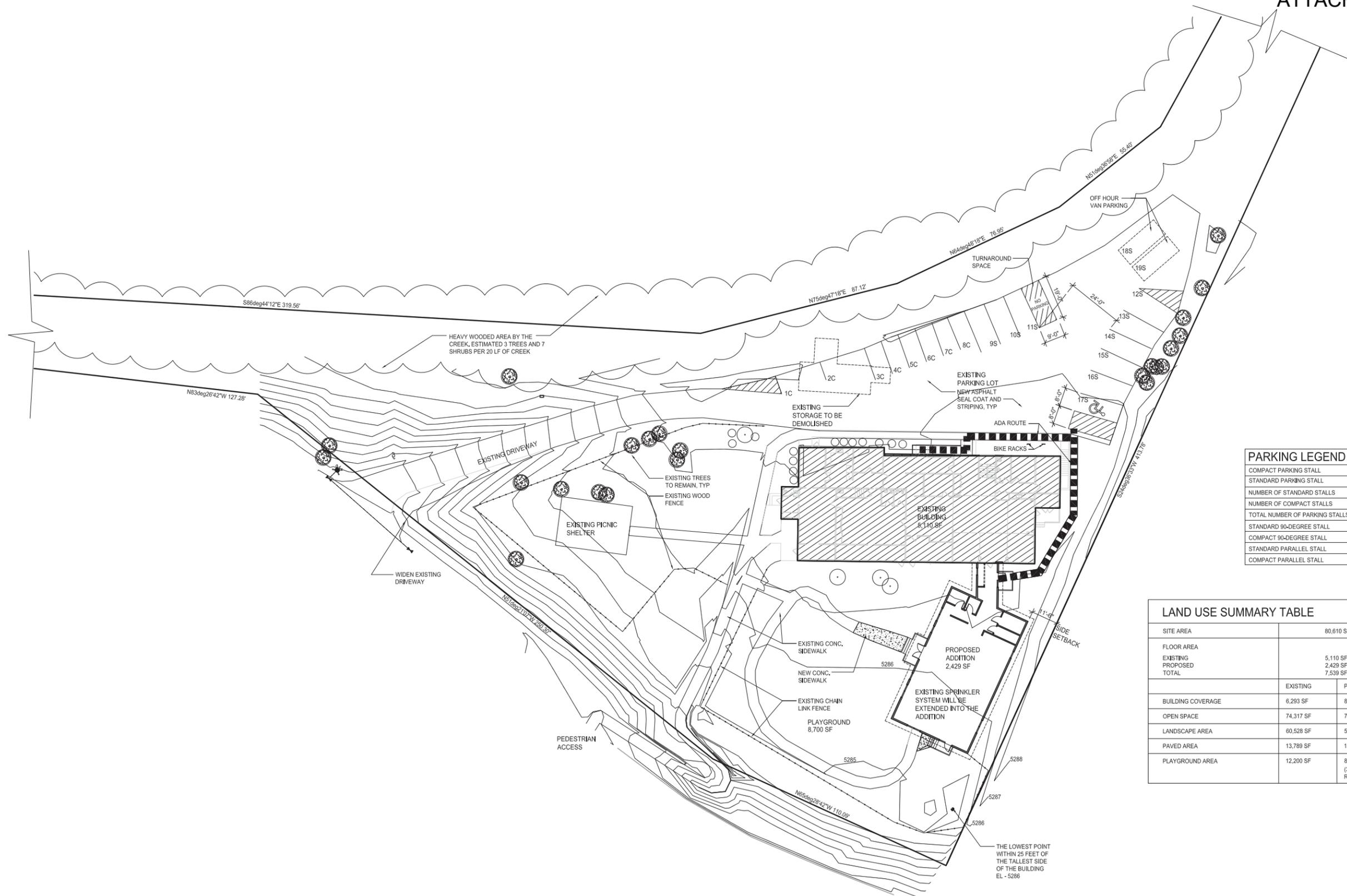
- A: [Applicant's Proposed Plans](#)
- B: [Parking Study](#)

2031 GROVE STREET
 DENVER, CO 80211
 303 . 333 . 1550
 www.kungarch.com

STRUCTURAL:
 S2E CONSULTING
 STRUCTURAL ENGINEERS
 5552 EAST 117th CIRCLE
 THORNTON, CO 80233
 303 . 252 . 0471

MECHANICAL & PLUMBING:
 McDONALD CONSULTING & DESIGN
 10698 AMESBURY WAY
 HIGHLANDS RANCH, CO 80126
 303 . 875 . 9293

ELECTRICAL:
 KAZIN & ASSOCIATES, INC.
 9364 TEDDY LANE, SUITE 101
 LONE TREE, CO 80124
 720 . 489 . 1609



PARKING LEGEND	
COMPACT PARKING STALL	#C
STANDARD PARKING STALL	#S
NUMBER OF STANDARD STALLS	11
NUMBER OF COMPACT STALLS	8
TOTAL NUMBER OF PARKING STALLS	19
STANDARD 90-DEGREE STALL	9' X 19'
COMPACT 90-DEGREE STALL	7.75' X 15'
STANDARD PARALLEL STALL	8' X 23'
COMPACT PARALLEL STALL	8' X 20'

LAND USE SUMMARY TABLE		
	EXISTING	PROPOSED
SITE AREA	80,610 SF	
FLOOR AREA		
EXISTING	5,110 SF	
PROPOSED	2,429 SF	
TOTAL	7,539 SF	
BUILDING COVERAGE	6,293 SF	8,182 SF
OPEN SPACE	74,317 SF	72,428 SF
LANDSCAPE AREA	60,528 SF	57,322 SF
PAVED AREA	13,789 SF	15,106 SF
PLAYGROUND AREA	12,200 SF	8,700 SF (3,000 SF REQUIRED)

Additions & Renovations For:
THE FAMILY LEARNING CENTER
 3164 34th Street
 Boulder, CO 80301

REV.	DATE	ISSUE
05.01.12		OWNER REVIEW
05.10.12		ZONING REVIEW
06.19.12		CLIENT REVIEW
10.01.12		CLIENT REVIEW
11.19.12		1ST PLANNING SUBMITTAL
09.12.13		2ND PLANNING SUBMITTAL
01.21.14		3RD PLANNING SUBMITTAL

PROJECT NO. 12-06
 SCALE AS NOTED

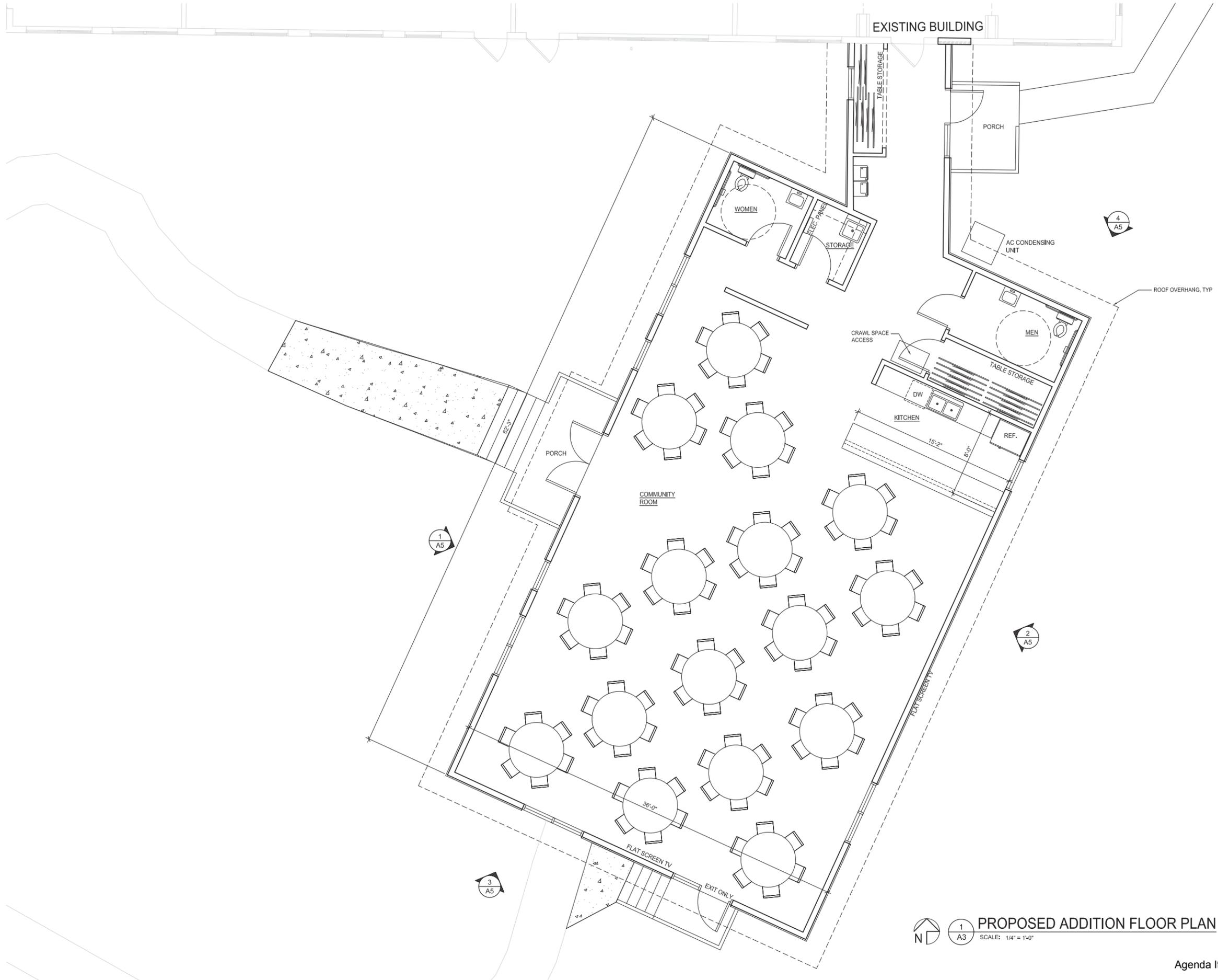
DRAWING TITLE
SITE PLAN



STRUCTURAL:
S2E CONSULTING
STRUCTURAL ENGINEERS
5552 EAST 117th CIRCLE
THORNTON, CO 80233
303 . 252 . 0471

MECHANICAL & PLUMBING:
McDONALD CONSULTING & DESIGN
10698 AMESBURY WAY
HIGHLANDS RANCH, CO 80126
303 . 875 . 9293

ELECTRICAL:
KAZIN & ASSOCIATES, INC.
9364 TEDDY LANE, SUITE 101
LONE TREE, CO 80124
720 . 489 . 1609



Additions & Renovations For:
THE FAMILY LEARNING CENTER
3164 34th Street
Boulder, CO 80301

REV.	DATE	ISSUE
	05.01.12	OWNER REVIEW
	05.10.12	ZONING REVIEW
	06.19.12	CLIENT REVIEW
	10.01.12	CLIENT REVIEW
	11.19.12	1ST PLANNING SUBMITTAL
	09.12.13	2ND PLANNING SUBMITTAL
	01.21.14	3RD PLANNING SUBMITTAL

PROJECT NO. 12-06

SCALE AS NOTED

DRAWING TITLE

PROPOSED FLOOR PLAN

PROPOSED ADDITION FLOOR PLAN
SCALE: 1/4" = 1'-0"
1/A3

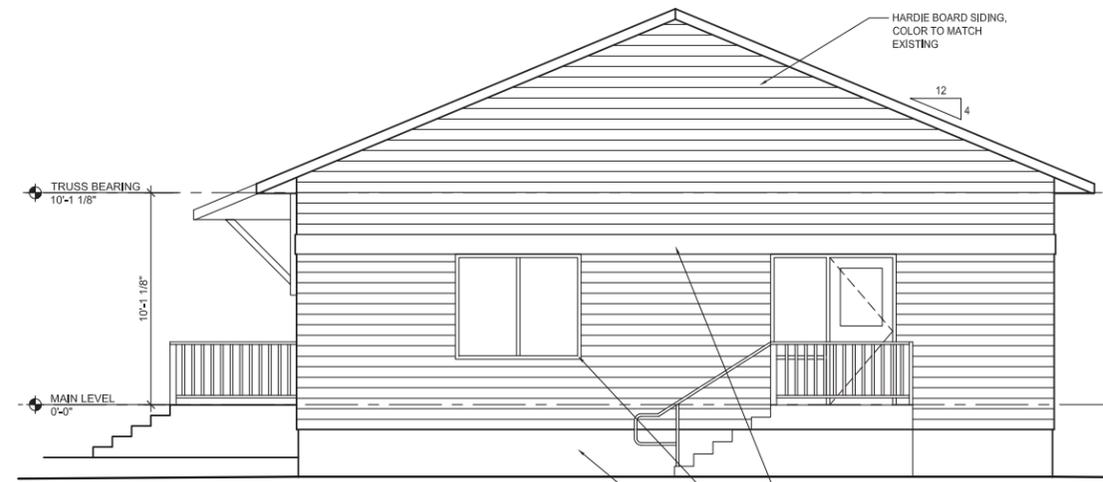
A3

REV. DATE	ISSUE
05.01.12	OWNER REVIEW
05.10.12	ZONING REVIEW
06.19.12	CLIENT REVIEW
10.01.12	CLIENT REVIEW
11.19.12	1ST PLANNING SUBMITTAL
09.12.13	2ND PLANNING SUBMITTAL
01.21.14	3RD PLANNING SUBMITTAL

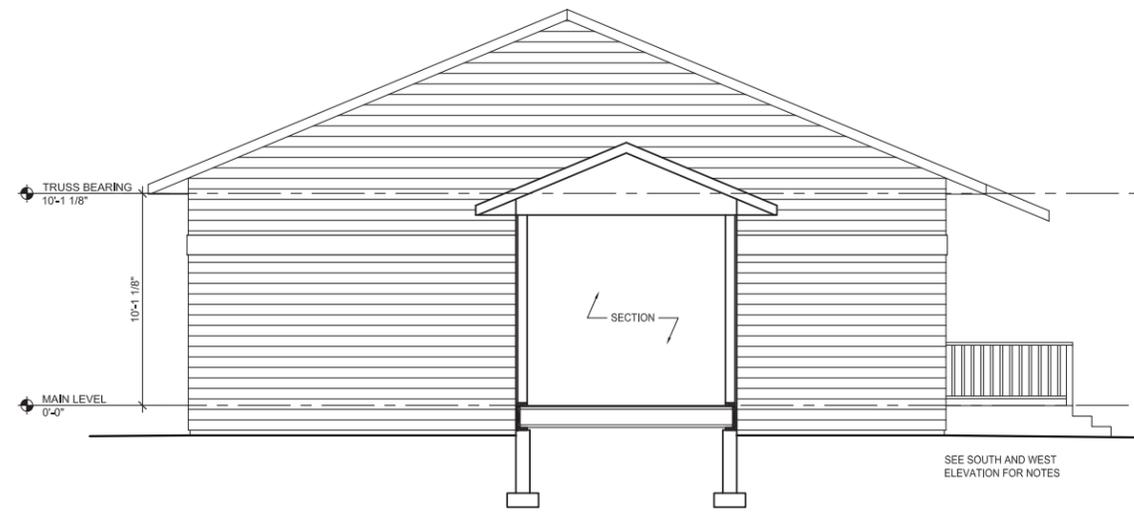
PROJECT NO. 12-06
SCALE AS NOTED
DRAWING TITLE

BUILDING ELEVATIONS ADDITION

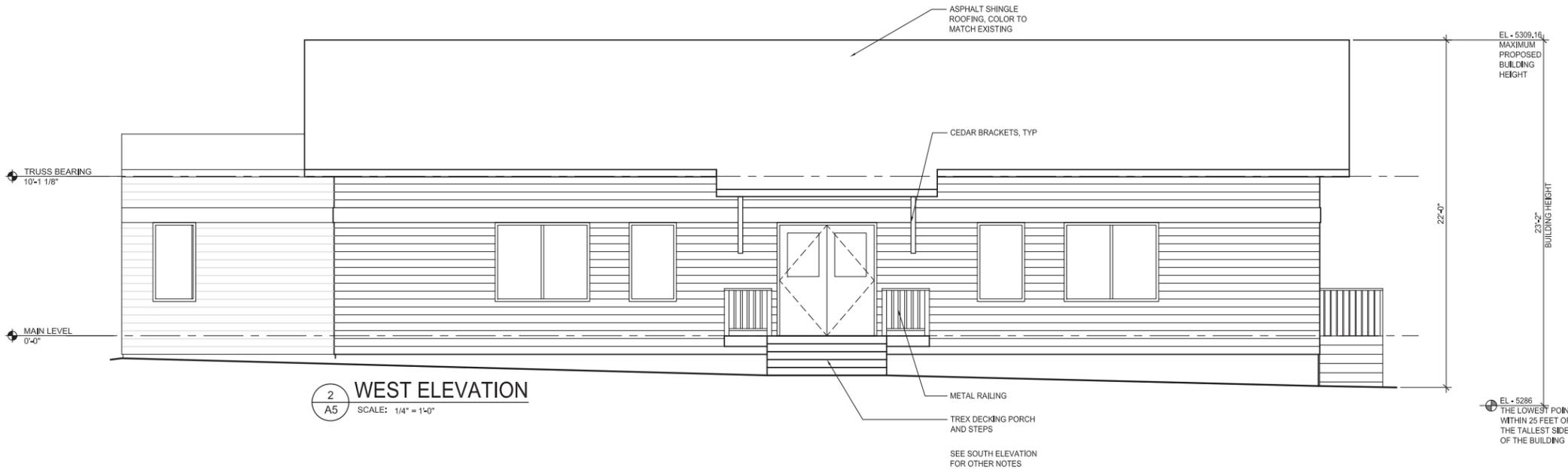
A5



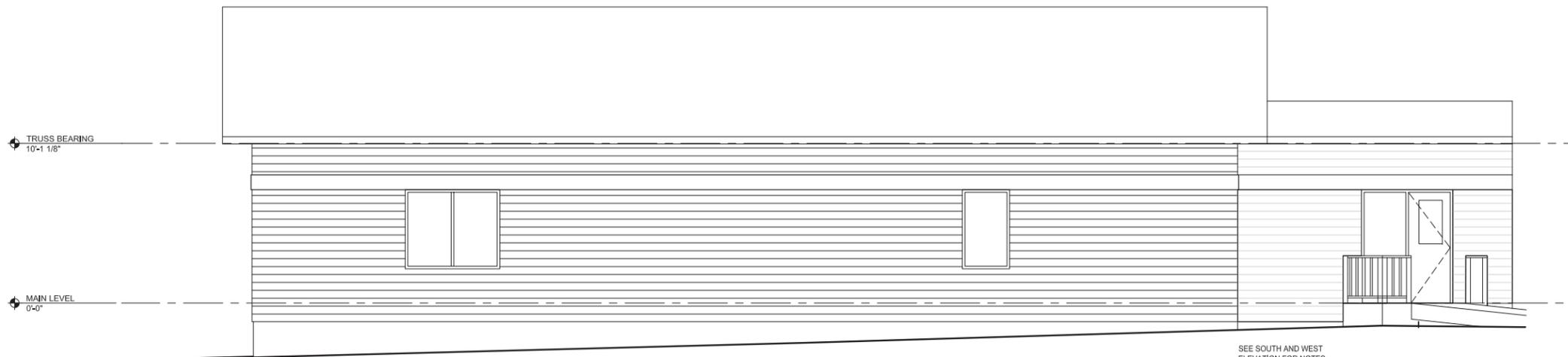
1 SOUTH ELEVATION
A5 SCALE: 1/4" = 1'-0"



2 NORTH ELEVATION
A5 SCALE: 1/4" = 1'-0"



2 WEST ELEVATION
A5 SCALE: 1/4" = 1'-0"



4 EAST ELEVATION
A5 SCALE: 1/4" = 1'-0"

1889 York Street
Denver, CO 80206
(303) 333-1105
FAX (303) 333-1107
E-mail: lsc@lscdenver.com



March 18, 2014

Mr. Mike Crase
Gilmore
4949 Ironton Street
Denver, CO 80239

Re: Family Learning Center
Boulder County, CO
(LSC #130080)

Dear Mr. Crase:

In response to your request, LSC Transportation Consultants, Inc. has prepared this parking analysis for the Family Learning Center. As shown on Figure 1, the site is located at 3164 34th Street in Boulder, Colorado.

REPORT CONTENTS

The report contains the following: a description of the current and planned land use, a survey of the existing parking utilization, the number of shared parking spaces that would likely be needed at peak times on a typical weekday, and recommendations to improve existing parking conditions.

LAND USE AND ACCESS

Figure 1 shows how 34th Street connects the site to Valmont Road. There is on-street public parking available on 34th Street north of Valmont Road. Figure 2 shows the conceptual site plan. The proposed improvements will include 18 standard parking spaces and one ADA parking space - a slight increase from existing conditions (13 standard spaces and one ADA space). The existing building is about 5,137 square feet with 18 full- or part-time employees. The intent of the 2,427 square-foot expansion is to better support existing services and operations of the Family Learning Center with no significant increase in staff size, traffic generation, or parking demand. The Family Learning Center has indicated there is a verbal understanding between the San Juan Del Centro apartment complex owner/ operator and the Family Learning Center to share the apartment complex parking spaces nearest to the Family Learning Center.

EXISTING PARKING SPACE UTILIZATION

There are 53 available parking spaces (including 2 ADA spaces) on or in the vicinity of the site as shown in Figure 3. An inventory was taken of these parking spaces between 7:45 AM and

6:15 PM on Tuesday through Thursday (March 5-7, 2013) to determine the existing parking space utilization. The results are summarized in Table 1 and show a maximum utilization of 66.0% on Tuesday, 88.7% on Wednesday, and 79.2% on Thursday. The parking spaces on the Family Learning Center site were occasionally fully occupied with some spillover parking in the San Juan Del Centro spaces which supports the need for some parking sharing with the San Juan Del Centro apartment complex.

ESTIMATE OF OVERFLOW PARKING DURING PEAK CONDITIONS

Two separate methods were used to estimate the parking space demand during peak operations. One method is based on data from the ITE *Parking Generation Manual* and the other is based on operational information from the Family Learning Center.

ITE Parking Generation Manual

The manual estimates the 85th percentile parking demand for a Day Care Center as 3.70 spaces per 1,000 square feet of gross floor area. This would relate to a demand of about 28 parking spaces. It is estimated this could be reduced about 20 percent due to many of the trips to/from the site being made by bus/shuttle or by pedestrians and cyclists from the surrounding neighborhood(s). Based on this method the estimated parking demand would be about 23 parking spaces which could be served by the 18 on-site standard parking spaces and five shared parking spaces from San Juan Del Centro.

Based on Operational Information

The Center typically has about three employee vehicles and three shuttle buses parked on or near the site. There are three main activities that generate weekday parking demand: daily pre-K, daily after school care for middle school students, and Wednesday night tutoring for high school students.

Pre-K (Weekdays):

About 15 of the 30 Pre-K students typically arrive/depart by passenger vehicle. During peak times the likely parking demand would be about 21 vehicles (15 for Pre-K, 3 for employees, and 3 for shuttle buses).

Typically vans drop off at 8:00 AM and parents drop off between 7:50 and 8:15 AM. Vans pick up at 11:30 AM and parents pick up between 11:30 and 11:40 AM. Vans drop off at 12:30 PM and parents drop off between 12:30 and 12:40 PM. Vans and parents pick up around 4:00 PM. The vans drop off after school students prior to picking up preschool students.

After School Care (Weekdays):

About 20 of the 45 after school care students depart by passenger vehicle - most arrive by van or are dropped off by bus and walk to the site. There is no defined pick-up time so it is expected only about 80 percent of the 20 vehicles would be on-site at any given time. During peak times the likely parking demand would be about 22 vehicles (16 for after school care, 3 for employees, and 3 for shuttle buses).

Elementary and middle school students are dropped off by bus around 3:00 PM on 34th Street and they walk to the Family Learning Center. Vans drop off students from Creekside Elementary and Centennial Middle School at 4:00 PM and pick up afternoon preschool students. Students typically walk home or are picked up between 5:30 and 6:00 PM.

Tutoring (Wednesday nights):

Students typically arrive at 6:00 PM and leave at 8:00 PM. Many walk to/from San Juan Del Centro and/or carpool. To be conservative it is assumed about 15 of the 30 tutored students arrive/depart by vehicle with very few parking spaces utilized for drop-off. Most of the spaces utilized for tutoring would be during pick-up when a parent or guardian is waiting for a student. During peak times the likely parking demand would be about 21 vehicles (15 for tutoring, 3 for employees, and 3 for shuttle buses).

SUMMARY AND CONCLUSIONS

1. There is on-street public parking on 34th Street north of Valmont Road.
2. The Family Learning Center site is proposing to expand by adding a 2,427 square-foot building to supplement the existing 5,137 square-foot building. The intent of the 2,427 square-foot expansion is to better support existing services and operations with no significant increase in staff size, traffic generation, or parking demand.
3. The Family Learning Center has indicated there is a verbal understanding between the San Juan Del Centro owner/operator and the Family Learning Center to share the apartment complex parking spaces nearest to the Family Learning Center.
4. The data collected shows the existing parking lot on site is occasional fully utilized with parking spillover into the adjacent apartment complex spaces. The data also shows there is capacity in the apartment complex to accommodate this overflow parking.
5. It is expected about five San Juan Del Centro parking spaces will typically be used by Family Learning Center vehicles during peak weekday times.

RECOMMENDATIONS

6. The City of Boulder has asked the Family Learning Center to formalize the parking sharing agreement with San Juan Del Centro apartment complex. It is recommended the agreement be for five shared parking spaces on a typical weekday with up to ten spaces allowed on a short-term basis - for example if a few spaces are utilized on Wednesday nights (6:00 PM) for high school student drop-off.
7. It is recommended that the Family Learning Center direct staff and volunteers to park in the shared parking spaces in the San Juan Del Centro apartment complex, particularly during times of peak parking demand.
8. It is recommended that the Family Learning Center park their shuttle vans in the shared parking spaces in the San Juan Del Centro apartment complex, particularly during times

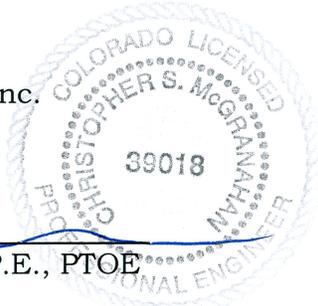
of peak parking demand. They could be moved into the on-site parking lot for overnight parking.

* * * * *

We trust this information will assist you in improving parking operations for the Family Learning Center.

Respectfully submitted,

LSC Transportation Consultants, Inc.



By: _____

Christopher S. McGranahan, P.E., PTOE
Principal

3-18-14

CSM/wc

Enclosures: Table 1
Figures 1-3
ITE Parking Generation Data for Day Care Center

Z:\LSC\Projects\2013\130080-FamilyLearningCenter\Report\March-2014\FamilyLearningCenter-031814.wpd

**Table 1
Parking Utilization
Family Learning Center
(LSC #130080; March, 2014)**

Time	Number of Vehicles Parked ⁽¹⁾					
	Tuesday 03/05/2013		Wednesday 03/06/2013		Thursday 03/07/2013	
	# parked	% parked	# parked	% parked	# parked	% parked
07:45 AM	27	50.9%	25	47.2%	23	43.4%
08:00 AM	26	49.1%	25	47.2%	22	41.5%
08:15 AM	29	54.7%	30	56.6%	30	56.6%
09:45 AM	32	60.4%	27	50.9%	30	56.6%
11:15 AM	32	60.4%	24	45.3%	30	56.6%
11:30 AM	35	66.0%	24	45.3%	30	56.6%
11:45 AM	33	62.3%	21	39.6%	29	54.7%
01:15 PM	33	62.3%	29	54.7%	28	52.8%
02:45 PM	33	62.3%	31	58.5%	28	52.8%
03:00 PM	33	62.3%	31	58.5%	28	52.8%
03:15 PM	31	58.5%	35	66.0%	29	54.7%
03:45 PM	31	58.5%	36	67.9%	36	67.9%
04:00 PM	32	60.4%	44	83.0%	42	79.2%
04:15 PM	29	54.7%	47	88.7%	34	64.2%
05:00 PM	32	60.4%	37	69.8%	23	43.4%
05:45 PM	28	52.8%	29	54.7%	26	49.1%
06:00 PM	21	39.6%	28	52.8%	25	47.2%
06:15 PM	23	43.4%	32	60.4%	27	50.9%

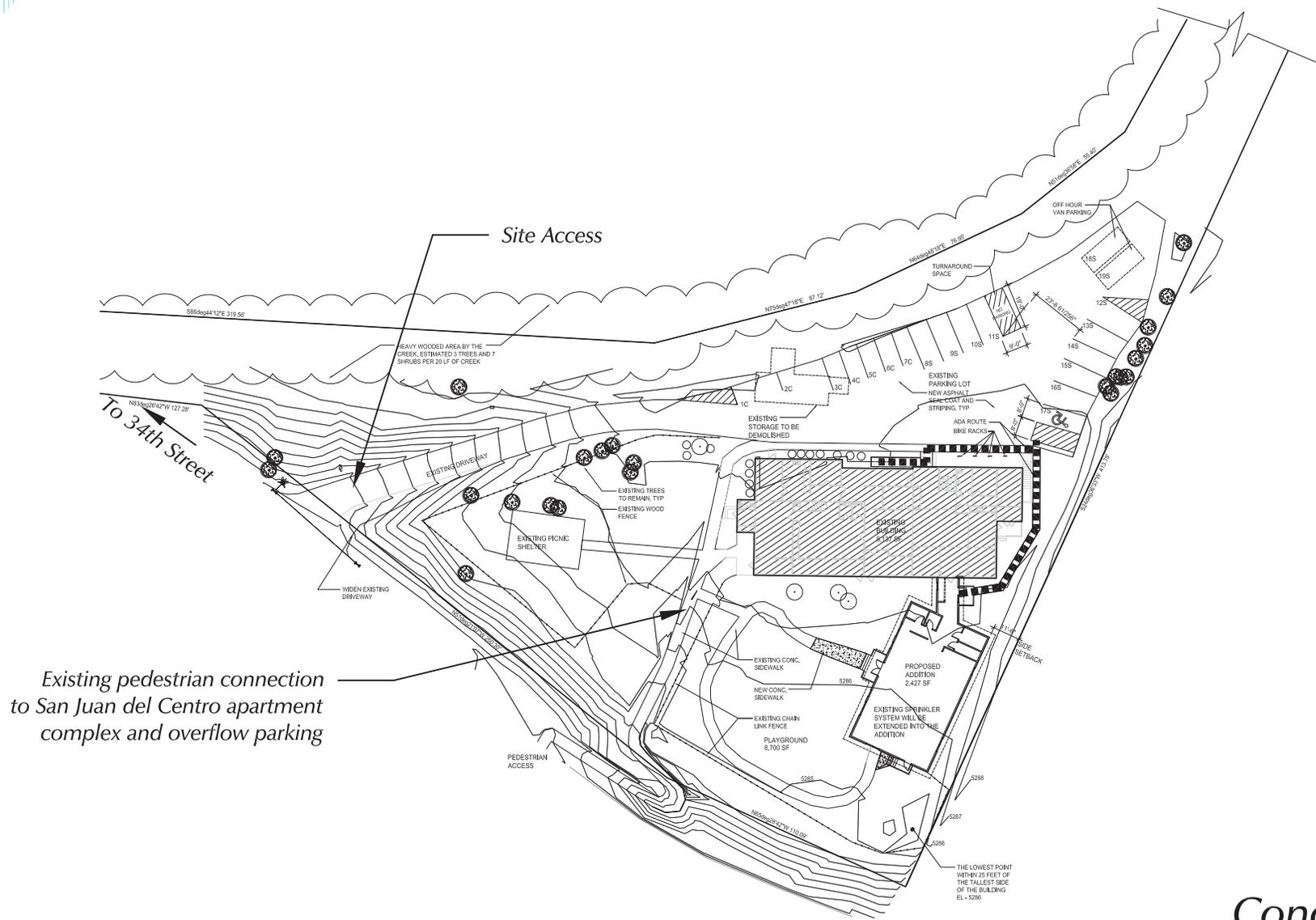
⁽¹⁾ There are 53 total available parking spaces on or in the vicinity of the site in the area shown in Figure 3.



Approximate Scale
Scale: 1"=2,500'

Figure 1
**Vicinity
Map**

Family Learning Center (LSC # 130080)



Existing pedestrian connection to San Juan del Centro apartment complex and overflow parking

Figure 2
Conceptual Site Plan

Family Learning Center (LSC # 130080)



Approximate Scale
Scale: 1"=100'

LEGEND:

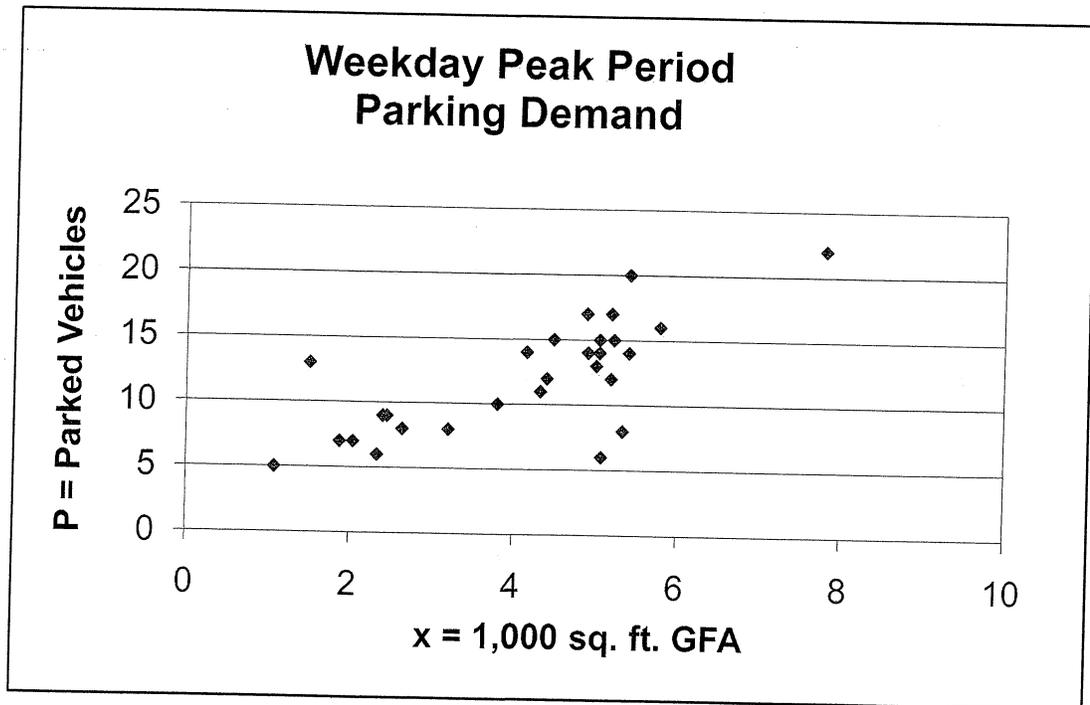
⑨ = Parking Spaces

Figure 3
**Limits of
Parking Space Survey**
Family Learning Center (LSC # 130080)

Land Use: 565 Day Care Center

Average Peak Period Parking Demand vs. 1,000 sq. ft. GFA On a Weekday

Statistic	Peak Period Demand
Peak Period	8:00–9:00 a.m.; 4:00–6:00 p.m.
Number of Study Sites	29
Average Size of Study Sites	4,200 sq. ft. GFA
Average Peak Period Parking Demand	3.16 vehicles per 1,000 sq. ft. GFA
Standard Deviation	1.26
Coefficient of Variation	40%
95% Confidence Interval	2.70–3.62 vehicles per 1,000 sq. ft. GFA
Range	1.18–8.67 vehicles per 1,000 sq. ft. GFA
85th Percentile	3.70 vehicles per 1,000 sq. ft. GFA
33rd Percentile	2.74 vehicles per 1,000 sq. ft. GFA



◆ Actual Data Points

CITY OF BOULDER
PLANNING BOARD AGENDA ITEM
MEETING DATE: April 3, 2013

AGENDA TITLE: Public hearing and consideration of a **Use Review** application, no. LUR2013-00065, for approval of an 11-space parking lot as a second principal use in addition to a proposed 3-unit residential townhome development at 2360 Grove St. in the RH-2 zone district (see LUR2013-00051 for associated Site Review).

Applicant: Peter Stewart
Owner: Grove 3, LLC

REQUESTING DEPARTMENT:

Community Planning & Sustainability

David Driskell, Executive Director

Susan Richstone, Deputy Director

Charles Ferro, Land Use Review Manager

Chandler Van Schaack, Planner I

OBJECTIVE:

1. Hear Staff and Applicant presentations
2. Hold Public Hearing
3. Planning Board discussion
4. Planning Board action to approve, approve with conditions, or deny

Proposal: Request for approval of an 11-space parking lot as a second principal use in addition to a proposed 3-unit residential townhome development at 2360 Grove St. in the RH-2 zone district (see LUR2013-00051 for associated Site Review).

Project Name: 2360 Grove

Location: 2360 Grove St.

Size of Tract: 10,990 s.f. (0.25-acres)

Zoning: Residential High - 2 (RH-2)

Comprehensive Plan: High Density Residential

PROCESS:

This Use Review request is to allow for a new residential townhome development project at 2360 Grove St. to include a second principal use consisting of 11 shared parking spaces for use by the neighboring commercial property to the east (1575 Folsom). Please see **Figure 1** below for a vicinity map. A separate Site Review application is required for the new residential townhome development due to requested setback modifications (LUR2013-00051); however, the proposed 3-unit townhome development is not included in this Use Review request due to the fact that it is a principal use allowed by-right in the RH-2 zone district. The shared parking is considered a separate principal use because it is intended to serve a separate property not included in the residential development proposal. Per section 9-6-1, "Use Standards," B.R.C. 1981, "*automobile parking lots, garages, or car pool lots as a principal use*" are permitted in the RH-2 zone only if approved through a Use Review process. Pursuant to section 9-2-15, "*Use Review*," B.R.C. 1981, the city manager shall review and submit a recommendation to the Planning Board for any application for a Use Review of a nonresidential use in a residential zone. It should be noted that while the residential development is not included in this Use Review request, approval

of the Site Review application is conditioned on the applicant receiving approval of the shared parking through this Use Review application. Please see [Attachment A](#) for the applicant's proposed plans.

BACKGROUND:

Existing Use / Area Context.

The project site is located on Grove Street near the intersection of Grove and Folsom Street within the RH-2 (Residential -High 2) zone district, which is defined in section 9-5-2(c)(1)(F) of the land use code as “High density residential areas primarily used for a variety of types of attached residential units, including without limitation, apartment buildings, and where complementary uses may be allowed.” The areas to the north and northwest of the subject site are a mix of one to three-story mixed density residential buildings, and include the

RMX-1 (Residential –Mixed 1)-zoned Goss-Grove residential neighborhood. The properties immediately to the south and east of the subject site contain primarily retail and service uses and are zoned BT-2 (Business-Transitional 2), with BR-1 (Business- Regional 1) zoning extending further to the south and east across Arapahoe Ave. and Folsom St., respectively.

Currently, the 10,990 square foot site contains a surface parking lot which serves the existing restaurant uses located on the neighboring property to the east (1575 Folsom St.). The existing restaurant uses located at 1575 Folsom St. were approved in 1985 through a Special Review (SR-85-32), which required that the development provide 13 parking spaces on site and also maintain a valid lease for an additional 11 parking spaces within 500 feet of the property (Please see [Attachment B](#) for the original 1575 Folsom disposition of approval). At the time of the original approval for 1575 Folsom in 1985, the parcel that is now 2360 Grove St. was part of a larger parcel under common ownership that included the 2333 Arapahoe property to the south (former Dairy Queen site) and had frontages on both Arapahoe Ave. and Grove St., with an additional access point off of Folsom St. just south of the 1575 Folsom property. The original site plan approval for 1575 Folsom showed the 11 parking spaces located just south of the property, on what is now 2333 Arapahoe; however, at some point following the closure of the Dairy Queen a new parking agreement was created and the spaces were relocated to the 2360 Grove property, which contained the northern portion of the former Dairy Queen parking lot. Currently, there is a shared parking and access agreement between the owners of 1575 Folsom and 2360 Grove requiring that 11 parking spaces be reserved on the subject site for use by patrons of 1575 Folsom.

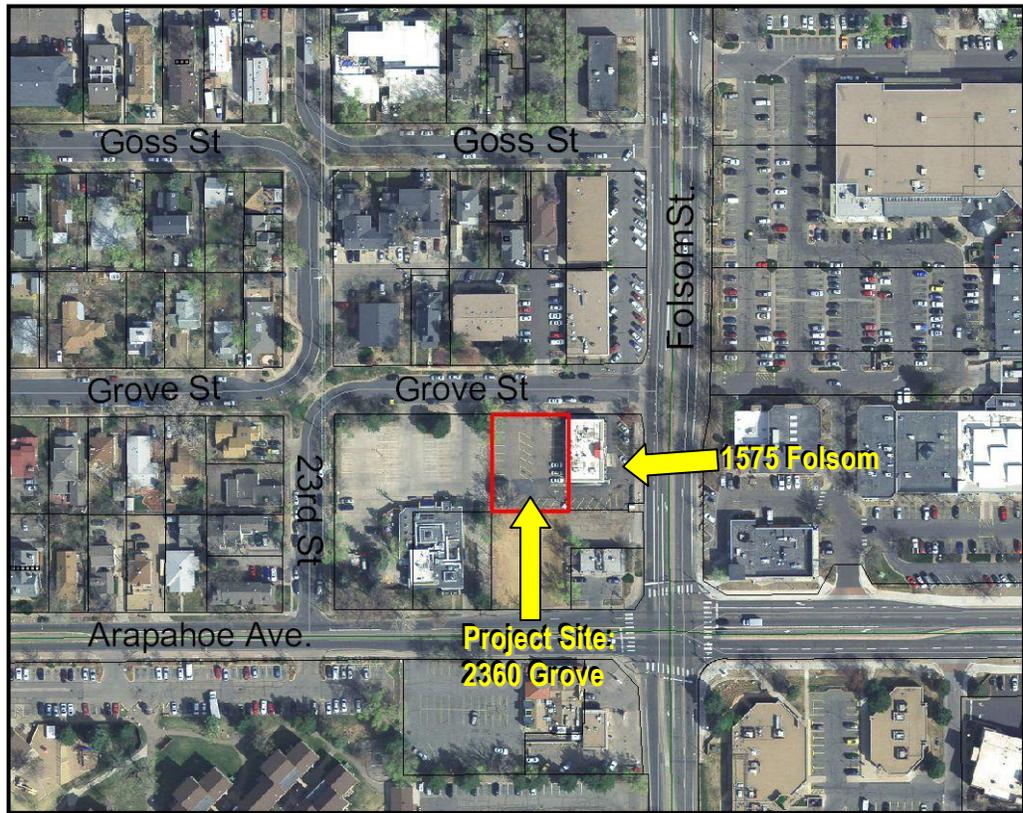


Figure 1: Vicinity Map

ANALYSIS OF USE REVIEW CRITERIA

Applications for Use Review are reviewed for consistency with the criteria set forth in subsection 9-2-15(e), "Criteria for Review," B.R.C. 1981.

1. **Is the proposed use consistent with the Use Review criteria set forth in subsection 9-2-15(e), "Criteria for Review," B.R.C. 1981?**

Criteria for Review: No use review application will be approved unless the approving agency finds all of the following:

✓ **(1) Consistency with Zoning and Non-Conformity: The use is consistent with the purpose of the zoning district as set forth in section 9-5-2(c), "Zoning Districts Purposes," B.R.C. 1981, except in the case of a non-conforming use;**

The project site is located within the RH-2 (Residential -High 2) zone district, which is defined in section 9-5-2(c)(1)(F) of the land use code as "High density residential areas primarily used for a variety of types of attached residential units, including without limitation, apartment buildings, and where complementary uses may be allowed."

The proposed shared parking is considered a separate principal use because it is intended to serve a separate property not included in the residential development proposal. Per section 9-6-1, "Use Standards," B.R.C. 1981, "automobile parking lots, garages, or car pool lots as a principal use" are permitted in the RH-2 zone only if approved through a Use Review process.

✓ **(2) Rationale: The use either:**

✓ **(A) Provides direct service or convenience to or reduces adverse impacts to the surrounding uses or neighborhood;**

The proposed shared parking would provide a direct service to the adjacent commercial use by allowing the owners of that property to maintain their existing shared parking agreement with the owners of 2360 Grove, thereby providing them with a close and convenient area for customers to park. The existing restaurant uses located at 1575 Folsom St. were approved in 1985 through a Special Review (SR-85-32), which required that the development provide 13 parking spaces on site and also maintain a valid lease for an additional 11 parking spaces within 500 feet of the property.

Maintaining the existing shared parking agreement will also help to reduce adverse impacts to the surrounding neighborhood by preventing parking spillover from the adjacent commercial uses. The subject site is located immediately adjacent to the residential Goss-Grove neighborhood, which can be impacted by overflow parking from Naropa University, Boulder High and other nearby commercial development. Given that the existing site has absorbed the parking impacts of the adjacent commercial uses for nearly 30 years, the proposal to maintain the shared parking as a second principal use while also redeveloping the underutilized site for residential represents an optimal way to increase intensity without increasing the impacts of the existing uses.

N/A (B) Provides a compatible transition between higher intensity and lower intensity uses;

Not applicable.

N/A (C) Is necessary to foster a specific city policy, as expressed in the Boulder Valley Comprehensive Plan, including, without limitation, historic preservation, moderate income housing, residential and non-residential mixed uses in appropriate locations, and group living arrangements for special populations; or

Not applicable.

N/A (D) Is an existing legal non-conforming use or a change thereto that is permitted under subsection (e) of this section;

Not applicable.

✓ **(3) Compatibility: The location, size, design, and operating characteristics of the proposed development or change to an existing development are such that the use will be reasonably compatible with and have minimal negative impact on the use of nearby properties or for residential uses in industrial zoning districts, the proposed development reasonably mitigates the potential negative impacts from nearby properties;**

As discussed above, the 2360 Grove site has been used as a parking lot for adjacent commercial uses for over 30 years. During that time, the site has helped to alleviate the impacts of the nearby commercial development on the adjacent residential neighborhood to the north and west. Formerly part of the Dairy Queen parking area and later serving the “Siamese Plate” restaurant to the east at 1575 Folsom, the site has continuously helped to create a buffer between the higher intensity uses of the Boulder Valley Regional Center (BVRC) to the east and the mixed density residential uses to the west. Under the current proposal, the site will be redeveloped in a manner consistent with the nearby residential areas while also maintaining its historic function as a parking buffer between the commercial and residential uses it lies between. Therefore, visually, the proposed development will be more compatible with the surrounding residential areas than it is currently, while in terms of impacts to nearby properties, the site will maintain its existing compatibility.

✓ **(4) Infrastructure: As compared to development permitted under section 9-6-1, "Schedule of Permitted Uses of Land," B.R.C. 1981, in the zone, or as compared to the existing level of impact of a non-conforming use, the proposed development will not significantly adversely affect the infrastructure of the surrounding area, including, without limitation, water, wastewater, and storm drainage utilities and streets;**

The proposed parking use does not require any infrastructure improvements, as the site is already developed as a parking lot. The associated residential development meets the use and intensity standards for the RH-2 zone district.

✓ **(5) Character of Area: The use will not change the predominant character of the surrounding area; and**

The areas to the north and northwest of the subject site are a mix of one to three-story mixed density residential buildings, and include the RMX-1 (Residential –Mixed 1)-zoned Goss-Grove residential neighborhood. The properties immediately to the south and east of the subject site contain primarily retail and service uses and are zoned BT-2 (Business- Transitional 2), with BR-1 (Business- Regional 1) zoning extending further to the south and east across Arapahoe Ave. and Folsom St., respectively. As such, redevelopment of the subject site as attached residential housing while maintaining the existing parking agreement with the adjacent commercial property is

in keeping with the character of both the existing residential uses to the north and northwest as well as the existing commercial uses to the south and east.

N/A (6) Conversion of Dwelling Units to Non-Residential Uses: There shall be a presumption against approving the conversion of dwelling units in the residential zoning districts set forth in subsection 9-5-2(c)(1)(a), B.R.C. 1981, to non-residential uses that are allowed pursuant to a use review, or through the change of one non-conforming use to another non-conforming use. The presumption against such a conversion may be overcome by a finding that the use to be approved serves another compelling social, human services, governmental, or recreational need in the community including, without limitation, a use for a day care center, park, religious assembly, social service use, benevolent organization use, art or craft studio space, museum, or an educational use.

Not Applicable, as there are no existing residential uses located on the subject site.

PUBLIC COMMENT AND PROCESS

Required public notice was provided in the form of written notifications to property owners within 600 feet of the subject property. In addition, a public notice sign was posted on the property and therefore, all public notice requirements of section 9-4-3, "Public Notice Requirements," B.R.C. 1981 were met. Staff received comments from a neighboring property owner concerned about the city's residential parking requirements in general; however, the citizen overall expressed support for the request to maintain shared parking spaces.

STAFF FINDINGS AND RECOMMENDATION

Staff recommends that the Planning Board approve the Use Review application LUR2013-00065, adopting the staff memorandum as findings of fact and subject to the recommended conditions of approval.

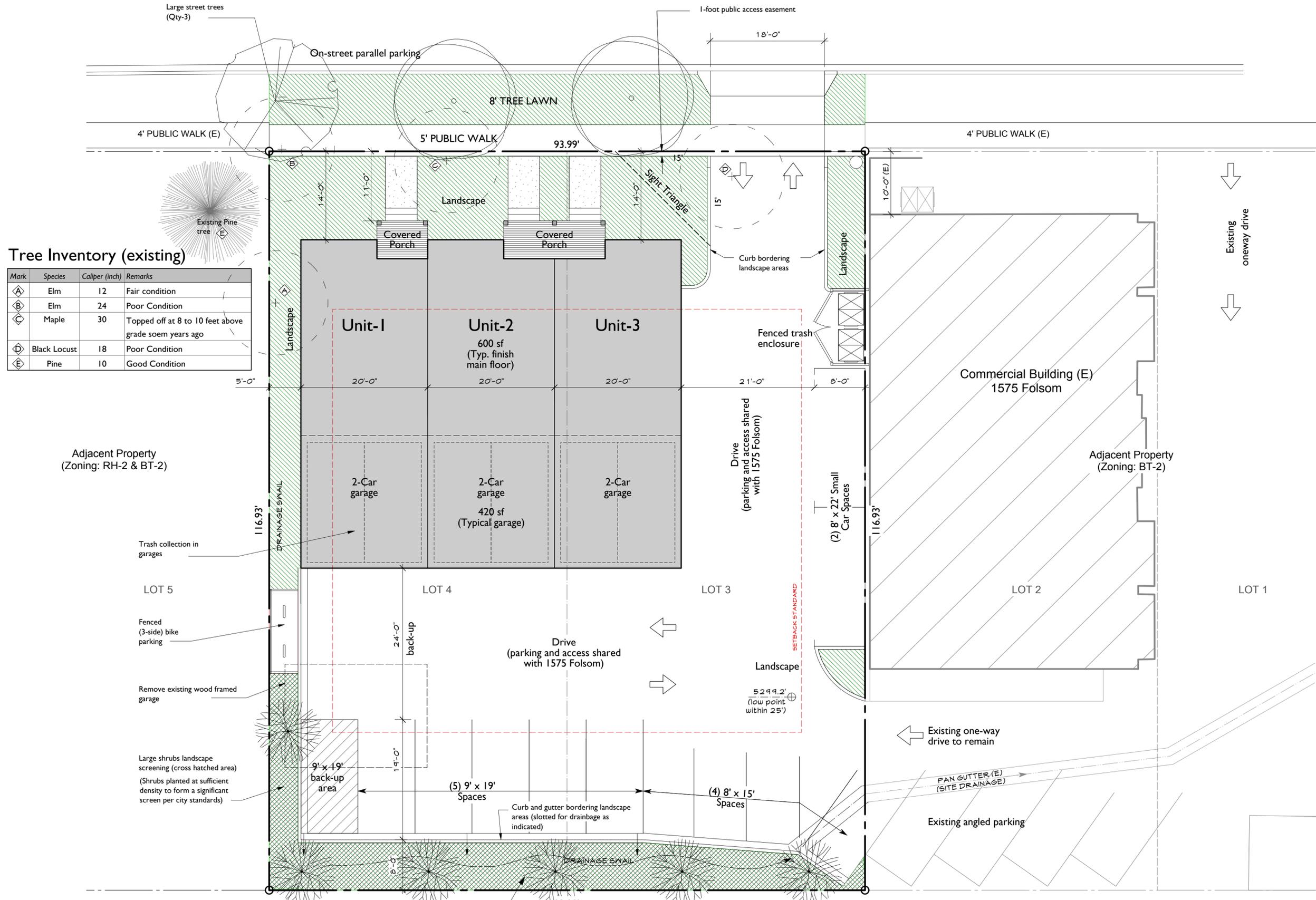
RECOMMENDED CONDITIONS OF APPROVAL:

1. The Applicant shall ensure that the **development shall be in compliance with all approved plans** dated January 3, 2014 on file in the City of Boulder Planning Department, except to the extent that the development may be modified by the conditions of this approval. Further, the Applicant shall ensure that the approved use is operated in compliance with the following restrictions:
 - a. Size of the approved use shall be limited to eleven (11) parking spaces for the use of the neighboring property at 1575 Folsom Street, including five (5) standard stalls and four (4) compact stalls located along the south side of the site as well as two (2) parallel compact stalls located along the east side of the subject property.
2. The Applicant **shall not expand or modify the approved use**, except pursuant to subsection 9-2-15(h), B.R.C. 1981.

ATTACHMENTS:

- A: [Applicant's Proposed Plans](#)
B: [Disposition of Approval for 1575 Folsom](#)

GROVE STREET



Tree Inventory (existing)

Mark	Species	Caliper (inch)	Remarks
A	Elm	12	Fair condition
B	Elm	24	Poor Condition
C	Maple	30	Topped off at 8 to 10 feet above grade soem years ago
D	Black Locust	18	Poor Condition
E	Pine	10	Good Condition

Adjacent Property
(Zoning: RH-2 & BT-2)

Trash collection in garages

LOT 5

Fenced (3-side) bike parking

Remove existing wood framed garage

Large shrubs landscape screening (cross hatched area)
(Shrubs planted at sufficient density to form a significant screen per city standards)

Small upright trees (Qty-7)

Large shrubs and landscape screening (Shrubs planted at sufficient density to form a significant screen per city standards)

Adjacent Property
(Zoning: BT-2)

Flood Zone Information
 This property is located in the 100-year flood plain of Boulder Creek
 Base Flood Elevation (BFE):
 Zone: AO
 Depth: 1-foot
 Flood Protection Elevation (FPE): 2-feet

Flood Zone information based on FIRM map 08013C0394J dated: Dec 18, 2012

Site Development Plan

1" = 10'-0"



Sheet Issue:

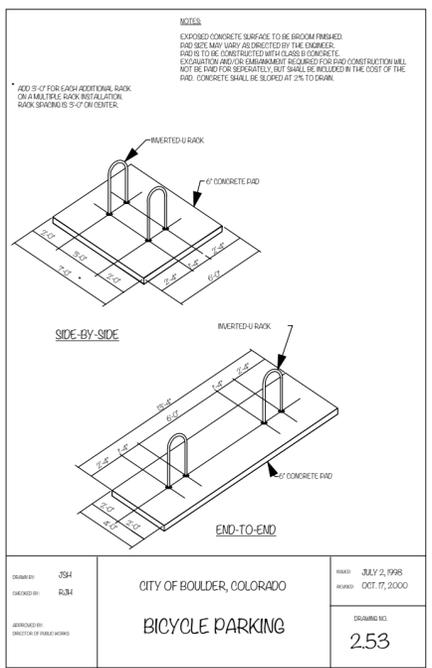
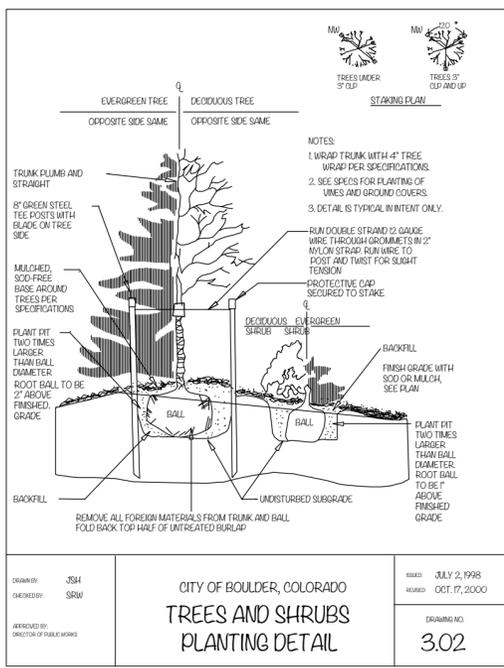
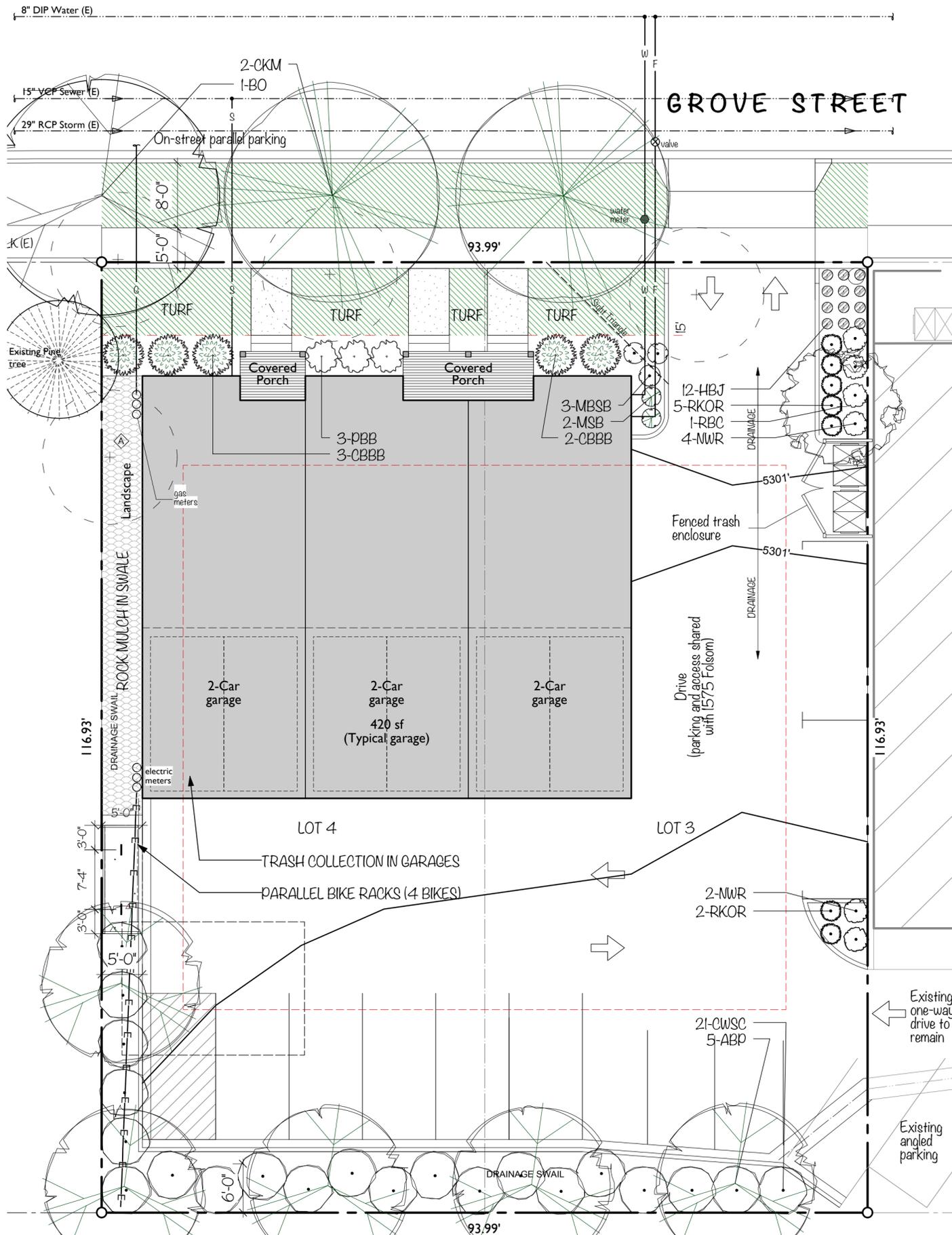
Date	Comments	Init
SEPT. 16, 2013	Submittal 1	PS
Dec. 2, 2013	Resubmittal 1	PS
Jan. 3, 2013	Resubmittal 2	PS

Prepared for:
Grove 3, LLC
 2291 Arapahoe, Boulder, Colorado 80302
 Contact: Gary Calderon 303-449-2900

2360 Grove Street, Boulder
 Lot 3 & 4, Block 20 Culvers Subdivision, City of Boulder, County of Boulder

Site & Use Review
 LUR2013-00051
 LUR2013-00065

Sheet Title
 SITE PLAN
 Sheet No.



LANDSCAPE REQUIREMENTS: 01/29/14

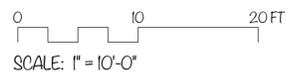
OVERALL SITE	REQUIRED	PROVIDED/COMMENTS
LANDSCAPING	TOTAL SITE: 10,990 SF Zone: RM-2	
STREETSCAPE:		
Detached Sidewalk - Grove Street	1 tree/30' - 64 LF = 3 trees	3 trees provided due to driveway and utility conflicts
MINIMUM PLANT SIZES: 1 tree & 5 shrubs/1500 sf - 2,087 sf = 2 trees & 10 shrubs		
Deciduous Trees	2" cal.	3
Evergreen Trees	6' ht.	0
Ornamental Trees	1.5' cal.	6
Shrubs	5 gallon container	51 5-gallon shrubs
PARKING LOT SCREENING:		
FROM THE STREET:		
Height & Opacity	Landscape Material 42" ht.	N/A parking lot in rear of property
Width	6' Buffer	6' along south edge, only 4'-6" provided along west side
Trees	1 tree/25-78 LF = 5 trees	5 provided
INTERIOR PARKING LOT		
LANDSCAPING:	12 spaces, 5% landscape required 4,917 sf = 246 sf required	
Amount:	1 tree/200 sf of landscape area	Parking area over sized due to agreement to share parking with adjacent property. 164 sf provided with shrubs. Islands are minimal due to shared agreement. 1 tree provided in entry island with larger perimeter screening.

PLANT LIST: 1/29/14

KEY	QTY	BOTANICAL NAME	COMMON NAME	SIZE	SPACING
SHADE TREES:					
BO	1	Quercus macrocarpa	Burr Oak	2" cal.	as shown
CKM	2	Acer platanoides 'Crimson King'	Crimson King Norway Maple	2" cal.	as shown
TOTAL:	3				
ORNAMENTAL TREES:					
ABP	5	Pyrus calleryana 'Autumn Blaze'	Autumn Blaze Pear	1.5" cal.	as shown
RBC	1	Malus 'Red Barron'	Red Barron Crabapple	1.5" cal.	as shown
TOTAL:	6				
SHRUBS:					
CBBB	5	Buddleia davidii 'Nanho Blue'	Compact Blue Butterflybush	5 gal.	5.5' o.c.
CWSC	21	Prunus besseyi 'Pawnee Buttes'	Creeping Western Sand Cherry	5 gal.	6' o.c.
MBSB	3	Symphoricarpos x doorenbosii 'Magic Berry'	Magic Berry Coralberry	5 gal.	30" o.c.
MSB	2	Symphoricarpos x doorenbosii 'Marlene'	Marlene Coralberry	5 gal.	30" o.c.
NWR	8	Rosa x 'Nearly Wild'	Nearly Wild Rose (Floribunda)	5 gal.	3.5' o.c.
PBB	3	Buddleia davidii nanhoensis 'Petite Plum'	Compact Purple Butterfly Bush	5 gal.	5.5' o.c.
RKOR	9	Rosa x 'Rador'	Rainbow Knock Out Rose	5 gal.	3' o.c.
TOTAL:	51				
PERENNIALS:					
HBJ	12	Persicaria affinis	Himalayan Border Jewel	4" pots	18" o.c.

- PLANT NOTES:**
- All plant material shall meet specifications of the American Association of Nurserymen (AAO) for number one grade. All trees shall be balled and burlapped or equivalent. All plant materials shall have all wire, twine or other containment materials, except for burlap, removed from trunk and root ball of the plant prior to planting.
 - Trees shall not be planted closer 10 feet to any sewer or water line. Tree planting shall be coordinated with all utilities - locations of all utilities shall be verified in the field prior to planting.
 - All shrubs shall be planted no less than 3' from any sidewalk or curb.
 - Grades shall be set to allow for proper drainage away from structures. Grades shall maintain smooth profiles and be free of surface debris, bumps, and depressions.
 - Developers shall ensure that the landscape plan is coordinated with the plans done by other consultants so that the proposed grading, storm drainage, or other constructions does not conflict nor preclude installation and maintenance of landscape elements on this plan.
 - All shrub beds adjacent to turf areas shall be edged with a perforated Ryerson or approved equivalent steel edger.
 - All shrub bed areas shall be mulched with a 4" layer of wood mulch. Install 3" of 2"-4" cobble to bed adjacent to structure. No fabric to be installed below perennials, groundcovers or ornamental grasses.
 - Prior to installation of plant materials, areas that have been compacted or disturbed by construction activity shall be thoroughly loosened; organic soil amendments shall be incorporated at the rate of at least three (3) cubic yards per 1000 square feet of landscape area.
 - All landscape plant materials and grass will be irrigated with an automatic system. See Irrigation Plan for details.
 - Contractor shall verify all material quantities prior to installation. Actual number of plant symbols shall have priority over the quantity designated.
 - Refer to the City of Boulder Design and Construction Streetscaping Standards for all work within public areas.

LANDSCAPE PLAN



Sheet Issue:

Date	Comments	Init.
SEPT. 16, 2013	Submittal	PS
DEC. 2, 2013	Re-submittal-1	PS
JAN. 3, 2014	Re-submittal-2	SG
JAN. 31, 2014	Re-submittal-3	SG

Prepared for:
Grove 3, LLC
2291 Arapahoe, Boulder, Colorado 80302
Contact: Gary Calderon 303-449-2900

2360 Grove Street, Boulder
Lot 3 & 4, Block 20 Cuivers Subdivision, City of Boulder, County of Boulder

Site & Use Review
LUR2013-00051
LUR2013-00065

Sheet Title
Landscape Plan
Sheet No.



**CITY
OF
BOULDER**

ATTACHMENT B

DEPARTMENT OF COMMUNITY PLANNING AND DEVELOPMENT

DEPARTMENT OF COMMUNITY PLANNING AND DEVELOPMENT
NOTICE OF DISPOSITION

You are hereby advised that the following action was taken by the Planning Department based on the purposes and intent of the Land Use Regulations as set forth in Section(s) 9-4, B.R.C. 1981, as applied to the proposed development:

NAME OF PROJECT: Siamese Plate
DESCRIPTION: Restaurant in an existing building.
LEGAL: See Attached Exhibit A
LOCATION: 1575 Folsom
APPLICANT: Sirichai Chanchaikulvet
TYPE AND NUMBER: Special Review SR-85-32
ZONING: TB-E
DECISION: Approval with conditions.

FOR CONDITIONS OF APPROVAL, SEE THE BACK OF THIS DISPOSITION.

The date of approval will be the date that the Notice of Disposition and the Mylar plan copy are signed by the Planning Director. Prior to approval, the Planning Department must receive the signed Mylar plan copy from the applicant, with Disposition conditions shown on the Mylar.

Approved on:

11-1-85
Date

By:

for Dennis Winkler
Planning Director
Dept. of Community Planning & Development

This decision may be appealed to the Planning Board by filing an appeal letter with the Planning Department within two weeks of the date of approval. If no such appeal is filed, the decision shall be deemed final on the date above mentioned.

Appeal to Planning Board expires: November 15, 1985
Date

JF/nb/DispoIII

P.O. BOX 791

BOULDER, COLORADO 80306

TELEPHONE (303) 441-3270

CONDITIONS OF APPROVAL

1. The approval is subject to the applicant's compliance with the approved site plan attached as Exhibit "B". The landscaping improvements shall be completed by June 1, 1986.
2. The approval is contingent on the applicant's maintaining a valid lease for 11 parking spaces within 500 feet of the restaurant.
3. Any signage for the restaurant use, including the leased parking area, shall be in compliance with the City sign regulation.

**CITY OF BOULDER
PLANNING BOARD AGENDA ITEM**

MEETING DATE: April 3, 2014

AGENDA TITLE:

Public hearing to consider application #LUR2014-00003 to amend the previous Site and Use Review approval (case #LUR2008-00083) for the **Washington Village** project, located at **1215 Cedar Avenue**, which allowed 33 dwelling units and 2,950 square feet of office/commercial space on the ground floor of a new building along Broadway and community facilities on the 3-acre site. The following modifications within the RH-2 (Residential High – 2) zoning district are requested:

- Addition of three dwelling units bringing the site total to 36 dwelling units (30 dwelling on the RH-2 side);
- Additional floor area within the Broadway Building and North Building totaling 5,059 square feet (1,152 square feet would be above grade with the remaining space in basements and storage).
- Reconsideration of the previously approved Use Review to change a condition of approval to reduce parking by one parking space in light of a proposed 300 square feet reduction in the commercial space; and
- A parking reduction of 11 percent.

Applicant: Adrian Sopher, Sopher Architects
Property Owner: Washington School Development Company, LLC

REQUESTING DEPARTMENT:

Community Planning & Sustainability
David Driskell, Executive Director
Susan Richstone, Deputy Director
Charles Ferro, Land Use Review Manager
Karl Guiler, Senior Planner/Code Amendment Specialist

OBJECTIVE:

1. Hear applicant and staff presentations
2. Hold public hearing
3. Planning Board discussion
4. Planning Board action to approve, approve with conditions, or deny the request to amend the Site and Use Review applications.

SUMMARY:

Proposal: Site Review Amendment and Use Review to allow modifications to previously approved Washington Village II Site and Use Review application # LUR2008-00083.

Project Name: Washington Village II
Location: 1215 Cedar Avenue
Size of Tract: 3 acres (130,710 square feet)
Zoning: RH-2 (High Density Residential) & RL-1 (Low Density Residential)
Comprehensive Plan: High Density Residential & Low Density Residential

KEY ISSUES:

Staff has identified the following key issues to help guide the board's discussion:

1. Does the proposed parking reduction meet the criteria of section 9-2-14(h)(2)(K), B.R.C. 1981?
2. With the addition of dwelling units and floor area, will the project continue to meet the Site Review criteria?
3. With the reduction of one commercial parking space, will the project continue to meet the Use Review criteria?

BACKGROUND:



Figure 1- Development context around 1215 Cedar (outlined)

The 1215 Cedar Avenue property is bounded by Broadway to the west, Cedar Avenue to the south, 13th Street to the east, and a mix of multi-family and single-family development to the north as shown above. It consists of three acres and is currently occupied by the vacant Washington Elementary School, which was closed in 2003 by the Boulder Valley School District (BVSD) due to school consolidation. Its closure prompted a comprehensive process to consider how the site should be developed. While there was initial interest from several groups to develop the site, the applicant, Wonderland Hill Development Company, was the only group that proceeded with a site design for city consideration.

The Washington School property was the subject of two Concept Plans and two Site and Use Review applications during the years 2007 through 2009. Site and Use Review application LUR2008-00083 was ultimately approved with conditions by City Council on February 25, 2009 after a referendum on the property on the previous Site Review and two call-up hearings by City Council.

The approved Washington Village II plans can be found as sheets 0.0 through A2.5 within the plan set in [Attachment C](#). The original notice of disposition containing the conditions of approval is found in [Attachment B](#).

The Site Review approval permitted redevelopment of the site with a total of 33 dwelling units (27 attached, some within the school building and others in new buildings on the west side of the site, and six detached on the east side of the site) and 2,950 square feet of office/commercial space on the ground floor of a new building along Broadway.

The residential uses were designed to comply with the underlying Boulder Valley Comprehensive Plan (BVCP) land use designation and zoning on the site, both of which are split between high density residential along Broadway and low density residential along 13th Street. The non-residential uses on the site required Planning Board approval of a Use Review application. Following call up of the Planning Board's approval of the project, City Council reviewed and approved the project on Feb. 25, 2009.

As projects are required to be substantially complete after a three year period (unless specific phasing is approved), the applicant had to request a one-year staff level extension in 2012 and a Planning Board level extension in 2013 considering the impact of the economic downturn. That extension was approved by the board on Feb. 7, 2013. The applicant also submitted a final plat application in 2011, which was approved. Staff has also been working with the applicant to come to agreement over how the outlot on the corner would be deeded over to the city to become a pocket park. While not explicitly required by the Site Review, dedication of the land for a park was something that was important for the neighborhood and the applicant expressed their willingness to design the project independent of the space for these purposes. Staff expects to have the land dedicated to the Department of Parks and Recreation within the coming months.

To date, the applicant has submitted construction drawings related to landscaping, drainage and utilities for the entire site and has constructed two of the carriage homes, three of the single-family homes, and is currently constructing both the duplex buildings and the new units within the historic school building. Building permits for the remaining single-family homes are either under review or issued. The only substantive work not yet under way is the Broadway Building and the North Building, which are the subject of the requests below:

REQUESTS:

Proposal to amend the previously approved Washington Village project (Site and Use Review application LUR2008-00083) as follows:

- Request to allow new partition walls within the Broadway Building resulting in three additional dwelling units increasing the total unit count from 27 to 30 on the RH-2 portion of the development.

- Request to add floor area in areas that would not change the massing of buildings, as follows:
 - 1,096 square feet within the confines of the Broadway Building (as previously noticed) and 471 square feet in subterranean storage space;
 - 3,436 square feet in a new basement under the North Building and 56 square feet above grade as basement access.
 - A total of 5,059 square feet is proposed. Roughly 30% of the floor area would be storage space and roughly 20% would be above grade.
- Request for Use Review approval to change a previous condition of approval, which required nine (9) parking spaces for commercial based on the square footage. The applicant requests eight (8) parking spaces as the land use code requires eight spaces based on the 300 square foot reduction to the commercial spaces from 2,950 square feet to 2,650 square feet.
- Request for approval of a parking reduction of 11%. Effectively, 49 parking spaces are provided where RH-2 zoning code parking standards require 55 parking spaces.

As discussed below, Planning Board is the granting authority in this case. The applicant's request and written statement are found in [Attachment A](#).

PROCESS:

- Site Review Amendment: A Site Review Amendment is required to permit the addition of units and floor area described above. Pursuant to section 9-8-3(b), "Density in the RH-1, RH-2, RH-3 and RH-7 Districts," B.R.C. 1981, Planning Board review and approval is required to approve the additional three units (see code excerpt below). By-right density would permit 24 dwelling units and up to 46 dwelling units can be requested per 9-8-3(b), B.R.C. 1981. Twenty-seven dwelling units on the RH-2 side were previously approved.

Code excerpt: (b) Additional Density in the RH-2 District: In the RH-2 zoning district, the planning board may reduce the minimum lot area of 3,000 square feet per dwelling unit to 1,600 square feet of lot area per dwelling unit pursuant to site review approval.

- Use Review: A new Use Review is required to alter a previous condition of approval that required nine (9) parking spaces for commercial uses. Nine spaces were required because of the parking requirements that require one parking space for every 300 square feet. With 2,965 square feet previously approved, this equates to nine parking spaces. The applicant is proposing to reduce the commercial space by 300 square feet and as a result, only eight (8) parking spaces are required. Pursuant to section 9-2-15(d)(1), "Use Review," B.R.C. 1981, only Planning Board may approve non-residential uses in a residential zoning district (see code excerpt below). Therefore, the amendment to the condition of approval to permit eight (8) parking spaces requires Planning Board approval.

Code excerpt: (1) The city manager will review applications for use review of a nonresidential use in residential zoning districts, or attached and detached dwelling units or a residential use in a P district, and will submit a recommendation to the planning board for its final action pursuant to subsection [9-2-7\(b\)](#), B.R.C. 1981.

- Parking reduction: An 11 percent parking reduction is requested. Parking reduction requests on residential properties must be processed as part of Site Review. Typically, parking reductions under 50 percent do not require Planning Board review; however, as the board is required to act on the two requests above, the reduction is under the board's purview. As the parking reduction is the largest key issue of the application, it will be discussed first in the 'Analysis' section below.

ANALYSIS

1. Does the proposed parking reduction meet the criteria of section 9-2-14(h)(2)(K), B.R.C. 1981?

When the project was originally approved, the previous RH-2 parking standards applied whereby one parking space was required for the first 800 square feet of a unit and one additional parking space was required for every 300 square feet (or portion thereof) afterwards. This was one of the highest parking requirements in the city and was generally meant to apply to RH-2 areas in proximity to the university where there were instances of students doubling up within rooms. This restrictive standard often resulted in requests for parking reductions. For the Washington Village II project, a parking reduction of over 50 percent was required and was approved in 2009.

This parking standard was amended in 2012 to be more synonymous with other RH zones with calculations based on bedroom counts. The current requirement is outlined below:

1-bedroom units	1 parking space
2-bedroom units	1.5 parking spaces
3-bedroom units	2 parking spaces
4 or more bedroom units	3 parking spaces

When applied to the Washington Village project, the following number of residential spaces is required:

Unit type	Unit count	Requirement	Total
1-bedroom units	6 units	1 parking space	6 spaces
2-bedroom units	18 units	1.5 parking spaces	27 spaces
3-bedroom units	6 units	2 parking spaces	12 spaces
Total Residential parking	30 units	NA	45 spaces
<i>Extra spaces for additional future bedrooms</i>	NA	NA	2 spaces
Total Commercial parking	NA	1 space per 300 sf	8 spaces
Total required spaces	30 units & 2,650 sf of commercial	See above	55 spaces
Total spaces provided			49 spaces

The current requirement is for a total of 55 parking spaces on the RH-2 side. Previous requirements required 105 spaces for both residential and non-residential. The applicant is proposing 49 on-site parking spaces. This amounts to an 11 percent parking reduction.

Staff finds that the parking reduction criteria are met based on the following factors:

- The site is located along a multi-modal corridor with frequent bus service.
- On-street parking is along two frontages of the site (i.e., Cedar & 13th).
- A car share program will be implemented on site.
- Bike parking in excess of that required will be available on site.

- The development would operate as a cohousing community where sharing of vehicles may be more likely than in a typical community.
- General ease of walkability to destinations like Community Plaza or downtown.
- Parking requirements may count rooms that will not be occupied as bedrooms despite meeting the definition of bedroom and may not represent the need for vehicles on site.
- While the unit count would increase by three, the bedroom count (as compared to the previous approval) would only increase by two bedrooms.

Further, the submitted parking and traffic study prepared by LSC Transportation Consultants, Inc. (see [Attachment F](#)) shows that the parking will be accommodated on the site with a peak demand of 47 on-site parking spaces where there will be 49 parking spaces available on the site. Table 1 of the analysis indicates that peak time occurs during the evening hours where a total of 47 parking spaces would be necessary. Table 1 indicates a minimal parking need for commercial during evening hours based on ITE (Institute of Transportation Engineers) numbers, but as the offices are required to be closed during this time period, there would generally be a surplus of spaces available to residential at that time. During the day demand would be 35 parking spaces (8 for the office reserved during business hours and 27 on-site spaces for residential) where 41 spaces would be available. Based on these conclusions and the ability to meet the criteria below, staff finds that the parking reduction can be approved.

Responses to the parking criteria are below for reference:

(ii) Criteria: Upon submission of documentation by the applicant of how the project meets the following criteria, the approving agency may approve proposed modifications to the parking requirements of [section 9-9-6](#), "Parking Standards," B.R.C. 1981 (see tables 9-1, 9-2, 9-3 and 9-4), if it finds that:

a. For residential uses, the probable number of motor vehicles to be owned by occupants of and visitors to dwellings in the project will be adequately accommodated;

The submitted parking study dated March 13, 2014 indicates that peak residential need during the peak time (i.e., night time) would be 42 parking spaces where 49 spaces would be available. During the day, the peak need would be 35 parking spaces, where 41 spaces would be available to residential (8 spaces would be allotted to commercial until 4pm by condition of approval). Based on these conclusions, the probable number of motor vehicles to be owned by occupants of and visitors will be adequately accommodated.

In addition, the applicant is required to employ TDM strategies to encourage residents to own less or have no cars. The TDM prepared by the applicant has been reviewed and found to be appropriate in its provision of on-site bicycle parking (which exceeds requirements), a bike pool and car share program. The TDM also indicates that incentives, such as the provision of transit passes, would be allotted to residents that do not have a car as an incentive to free up parking spaces on the site and to reduce vehicles trips from the site, which is the principal goal of TDM strategies.

b. The parking needs of any nonresidential uses will be adequately accommodated through on-street parking or off-street parking;

Eight (8) off-street parking spaces would be allotted to the non-residential uses on the site. They would be restricted to non-residential uses for the majority of the day (8am to 4pm). Eight spaces meet the required number of spaces for the proposed office use. Further, office spaces would be required to close at 6pm to ensure spaces are available during times of peak need for residential.

c. A mix of residential with either office or retail uses is proposed, and the parking needs of all uses will be accommodated through shared parking;

The submitted parking study prepared by Transportation Consultants, Inc. and dated March 13, 2014 indicates a peak need of 47 parking spaces for residential and office uses on the site. With 49 on-site parking spaces available and considering the conditions of approval referenced below, the parking needs of both uses, which would rely on shared parking, would be met on the site.

-A condition of approval requires that the 8 spaces allotted to office uses during daytime hours be open for residential use after 4pm.

-A condition of approval restricts the office use to 8am and 6pm, thus eliminating any office need between 10pm and 8am.

d. If joint use of common parking areas is proposed, varying time periods of use will accommodate proposed parking needs; and

Parking for office uses would be reserved during the hours of 8am and 4pm. After 4pm, the parking spaces would be available for residential uses, which enable the project to better meet the peak demand during evening hours. Office spaces would be required to close at 6pm to ensure spaces are available during times of peak need for residential.

e. If the number of off-street parking spaces is reduced because of the nature of the occupancy, the applicant provides assurances that the nature of the occupancy will not change.

Should the occupancy of the development change such that its occupancy would require more parking than a standard residential development, a reconsideration of the Site Review and parking reduction would be required. The applicant is aware of this condition.

2. With the addition of dwelling units and floor area, will the project continue to meet the Site Review criteria?

Yes. The Site Review criteria require high quality design and are meant to avoid undue impact, ensure neighborhood compatibility and assess consistency with surrounded context. With three additional dwelling units and floor area, the following Site Review criteria are the most relevant to this evaluation:

- Boulder Valley Comprehensive Plan (BVCP) compliance (section 9-2-14(h)(1), B.R.C. 1981)
- Parking (section 9-2-14(h)(2)(K), B.R.C. 1981) [see Key Issue no. 1]
- Building Design (section 9-2-14(h)(2)(F), B.R.C. 1981)

As there are no other proposed changes that impact compliance with the other criteria, the full list of criteria is not discussed in depth in this analysis, but can be found within [Attachment D](#) with full staff responses for reference.

BVCP compliance

Staff continues to find the project, on balance, consistent with BVCP policies, including but not limited those on compact development pattern (BVCP 2.03, Compact Development Pattern), sensitive infill and redevelopment (BVCP 2.30, Sensitive Infill and Redevelopment), mixed-use (BVCP 2.16, Mixed Use and Higher Density Development), affordable housing (BVCP 7.02, Permanently Affordable Housing), mixture of housing types (BVCP 7.06, Mixture of Housing Types), and historic preservation (BVCP 2.28, Historic Preservation/Conservation Tools). Further, the density continues to be consistent with that permitted per the BVCP High Density Residential land use designation and the RH-2 zone.

The relevant criteria are below with the changes shown underlined:

(1) Boulder Valley Comprehensive Plan:

(A) The proposed site plan is consistent with the land use map and the service area map and, on balance, the policies of the Boulder Valley Comprehensive Plan.

The Washington Village project has been found to be compatible with the policies of the comprehensive plan, including but not limited to, policies related to compact land use pattern, infill development, mixed-use, affordable housing, mixture of housing types, historic preservation, and sensitive infill and development.

(B) The proposed development shall not exceed the maximum density associated with the Boulder Valley Comprehensive Plan residential land use designation. Additionally, if the density of existing residential development within a three-hundred-foot area surrounding the site is at or exceeds the density permitted in the Boulder Valley Comprehensive Plan, then the maximum density permitted on the site shall not exceed the lesser of:

(i) The density permitted in the Boulder Valley Comprehensive Plan, or ...

The density permitted in the BVCP for the western portion of the site is 14 units or greater. The subject project would be approximately 17.6 dwelling units per acre (du/ac) on that side- an increase from the previous approval which was for 15.8 du/ac. The applicant has requested additional density pursuant to Section 9-8-3(d), B.R.C. 1981. Based on the ability of the project to meet the other aspects of these criteria, the amount of density is appropriate on the Broadway multi-modal corridor. On the eastern portion where low density residential development is intended, the density would be approximately 4.6 dwelling units per acre, which is within the 2 to 6 units per acre range for Low Density Residential.

Building design

With respect to building design and massing, the additional units and floor area will not result in diminished building quality and will not increase impacts above what was previously assessed. This is because the new units and floor area would be contained entirely within the approved massing of buildings and underneath buildings as shown in Figures 2 and 3 below:

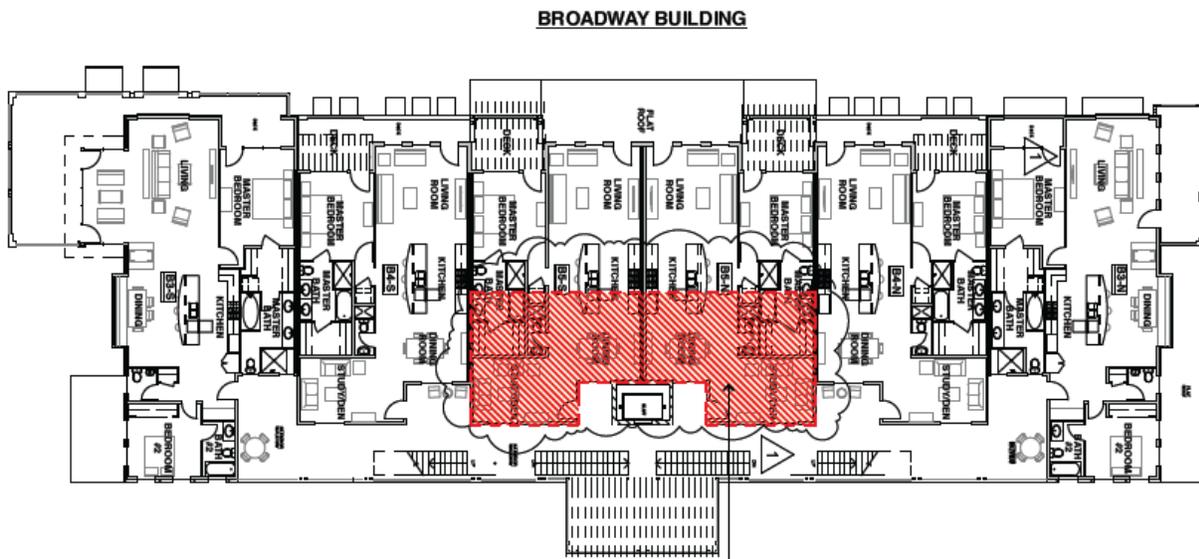


Figure 2- Proposed Broadway Building additions within the approved footprint and massing of the building.

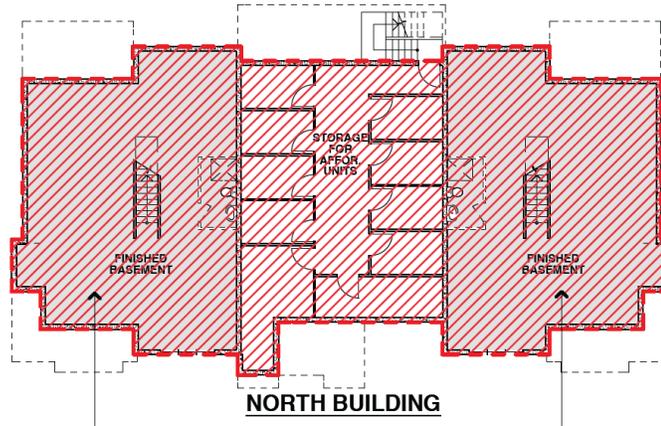


Figure 3- Floor area proposed within the basement of the North Building.

As the additional units and floor area for the development would be entirely within the massing, footprints and basements of buildings, there would be no additional perceivable bulk. While additional floor area is proposed, the massing of buildings would actually decrease as a result of design changes to the Broadway Building. For instance, the Broadway Building has been reduced in height by 5 feet per the previous condition of approval. The massing is less, because the reduced height was achieved by sinking the building and subterranean parking deeper into the ground. This design enables the retention of the hip roof as opposed to what may have been a perceivably taller building with a flat roof. Figure 4 depicts the change:

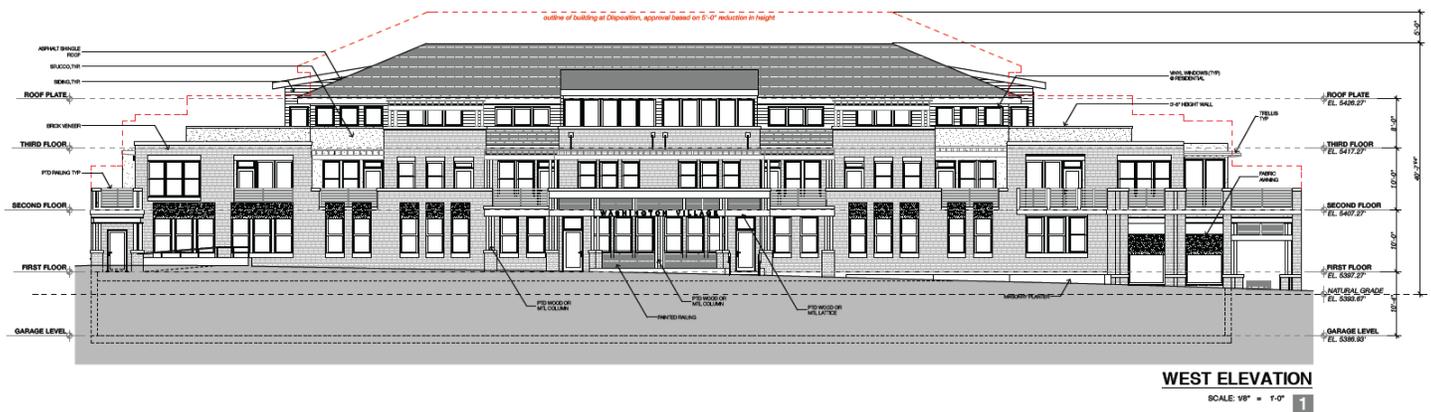


Figure 4- Approved Broadway Building design with overall height reduced by 5 feet.

For the reasons outlined above and within the criteria responses, staff continues to find the project consistent with the Site Review criteria.

(F) Building Design, Livability, and Relationship to the Existing or Proposed Surrounding Area:

(i) The building height, mass, scale, orientation, architecture and configuration are compatible with the existing character of the area or the character established by adopted design guidelines or plans for the area;

This factor is met as follows:

- The height of the majority of the proposed buildings on the site would conform to the 35-foot height limitation for the RH-2 and RL-1 districts. In fact, most of the multi-family buildings would not exceed 30 feet. The Broadway Building would be built at 41 feet (per previous condition of approval).
- The mass of the interior buildings would be appropriate, since they are all well articulated and would not appear imposing, since most are only two stories and those over two stories are of similar stature to large single family homes, which exist in the neighborhood.
- Additional floor area proposed for the development would be within the massing, footprints and basements of

buildings and would not contribute to any additional perceivable bulk. While additional floor area is proposed, the massing of buildings would actually decrease as a result of design changes to the Broadway Building. For instance, the Broadway Building has been reduced in height by 5 feet per the previous condition of approval. The massing is less, because the reduced height was achieved by sinking the building and subterranean parking deeper into the ground. This design enables the retention of the hip roof as opposed to what may have been a perceivably taller building with a flat roof.

- Aside from the historic school, the Broadway Building would be the most massive on the site. Its size would be noticeable by virtue of its length along Broadway at 194 feet. This compares to a roughly 170 foot length of the Broadway Brownstones project to the south. The length is not considered out of context since it is mostly two story massing along the streetscape with relief provided by upper floor setbacks. Where the first story is at 17 feet at its closest point to the Broadway lot line (this correlates to setbacks of buildings across the street), the second story would be set back 22 feet (which matches the setbacks of the Broadway Brownstones one block down) and the third floor would be set back 34 feet, which is 9 feet more than the required setback on that side. The entire length of the building is well articulated and will not present any more mass than the Broadway Brownstones building along Broadway. Concentrating massing along Broadway is considered a more practical way of accommodating the density on the site, as the alternative could present greater impacts to the single-family neighborhood on the project's east side.
- The orientation and configuration of buildings on the site are found compatible, in that all would orient to the three streetscapes (a condition is proposed that would require this) and the configuration of the buildings are appropriate to the different contexts on each side of the site. For example, buildings on the RH-2 side have larger footprints similar to other multi-family projects along the high density residential Broadway corridor, whereas moving eastward on the sites, the form and footprints of buildings decrease to match the RL-1 single-family character on the east side.

(ii) The height of buildings is in general proportion to the height of existing buildings and the proposed or projected heights of approved buildings or approved plans or design guidelines for the immediate area;

The heights of the buildings, as noted above, have been found to be compatible with the height of existing for the immediate area and other buildings proposed for the site. The existing Washington School building is over the height limit at nearly 50-feet and would continue to be the most prominent building on the site. The next tallest building would be the Broadway Building, which has been limited to no taller than 41 feet per previous condition of approval. The applicant has already adjusted the design of the building to meet this height limitation while also retraining a hip roof design lending to more consistency in form to adjacent buildings on and around the site. The proposed building at that height will be in general proportion to the height of existing buildings considering the school building the height of Broadway Brownstones and the BHP building at 3120 Broadway, which were both built to a height of 41-feet.

Site Design

Lastly, conversations related to permeability were raised during the original Site Review in 2009 due to the length of the Broadway Building (discussed in the criteria above) and the perception that it be a barrier to pedestrian permeability through the site. This was considered important because of BVCP policy 2.37, Enhanced Design for Private Sector Projects which states:

Permeability. Projects should provide multiple opportunities to walk from the street into projects, thus presenting a street face that is permeable. Where appropriate, they should provide opportunities for visual permeability into a site to create pedestrian interest.

To address this, a pathway entering the site on the north end of the Broadway Building was added, which would enable persons to walk into the site and potentially continue on pathways through the site towards 13th Street to the east. This was considered consistent with the BVCP policy above. Since the approval, the applicant has raised concerns about having non-residents entering the site as if it were a public trail and neighbors at Red Arrow Townhomes to the north have also raised privacy concerns. To address this, the applicant has changed the pathway surface from concrete to sandstone in efforts to make the path look less like a city public multi-use path and more as a private path while at the same time not prohibiting entry. Staff finds that this design solution is an acceptable compromise to ensure permeability while also making it clear that persons would be entering a private site. However, as part of this application the applicant has reconfigured the pathway towards an exterior elevated

walkway in the Broadway Building rather than having a direct pathway to the open space within the site. Staff is concerned that this diminishes the more fluid permeability offered in the previous design. Staff understands that this is a delicate balance of achieving overall design quality while avoiding disproportionate intrusion into the site by those that do not live there. Nevertheless, staff believes that the sandstone surfacing achieves this balance and thus, staff is proposing a condition of approval (see 3(b) on page 12) that would require the previous path configuration to meet the intent of BVCP policies and the Site Review criteria.

3. With the reduction of one commercial parking space, will the project continue to meet the Use Review criteria?

The full language of the Use Review criteria of section 9-2-15(e), B.R.C. 1981 and staff responses to each are found within [Attachment E](#). Use Reviews can be approved if the criteria (summarized below) are met:

- Consistency with zoning
- Use meeting specified rationale
- Compatibility with surroundings
- Minimal impact to infrastructure
- Consistency with character of the area

Staff continues to find the proposed establishment of commercial uses on the site along Broadway to be consistent with the criteria, as the uses would create a mix of uses along a multi-modal corridor (consistent with city policies) and to an extent that is consistent with the size and location of other office uses in the immediate area along Broadway. Further, the uses are restricted, by previous condition of approval, to Professional and Technical Offices, which typically have fewer visits from clients as compared to other types of office uses.

Staff finds that the modification to the condition relative to the amount of parking allotted to the commercial use is also supportable as the specified amount (i.e., 8 parking spaces) will continue to be reserved for commercial uses and in an amount that meets the parking regulations of section 9-9-6, "Parking Standards," B.R.C. 1981, for the proposed uses. The condition only requires alteration because of the applicant's decision to reduce the commercial space and, in turn, the parking requirement.

Page 12 contains the updated Use Review conditions of approval, which reflect the readjusted number of parking spaces and the reduced commercial space size (i.e., 2,650 square feet).

PUBLIC COMMENT AND PROCESS:

Required public notice was given in the form of written notification mailed to all property owners within 600 feet of the subject site and a sign posted on the property for at least 10 days. All notice requirements of Section 9-4-3, "Public Notice Requirements," B.R.C. 1981 have been met. Staff has also contacted those neighbors that requested to be notified of any upcoming meetings or submittals and also sent mailed notice of the Planning Board hearing.

Staff also provided mailed public notice of the hearing to properties within 600 feet of the subject site and emailed notice of the public hearing to interested parties and the Washington School Neighborhood Association.

Staff has received a number of email communications from the neighborhood in support and against the proposed changes. In summary, comments in support of the project cite the addition of smaller units, which would be relatively more affordable than the previous larger units and how making the project more aligned with the current market demand would enable a swifter completion of the project. Members of the public in opposition to the proposed changes raise concerns about inadequate parking and frustrations on the ongoing construction on the site. One neighbor within the Red Arrow townhome neighbor continues to object to the location and height of the Broadway Building and requests that it be moved further south to minimize shadow impacts. Public comments are found within [Attachment G](#).

STAFF RECOMMENDATION:

Staff recommends that the Planning Board approve Site and Use Review application # LUR2014-00003 with the conditions listed below and adopting the findings provided in this memorandum and its attachments.

PROPOSED CONDITIONS OF APPROVAL

1. The Applicant shall ensure that the **development shall be in compliance with all approved plans and documents**, including the approved plans entitled Washington Village II dated November 17, 2008 and January 8, 2009, the amended plans entitled Washington School Developments dated January 6, 2014, the written statement dated September 12, 2008 as amended in the written statement dated January 6 and March 14, 2014, and the conditions of approval within the Notice of Disposition dated February 25, 2009 on file in the City of Boulder Planning Department, except to the extent that the development may be modified by this approval.
2. The Applicant shall **comply with all previous conditions** contained in any previous approvals, except to the extent that any previous conditions may be modified by this approval, including, but not limited to, the following: Development Agreement recorded at Reception No. 03058023 on February 10, 2010.
3. Prior to a building permit application for the Broadway Building and the North Building, whichever occurs first, the Applicant shall submit a Technical Document Review application or applications for the following items, subject to the approval of the City Manager:
 - a) **Final architectural plans**, including materials and colors, to insure compliance with the intent of this approval and compatibility with the historic school and surrounding area.
 - b) A **final site plan** illustrating the approved site configuration for the environs of the Broadway Building and North Building, including the original path configuration north of the Broadway Building, consistent with the approved site plans dated November 17, 2008 and January 8, 2009 as well as the curb ramp to be constructed for the Broadway pedestrian signal.
 - c) A **detailed landscape and tree protection plan**, including size, quantity, and type of plants existing and proposed; type and quality of non-living landscaping materials; any site grading proposed; and the proposed irrigation system, to insure compliance with this approval and the City's landscaping requirements. The plans must conform to the preliminary Tree Preservation Plan and arborist assessment attached to the approved plans. Any construction that affects the existing trees, including but not limited to foundations, grading, impervious surfaces, and the erection of walls within the vicinity of trees to be preserved that result in unanticipated damage to existing trees, shall require mitigation pursuant to the detailed landscape and tree protection plan.

- d) A **detailed outdoor lighting plan** showing location, size, and intensity of illumination units, showing compliance with Section 9-9-16, B.R.C. 1981.
 - e) A **detailed parking plan** showing the arrangement, locations, dimensions, and type of parking stalls (including any areas of the site for bicycle parking or reserved for deferred parking) to insure compliance with this approval and Section 9-9-6, B.R.C. 1981. This plan shall accommodate three additional deferred parking spaces on the RL-1 portion of the lot. The three spaces, if constructed, shall be signed appropriately to designate for multi-family residents or guests, shall not require the removal of any trees, and shall be surfaced with permeable pavement and/or pavers consistent with the City of Boulder Design and Construction Standards.
 - f) A **detailed shadow analysis** to insure compliance with the City's solar access requirements of Section 9-9-17, B.R.C. 1981.
4. Prior to a building permit application for the Broadway Building or North Building, the Applicant shall **dedicate to the City**, at no cost to the City, a public access easement for 1 foot area along the edge of the transit shelter pad on Broadway, meeting the City of Boulder Design and Construction Standards, as part of Technical Document Review applications, subject to the approval of the City Manager.
 5. Prior to a building permit application for the Broadway Building or North Building, whichever occurs first, the Applicant shall submit a **financial guarantee**, in a form and amount acceptable to the Director of Public Works, to guarantee those items proposed in the Applicant's Transportation Demand Management (TDM) plan, including transit passes.
 6. Prior to a building permit application for the Broadway Building or North Building, whichever occurs first, the Applicant shall submit a **financial guarantee**, in a form and amount acceptable to the Director of Public Works, in an amount equal to the cost of providing eco-passes to the employees who work in the commercial office space for a period of three years after the issuance of a certificate of occupancy for the tenant finish of the commercial space.
 7. Prior to Certificate of Occupancy for any of the three single-family homes closest to the north lot line on the RL-1 portion of the property, the Applicant shall ensure **that a landscaped buffer of no less than 5 feet between the existing chain link fence location and the access drive** shall be added and planted with shrubs and small to medium sized trees to increase compatibility and buffering between the subject site and the single family residence to the immediate north. Also, to enhance compatibility, the Applicant shall ensure that the existing chain link fence be replaced with a wood fence of 100% opacity, at least 6 feet in height, and otherwise consistent with Section 9-9-15, B.R.C. 1981, in the exact location as the chain link fence and without detriment to existing vegetation. Further, the addition of the landscaped buffer shall not affect the size and location of the southeastern open space.

CONDITION OF APPROVAL – USE REVIEW

1. The Applicant shall ensure that the **development shall be in compliance with all approved plans and documents** including the approved plans entitled Washington Village II dated November 17, 2008 and January 8, 2009, the amended plans entitled Washington School Developments dated January 6, 2014, the written statement dated September 12, 2008 as amended in the written statement dated January 6 and March 14, 2014, and the conditions of approval within the Notice of Disposition dated February 25, 2009 on file in the City of Boulder Planning Department, except to the extent that the development may be modified by this approval

2. The Applicant shall **comply with all previous conditions** contained in any previous approvals, except to the extent that any previous conditions may be modified by this approval, including, but not limited to, the following: Development Agreement recorded at Reception No. 03058023 on February 10, 2010.
3. The Applicant **shall not expand or modify the approved use**, except pursuant to Subsection 9-2-15(h), B.R.C. 1981.
4. The Applicant shall ensure that the **approved office uses** are operated in compliance with the written statement dated September 12, 2008 as amended in the written statement dated January 6 and March 14, 2014, pursuant to the following restrictions:
 - a) Professional and Technical Offices are approved in the non-residential space along Broadway are not to exceed 2,650 square feet in size.
 - b) The office uses shall be closed from 6:00 p.m. to 8 a.m., Monday through Friday.
 - c) Eight (8) parking spaces shall be designated within the Broadway Building for the office uses during the hours of 8:00 a.m. to 4:00 p.m. and shall otherwise be available for residential uses outside these hours.

ATTACHMENTS:

- A. Applicant's written statement dated January 6, 2014
- B. Notice of Disposition dated Feb. 25, 2009
- C. Proposed plans dated January 6, 2014 (also contains the previously approved plans)
- D. Staff responses to the Site Review criteria (includes parking reduction criteria)
- E. Staff responses to the Use Review criteria
- F. Studies related to parking and traffic dated March 13, 2014 prepared by LSC Transportation Consultants, Inc.
- G. Public comments



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www.sopherarchitects.com
 O: 303 444-6902 asopher@sopherarchitects.com
 1919 14th STREET, SUITE 610 BOULDER, CO 80302

MEMORANDUM

To: Charles Ferro, Land Use Review Manger, CITY OF BOULDER
 Karl Guiler, Case Manager

From: Adrian Sopher, SOPHER ARCHITECTS, LLC

Project: WASHINGTON VILLAGE II – Site Plan Review Amendment

Date: 6 January 2014

Re: SPR AMENDMENT SUBMITTAL & USE REVIEW – WRITTEN STATEMENT

SUMMARY

Brief description of approved Site Plan Review Disposition – basis for proposed amendment

Washington Village II was approved by City Council on 25 February 2009. The approval was for the construction of a residential co-housing community on the site of the 3-acre Washington School property. The project incorporated:

- The landmarking of the original school structure and its subsequent renovation
- The construction of 27 multi-family residences on the western 57% of the site, which is zoned RH-2
- The construction of 6 single-family residences on the eastern 43% of the site, which is zone RL-1
- The creation of a pocket park on the far eastern portion of the site, to be dedicated to the city for public use.

For a detailed description of the project, please refer to the project Disposition dated 25 February 2009.

the RH-2 zone & Washington Village

The approval of the Washington Village project was a long, arduous, and contentious process. This is a matter of public record and public memory. The reasons for this are long and varied. But upon completion of that process, it was apparent to city staff, the planning board, and to council, that some of that contention could be ascribed to confusion created by the nature of the RH-2 zone itself.

The history of the zone and its evolution under many names, was originally established in the Goss-Grove neighborhood to minimize the impact of many students sharing small residences with insufficient off-street parking to support them. The zone as then defined, also strictly limited the amount of units, the size of units, and the overall Floor Area Ratio on any RH-2 site.

For reasons that are not obvious, the zone designation was applied to a few areas around town, some of which had no relationship to the issues for which the zone was originally created, and in particular, along a portion of the Broadway corridor adjacent to Washington School.

Consequently, when sites were redeveloped in this area, a Site Review process became an ordinary occurrence, simply to get relief from:

- extremely limited allowable floor areas (a maximum of .25, or .5 with Site Review)

- extremely high parking requirement (4 cars for a 1400 square foot residence, which could be reduced significantly with Site Review)
- limits on the amount of total allowable units on site.

Please note that even with Site Review approval, the increased limit from .25 to .5 FAR for a high density multi-family residential zone is very low, particularly when we consider that a .5 FAR is what single-family residential zone was reduced to in the RL-1 zone under Compatible Development. And likewise, to meet the parking standards which apply to all other high density residential zones in the city, required a 52% parking reduction for this project.

Requesting and receiving reasonable relief from these requirements added confusion to a project that already had a high degree of neighborhood scrutiny, because of the school board’s decision to remove the school and its grounds from the public realm.

Subsequent code changes to the RH-2 zone and its affect on Washington village

Before Washington Village, the RH-2 zone had already been identified as being an extremely difficult zone. The unnecessary added confusion it created in such a visible project made its review and revision a higher priority. Within three years of Washington Village II’s approval, the RH-2 provisions were revised to become more in keeping with the requirements of other high density residential zones in the city.

The code changes adopted include:

- ***the standard parking requirements as they apply to all other high density residential zones***
- ***the removal of FAR limits, replaced by requirements for open space minimums***
- ***a far less stringent limit on total number of units on the site.***

Differences between approved Site Plan Review Disposition and the project today

Since Washington Village originally went through a discretionary review and public hearing, staff determined that with the changes to the RH-2 zone, some aspects of the project could be modified under the Minor Modification provisions and staff review process. But these were limited to those items that did not affect the outlines of the original approval.

Therefore, by means of three separate applications and staff reviews, we were able to increase the total floor area of the multi-family portion of the site by up to 10%, as long as we did not appreciably modify the SPR approved scale, character, general footprint and setbacks, etc. of the original approval.

In the process of applying for these changes, we also reduced by five feet, the overall height and mass of the Broadway Building on the western part of the site. In doing so, we likewise reduced the impact of the solar shadow on our neighbors to the north, while maintaining the overall look and feel of the structure as a whole.

This was done by:

- lowering the below-grade garage slab elevation by one foot
- changing the structural system of the garage deck to a post-tensioned concrete slab, and thereby reducing its structural depth
- because of the change in the above-mentioned deck, we were able to reduce the spans of the wood framing, thereby reducing the depth of its construction at each framed floor level.
- These changes were all incorporated into the minor modification applications and approved by staff.

Differences between approved project and Proposed Amendment

The current application requests the following:

- an additional 5059 square feet in total area on the multi-family portion of the site, with 77% of this addition being underground (basement) and 31% being storage space. Only 1152 square feet will be above ground (1096 on the second floor of the Broadway Building and 56 square feet on the first level entries of the North Building end unit townhomes.)
- a change from a total of 27 units to 30 units in the multi-family portion of the site, to be accomplished by

- reducing the size of currently approved units
- a reduction of 300 square feet in the amount of previously approved office use
- because of the reduction in commercial/office square footage we are also reducing the amount of parking dedicated to the commercial/office use from 9 to 8 (requires a reconsideration of the previously approved Use Review.)
- a reduction in the overall size of individual residential units.
- a parking reduction of 11%

Impacts of the proposal:

- all of the additional area being proposed in the Broadway building will be within the existing footprint and massing of the previously approved Broadway Building, and therefore has no impact on any neighboring property (1096 square feet will be within the approved structure on the second floor, and 471 square feet of storage in the underground garage.)
- most (99%) of the additional area being proposed in the North building will be underground, within the existing footprint and massing of the previously approved North Building, and therefore has no impact on any neighboring property. (3436 square feet will be within the approved crawl space underground, changing the originally approved crawl space to a finished basement and storage area. 56 square feet will be at the entries to the end unit townhomes.)
- the parking reduction of 11% is well within reason for a site that is on a major bus route, within one block of a retail plaza, includes a car share/bike share program, and will participate in Neighborhood and Commercial EcoPass programs. (The previous approval LUR 2008-00083 was granted a 55% parking reduction through the previous code.)
- all changes proposed have no impact on the building perimeters, have no impact on the street, and have no impact on neighboring properties.
- with a total of 33 units, now increasing to 36, we already exceed our 20% Inclusionary Zoning requirement, with existing provision of 9 affordable units, all on-site.
- the addition of units within the already approved massing of the Broadway Building enables smaller and more affordable market rate units, better serving the needs of Boulder residents (empty nesters interested in downsizing and younger singles and couples) that are interested in living in a walkable urban location.

additional information – Boulder valley comprehensive plan policy conformance

In addition to what has already been incorporated into the approved Washington Village II project, the current SPR Amendment application is in keeping with the following BVCP policy goals...

***Local Support for Community Housing Needs
Housing Choices***

Growth and Community Housing Goals

Social Equity

Consistent with BVCP Policy...

7.01 Local Solutions to Affordable Housing

7.06 Mixture of Housing Types

7.09 Housing for a Full Range of Households

7.13 Integration of Permanently Affordable Housing

8.03 Equitable Distribution of Resources

8.04 Addressing Community Deficiencies

WV is committed to providing a long-term solution to the many and diverse issues surrounding the construction of mixed income housing options, here specifically based on a community oriented co-housing model, with on-site low and moderate rate affordable units. The goal is to accommodate housing options to a broad cross section of its constituents, in fostering community.

Energy and climate

Consistent with BVCP Policy...

4.04 Energy-Efficient Land Use

4.05 Energy-Efficient Building Design

The Washington Village project has consistently endeavored to use the most energy efficient materials and practices throughout the project's construction, including high-performance glazing, exceeding the requirements for building insulation, geo-thermal heating and PV panels throughout the project.

Transportation

Consistent with BVCP Policy...

6.02 Reduction of Single Occupancy Auto Trips

The Washington Village project has consistently attempted to create a community with minimal requirements for SOV usage, that shares resources and is eminently suited, by population and intention, towards minimizing auto dependency. The co-housing model is well-suited to sharing resources on a daily basis, simply by virtue of its lifestyle model. The community is also incorporating a car share program to further decrease the demand of cars/ parking on site.

zoning

Community Identity/Land Use Pattern

Neighboring structures

Mixed Use and Higher Density Development

Urban Design Linkages

Consistent with BVCP Policy...

2.03 Compact Development Pattern

2.09 Neighborhoods as Building Blocks

2.15 Compatibility of Adjacent Land Uses

2.16 Mixed Use and Higher Density Development

2.21 Commitment to a Walkable and Accessible City

The Washington School site is located on one of the most highly traveled corridors on the city. Consequently maximizing the the scale and density of use along that corridor, minimizes the impacts on adjacent single family residences. Likewise by increasing the intensity of use along those corridors highly serviced by multiple modes of transit, we are able to minimize the necessity for single vehicle use. Further, the immediate access within a 10-minute walk to all essential services to residents of the site make this a key location to maximize density in the city. And we have done so here, without significant long-term impact to the adjacent single family neighborhoods, while at the same, buffering the adjacent neighborhood from the Broadway activity.

Community Conservation

2.24 Preservation of Historic and Cultural Resources

2.27 Eligible Historic Districts and Landmarks

The preservation, landmarking, and renovation of the Washington School Building has always been at the center of the Washington Village project, and it remains the identifying heart and center of the co-housing community.

Design Quality

Consistent with BVCP Policy...

2.30 Sensitive Infill and Redevelopment

2.32 Physical Design for People

2.37 Enhanced Design for Private Sector Projects

Altogether, we have endeavored to create a pocket neighborhood on a co-housing model, that fully integrates the existing Washington School site into one that meets the possibilities of a heavily traffic multi-modal corridor, and is sensitive to the adjacent single-family residential neighborhood

additional information – Site Design

- The proposed SPR amendment makes no change to the approved site design because all of the changes are within the existing building footprint, massing and roof area. Therefore...
- Open Space – there are no changes to the open space areas on site

- Landscape – there are no changes to the landscape design on site
- Circulation – there are no changes to the circulation layout on site
- Parking – the proposed amendment is requesting a minimal 11% parking reduction which is well within reason for this particular location.
- Building Design, Livability, and Relationship to Existing Surrounding Area – the proposed amendment has no impact on the surrounding area or other issues related to this topic
- Solar Siting – the proposed amendment has no impact on solar shadows
- Placement of Open Space & Streets – proposed amendment has no impact
- Lot Layout – proposed amendment has no impact
- Building Form – proposed amendment has no impact, because the entire proposal fits within the existing building mass



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 O: 303 444-6902 asopher@sopherarchitects.com
 1919 14th STREET, SUITE 610 BOULDER, CO 80302

MEMORANDUM

To: Karl Guiler

From: Erin Bagnall, SOPHER ARCHITECTS, LLC
 Project: WASHINGTON VILLAGE II – Site Plan Review Amendment
 Date: 14 MAR 2014

Re: SPR AMENDMENT RE-SUBMITTAL – PARKING REDUCTION CRITERIA 2014.03.14

(K) Additional Criteria for Parking Reductions: The off-street parking requirements of section 9-9-6, "Parking Standards," B.R.C. 1981, may be modified as follows:

(i) Process: The city manager may grant a parking reduction not to exceed fifty percent of the required parking. The planning board or city council may grant a reduction exceeding fifty percent.

(ii) Criteria: Upon submission of documentation by the applicant of how the project meets the following criteria, the approving agency may approve proposed modifications to the parking requirements of section 9-9-6, "Parking Standards," B.R.C. 1981 (see tables 9-1, 9-2, 9-3 and 9-4), if it finds that:

- a. For residential uses, the probable number of motor vehicles to be owned by occupants of and visitors to dwellings in the project will be adequately accommodated;*
- b. The parking needs of any nonresidential uses will be adequately accommodated through on-street parking or off-street parking;*
- c. A mix of residential with either office or retail uses is proposed, and the parking needs of all uses will be accommodated through shared parking;*
- d. If joint use of common parking areas is proposed, varying time periods of use will accommodate proposed parking needs; and*
- e. If the number of off-street parking spaces is reduced because of the nature of the occupancy, the applicant provides assurances that the nature of the occupancy will not change.*

On February 25, 2009 the Washington Village project was granted a **55%** parking reduction by City Council. In the years since, the project was able to add an additional 2 spaces on site (the total count on site went from 59 to 61). The Land Use Code has changed as well. When the project was initially approved the RH-2 parking standards were based on 1 space being provided for the first 800 square feet of a unit and one additional parking space for every 300 square feet (or portion thereof afterwards) in the unit. The current code calculates according to bedroom and is outlined below. (It is worth noting that the new code is based on bedroom count, where the City’s definition of ‘bedroom’ means a room that is not a garage, kitchen, bathroom, dining area, or living room, that has over seventy square feet of floor area, and that is used for sleeping or capable of being used for sleeping.)

1 bedroom unit	1 parking space
2 bedroom unit	1.5 parking spaces
3 bedroom unit	2 parking spaces
4 or more bedroom units	3 parking spaces

When applied to Washington Village, the following number of residential spaces are required:

RH-2 PARKING REQUIRED UNDER SITE REVIEW AMENDMENT REQUESTS

PRIVATE RESIDENTIAL AREAS (count toward total required spaces)	BEDROOMS PER UNIT	CARS PER UNIT	NUMBER OF UNITS	TOTAL CARS
HISTORIC SCHOOL				
BASEMENT LEVEL				
S1-NE AFFORDABLE (NORTH EAST)	2	1.5	1	1.5
S2-SE AFFORDABLE (SOUTH EAST)	2	1.5	1	1.5
MAIN LEVEL				
S3 MARKET FLAT SOUTHWEST AND SOUTHEAST	1	1	2	2
S4 MARKET FLAT NORTH	3	2	1	2
UPPER LEVEL				
S5-SE MARKET FLAT SOUTH (SOUTHEAST)	1	1	1	1
S5-SW MARKET FLAT SOUTH (SOUTHWEST)	1	1	1	1
S6 MARKET FLAT NORTH	3	2	1	2
BROADWAY BUILDING				
MAIN LEVEL				
B1 AFFORDABLE CENTRAL UNITS	1	1	2	2
B2 AFFORDABLE END UNITS	2	1.5	2	3
SECOND LEVEL				
B3-S MARKET FLAT SOUTH	2	1.5	1	1.5
B3-N MARKET FLAT NORTH	2	1.5	1	1.5
B4 MARKET FLAT (1-BED)	2	1.5	1	1.5
B4 MARKET FLAT (1-BED)	2	1.5	1	1.5
B4 MARKET FLAT (1-BED)	2	1.5	1	1.5
B4 MARKET FLAT (1-BED)	2	1.5	1	1.5
THIRD LEVEL				
B6-N PENTHOUSE FLAT	3	2	1	2
B6-S PENTHOUSE FLAT	3	2	1	2
NORTH BUILDING				
MAIN LEVEL				
N1 MARKET TOWN HOMES EAST AND WEST	4	3	2	6
N2 AFFORDABLE MODERATE LOWER	2	1.5	1	1.5
N3 AFFORDABLE MODERATE UPPER	2	1.5	1	1.5
DUPLEX				
E1 MARKET TOWN HOMES	2	1.5	4	6
CARRIAGE				
C1 AFFORDABLE INCL. NORTH	2	1.5	1	1.5
C2 AFFORDABLE MODERATE SOUTH	2	1.5	1	1.5
TOTAL CARS IN RH-2 ZONE				47

number of bedrooms (includes potential for owners of N1-E and N1-W to add a bedroom to the unfinished basement)

62

OFFICE USE	OFFICES	SF/CAR		
OFF-STREET PARKING REQUIRED (typically 1 car/ 300 sf of office use)	2646	300	8.82	
RH-2 ZONE TOTAL OFFICE PARKING REQUIRED				8

TOTAL CARS REQUIRED IN RH-2 ZONE	55
---	-----------

TOTAL CARS PROVIDED IN RH-2 ZONE	49
---	-----------

PARKING REDUCTION REQUESTED	11%
------------------------------------	------------

NUMBER OF UNITS	30
RESIDENTIAL ACCESSIBLE SPACES REQ'D	3.29
NON-RESIDENTIAL ACCESSIBLE SPACES REQ'D	1
TOTAL HANDICAP SPACES REQ'D	4

The resulting parking reduction with the new code and the additional units requested with this Site Review Amendment is now 11%.

PARKING SUMMARY

RH-2 ZONE -- ON SITE

BELOW BUILDING STRUCTURED PARKING

BROADWAY BUILDING			
STANDARD			16
COMPACT			11
HANDICAP			2

ON-GRADE PARKING

COMPACT (NORTH OF NORTHERN DUPLEX)			4
HANDICAP (NORTH OF NORTHERN DUPLEX)			1
HANDICAP VAN (NORTH OF NORTHERN CARRIAGE HOUSE)			1

TUCK-UNDER PARKING

STANDARD (DUPLEXES)			8
STANDARD (CARRIAGE UNITS)			6

TOTAL STANDARD CARS			30
TOTAL COMPACT CARS	PERCENTAGE OF COMPACT	31%	15
TOTAL HANDICAP CARS			4

TOTAL CARS PROVIDED FOR RH-2 ZONE ON-SITE	49
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RL-1 ZONE -- ON SITE

ON-GRADE INTERIOR GARAGE PARKING

SINGLE FAMILY RESIDENCES			12
(presumed as 2 cars for each single family detached unit)			

TOTAL CARS PROVIDED FOR RL-1 ZONE ON-SITE	12
--	-----------

TOTAL AMOUNT OF PARKING SPACES PROVIDED ON SITE	61
--	-----------

ON STREET PARKING

CEDAR AVENUE			13
13th STREET			11

TOTAL AMOUNT OF PARKING SPACES AVAILABLE ON STREET	24
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***11% PARKING REDUCTION REQUESTED WITH SPR AMENDMENT REQUESTS**

RESPONSE TO PARKING CRITERIA:

a. For residential uses, the probable number of motor vehicles to be owned by occupants of and visitors to dwellings in the project will be adequately accommodated;

There are two zones on this site. The parking for the RH-2 (multifamily) portion of the site amounts to 41 spaces for 30 units. According to the parking study compiled by LSC Transportation Consultants, INC. the peak demand (between the hours of 10 PM and 7 AM) for these units will be 1.38 spaces per dwelling unit. That amounts to a demand during night time hours (peak time) of 42 spaces. Since the 8 office designated spaces will be shared with the residential units during the night time hours, there will be more than 42 spaces available to the residential units during this peak demand time.

The parking for the RL-1 portion of the site (6 single family homes) will be provided by way of 6 double car garages, one for each home, amounting to 12 total spaces.

b. The parking needs of any nonresidential uses will be adequately accommodated through on-street parking or off-street parking;

The RH-2 portion of the site will include 2,646 square feet of office space, parking for which will be assigned to 8 spaces (one handicap) in the underground garage in the Broadway Building. According to the parking study compiled by LSC Transportation Consultants, INC. the peak demand (between the hours of 8 AM and 5 PM) for the office use will be 8 cars, already provided for on site. The office spaces will have a demand of 5 spaces at night, 8 during mid-day, and 2 in the evening.

c. A mix of residential with either office or retail uses is proposed, and the parking needs of all uses will be accommodated through shared parking;

The RH-2 portion of the site will have 30 residential units and 2,646 square feet of office space, as stated previously. There will be a total of 49 spaces on site to accommodate these uses. During the peak office hours (daytime) the 8 spaces designated for the office use will only be available to the office use. All other hours of the day the office spaces will be shared with the residential use. According to the parking study compiled by LSC Transportation Consultants, INC. the peak demand on site will be during the hours between 10 PM and 7 AM where the residential units will need 42 spaces and the office use will need 5. This amounts to a peak demand during the night of 47 spaces, the site is providing 49.

d. If joint use of common parking areas is proposed, varying time periods of use will accommodate proposed parking needs; and

The 8 parking spaces reserved for the office uses are designated for ONLY office use between the hours of 8 AM and 4 PM. After 4 PM these spaces will be available for the residential use as well. This shared parking will help to meet the peak demand hours for residential which are opposite the peak demand for office use (peak demand for residential use is between 10 PM and 7 AM and peak office use is between 8 AM and 5 PM.) This shared parking strategy should allow for plenty of parking provided on site. In addition there are 24 street parking spaces available if needed.

e. If the number of off-street parking spaces is reduced because of the nature of the occupancy, the applicant provides assurances that the nature of the occupancy will not change.

Any change to the occupancy that would require more parking would require a reconsideration of the Site Review. The applicant does not anticipate any change to the co-housing/office/single family uses on site.

Washington Village Co-housing is a planned community of active adults who are interested in living in a community that shares work and property. We have asked for a parking reduction because this is common in co-housing communities. (Nomad Co-housing in North Boulder has 11 units on site and there are 19 parking spaces designated for the co-housing.) With an implemented TDM plan and a traveler friendly site (the site is located on a multi-modal corridor with immediate access to shopping, hospital/doctors offices, bike path, Recreation Center, on site park, and major bus route access) the applicant feels that the small parking reduction being requested is sustainable and justified given the circumstances of the site.

Washington Village has a TDM plan in place that is providing the following:

- Neighborhood EcoPasses are being provided to residents when they move in
- EcoPasses will be provided to the office tenants (for 3 years after occupancy)
- Car Share program managed within the Co-housing community through an info center in the common house
- On site bicycle storage greatly exceeds the amount required by code (45 bikes throughout the site)
- Bike share program managed within the Co-housing community through an info center in the common house



CITY OF BOULDER
Planning and Development Services

1739 Broadway, Third Floor • P.O. Box 791, Boulder, CO 80306-0791
 phone 303-441-1880 • fax 303-441-3241 • web boulderplandevlop.net

CITY OF BOULDER CITY COUNCIL
NOTICE OF DISPOSITION

You are hereby advised that on February 25, 2009 the following action was taken by the City Council based on the standards and criteria of the Land Use Regulations as set forth in Chapter 9-2, B.R.C. 1981, as applied to the proposed development.

DECISION: APPROVED WITH CONDITIONS
PROJECT NAME: 1215 CEDAR AVENUE
DESCRIPTION: 1) **SITE REVIEW:** Request to construct a total of 33 residential units on a 3 acre site. More specifically, 27 residential units and common facilities are proposed in a co-housing type community on the high density portion of the site along Broadway and 6 single-family residences are proposed on the low density portion along 13th Street. This approval includes the following modifications:

- A reduction in the minimum lot area per dwelling unit in the RH-2 zone pursuant to Section 9-8-3(b), B.R.C. 1981.
- A height modification to permit the Broadway Building at a height of 41 feet (from the lowest point within 25 feet of the structure, not from grade), where 35 feet is the "by-right" standard for the zoning district.
- Modification to the RH-2 front 25-foot setback to permit the first floor of the Broadway Building at 17 feet and the second floor at 21 feet.
- A 55% parking reduction.

2) **USE REVIEW:** Request to permit 2,950 square feet of office space on the ground floor of a new building along Broadway.

3) **VESTED RIGHTS:** Request for creation of vested rights pursuant to Section 9-2-19, B.R.C. 1981.

LOCATION: 1215 CEDAR AVENUE
COOR: N4W6
LEGAL DESCRIPTION: See Exhibit A attached.
APPLICANT/OWNER: WONDERLAND HILL DEVELOPMENT COMPANY
APPLICATION: Site and Use Review, LUR2008-00083
ZONING: RH-2 and RL-1
CASE MANAGER: Karl Guiler

FOR CONDITIONS OF APPROVAL, SEE THE FOLLOWING PAGES OF THIS DISPOSITION.

IN ORDER FOR A BUILDING PERMIT APPLICATION TO BE PROCESSED FOR THIS PROJECT, A SIGNED DEVELOPMENT AGREEMENT AND SIGNED MYLAR PLANS MUST BE SUBMITTED TO THE PLANNING DEPARTMENT WITH DISPOSITION CONDITIONS AS APPROVED SHOWN ON THE MYLAR PLANS, IF THE DEVELOPMENT AGREEMENT IS NOT SIGNED WITHIN NINETY (90) DAYS OF THE FINAL DECISION DATE, THE CITY COUNCIL APPROVAL AUTOMATICALLY EXPIRES.

Pursuant to Section 9-2-12 of the Land Use Regulations (Boulder Revised Code, 1981), the applicant

must begin and substantially complete the approved development within three years from the date of final approval. Failure to "substantially complete" (as defined in Section 9-2-12) the development within three years shall cause this development approval to expire.

Council Member Cowles moved, seconded by Espinoza to approve (6-2, Gray and Morzel opposed) the Site and Use Review application # LUR2008-00083 for Washington Village II incorporating the staff memorandum and the attached Site and Use Review Criteria Checklist as findings of fact, subject to the conditions of approval below, and modified as follows: Addition of Condition No. 7 from Planning Board Notice of Disposition dated Jan. 9, 2009 requiring that the applicant provide EcoPasses to employees in the commercial space, adding the word "deferred" in paragraph 3(d) to state that "The plan shall accommodate three additional "deferred" parking spaces on the RL-1 portion of the lot", and to direct that an additional condition be added to state that the southeastern open space shall be protected from the impact of construction and shall remain in its current or improved state and open during construction.

Mayor Appelbaum moved, seconded by Morzel to amend section 5(a)ii approving the 41 foot height limit with no parapets and limiting mechanical equipment to not exceed the maximum height by more than 18 inches. The motion carried 8:0.

CONDITION OF APPROVAL – SITE REVIEW

1. The Applicant shall be responsible for ensuring that the **development shall be in compliance with all approved plans** entitled Washington Village II dated November 17, 2008 and January 8, 2009 and on file in the City of Boulder Planning Department, except as may be modified by this approval.
2. The portion of the site that is zoned RH-2 shall be **operated as a co-housing community** consistent with the Applicant's written statement dated September 12, 2008 on file with the City of Boulder Planning Department.
3. Prior to a building permit application, the Applicant shall submit Technical Document Review applications for the following items, subject to the approval of the City of Boulder Planning and Development Services Division:
 - a) A **final site development plan** for the entire site that is consistent with the approved site development plan with the following revisions:
 - i) Benches shall be added to the perimeter of the southeastern open space.
 - ii) A landscaped buffer of no less than 5 feet between the existing chain link fence location and the access drive shall be added and planted with shrubs and small to medium sized trees to increase compatibility and buffering between the subject site and the single family residence to the immediate north. Also, to enhance compatibility, the existing chain link fence shall be replaced with a wood fence of 100% opacity, at least 6 feet in height, and otherwise consistent with Section 9-9-15, B.R.C. 1981 in the exact location as the chain link fence without detriment to existing vegetation. Further, the addition of the landscaped buffer shall not affect the size and location of the southeastern open space.
 - iii) The detention area and storm water quality facility on the southeastern open space shall be eliminated. Any necessary drainage outfall piping may be located in the southeastern open space, as necessary, and shall not negatively impact existing trees. The stormwater management system shall be:
 - A) A detention area located on one of the following locations:
 - I. The proposed south carriage house site;
 - II. The proposed Single Family Building Area No. 6 shown on Sheet C1.0 of the approved plans, or
 - III. Both of the sites described in subsections A.I and A.II above;

or

- B) Another method of stormwater management consistent with the *City of Boulder Design and Construction Standards* that avoids any significant changes to the footprints of buildings or the general site design.
- b) A **detailed landscape and tree protection plan**, including size, quantity, and type of plants existing and proposed; type and quality of non-living landscaping materials; any site grading proposed; and the proposed irrigation system, to insure compliance with this approval and the City's landscaping requirements. The plans must conform to the preliminary Tree Preservation Plan and arborist assessment attached to the approved plans. Any construction that affects the existing trees, including but not limited to foundations, grading, impervious surfaces, and the erection of walls within the vicinity of trees to be preserved that result in unanticipated damage to existing trees, shall require mitigation pursuant to the detailed landscape and tree protection plan.
 - c) A **detailed lighting plan** showing location, size, and intensity of illumination units, showing compliance with Section 9-9-16, B.R.C. 1981.
 - d) A **detailed parking plan** showing the arrangement, locations, dimensions, and type of parking stalls (including any areas of the site for bicycle parking or reserved for deferred parking) to insure compliance with this approval and Section 9-9-6, B.R.C. 1981. This plan shall accommodate three additional deferred parking spaces on the RL-1 portion of the lot. The three spaces, if constructed, shall be signed appropriately to designate for multi-family residents or guests, shall not require the removal of any trees, and shall be surfaced with permeable pavement and/or pavers consistent with the *City of Boulder Design and Construction Standards*.
 - e) A **detailed shadow analysis** to insure compliance with the City's solar access requirements of Section 9-9-17, B.R.C. 1981.
 - f) **Final Storm Water Plans and Report** meeting the *City of Boulder Design and Construction Standards*.
 - g) **Final Utility Plans and Report** meeting the *City of Boulder Design and Construction Standards*. The revisions required to the preliminary utility report may be completed as part of the Final Utility Report. The revisions to the report require elimination of the proposed water main along the north side of the site and an upgrade of the existing water main in Cedar Avenue from a 6" main to an 8" main.
 - h) **Final transportation engineering plans** meeting the *City of Boulder Design and Construction Standards* for all transportation improvements.
4. Prior to a building permit application, the Applicant shall **dedicate to the City, at no cost, the following** as part of Technical Document Review applications, subject to the approval of the City of Boulder Planning and Development Services Division:
- a) A 20-foot public access easement along the westerly and northerly edges of the portion of the site zoned RL-1 in the location of the shared access drive;
 - b) A public access easement for 1 foot beyond the edge of the transit shelter pad on Broadway; and
 - c) A public access easement for 1 foot beyond the sidewalk limits along Broadway and 13th Street.
5. Prior to a building permit application on the portion of the site that is zoned **RH-2**, the Applicant shall submit a Technical Document Review application, subject to the approval of the Planning Director, for the following items:

- a) **Final architectural plans**, including materials and colors, to insure compliance with the intent of this approval and compatibility with the historic school and surrounding area and including the following revisions:
 - i) The final architectural plans and elevations shall include **revisions that modify the façade of the west elevation of the south carriage house** (if retained) to ensure that the building presents an attractive streetscape appropriate to the pedestrian scale.
 - ii) The **Broadway Building shall be modified to not exceed 41-feet in height** pursuant to the City's definition of "height" in Chapter 9-16, B.R.C. 1981 and parapets and mechanical equipment shall not exceed that height more than 18 inches.
6. Prior to a building permit application on the portion of the site that is zoned **RH-2**, the Applicant shall submit a **financial guarantee**, in a form and amount acceptable to the Director of Public Works, to guarantee those items proposed in the Applicant's Transportation Demand Management (TDM) plan, including transit passes.
7. The applicant shall provide **EcoPasses to employees that work in the commercial office space**. The applicant shall provide a financial guarantee for a period of three years after the issuance of a certificate of occupancy for the tenant finish of the commercial space in an amount to guarantee the distribution of EcoPasses.
8. Prior to a building permit application on the portion of the site that is zoned **RH-2** and is part of the proposed individual landmark site, the Applicant shall **apply to landmark the historic school building and secure a landmark alteration certificate** required by Chapter 9-11, "Historic Preservation," B.R.C. 1981.
9. Prior to application for a building permit on the portion of the site that is zoned **RL-1**, the Applicant shall submit a Technical Document Review application, subject to the approval of the Planning Director, for **final architectural plans** that demonstrate compliance with approved design guidelines prepared by the Applicant and include the following limitations:
 - a) The maximum floor area ratio (FAR) for the single-family homes shall be 0.5:1 averaged across the land area included within the RL-1 portion and within the confines of the project, or the underlying RL-1 FAR limit at time of building permit, if less than 0.5:1.
 - b) Each principal dwelling shall not exceed a floor area of 2,500 square feet, the garage shall not exceed 500 square feet, and any studio space above the garage may not exceed 350 square feet.
 - c) The second level of the principal structure shall not exceed 75% of the ground level floor area of said structure.
 - d) To ensure appropriate massing and architectural compatibility, the majority of each single family roof forms shall be gable and/or hip roofs with the uppermost portion of the roofs having a roof pitch no less than 5:12.
 - e) The single family dwellings shall present attractive street faces to the southeastern open space and all streetscapes to ensure that the buildings present an attractive streetscape appropriate to the pedestrian scale. Attractive street faces may include any combination of porches, detailing, and appropriate fenestration.
10. The Applicant shall protect southeastern open space from damage associated with construction activities and keep the area visually open during the construction of the development.

CONDITION OF APPROVAL – USE REVIEW

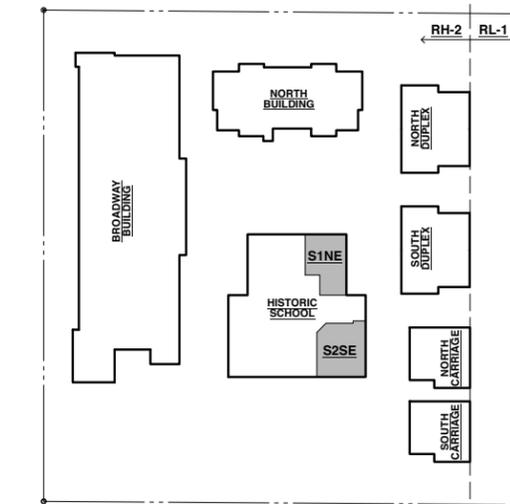
1. The Applicant shall be responsible for ensuring that the **development shall be in compliance with all approved plans** entitled Washington Village II dated November 17, 2008 and January 8, 2009 and the

written statement dated September 12, 2008, on file in the City of Boulder Planning Department, except as may be modified by this approval.

2. The Applicant **shall not expand or modify the approved use**, except pursuant to Subsection 9-2-15(h), B.R.C. 1981.
3. The Applicant shall ensure that the **approved office uses** are operated in compliance with the written statement dated September 12, 2008, pursuant to the following restrictions:
 - a) Professional and Technical Offices are approved in the non-residential space along Broadway not to exceed 2,950 square feet.
 - b) The office uses shall be closed from 6:00 p.m. to 8 a.m., Monday through Friday.
 - c) Nine (9) parking spaces shall be designated within the Broadway Building for the office uses during the hours of 8:00 a.m. to 4:00 p.m. and shall otherwise be available for residential uses outside these hours.

WASHINGTON SCHOOL DEVELOPMENT LLC

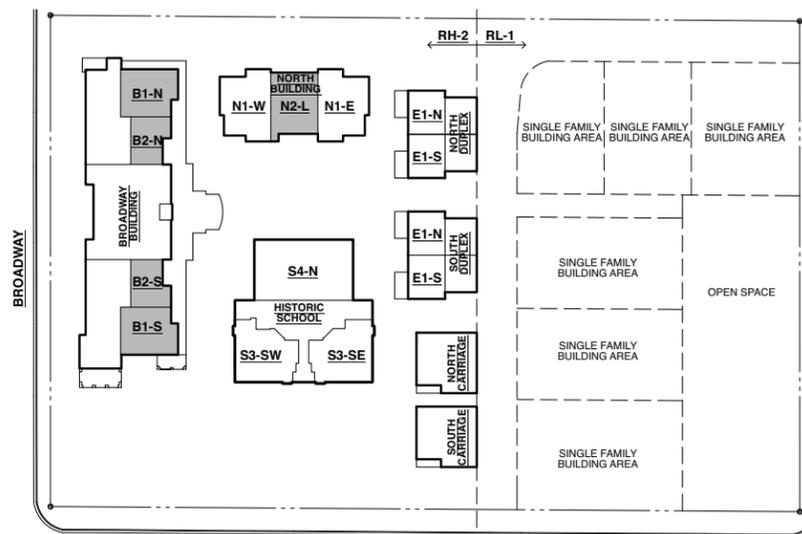
SITE PLAN REVIEW AMENDMENT JANUARY 6, 2014



LOWER LVL UNIT LOCATION PLAN

SCALE: 1" = 50'

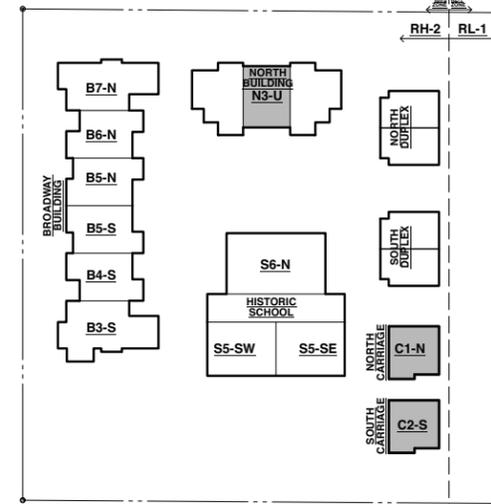
4



FIRST LVL UNIT LOCATION PLAN

SCALE: 1" = 50'

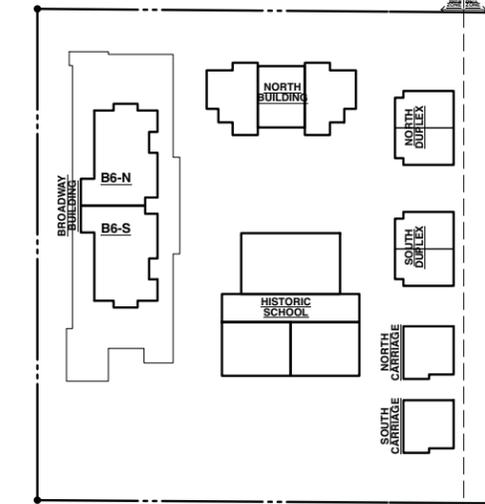
3



SECOND LVL UNIT LOCATION PLAN

SCALE: 1" = 50'

2



THIRD LVL UNIT LOCATION PLAN

SCALE: 1" = 50'

1

PLAN LEGEND:

- NEW WALL - FULL HEIGHT - WOOD FRAMED
- NEW WALL - CONCRETE
- NEW WALL - 8" CMU
- NEW WALL - PARTIAL HEIGHT
- NEW WALL - LANDSCAPE
- EXISTING WALL TO REMAIN
- EXISTING WALL TO BE REMOVED
- SECTION MARKER
- WALL ELEVATION MARKER
- NEW GRADE ELEVATION MARKER
- EXISTING GRADE ELEVATION MARKER
- PROPERTY LINE
- BUILDING BELOW
- BUILDING / ROOF ABOVE
- CENTERLINE
- FENCE LINE
- AREA OF DRAWING REVISION
- REVISION NUMBER MARKER

ABBREVIATIONS:

- ABV ABOVE
- ADD ADDITIONAL
- AFF ABOVE FINISHED FLOOR
- BLW BELOW
- BLK BLOCK, BLOCKING
- BOT BOTTOM
- BOW BOTTOM OF WALL
- CAB CABINET
- CL CENTERLINE
- CLG CEILING
- COL COLUMN
- CONC CONCRETE
- CONT CONTINUOUS
- CP CENTERPOINT
- DBL DOUBLE
- DET DETAIL
- DIA DIAMETER
- DIM DIMENSION
- DN DOWN
- DR DOOR
- DS DOWNSPOUT
- DW DISHWASHER
- EA EACH
- EKH EXHAUST
- EX EXISTING
- EXT EXTERIOR
- FOC FACE OF CONCRETE
- FOM FACE OF MASONRY
- FOS FACE OF STUD
- FN FINISHED GRADE
- FN FOUNDATION
- FLR FLOOR
- FRZ FRIGERATOR
- GL GLASS
- GR GRADE
- GWB GYPSUM WALL BOARD
- HB HOSE BIBB
- HGT HEIGHT
- HWH HOT WATER HEATER
- INFO INFORMATION
- INSUL INSULATION
- INT INTERIOR
- LT LIGHTING
- LV LOW VOLTAGE
- MAX MAXIMUM
- MDF MEDIUM DENSITY FIBERBOARD
- MIN MINIMUM
- MTL METAL
- MFR MANUFACTURER
- NA NOT APPLICABLE
- NIC NOT IN CONTRACT
- NFC NOT FOR CONSTRUCTION
- NO NUMBER
- NOM NOMINAL
- OC ON CENTER
- PL PLATE, PROPERTY LINE
- PTD PAINT, PAINTED
- PWD PLYWOOD
- R RISER(S); RADIUS
- R RADIUS
- RAD RADIUS
- REF REFERENCE
- REFR REFRIGERATOR
- REGS REGULATION(S)
- REQ'D REQUIRED REQUIREMENTS
- SCH,SCHED SCHEDULE
- SIM SIMILAR
- SST STAINLESS STEEL
- STL STEEL
- T TREAD(S)
- TBD TO BE DETERMINED
- T&G TONGUE & GROOVE
- THK THICKNESS
- TOP TOPPING
- TOP TOP OF CONCRETE
- TOS TOP OF SLAB
- TOW TOP OF WALL
- TYP TYPICAL
- UNO UNLESS NOTED OTHERWISE
- V VOLT(AGE)
- VCB VINYL COVE BASE
- VIF VERIFY IN FIELD
- WD WOOD
- WDW WINDOW

SHEET INDEX:

- N/A COVER SHEET, REVISED
- 0.0 SITE PLAN, SETBACK INFORMATION
- C1.0 PRELIMINARY GRADING & DRAINAGE PLAN
- C2.0 PRELIMINARY UTILITY PLAN
- LP-1 EXISTING TREE INVENTORY & PRELIMINARY TREE PRESERVATION PLAN
- LP-2 LANDSCAPE PLAN
- LP-3 LANDSCAPE DETAILS
- LP-4 LANDSCAPE DETAILS
- LP-5 BICYCLE PARKING PLAN & DETAILS
- 0.1 SOLAR SHADOW ANALYSIS
- 1.0 GARAGE/BASEMENT/PARKING PLAN
- 1.1 FIRST FLOOR PLAN (WITH AREA CALCULATIONS)
- 1.2 SECOND FLOOR PLAN (WITH AREA CALCULATIONS)
- 1.3 THIRD FLOOR/ROOF PLAN (WITH AREA CALCULATIONS)
- 1.4 ROOF PLAN
- A2.1 HISTORIC SCHOOL ELEVATIONS
- A2.2 BROADWAY BUILDING ELEVATIONS WEST AND SOUTH
- A2.3 BROADWAY BUILDING ELEVATIONS EAST AND NORTH
- A2.4 NORTH BUILDING ELEVATIONS
- A2.5 DUPLEX AND CARRIAGE HOUSE ELEVATIONS
- 0.0.R SITE PLAN, SETBACK INFORMATION
- C1.0.R PRELIMINARY GRADING & DRAINAGE PLAN
- LP-2.R LANDSCAPE PLAN
- 0.1.R SOLAR SHADOW ANALYSIS
- 1.0.R GARAGE/BASEMENT/PARKING PLAN
- 1.1.R FIRST FLOOR PLAN (WITH AREA CALCULATIONS)
- 1.2.R SECOND FLOOR PLAN (WITH AREA CALCULATIONS)
- 1.3.R THIRD FLOOR/ROOF PLAN (WITH AREA CALCULATIONS)
- A2.2.R BROADWAY BUILDING ELEVATIONS WEST AND SOUTH
- A2.3.R BROADWAY BUILDING ELEVATIONS EAST AND NORTH

PROJECT TEAM:

OWNER / DEVELOPER:

WASHINGTON SCHOOL DEVELOPMENT LLC
 4576 BROADWAY
 BOULDER, CO 80304
 303-449-3232
 CONTACT: PETER SPAULDING

ARCHITECT:

SOPHER ARCHITECTS LLC
 1919 14TH ST SUITE 610
 BOULDER, CO 80302
 303-444-6902
 CONTACT: ADRIAN SOPHER

CIVIL ENGINEER:

JVA, INCORPORATED
 1319 SPRUCE STREET
 BOULDER, CO 80302
 303-444-1951
 CONTACT: CHARLIE HAGER

LANDSCAPE DESIGNER:

303 ARCHITECTURE
 1942 BROADWAY, SUITE 314
 BOULDER, CO 80302
 303-447-6448
 CONTACT: STEPHANIE RIDGWAY

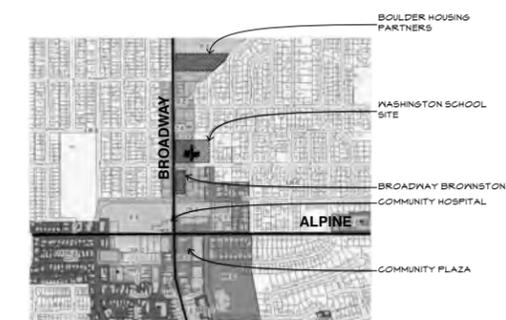
SURVEYOR:

FLATIRONS, INC.-SURVEYING & ENGINEERING
 3825 IRIS AVE #100
 BOULDER, CO 80301
 303-443-7001
 CONTACT: GREG MEYER

LOCATIONS AND CHARACTERISTICS OF AFFORDABLE UNITS

Building	Unit Designation	Unit Access Level	Type of Unit	Affordable Type	Finished SF	Bedrooms	Baths	Proposed Price
Broadway	B1 North	First	End	Low	919	2	1.75	\$133,400
	B1 South	First	End	Low	919	2	1.75	\$133,400
	B2 North	First	Mid	Low	606	1	1	\$83,700
North	B2 South	First	Mid	Low	606	1	1	\$83,700
	N2 Lower	First	Mid	Moderate	1001	2	1.75	\$275,000
	N3 Upper	First	Mid	Moderate	1024	2	1.75	\$295,000
School	S1-NE	Lower	Corner	Low	801	2	1	\$117,800
	S2-SE	Lower	Corner	Moderate	801	2	1	\$245,000
Carriages	C1 North	Second	Carriage	Low	901	2	1.75	\$133,400
	C2 South	Second	Carriage	Moderate	901	2	1.75	\$265,000

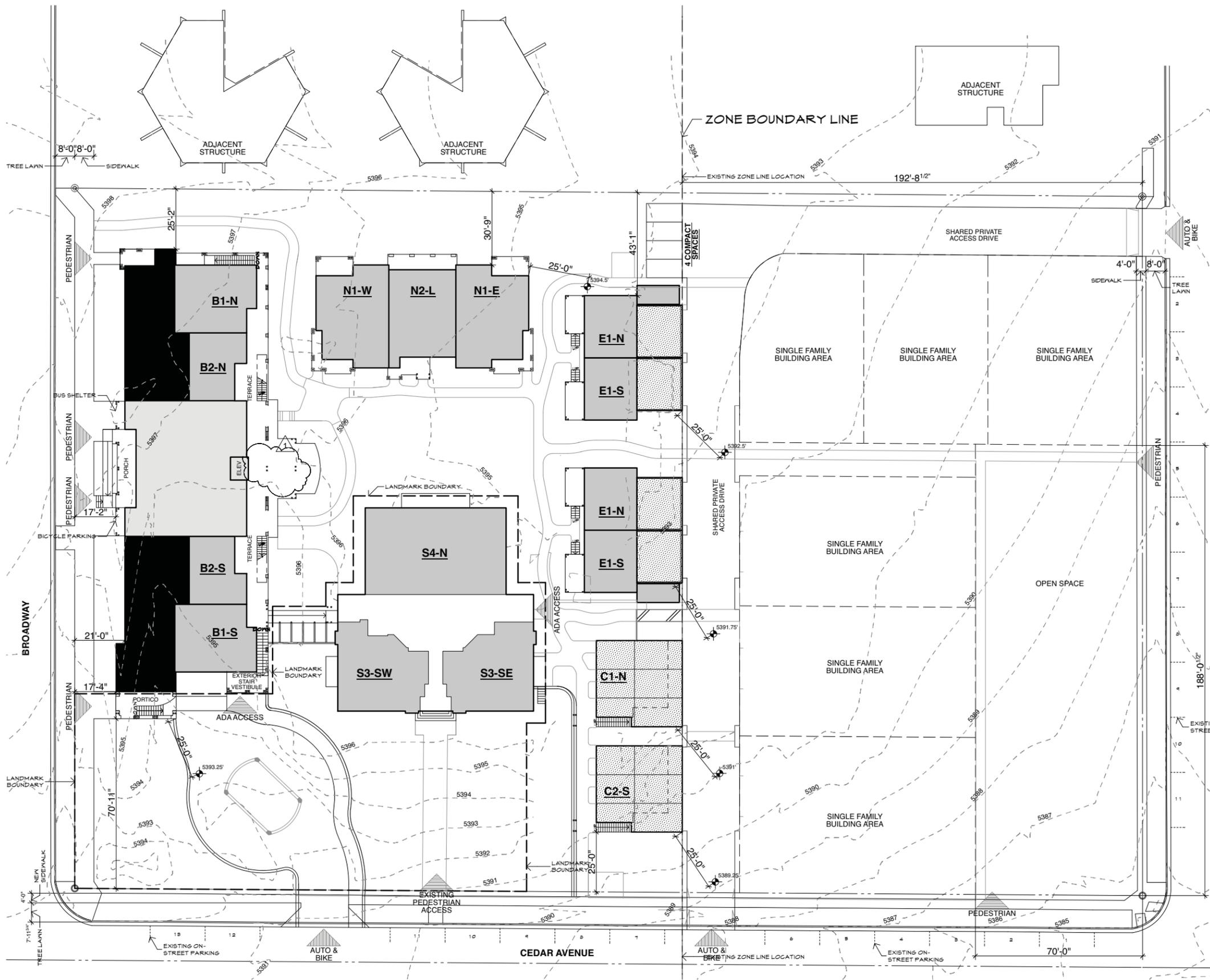
DENOTES AFFORDABLE UNIT



VICINITY MAP

NOT TO SCALE

SITE REVIEW SUBMITTAL	12 SEPT 2008
SITE REVIEW RESUBMITTAL	17 NOV 2008
SITE REVIEW AMENDMENT	6 JAN 2014



BUILDING PLACEMENT & SETBACKS

NOTE: ALL HEIGHTS ARE MEASURED TO EXISTING GRADE AT A 25' RADIUS FROM THE LOWEST POINT ON THE STRUCTURE (TYPICALLY, THE SOUTH EASTERN CORNER OF THE BUILDING)

RH-2 ZONE		
BROADWAY	MIXED USE STRUCTURE	17.0'
SETBACK FOR PORCHES & SINGLE STORY MASSES		21.0'
SETBACK FOR TWO STORY BUILDING MASSES		25.0'
SETBACK FOR THIRD STORY BUILDING MASSES		47.5'
BUILDING HEIGHT (for Broadway Building)		
CEDAR AVENUE	CARRIAGE HOUSE	25.0'
SETBACK FOR PORCHES		25.0'
SETBACK FOR PRINCIPAL BUILDING		25.0'
NORTH PROPERTY LINE	NORTH BUILDING & DUPLEX	25.0'
SETBACK FOR STOOPS & PORCHES & SINGLE STORY MASSES		25.0'
SETBACK FOR TWO STORY BUILDING MASSES		25.0'
BUILDING HEIGHT (for North Building)		±60'
SETBACK FOR THIRD STORY BUILDING MASSES (for Broadway Building)		30' max
20' SHARED PRIVATE ACCESS DRIVE	EAST BUILDINGS	0.0'
SETBACK FOR SINGLE STORY MASSES & BALCONIES ABOVE		6.0'
SETBACK FOR TWO & THREE STORY BUILDING MASSES		0.0'
SETBACK FOR ACCESSORY USES		±31'
BUILDING HEIGHT (for Duplexes)		±28'
BUILDING HEIGHT (for Carriage Houses)		
EXISTING HISTORIC SCHOOL		±47.0'
EXISTING BUILDING HEIGHT (excluding tower)		

RL-1 ZONE		
CEDAR AVENUE	SINGLE FAMILY RESIDENCES	25.0'
SIDEWALK SETBACK ADJACENT TO STREET		25.0'
13TH STREET	SINGLE FAMILY RESIDENCES	25.0'
REAR YARD SETBACK ADJACENT TO STREET		
PRIVATE ACCESS DRIVE	SINGLE FAMILY RESIDENCES & ACCESSORY BLDGS	4.0'
DISTANCE TO A ONE OR TWO STORY ACCESSORY STRUCTURE		4.0'
DISTANCE TO A ONE STORY PRINCIPAL STRUCTURE ELEMENT		10.0'
DISTANCE TO A TWO STORY PRINCIPAL STRUCTURE ELEMENT		
ADJACENT SINGLE FAMILY BUILDING MASSES		10.0'
DISTANCE FROM A ONE STORY MASS TO NEIGHBORING BUILDING ELEMENTS		15.0'
DISTANCE FROM A TWO STORY MASS TO NEIGHBORING BUILDING ELEMENTS		15.0'
DISTANCE BETWEEN PRINCIPAL & ACCESSORY STRUCTURE (under single ownership)		15.0'
MAXIMUM BUILDING HEIGHT		15.0'
ALL SINGLE FAMILY RESIDENCE PARKING MUST BE ACCESSED FROM THE SHARED ACCESS DRIVE		

OPEN SPACE CALCULATIONS

RH-2 ZONE TOTAL SQUARE FOOTAGE	74398
REQUIRED OPEN SPACE FOR BUILDING OVER 45' UNDER 55' - 20%	14880
RH-2 ZONE PROPOSED OPEN SPACE (NOT INCLUDING PRIVATE DECKS)	40152
MINIMUM PERCENTAGE OF PROPOSED OPEN SPACE FOR RH-2 ZONE	54%
RH-2 ZONE PROPOSED PRIVATE OPEN SPACE (DECKS, PORCHES)	
PRIVATE OPEN SPACE ALLOWABLE IN CALCULATIONS -25% OF TOTAL REQUIRED (14880 X .25)	3720
	43872 59%

DRAWING KEY

- GARAGE / PARKING / STORAGE SPACE
- COMMERCIAL / OFFICE SPACE
- CO-HOUSING COMMONS SPACE
- RESIDENTIAL SPACE

SITE PLAN

SCALE: 1" = 20'



ARCHITECTURE
PLANNING &
INTERIOR DESIGN

3008 FOLSOM STREET
BOULDER, CO 80304
PHONE: 303-444-6902
FAX: 303-442-6100



WASHINGTON VILLAGE II
1215 CEDAR AVE
BOULDER, COLORADO

SITE PLAN, SETBACK
INFORMATION

SITE REVIEW 12 SEP 2008
SUBMITTAL 2008
SITE REVIEW 17 NOV 2008
RE-SUBMITTAL 2008

0.0

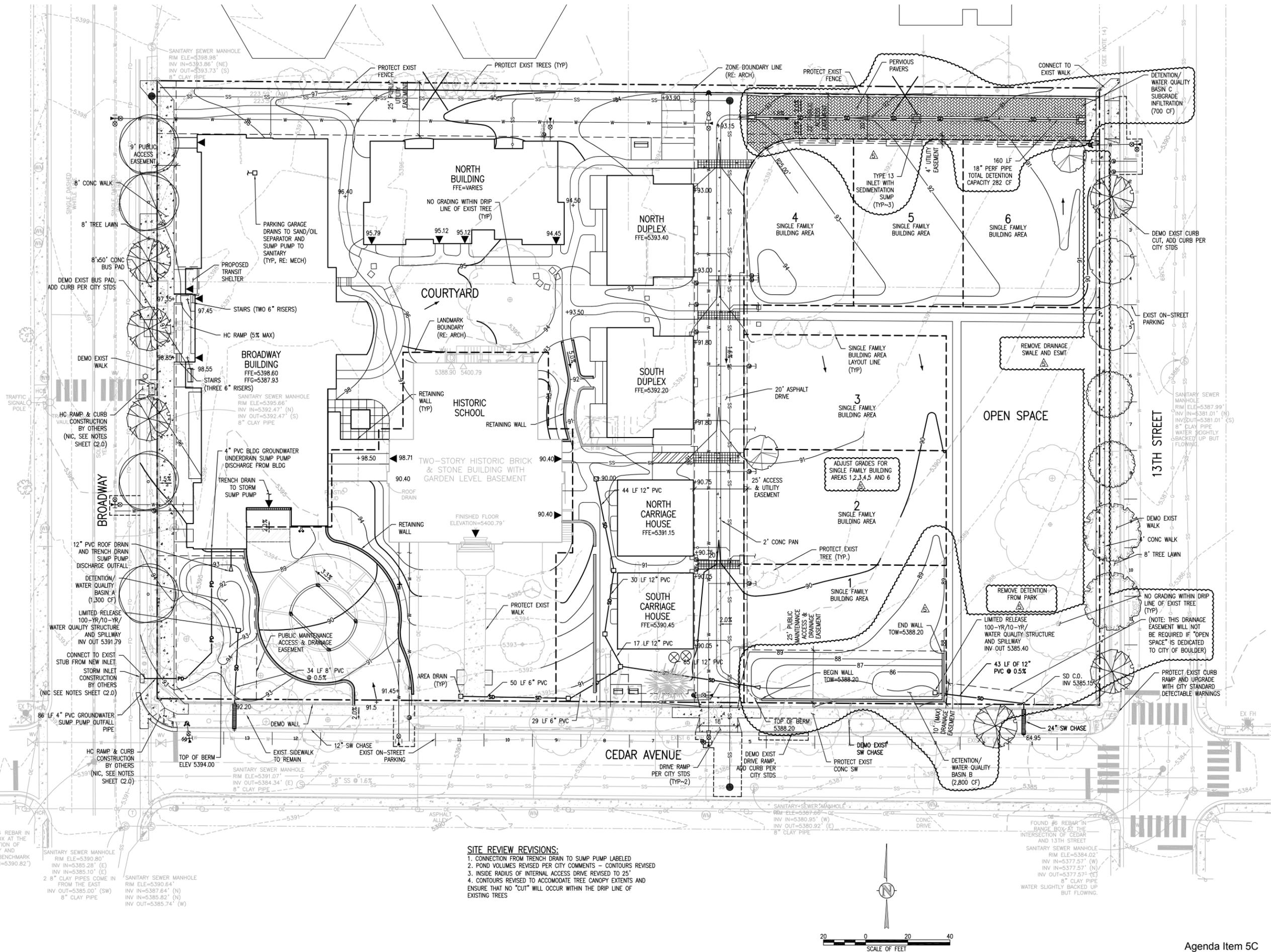
WASHINGTON VILLAGE II
1215 CEDAR AVE
BOULDER, COLORADO

**PRELIMINARY GRADING
AND DRAINAGE PLAN
NOT FOR CONSTRUCTION**

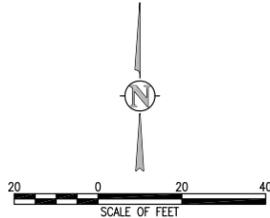
DATE: 25 FEB 09

- 13 SEP 07
- 11 SEP 08
- 17 NOV 08
- 08 JAN 09
- 25 FEB 09

C1.0



SITE REVIEW REVISIONS:
 1. CONNECTION FROM TRENCH DRAIN TO SUMP PUMP LABELED
 2. POND VOLUMES REVISED PER CITY COMMENTS - CONTOURS REVISED
 3. INSIDE RADIUS OF INTERNAL ACCESS DRIVE REVISED TO 25'
 4. CONTOURS REVISED TO ACCOMMODATE TREE CANOPY EXTENTS AND ENSURE THAT NO "CUT" WILL OCCUR WITHIN THE DRIP LINE OF EXISTING TREES



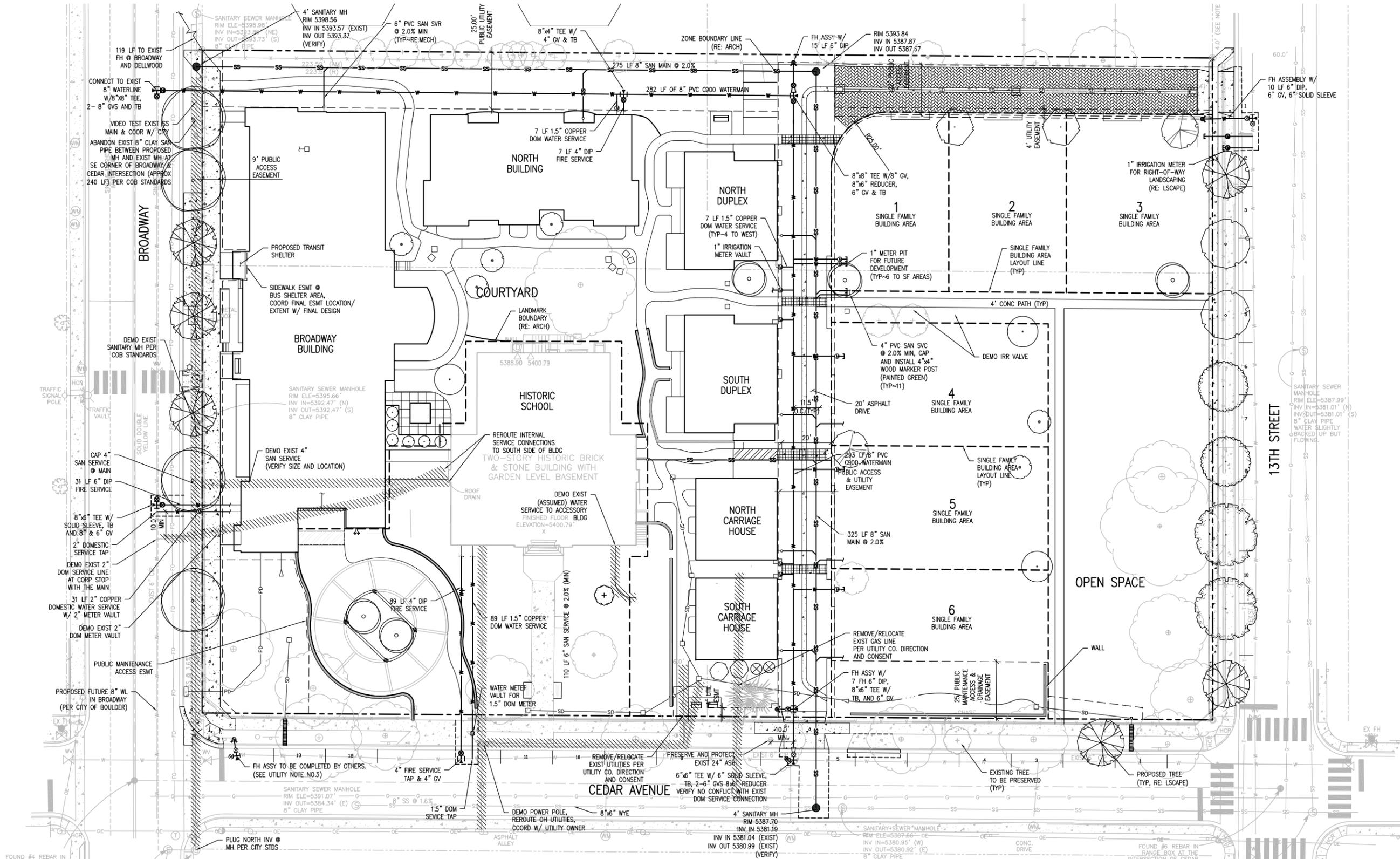
WASHINGTON VILLAGE II
1215 CEDAR AVE
BOULDER, COLORADO

PRELIMINARY UTILITY PLAN

DATE: 25 FEB 09

13 SEP 07	△
11 SEP 08	△
17 NOV 08	△
08 JAN 09	△
25 FEB 09	△

C2.0



CITY OF BOULDER CONSTRUCTION NOTES:

1. ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE "DESIGN AND CONSTRUCTION STANDARDS" OF THE CITY OF BOULDER, AND SHALL BE COMPLETED TO THE SATISFACTION OF THE DIRECTOR OF PUBLIC WORKS. IN THE EVENT THAT A DESIGN ELEMENT DOES NOT REFLECT CITY STANDARDS, THE MATTER MUST BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE ENGINEER AND THE DIRECTOR OF PUBLIC WORKS. THE ENGINEER SHALL BE RESPONSIBLE FOR RECOMMENDING A SOLUTION OR ALTERNATIVE SOLUTIONS TO THE CITY FOR REVIEW AND APPROVAL.
2. THE APPROVAL OF A CONSTRUCTION PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF CONSTRUCTING WORKABLE PUBLIC IMPROVEMENTS. ALL REVISIONS AND/OR CORRECTIONS REQUIRED WILL BE SOLELY THE CONTRACTOR'S RESPONSIBILITY, AND AT THEIR EXPENSE.
3. THESE PLANS HAVE BEEN CHECKED BY THE CITY OF BOULDER ONLY FOR CONFORMANCE WITH THE "DESIGN AND CONSTRUCTION STANDARDS," COMPLIANCE WITH DEVELOPMENT AGREEMENT CONDITIONS, AND FOR GENERAL CONCEPTUAL APPROVAL OF PUBLIC IMPROVEMENTS AS SHOWN. THE CITY'S REVIEW DOES NOT VERIFY OR ENSURE THE ACCURACY OF EXISTING OR PROPOSED DIMENSIONS, LINES, COORDINATES, OR GRADES SHOWN, INCLUDING ALL EXISTING UTILITIES SHOWN OR NOT SHOWN.
4. UTILITY LOCATIONS SHOWN REFLECT AVAILABLE RECORD DATA. THE CONTRACTOR SHALL TAKE PRECAUTIONARY MEASURES TO PROTECT ALL UTILITY LINES SHOWN AND OTHERWISE LOCATED. THE CONTRACTOR SHALL CONTACT THE "UTILITY NOTIFICATION CENTER OF COLORADO" AT 1-800-922-1987 FOR UTILITY LOCATES 24 HOURS PRIOR TO BEGINNING CONSTRUCTION.
5. BEFORE WORK BEGINS, THE CONTRACTOR SHALL OBTAIN A PERMIT TO WORK IN THE RIGHT-OF-WAY FROM THE CITY AND MUST NOTIFY THE CITY RIGHT-OF-WAY INSPECTION STAFF AT LEAST 24 HOURS IN ADVANCE OF COMMENCING CONSTRUCTION ACTIVITIES.

CITY OF BOULDER CONSTRUCTION NOTES CONT:

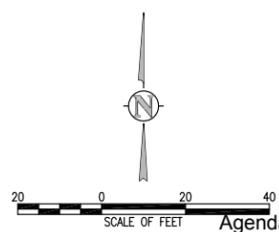
6. THE CONTRACTOR SHALL OBTAIN AND MAINTAIN A COMPLETE AND APPROVED SET OF CONSTRUCTION PLANS. THESE DRAWINGS, AND ANY REQUIRED PERMITS, SHALL BE AVAILABLE AT THE PROJECT SITE AT ALL TIMES AND SHALL BE MADE AVAILABLE TO CITY STAFF UPON REQUEST. IF CONSTRUCTION PLANS ARE NOT READILY AVAILABLE AT THE PROJECT SITE, THE DIRECTOR OF PUBLIC WORKS MAY ISSUE A STOP WORK ORDER AND HALT ALL CONSTRUCTION ACTIVITIES PENDING COMPLIANCE BY THE CONTRACTOR.
7. THE CONTRACTOR AGREES TO COMPLY WITH THE PROVISIONS OF THE TRAFFIC CONTROL PLAN AND THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES," PART IV, FOR CONSTRUCTION SIGNAGE AND TRAFFIC CONTROL.
8. ALL SURPLUS MATERIALS, TOOLS, AND TEMPORARY STRUCTURES, FURNISHED BY THE CONTRACTOR, SHALL BE REMOVED FROM THE PROJECT SITE BY THE CONTRACTOR. ALL DEBRIS AND RUBBISH CAUSED BY THE OPERATIONS OF THE CONTRACTOR SHALL BE REMOVED, AND THE AREA OCCUPIED DURING CONSTRUCTION ACTIVITIES SHALL BE RESTORED TO ITS ORIGINAL CONDITION, WITHIN 48 HOURS OF PROJECT COMPLETION, UNLESS OTHERWISE DIRECTED BY THE DIRECTOR OF PUBLIC WORKS.
9. THE CONTRACTOR SHALL PROVIDE TREE AND LANDSCAPE PROTECTION AS SET FORTH IN CHAPTER 6-6, "PROTECTION OF TREES AND PLANTS," BOULDER REVISED CODE (B.R.C.) 1981 AND THE CITY OF BOULDER DESIGN AND CONSTRUCTION STANDARDS (DCS). ALL LANDSCAPING SHALL BE PROVIDED AND MAINTAINED IN COMPLIANCE WITH THE APPROVED LANDSCAPING PLAN, B.R.C. AND DCS.
10. THE CONTRACTOR IS REQUIRED TO PROVIDE AND MAINTAIN EROSION AND SEDIMENT CONTROL MEASURES IN ACCORDANCE WITH THE URBAN DRAINAGE AND FLOOD CONTROL DISTRICT "URBAN STORM DRAINAGE CRITERIA MANUAL VOLUME 3", THE M STANDARD PLANS OF THE COLORADO DEPARTMENT OF TRANSPORTATION, AND THE APPROVED EROSION CONTROL PLAN. THE DIRECTOR OF PUBLIC WORKS MAY REQUIRE THE CONTRACTOR TO PROVIDE ADDITIONAL EROSION CONTROL MEASURES DUE TO UNFORESEEN EROSION PROBLEMS OR IF THE PLANS DO NOT FUNCTION AS INTENDED.

UTILITY NOTES:

1. OWNER AND CONTRACTOR TO VERIFY FINAL TAP SIZES, BASED ON FINAL FUTURE COUNTS, AT THE TIME OF BUILDING PERMIT APPLICATION AND ADJUST TAP SIZES AND TAP FEES ACCORDINGLY.
2. FINAL SANITARY, WATER, AND OTHER UTILITY ALIGNMENTS TO BE DETERMINED IN THE FIELD BASED ON ENCOUNTERED FIELD CONDITIONS. NOTIFY ENGINEER OF ANY DISCREPANCIES AND ADJUST INVERTS, MANHOLE ELEVATIONS, ETC. BASED ON EXISTING UTILITY ELEVATIONS.
3. DEVELOPER/OWNER SHALL COORDINATE WITH CITY OF BOULDER TO ENSURE THAT WORK TO BE DONE IN R.O.W. BY CITY OF BOULDER (INCLUDING NEW FIRE HYDRANT, RAMPS, ETC.) IS COMPLETED BY DESIRED DATE OF ISSUANCE OF CERTIFICATE OF OCCUPANCY. THE DEVELOPER/OWNER SHALL BE RESPONSIBLE FOR SIDEWALK CONSTRUCTION ALONG BROADWAY, AND THE CITY OF BOULDER WILL CONSTRUCT THE OTHER ITEMS AS SHOWN ON THE PROJECT PLANS FOR THE BROADWAY RECONSTRUCTION.
4. DEVELOPER TO COORDINATE RELOCATION AND/OR UPGRADE OF ALL NON-CITY UTILITIES WITH RESPECTIVE UTILITY OWNER (GAS, ELEC, TELE, ETC.). ON-SITE OVERHEAD UTILITIES AND OVERHEAD UTILITIES IN THE IMMEDIATELY ADJACENT RIGHT-OF-WAY AREA TO BE BURIED.

SITE REVIEW REVISIONS:

1. 4" WIDE UTILITY EASEMENT NOT SHOWN WHERE OVERLAPPING 25' UTILITY EASEMENT.
2. UTILITY EASEMENT REVISED FROM 30' TO 25' AND COMBINED WITH ACCESS EASEMENT.
3. 2" DOMESTIC METER, VAULT, AND SERVICE ALONG BROADWAY TO BE ABANDONED AT CORP STOP WITH MAIN.
4. 6" FIRE SERVICE TO BROADWAY BUILDING TIE FITTINGS REVISED.
5. UTILITY NOTE 3 REVISED.
6. FIRE HYDRANT NEAREST NORTH DUPLEX RELOCATED TO NORTH EDGE OF SITE.
7. 8" WATERMAIN IS "LOOPED" THROUGH SITE FROM 6" MAIN IN CEDAR AVE TO FUTURE 8" MAIN IN BROADWAY STREET. NORTH BUILDING DOMESTIC AND FIRE SERVICED SHIFTED TO NORTH SIDE AND SANITARY MAIN SHIFTED NORTH TO ACCOMMODATE.





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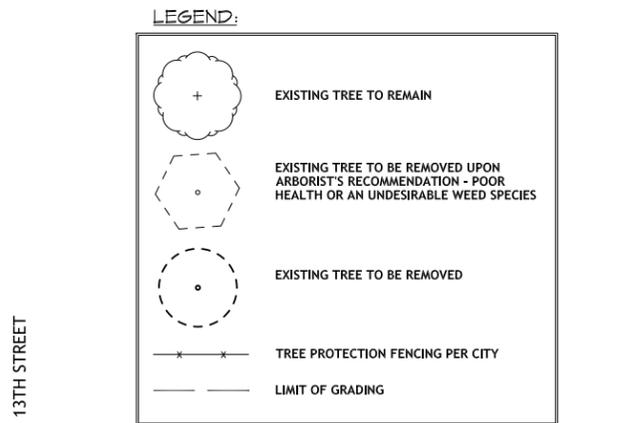
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- 08 JAN 09
- 25 FEB 09

**EXISTING TREE INVENTORY PREPARED BY AN
ISA CERTIFIED ARBORIST**

Tree Survey at Washington School				
TREE#	SPECIES	DBH	CONDITION	COMMENTS
1	American Linden	15	50%	One lead of two is broken
2	Green Ash	15	30%	Poor Health, Drought stressed from years of drought
3	Little leaf Linden	39	80%	Good condition
4	American Linden	22	80%	Co-Dominant main crotch
5	Green Ash	20	20%	Very drought stressed, poor health, the tree has been topped
6	American Elm	26	50%	drought stressed. Many dead limbs near the top of the tree.
7	Green Ash	19	35%	Very drought stressed. Poor health
8	Green Ash	25	65%	Drought stressed.
9	Green Ash	22	70%	Drought stressed
10	Green Ash	8	70%	Good condition
11	Green Ash	22	50%	Drought stressed. Bad basal trunk wound
12	Black Locust	11	80%	Good condition
13	Green Ash	25	75%	Good condition
14	Green Ash	22	65%	Fair to good condition. One lead has been topped.
15	Green Ash	23	50%	Drought stressed. Poor to fair condition.
16	Green Ash	22	35%	Very drought stressed. Poor condition.
17	Green Ash	21	60%	Drought stressed. Fair to Good condition. Small basal trunk wound.
18	Green Ash	26	40%	Very drought stressed. Poor condition.
19	Green Ash	29	0%	Very large trunk wound. This tree will become a hazard. Remove.
20	Siberian Elm	Multi	0%	Poor specimen. Multi-stemmed near. Remove.
21	Crabapple	16	90%	Very good condition. Needs pruning.
22	American Elm	5.6	80%	Good condition. Full of European elm scale. Two stems.
23	Mountain Ash	Multi	0%	Most stems are dead. Remove
24	Black Walnut	14	70%	Good buds appear to be alive but a few dedicated leaves persist.
25	Green Ash	4	35%	Fair condition but tree bends over badly
26	Green Ash	3	0%	Remove. Trunk bent and too close to sapling.
27	Cottonwood	14	90%	Very good condition
28	Cottonwood	13	90%	Very good condition
29	Cottonwood	13	90%	Very good condition
30	Silver Maple	7	45%	Fair condition. Poor structure.
31	Green Ash	6	30%	Poor condition. Bad wound where tree grew over fence.
32	Green Ash	8	45%	Fair condition. Original lead girdled and dead
33	Green Ash	9.11	30%	Leans badly. Keep only for screening for neighbor.
34	American Elm	9.7	40%	Fair condition but too crowded to stay.
35	Green Ash	11	35%	Bends badly
36	Green Ash	11	60%	Fair to good condition. In crowded clump.
37	Green Ash	11	60%	Fair to good condition.
38	Green Ash	11	50%	Fair condition. Poor structure.
39	Green Ash	8	60%	Fair to good condition.
40	Black Walnut	9	60%	Some dead leaves persist. Possible 1000 cankers.
41	Green Ash	8	65%	Fair to good condition.
42	Black Walnut	12	75%	Appears to be in good health.
43	Little leaf Linden	25	60%	Fair to good condition. Trunk wound and some internal decay.
44	Green Ash	8	65%	Fair to good condition.
45	Colo Blue Spruce	10	95%	Very good condition.
46	Norway Maple	9	0%	Poor condition. Broken lead, perennial canker, split limb
47	Sugar Maple	26	80%	Good condition. Needs pruning.
48	Honeylocust	17	75%	Good condition.
49	Honeylocust	13	80%	Good condition.
50	Crabapple	16	85%	Good condition. Co-dominant main crotch.
51	Plum	7	25%	Poor condition. Gummosis all over trunk - possible cankers.
52	Honeylocust	15	70%	Good condition. Co-dominant crotch.
53	Siberian Elm	multi	0%	Poor condition, poor structure.
54	Colo Blue Spruce	14	85%	Fair to good condition. Foliage sparse from years of drought.
55	Colo Blue Spruce	8	45%	Fair condition. Crowded by bigger trees on either side.
56	American Linden	20	90%	Very good condition. Nice tree.
57	Siberian Elm	6	45%	Fair condition.
58	Green Ash	9	80%	Good condition.
59	Honeylocust	11	50%	Fair condition. Poor structure.
60	Crabapple	14	70%	Good condition.
61	Crabapple	14	70%	Good condition.
62	Honeylocust	10	70%	Good condition.
63	Siberian Elm	6	35%	Fair condition. Poor structure.
64	Siberian Elm	6.4	35%	Fair condition.
65	Siberian Elm	20	0%	Dead except for some lower re-sprouting. Remove.

This tree survey was performed by Stef Ringgenberg on October 28, 2008 when the trees were mostly dormant, from the ground, with no visual aids or probing inside the trees. These trees have been drought stressed for years and some are in declining health. The condition of the trees was determined, in part, by observing the apparent health of the dormant buds. It was assumed that the appearance of the buds reflects the health of the trees. That assumption could prove incorrect in some of these drought stressed trees.

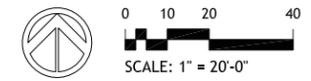
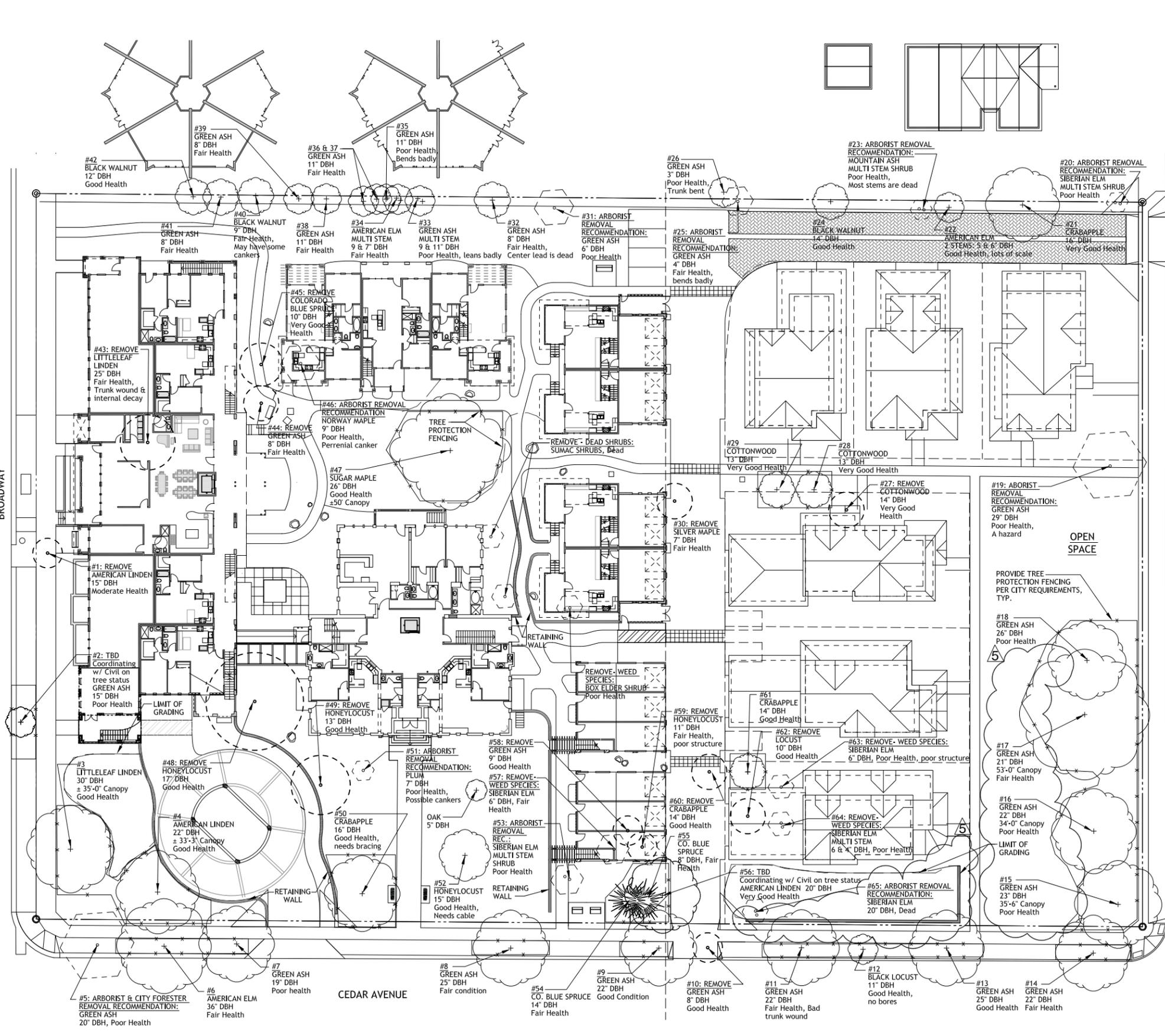
Stef Ringgenberg 303-530-0640 Boulder Tree & Landscape Consulting



NOTE: ALL REMAINING TREES TO MEET TREE PROTECTION STANDARDS IN B.R.C. CHAPTER 6-6 PROTECTION OF TREES AND PLANTS AND CITY OF BOULDER'S DESIGN AND CONSTRUCTION STANDARDS AS REQUIRED.

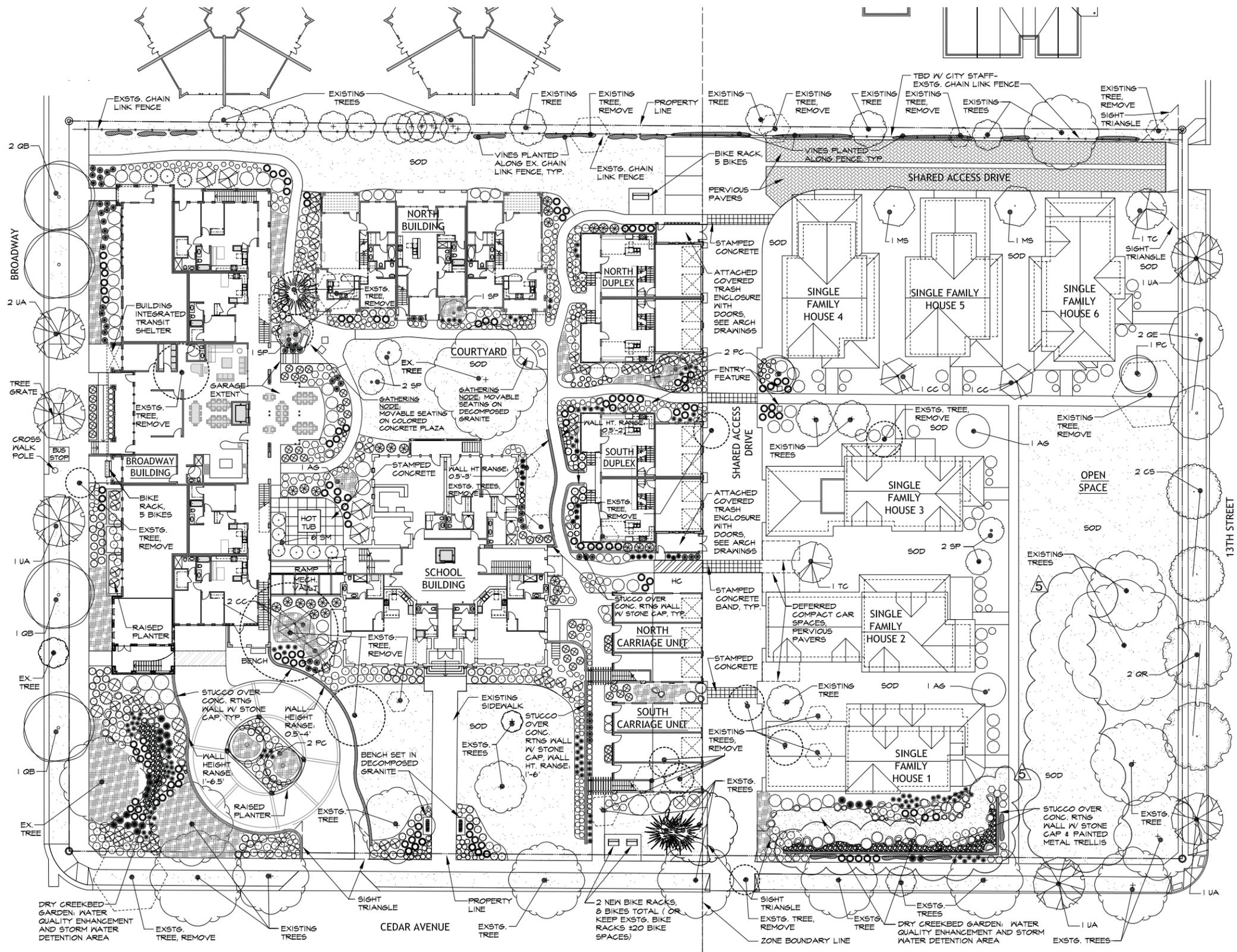
DETAILS ON LP-3 SHEET

APPLICANT AND CITY FORESTRY STAFF TO COORDINATE ON TREE PRESERVATION AND PRUNING SPECIFICATIONS FOR ALL REMAINING TREES.



NOTE: PRELIMINARY TREE PRESERVATION PLAN, NOT FOR CONSTRUCTION

PLANT PALETTE				
DECIDUOUS TREES				
SIZE	QUANTITY	SYMBOL	BOTANICAL NAME	COMMON NAME
15' GAL. MULTI STEM	3	AG	ACER GINNALA	AMUR MAPLE
2' GAL.	2	CS	CATALPA SPECIOSA	WESTERN CATALPA
15' GAL. MULTI STEM	4	CC	CERCIS CANADENSIS	EASTERN REDBUD
2' GAL.	2	MS	MALUS X 'SPRING SNOW'	SPRING SNOW FLOWERING CRABAPPLE
2' GAL.	4	QB	QUERCUS BICOLOR	SWAMP WHITE OAK
2' GAL.	2	QE	QUERCUS ELLIPSOIDALIS	NORTHERN PIN OAK
2' GAL.	2	QR	QUERCUS RUBRA	NORTHERN RED OAK
2' GAL.	5	PC	PYRUS CALLERYANA CHANTICLEER	CHANTICLEER PEAR
2' GAL.	5	SP	SOPHORA JAPONICA	JAPANESE PAGODA TREE
2' GAL.	2	TC	TILIA CORDATA 'GREENSPIRE'	GREENSPIRE LINDEN
2' GAL.	6	UA	ULMUS AMERICANA 'VALLEY FORGE'	VALLEY FORGE AMERICAN ELM
DECIDUOUS SHRUBS				
SIZE	QUANTITY	SYMBOL	BOTANICAL NAME	COMMON NAME
5 GAL.		CCD	CARYOPTERIS X CLANDONENSIS 'DARK KNIGHT'	DARK KNIGHT BLUE MIST SPIRAEA
5 GAL.		HS	HIBISCUS SYRIACUS 'WOODBRIDGE'	SINGLE RED-PURPLE ALTHEA
5 GAL.		LT	LONICERA TATARICA 'ARNOLD'S RED'	ARNOLD'S RED HONEYSUCKLE
5 GAL.		PA	PEROVSKIA ATRIFLIGIFOLIA	RUSSIAN SAGE
5 GAL.		PF	POTENTILLA FRUTICOSA 'PINK BEAUTY'	PINK BEAUTY POTENTILLA
5 GAL.		RAG	RHUS AROMATICA 'GROWN LOW'	GROWN LOW SUMAC
5 GAL.		RA	RIBES AUREUM	GOLDEN CURRANT
5 GAL.		RMR	ROSA X MEIDLAND RED	SINGLE RED SHRUB ROSE
5 GAL.		RWH	ROSA X MEIDLAND WHITE	DOUBLE WHITE SHRUB ROSE
5 GAL.		SJ	SPIREA JAPONICA 'NEON FLASH'	NEON FLASH SPIREA
5 GAL.		SO	SYMPHORICARPUS OREOPHILUS	MOUNTAIN SNOWBERRY
5 GAL.		SM	SYRINGA MEYERI 'MISS KIM'	MISS KIM DWARF KOREAN LILAC
5 GAL.		VL	VIBURNUM LENTAGUM	NANNYBERRY VIBURNUM
5 GAL.		VT	VIBURNUM TRILOBUM 'COMPACTA'	COMPACT AMERICAN CRANBERRY
EVERGREEN SHRUBS				
SIZE	QUANTITY	SYMBOL	BOTANICAL NAME	COMMON NAME
5 GAL.		AC	ARCTOSTAPHYLOS X COLORADENSIS	PANCHITO MANZANITA
5 GAL.		BM	BUXUS MICROPHYLLA 'INSULARIS WINTER GEM'	WINTER GEM BOXWOOD
5 GAL.		CA	COTONEASTER APICULATUS	CRANBERRY COTONEASTER
5 GAL.		EFV	EUONYMUS FORTUNEI 'COLORATUS'	PURPLE LEAF WINTERCREEPER
5 GAL.		EFE	EUONYMUS FORTUNEI 'EMERALD SIAETY'	EMERALD SIAETY EUONYMUS
1 GAL.		JHH	JUNIPERUS HORIZONTALIS 'HUGHES'	HUGHES' JUNIPER
1 GAL.		JS	JUNIPERUS SABINA 'CALGARY CARPET'	CALGARY CARPET JUNIPER
5 GAL.		JST	JUNIPERUS SCOPOLORUM 'TABLE TOP BLUE'	TABLE TOP BLUE JUNIPER
5 GAL.		PPM	PICEA PENSILVANA 'MESA VERDE'	MESA VERDE SPRUCE
5 GAL.		PHS	PINUS MISO 'SLOWMOUND'	SLOWMOUND MISO PINE
5 GAL.		PHW	PINUS MISO 'WHITE BUD'	WHITE BUD MISO PINE
PERENNIALS				
SIZE	QUANTITY	SYMBOL	BOTANICAL NAME	COMMON NAME
1 GAL.		AR	AGASTACHE RUPESTRIS	SUNSET HYSSOP
1 GAL.		BL	BERLANDIERA LYRATA	CHOCOLATE FLOWER
1 GAL.		GA	GAILLARDIA ARISTATA	NATIVE BLANKET FLOWER
1 GAL.		HSD	HEMEROCALLIS 'STELLA DE ORO'	STELLA DE ORO DAYLILY
1 GAL.		IH	IRIS HYBRIDS	BEARDED IRIS
1 GAL.		LAM	LAVENDULA ANGUSTIFOLIA 'MUNSTEAD'	LAVENDER
1 GAL.		MM	MIRABILIS MULTIFLORA	DESERT FOUR O' CLOCK
1 GAL.		NP	NEPETA X FAASSENII	CATMINT
1 GAL.		OCM	OENOTHERA CAESPITOSA 'MARGINATA'	WHITE EVENING PRIMROSE
1 GAL.		PF	PENSTEMON PINIFOLIUS	PINELEAF PENSTEMON
1 GAL.		PS	PENSTEMON STRICTUS	ROCKY MOUNTAIN PENSTEMON
5 GAL.	3	PAT	PEROVSKIA ATRIFLIGIFOLIA	RUSSIAN SAGE
1 GAL.		RC	RATIBIDA COLIMNIFERA 'MEXICAN HAT'	MEXICAN HAT CONEFLOWER
1 GAL.	4	SAP	SALVIA PACHYPHYLLA	MOJAVE SAGE
1 GAL.	5	SB	STACHYS BYANTINA	LAMB'S EAR
1 GAL.		VS	VERONICA SPICATA 'TALL BLUE'	BLUE SPIKE SPEEDWELL



ORNAMENTAL GRASSES				
SIZE	QUANTITY	SYMBOL	BOTANICAL NAME	COMMON NAME
2 GAL.		CAO	GALAMAGROSSTIS ACUTIFLORA 'OVERDAM'	VARIATED FEATHER REED GRASS
2 GAL.		CAK	GALAMAGROSSTIS ARADINACEA 'KARL FORESTER'	FEATHER REED GRASS
2 GAL.		HB	HELICTOTRICHON SEMPERVIRENS	BLUE AVENA GRASS
2 GAL.		MS	MISCANTHUS SINENSIS 'PURPURESCENS'	PURPLE MAIDEN GRASS
2 GAL.		PV	PANICUM VIRGATUM 'SHENANDOAH'	SHENANDOAH RED SWITCH GRASS
2 GAL.		PAH	PENNISETUM ALOPECUROIDES 'HAMELIN'	HARDY FOUNTAIN GRASS

VINES				
SIZE	QUANTITY	SYMBOL	BOTANICAL NAME	COMMON NAME
1 GAL.		LJH	LONICERA JAPONICA 'HALLIANA'	HALL'S HONEYSUCKLE
1 GAL.		PQ	PARTHENOCISSUS QUINGUEFOLIA	VIRGINIA CREEPER
1 GAL.		PA	POLYGONUM AUBERTII	SILVER LACE VINE

TURF GRASS
SOD - LOCAL GROWN REVELLE LOW-WATER BLUEGRASS BLEND



NOTE: LANDSCAPE PLAN FOR CITY REVIEW, NOT FOR CONSTRUCTION

REFER TO LANDSCAPE DETAIL SHEETS FOR LANDSCAPE REQUIREMENTS TABLE, PLANTING NOTES AND ADDITIONAL INFORMATION

303
ARCHITECTURE,
INC.

ARCHITECTURE LANDSCAPE ARCHITECTURE PLANNING
1942 BROADWAY, SUITE 314 BOULDER, COLORADO 80302
P: 303-447-6448 C: 303-641-6708 F: 303-648-5683
Stephanie@303Architecture.com

ARCH
ITECTURE
INCORPORATED

ARCHITECTURE
PLANNING &
INTERIOR DESIGN
3008 FOLSOM STREET
BOULDER, CO 80304
PHONE: 303-444-6909
FAX: 303-448-6160



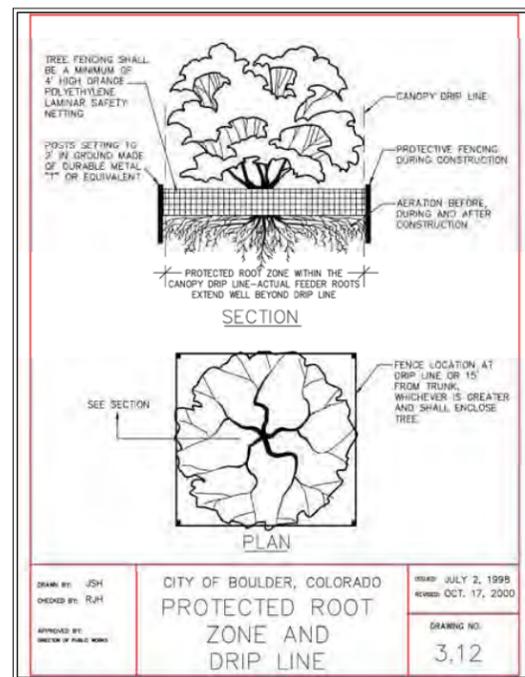
WASHINGTON VILLAGE II
1215 CEDAR AVE
BOULDER, COLORADO

REVISED LANDSCAPE
CONCEPT - DETENTION
BASIN ALTERNATIVE

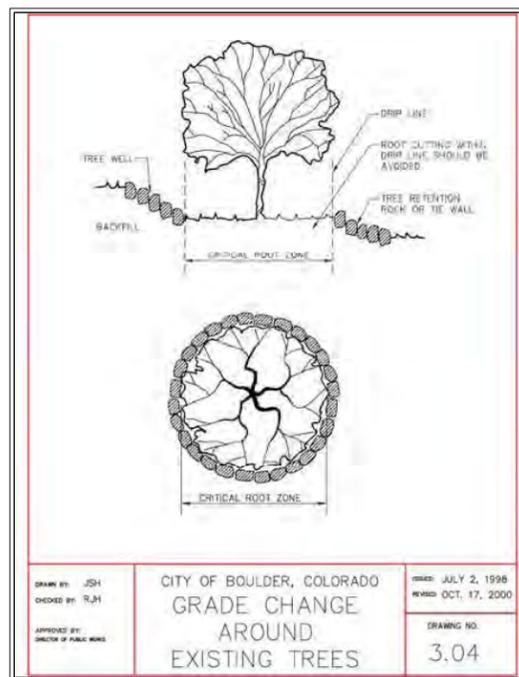
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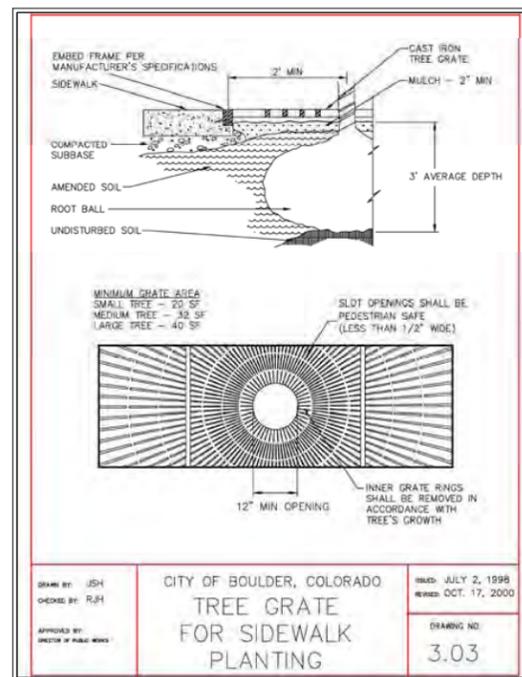
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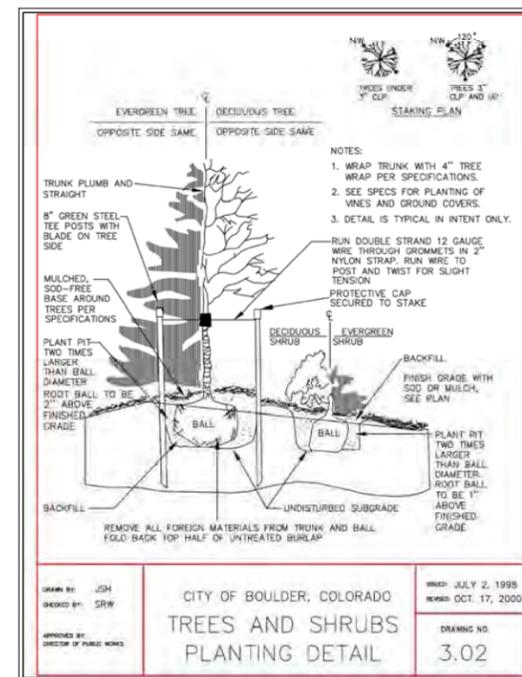
CITY OF BOULDER DETAIL 3.12:
PROTECTED ROOT ZONE AND DRIP LINE



CITY OF BOULDER DETAIL 3.04:
GRADE CHANGE AROUND EXISTING TREES



CITY OF BOULDER DETAIL 3.03:
TREE GRATE FOR SIDEWALK PLANTING



CITY OF BOULDER DETAIL 3.02:
TREES & SHRUBS PLANTING DETAIL

PLANTING NOTES

1. ALL PLANT MATERIAL SHALL MEET SPECIFICATIONS OF THE AMERICAN ASSOCIATION OF NURSERYMEN (AAN) FOR NUMBER ONE GRADE. ALL TREES SHALL BE BALLED AND BURLAPPED OR EQUIVALENT.
2. TREES SHALL NOT BE PLANTED CLOSER THAN 4 FEET TO ANY GAS OR ELECTRICAL LINE AND NO CLOSER THAN 10 FEET TO ANY WATER OR SEWER LINE. LARGE SHRUBS SHALL NOT BE PLANTED CLOSER THAN 4 FEET TO ANY WATER OR SEWER LINE. LOCATIONS OF ALL UTILITIES SHALL BE VERIFIED IN THE FIELD PRIOR TO PLANTING TREES AND LARGE SHRUBS.
3. GRADES SHALL BE SET TO ALLOW FOR PROPER DRAINAGE AWAY FROM ALL STRUCTURES. GRADES SHALL MAINTAIN SMOOTH PROFILES AND BE FREE OF SURFACE DEBRIS, BUMPS & DEPRESSIONS.
4. ALL SHRUB BEDS ADJACENT TO TURF AREAS SHALL BE EDGED WITH RYERSON OR APPROVED EQUIVALENT ROLLED STEEL EDGER.
5. ALL SHRUB BED AREAS SHALL BE MULCHED WITH A 3" LAYER OF WOODBARK MULCH OVER WEED BARRIER FABRIC. PERENNIALS AND GROUND COVER AREAS SHALL BE MULCHED WITH A 3" LAYER OF SHREDDED WOODBARK MULCH.
6. DURING CONSTRUCTION, PREVENT CLEANING OF EQUIPMENT, THE STORAGE OR DISPOSAL OF WASTE BUILDING MATERIALS (I.E. PAINT, OILS, SOLVENTS, ASPHALT, CONCRETE, MORTAR, ETC) WITHIN THE DRIFLINE OF ANY PROTECTED TREE.
7. ALL AREAS DESIGNATED FOR PLANTING BEDS SHALL BE AMENDED WITH 6 CUBIC YARDS PER 1,000 SQUARE FEET OF MANURE COMPOST. THE CONTRACTOR SHALL PREPARE THE SUBGRADE BY ROTOTILLING TO A DEPTH OF 8" THEN ADDING THE SOIL AMENDMENTS AND ROTOTILLING TO A DEPTH OF 8".
8. LANDSCAPE CONTRACTOR SHALL VERIFY ALL MATERIAL QUANTITIES PRIOR TO INSTALLATION. ACTUAL NUMBER OF SYMBOLS SHALL HAVE PRIORITY OVER THE QUANTITY DESIGNATED.
9. REFER TO THE CITY OF BOULDER'S DESIGN AND CONSTRUCTION STANDARDS' AND 'LANDSCAPE REQUIREMENTS FOR STREETSAPES, PARKING LOTS AND ALL OTHER DEVELOPMENTS'
10. REFER TO THE CIVIL ENGINEERING DRAWINGS FOR GRADING AND UTILITY INFORMATION

LANDSCAPE REQUIREMENTS TABLE

ENTIRE SITE - TOTAL LOT SIZE:	130,710 S.F.	
TOTAL PARKING LOT SIZE:	14,181 S.F.	
SURFACE PARKING AREA	712 S.F.	
VEHICLE DROPOFF & SHARED ACCESS DRIVE	13,469 S.F.	
TOTAL AREA COVERED BY BUILDINGS	40,024 S.F.	
RH2 SITE: TOTAL BUILDING COVERAGE	30,124 S.F.	
RH1 SITE: TOTAL SINGLE FAMILY BUILDING COVERAGE	9,000 S.F.	
RH1 SITE: TOTAL SINGLE FAMILY PORCH COVERAGE	400 S.F.	
TOTAL AREA NOT COVERED BY A BUILDING OR PARKING LOT	76,505 S.F.	
	REQUIRED	PROVIDED
TOTAL NUMBER OF SURFACE PARKING STALLS	8	8
TOTAL INT. PARKING LOT LANDSCAPED AREA	NA	NA
TOTAL INTERIOR PARKING LOT LANDSCAPED AREA AS A % OF TOTAL PARKING LOT AREA	NA	NA
TOTAL # OF TREES IN INTERIOR LOT LANDSCAPED AREA	26	53
TOTAL PERIMETER PARKING LOT LANDSCAPED AREA	NA	NA
TOTAL NUMBER OF STREET TREES	25	25
TOTAL QUANTITY OF PLANT MATERIAL ON SITE PLAN (* INCLUDES KEPT EXISTING TREES)	TREES / SHRUBS 51 / 255	TREES / SHRUBS 77 / 302
TOTAL QUANTITY OF KEPT EXISTING TREES	15 TREES	40 TREES

303
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P: 303-447-6448 C: 303-641-6708 F: 303-648-5683
Stephanie@303Architecture.com

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PLANNING &
INTERIOR DESIGN



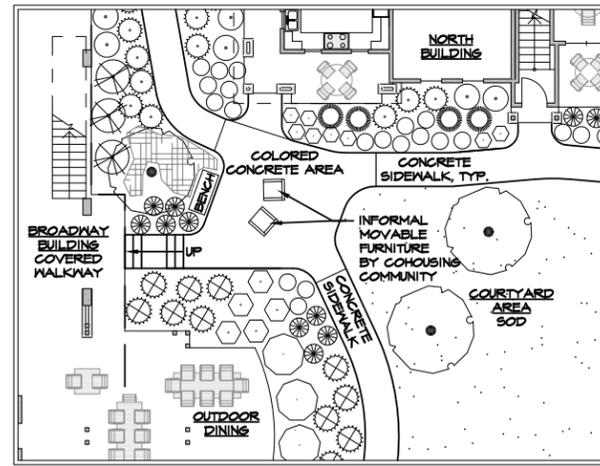
WASHINGTON VILLAGE II
1215 CEDAR AVE
BOULDER, COLORADO

LANDSCAPE DETAILS

DATE: 17 NOV 08

LP-3

JOB: WVI



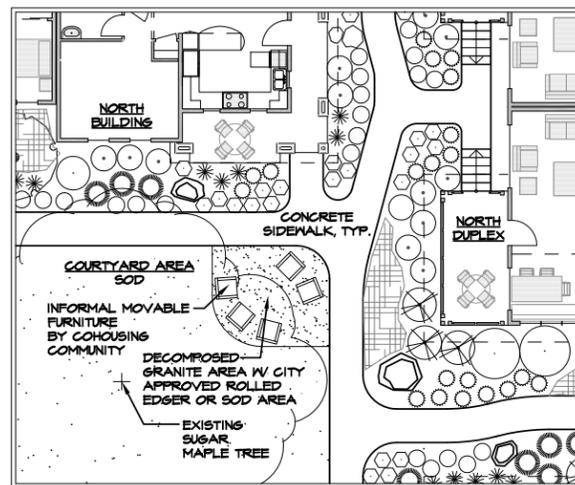
INFORMAL GATHERING NODE
NEAR BROADWAY BUILDING & INTERIOR COURTYARD
SCALE: 1"=10'-0"



PHOTO EXAMPLE 2:
BIRTHDAY PARTY AT INFORMAL GATHERING NODE
AT NOMAD COHOUSING IN BOULDER, COLORADO



PHOTO EXAMPLE 1:
INFORMAL GATHERING NODE AT
NOMAD COHOUSING IN BOULDER, COLORADO



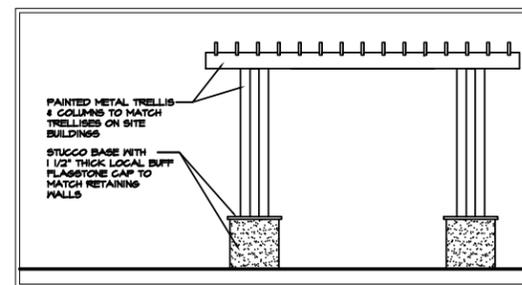
INFORMAL GATHERING NODE NEAR NORTH DUPLEX
BUILDING & INTERIOR COURTYARD
SCALE: 1"=10'-0"



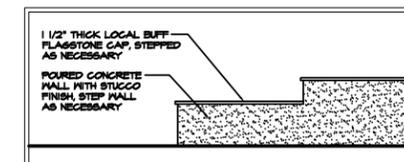
PHOTO EXAMPLE 4:
INFORMAL GATHERING NODE EXAMPLE,
SUN AND WIND COHOUSING, DENMARK



PHOTO EXAMPLE 3:
INFORMAL GATHERING NODE EXAMPLE,
COHOUSING COMMUNITY, EAST COAST



ENTRY FEATURE EXAMPLE
SCALE: 1/4"=1'-0"



RETAINING WALL EXAMPLE
SCALE: 1/4"=1'-0"



BENCH EXAMPLE:
DUMOR 5' LONG STEEL BENCH
MODEL 160 OR SIMILAR

NOTE: BIKE RACK LAYOUT AND
SPECS FOUND ON SHEET LP-5

303
ARCHITECTURE,
INC.

ARCHITECTURE LANDSCAPE ARCHITECTURE PLANNING

1942 BROADWAY, SUITE 314 BOULDER, COLORADO 80302
P: 303-447-6448 C: 303-641-6708 F: 303-648-5683
Stephanie@303Architecture.com

ARCH
ITECTURE
INCORPORATED

ARCHITECTURE
PLANNING &
INTERIOR DESIGN

3008 FOLSOM STREET
BOULDER, CO 80304
PHONE: 303-444-6908
FAX: 303-442-6160



WASHINGTON VILLAGE II

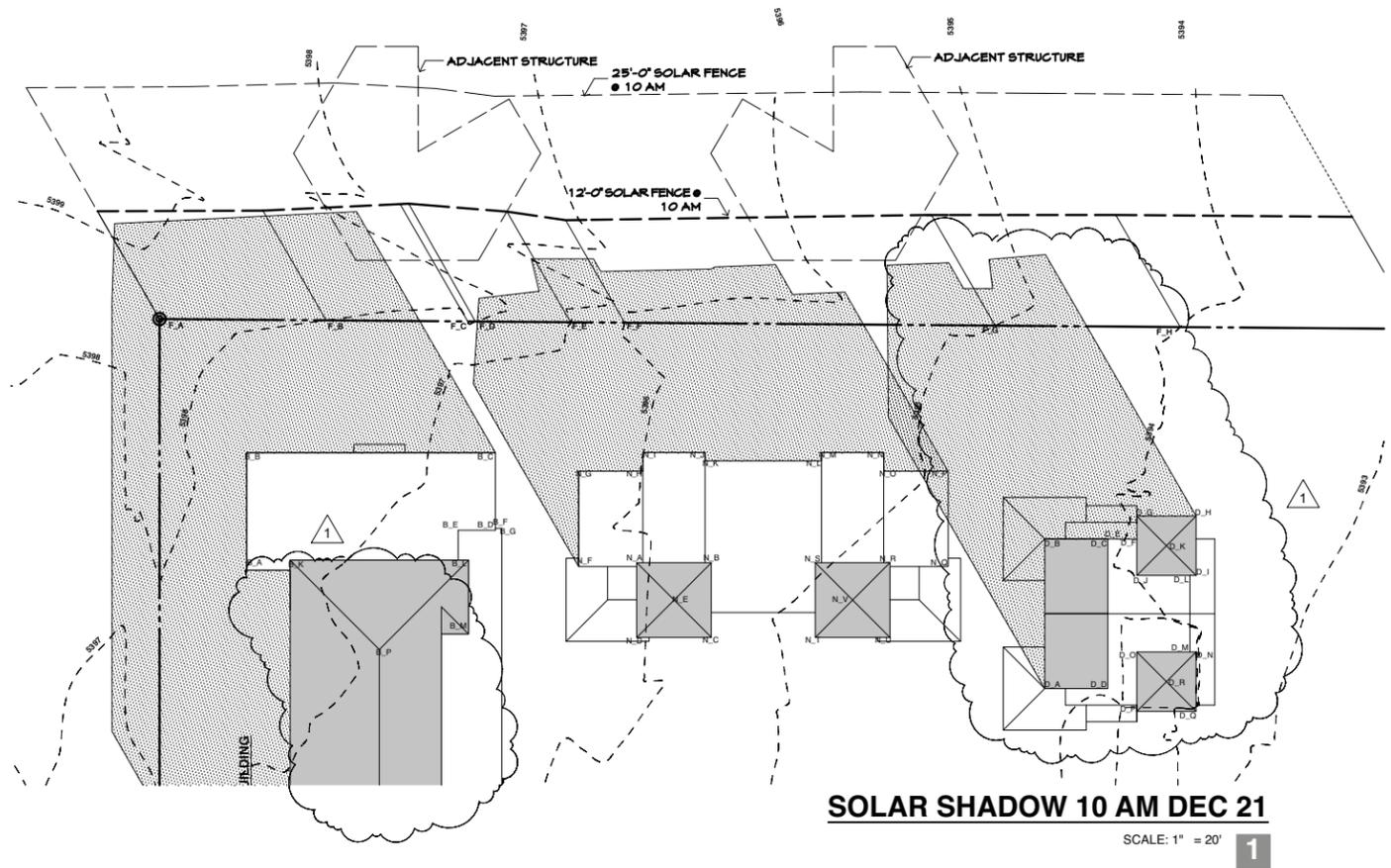
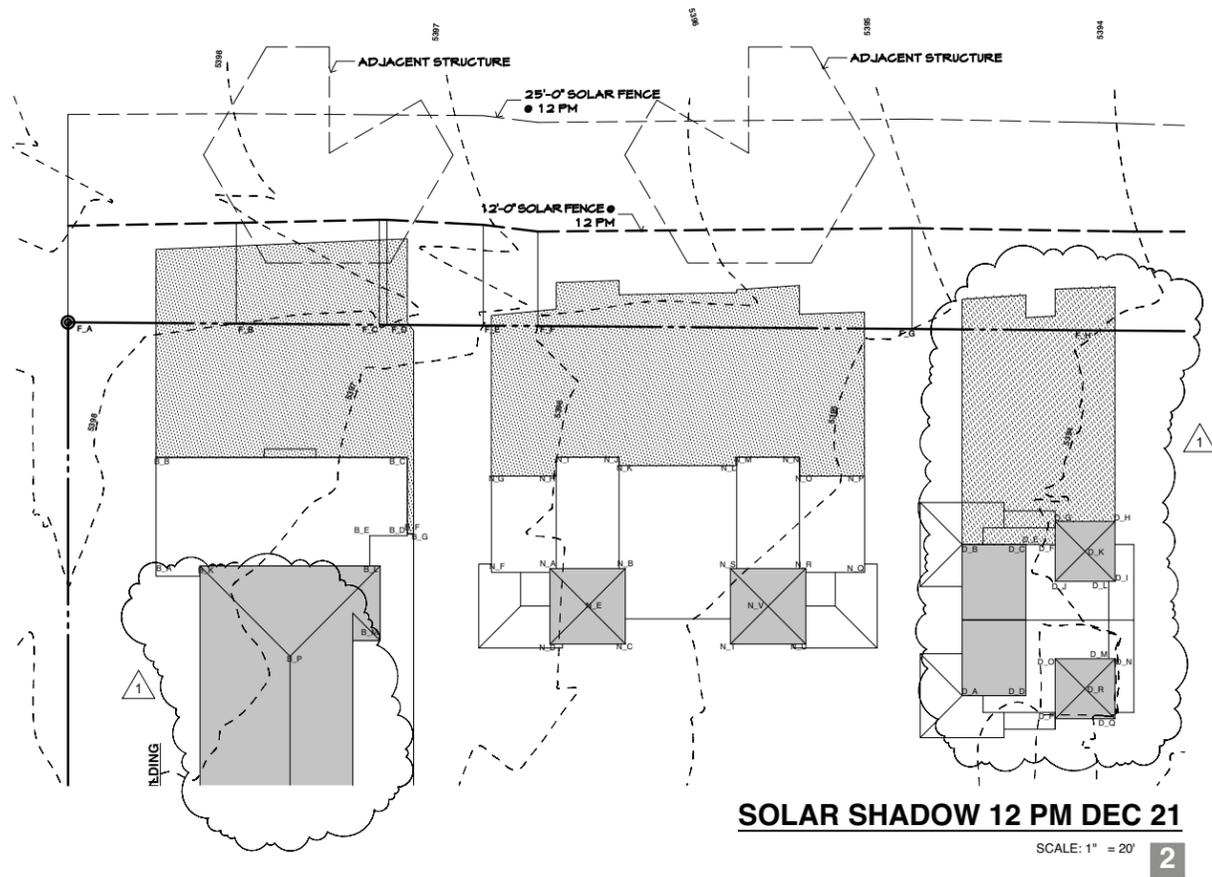
1215 CEDAR AVE
BOULDER, COLORADO

LANDSCAPE DETAILS

DATE: 11 NOV 08

LP-4

JOB: WW-II



SOLAR ANALYSIS CHART @ 25'-0" FENCE & 12'-0" FENCE

POINT	HGT ABV GRADE	ADJUSTED SHADOW LENGTH		
		10AM	NOON	2PM
F_A	25.00	64.26	49.50	66.91
F_B	25.00	64.93	50.00	67.58
F_C	25.00	66.25	51.00	69.56
F_D	25.00	66.25	51.00	69.56
F_E	25.00	64.93	52.00	68.24
F_F	25.00	62.94	48.50	66.25
F_G	25.00	64.93	50.00	67.58
F_H	25.00	64.26	49.50	66.91
F_I	25.00			
F_J	25.00			
F_A	12.00	29.95	23.00	31.14
F_B	12.00	30.48	24.00	32.46
F_C	12.00	32.60	25.00	33.79
F_D	12.00	32.60	25.00	33.79
F_E	12.00	30.74	24.00	32.46
F_F	12.00	28.49	22.50	30.48
F_G	12.00	30.74	24.00	32.46
F_H	12.00	30.48	23.50	32.46
F_I	12.00	30.48	23.50	31.14

SOLAR ANALYSIS CHART @ BROADWAY BLDG

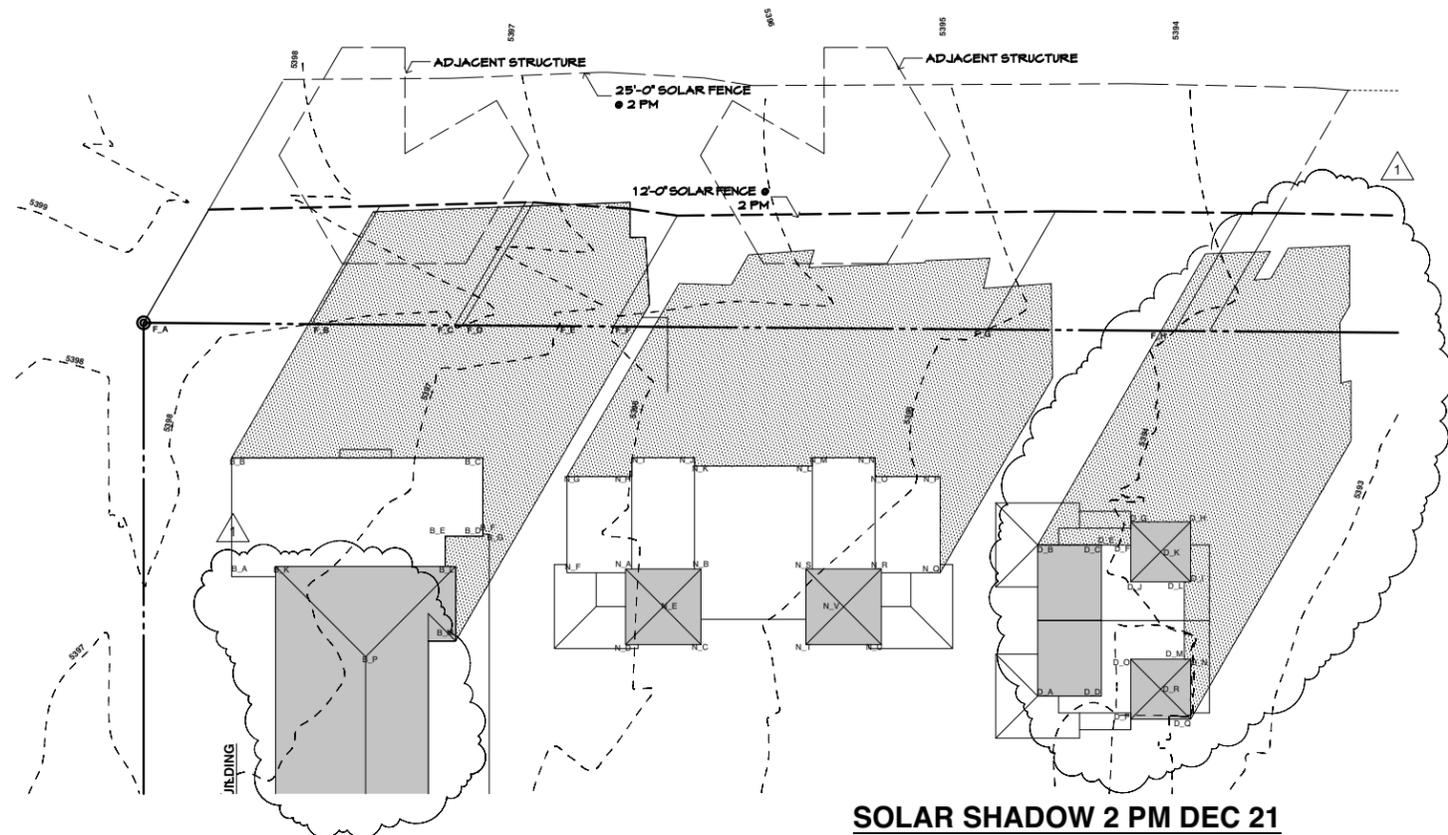
POINT	HGT ABV GRADE	ADJUSTED SHADOW LENGTH		
		10AM	NOON	2PM
B_A	25.77	66.30	50.54	67.63
B_B	25.52	63.65	49.54	67.63
B_C	26.52	66.97	52.04	70.28
B_D	26.52	66.30	50.54	70.28
B_E	26.52	66.30	50.54	70.28
B_F	25.18	62.75	47.86	66.73
B_G	25.18	62.75	48.36	66.73
B_K	33.68	84.61	65.56	89.25
B_L	34.18	85.94	65.86	90.58
B_M	34.18	86.60	67.84	92.56
B_P	44.68	113.10	86.86	119.09

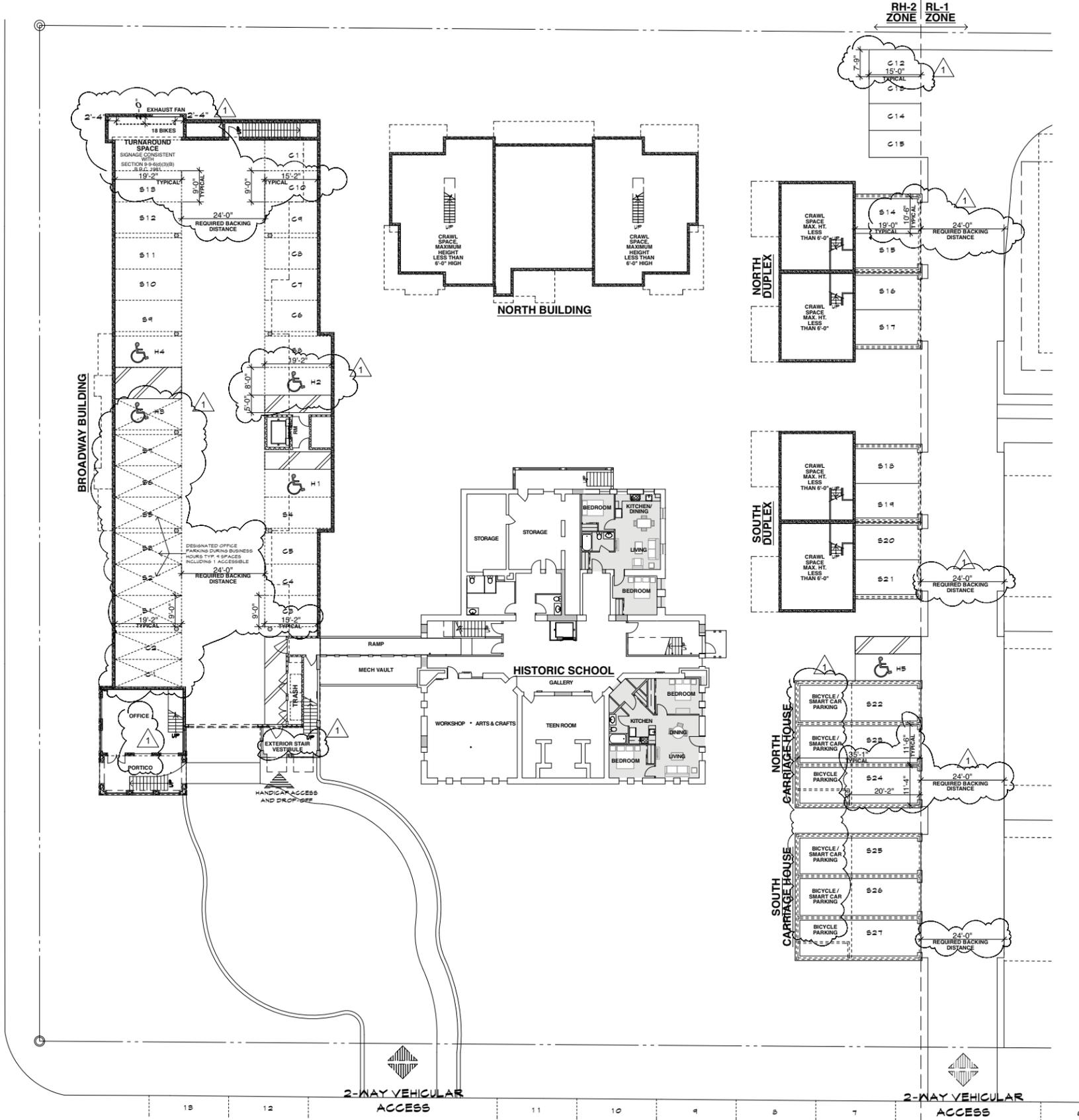
SOLAR ANALYSIS CHART @ NORTH BLDG

POINT	HGT ABV GRADE	ADJUSTED SHADOW LENGTH		
		10AM	NOON	2PM
N_A	22.58	56.52	44.66	60.50
N_B	23.08	57.85	45.66	61.16
N_C	23.08	58.51	45.66	61.82
N_D	22.58	57.85	45.16	61.16
N_E	27.25	68.24	54.00	72.21
N_F	19.87	50.67	39.24	53.98
N_G	19.87	48.02	38.24	53.32
N_H	20.12	50.01	39.74	52.66
N_I	21.29	53.77	41.58	55.76
N_J	21.54	53.77	42.08	57.08
N_K	20.85	52.60	40.70	54.59
N_L	21.35	53.27	41.20	55.92
N_M	20.70	51.54	39.90	54.19
N_N	20.70	52.20	40.90	54.85
N_O	19.53	49.10	38.56	51.75
N_P	19.78	49.77	39.06	53.08
N_Q	20.03	50.96	40.06	53.74
N_R	22.49	56.95	43.98	59.60
N_S	22.24	56.95	43.48	58.94
N_T	22.49	56.95	43.48	59.60
N_U	22.49	57.61	43.98	60.26
N_V	27.00	68.90	52.50	70.89

SOLAR ANALYSIS CHART @ NORTH DUPLEX BLDG

POINT	HGT ABV GRADE	ADJUSTED SHADOW LENGTH		
		10AM	NOON	2PM
D_A	29.20	75.39	59.40	80.69
D_B	29.70	76.05	58.40	80.03
D_C	30.20	76.72	59.40	80.69
D_D	29.20	77.38	59.90	81.35
D_E	27.50	69.56	54.00	72.88
D_F	27.50	69.56	54.50	73.54
D_G	28.15	71.28	55.30	75.26
D_H	28.40	72.61	55.80	75.92
D_I	28.40	71.95	56.80	75.92
D_J	28.15	71.28	56.30	75.26
D_K	31.25	78.84	61.50	83.48
D_L	27.75	70.23	55.50	74.20
D_M	26.50	71.55	55.50	74.86
D_N	27.15	73.27	56.80	76.58
D_O	27.15	72.61	55.80	75.92
D_P	27.15	72.61	56.30	75.92
D_Q	28.15	73.27	56.80	76.58
D_R	30.00	80.16	62.00	84.14





PARKING SUMMARY

RH-2 ZONE - ON SITE

BELOW BUILDING STRUCTURED PARKING	
BROADWAY BUILDING	13
STANDARD	11
COMPACT	4
HANDICAP	1
ON-GRADE PARKING	
COMPACT (NORTH OF DUPLEX)	4
HANDICAP (NORTH OF NORTHERN CARRIAGE HOUSE)	1
TUCK-UNDER PARKING	
STANDARD (DUPLEXES)	8
STANDARD (CARRIAGE UNITS)	6
TOTAL STANDARD CARS	27
TOTAL COMPACT CARS	15
TOTAL HANDICAP CARS	5
TOTAL CARS PROVIDED FOR RH-2 ZONE ON-SITE	47

RL-1 ZONE - ON SITE

ON-GRADE INTERIOR GARAGE PARKING	
SINGLE FAMILY RESIDENCES (presumed 2 cars for each single family detached unit)	12
TOTAL CARS PROVIDED FOR RL-1 ZONE ON-SITE	12
TOTAL AMOUNT OF PARKING SPACES PROVIDED ON SITE	59
ON STREET PARKING	
CEDAR AVENUE	13
13th STREET	11
TOTAL AMOUNT OF PARKING SPACES AVAILABLE ON STREET	24

PARKING ANALYSIS BASED ON RH-2 ZONE REQUIREMENTS

RH-2 ZONE CONTRIBUTING AREAS	UNIT SIZE	CARS/UNIT (4 MAX)	UNITS	TOTAL CARS
PRIVATE RESIDENTIAL AREAS (count toward total required spaces)				
HISTORIC SCHOOL				
BASEMENT LEVEL				
S1NE 1-STORY AFFORDABLE UNIT (NORTHEAST)	801	3	1	3
S2SE 1-STORY AFFORDABLE UNIT (SOUTHEAST)	801	3	1	3
FIRST FLOOR (exits to interior corridor)				
S1W 1-STORY MARKET RATE UNIT (SOUTH-WEST)	1,175	4	1	4
S3E 1-STORY MARKET RATE UNIT (SOUTH-EAST)	1,175	4	1	4
S4N 1-STORY MARKET RATE UNIT (NORTH)	2,084	4	1	4
SECOND FLOOR (exits to interior corridor)				
S5N 1 STORY MARKET RATE UNIT (SOUTH)	2,282	4	1	4
S6S 1 STORY MARKET RATE UNIT (NORTH)	2,084	4	1	4
BROADWAY BUILDING				
FIRST FLOOR (exits to grade)				
B1S 1-STORY AFFORDABLE UNIT (SOUTH)	919	3	1	3
B1N 1-STORY AFFORDABLE UNIT (NORTH)	919	3	1	3
B2S 1-STORY MODERATE UNIT (SOUTH)	606	2	1	2
B2N 1-STORY MODERATE UNIT (NORTH)	606	2	1	2
SECOND FLOOR (exits via outdoor porch to stair towers)				
B3S 1-STORY MARKET RATE UNIT (SOUTH)	1,601	4	1	4
B4S 2-STORY MARKET RATE UNIT (SOUTH)	2,151	4	1	4
BSN&S1-STORY MARKET RATE UNITS (MIDDLE)	1,182	4	2	8
B6 2-STORY MARKET RATE UNIT (NORTH)	2,151	4	1	4
B7 1-STORY MARKET RATE UNIT (NORTH)	1,471	4	1	4
NORTH BUILDING				
FIRST FLOOR (exits to grade)				
N1W 2-STORY MARKET RATE UNIT (WEST)	1,785	4	1	4
N1E 2-STORY MARKET RATE UNIT (EAST)	1,785	4	1	4
N2 1-STORY AFFORDABLE UNIT (LOWER)	1,001	3	1	3
SECOND FLOOR (exits via outdoor porch to stair towers)				
N3 1-STORY AFFORDABLE UNIT (UPPER)	1,024	3	1	3
DUPLEXES				
E1N&S3-STORY MARKET TOWNHOMES (NORTH)	1640	4	4	16
CARRIAGE HOUSES				
C1N&S1-STORY AFFORDABLE FLAT (ABOVE GARAGES)	901	3	2	6

RH-2 ZONE RESIDENTIAL PARKING REQUIRED (per Land Use Code) 27 96

RH-2 ZONE PARKING REDUCTION REQUESTED FOR RESIDENTIAL USE (refer to PARKING SUMMARY)

TOTAL NUMBER OF SPACES PROVIDED FOR RESIDENTIAL USE	38
PERCENTAGE PARKING REDUCTION FOR RESIDENTIAL USE	60%

RH-2 ZONE TOTAL OFFICE PARKING REQUIRED

OFFICE USE	OFFICES	SFCAR	
OFF-STREET PARKING REQUIRED (typically 1 car/300 sq ft of office use)	2,246	300	9
RH-2 ZONE TOTAL OFFICE PARKING REQUIRED			9

RH-2 ZONE TOTAL PARKING REQUIRED 105

RH-2 ZONE TOTAL PARKING REDUCTION REQUESTED (refer to PARKING SUMMARY)

TOTAL NUMBER OF SPACES PROVIDED IN RH-2 ZONE	47
TOTAL PERCENTAGE PARKING REDUCTION REQUESTED	55.2%

PARKING ANALYSIS BASED ON OTHER RH ZONES

1 BEDROOM = 1 CAR, 2 BEDROOM = 1.5 CARS, 3 BEDROOM = 2 CARS, 4+ BEDROOM = 4 CARS

RH-2 ZONE CONTRIBUTING AREAS	BEDROOMS PER UNIT	CARS PER UNIT	NUMBER OF UNITS	TOTAL CARS
PRIVATE RESIDENTIAL AREAS (count toward total required spaces)				
HISTORIC SCHOOL				
BASEMENT LEVEL				
S1 AFFORDABLE (NORTH EAST)	2	1.5	1	1.5
S2 AFFORDABLE (SOUTH EAST)	2	1.5	1	1.5
MAIN LEVEL				
S3 MARKET FLAT SOUTHWEST AND SOUTHEAST	1	1	2	2
S4 MARKET FLAT NORTH	3	2	1	2
UPPER LEVEL				
S5 MARKET FLAT SOUTH	3	2	1	2
S6 MARKET FLAT NORTH	3	2	1	2
BROADWAY BUILDING				
MAIN LEVEL				
B1 AFFORDABLE CENTRAL UNITS	1	1	2	2
B2 AFFORDABLE END UNITS	2	1.5	2	3
UPPER LEVEL				
B3 MARKET FLAT SOUTH	2	1.5	1	1.5
B4 MARKET FLAT NORTH	3	2	1	2
B5 MARKET SOUTH TOWNHOUSE	2	1.5	1	1.5
B6 MARKET NORTH TOWNHOUSE	3	2	1	2
B7 MARKET FLAT WITH HIGH LIVING	1	1	2	2
NORTH BUILDING				
MAIN LEVEL				
N1 MARKET TOWN HOMES EAST AND WEST	3	2	2	4
N2 AFFORDABLE MODERATE LOWER	2	1.5	1	1.5
N3 AFFORDABLE MODERATE UPPER	2	1.5	1	1.5
DUPLEX				
E1 MARKET TOWN HOMES	2	1.5	4	6
CARRIAGE				
C1 AFFORDABLE INCL NORTH	2	1.5	1	1.5
C2 AFFORDABLE MODERATE SOUTH	2	1.5	1	1.5

TOTAL CARS IN RH-2 ZONE USING RH-3,4,5 REQUIREMENTS 41

OFFICE USE

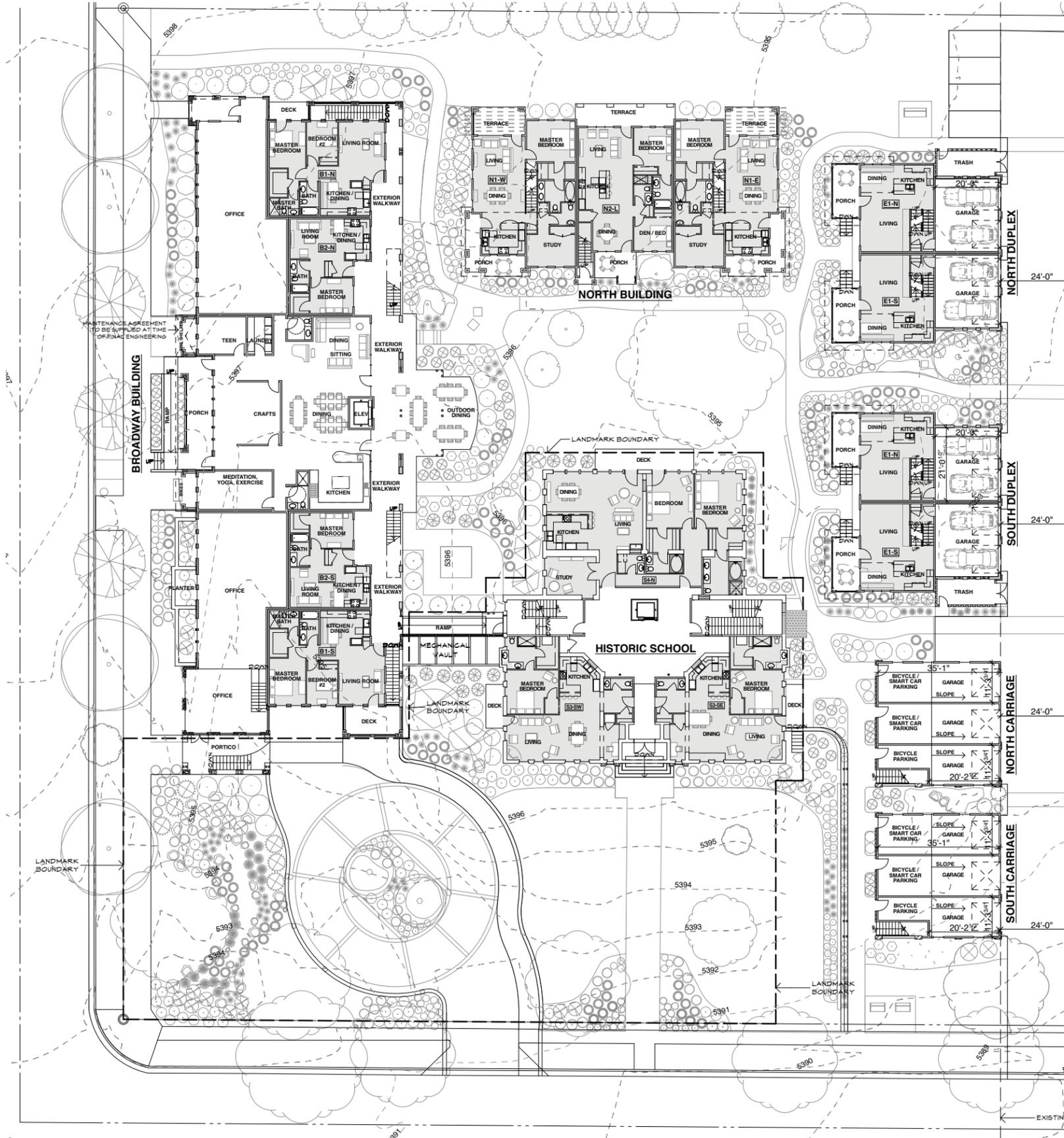
OFF-STREET PARKING REQUIRED (typically 1 car/300 sq ft of office use)	2,246	300	9,820
RH-2 ZONE TOTAL OFFICE PARKING REQUIRED			9

TOTAL CARS REQUIRED IN RH-2 ZONE 50

TOTAL PARKING REQUIRED USING OTHER RH ZONES	50
TOTAL NUMBER OF SPACES PROVIDED IN RH-2 ZONE	47
TOTAL PERCENTAGE PARKING REDUCTION	6%

BASEMENT/GARAGE PLAN

SCALE: 1/16" = 1'-0"



TOTAL SITE AREA	130,710 SF
RH-2 ZONE	74,398 SF
RL-1 ZONE	56,312 SF

FLOOR AREA CALCULATIONS (Res)

RH-2 ZONE	
BROADWAY BUILDING	
FIRST FLOOR	3050
SECOND FLOOR	7570
THIRD FLOOR	2014
TOTAL	12634
NORTH BUILDING	
FIRST FLOOR	3235
SECOND FLOOR	2344
TOTAL	5579
EAST BUILDINGS	
FIRST FLOOR	2210
LIVING ROOM	1440
SECOND FLOOR	2910
TOTAL	6560
SCHOOL BUILDING	
BASEMENT	1669
FIRST FLOOR	4474
SECOND FLOOR	4481
TOTAL	10624
CARRIAGE HOUSES	
FIRST FLOOR	1802
TOTAL	1802
TOTAL UNITS SF	37199

TOTAL SF	37199
TOTAL RESIDENTIAL SF	37199
TOTAL ALLOWABLE RESIDENTIAL SF IN RH-2	74398 X .5 FAR = 37199

FLOOR AREA CALCULATIONS (Non-Res)

RH-2 ZONE	
BROADWAY BUILDING	
GARAGE	10363
FIRST FLOOR COMMERCIAL	2946
FIRST FLOOR COMMONS	2647
TOTAL	15956
SCHOOL BUILDING	
BASEMENT COMMONS	2217
BASEMENT OTHER (MECHANICAL / STORAGE)	1538
TOTAL	3755
TOTAL SF	19711

AFFORDABLE UNIT LOCATIONS

BASEMENT	
S1NE - AFFORDABLE	SEE PAGE 1.0
S2SE - AFFORDABLE	SEE PAGE 1.0
FIRST FLOOR	
B1N - MODERATE	SEE PAGE 1.1
B1S - MODERATE	SEE PAGE 1.1
B2N - AFFORDABLE	SEE PAGE 1.1
B2S - AFFORDABLE	SEE PAGE 1.1
N2 - AFFORDABLE	SEE PAGE 1.1
C1N - AFFORDABLE	SEE PAGE 1.2
C2S - AFFORDABLE	SEE PAGE 1.2
SECOND FLOOR	
N3 - AFFORDABLE	SEE PAGE 1.2



ARCHITECTURE
PLANNING &
INTERIOR DESIGN

3008 FOLSOM STREET
BOULDER, CO 80304
PHONE: 303-444-6902
FAX: 303-447-6100



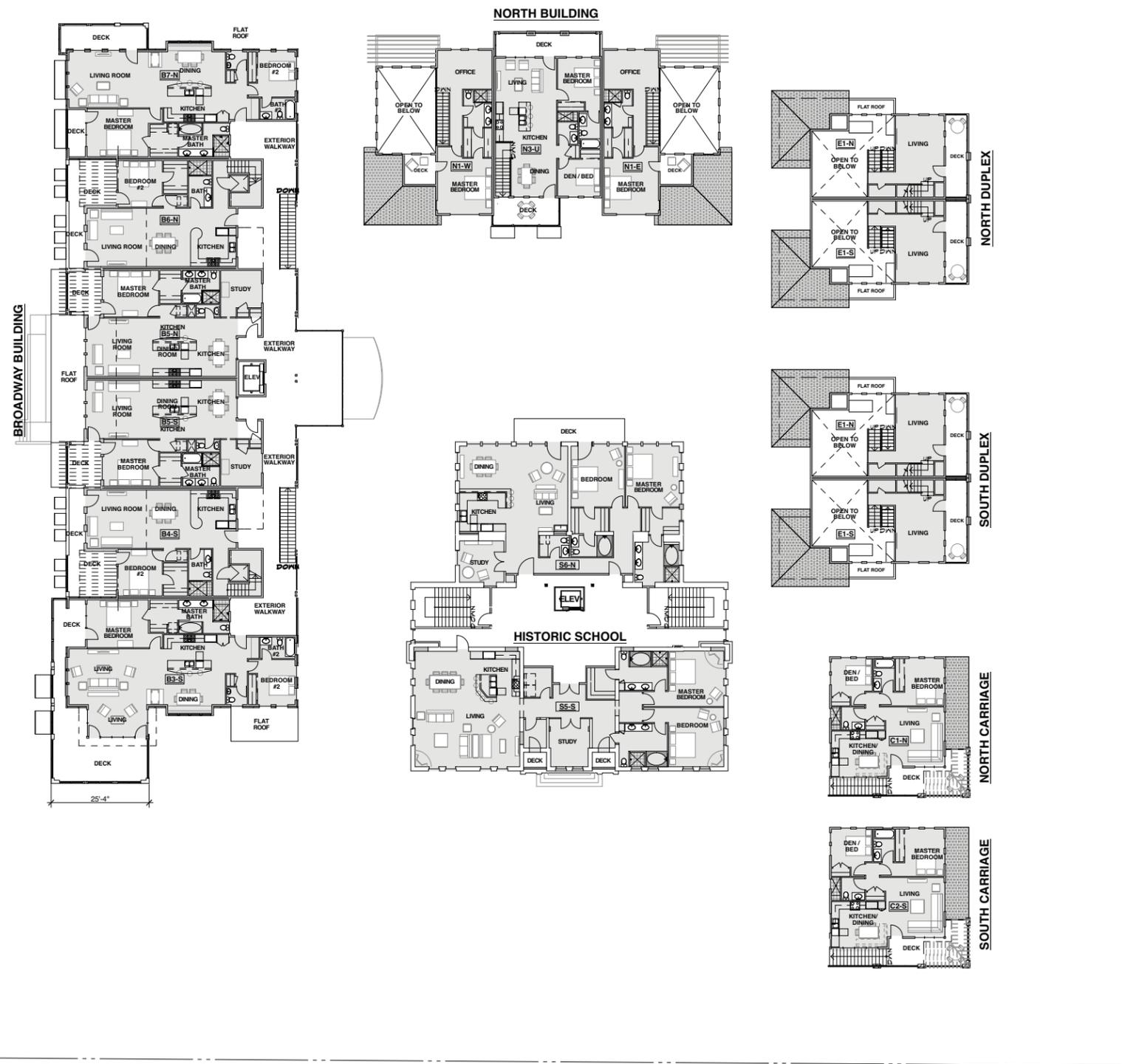
WASHINGTON VILLAGE II
1215 CEDAR AVE
BOULDER, COLORADO

FIRST LEVEL PLANS

SITE REVIEW 12 SEP 2008
SUBMITTAL 2008
SITE REVIEW 17 NOV 2008
RE-SUBMITTAL 2008

FIRST LEVEL PLANS

SCALE: 1/16" = 1'-0"



TOTAL SITE AREA	130,710 SF
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PLANNING &
INTERIOR DESIGN

3008 FOLSOM STREET
BOULDER, CO 80304
PHONE: 303-444-6902
FAX: 303-449-6100



WASHINGTON VILLAGE II

1215 CEDAR AVE
BOULDER, COLORADO

SECOND LEVEL PLANS

SITE REVIEW 12 SEP 2008
SUBMITTAL
SITE REVIEW 17 NOV 2008
RE-SUBMITTAL

SECOND LEVEL PLANS

SCALE: 1/16" = 1'-0"



TOTAL SITE AREA	130,710 SF
RH-2 ZONE	74,398 SF
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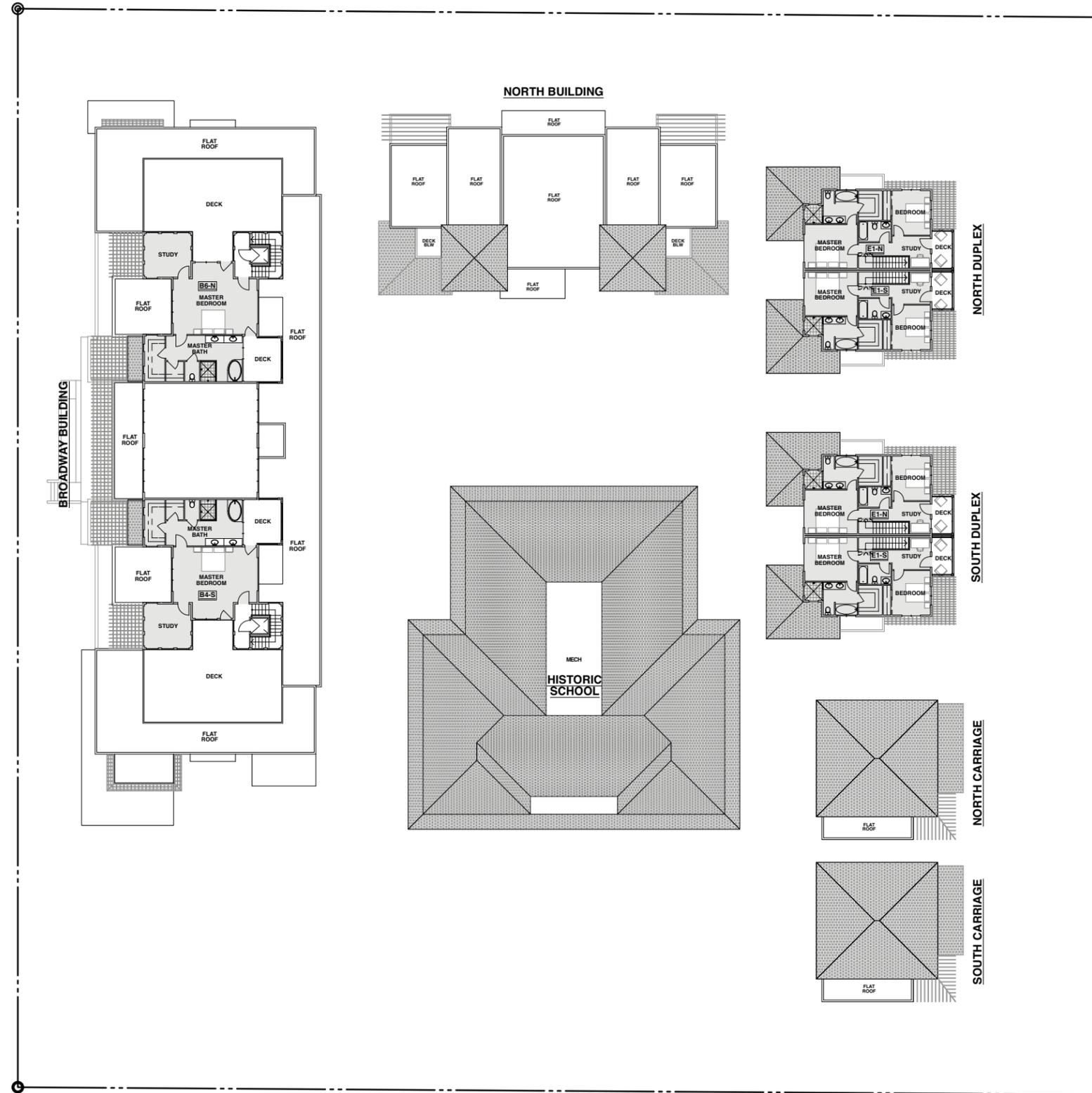
FLOOR AREA CALCULATIONS (Res)

RH-2 ZONE	
BROADWAY BUILDING	
FIRST FLOOR	3050
SECOND FLOOR	7570
THIRD FLOOR	2014
TOTAL	12634
NORTH BUILDING	
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SECOND FLOOR	2344
TOTAL	5579
EAST BUILDINGS	
FIRST FLOOR	2210
LIVING ROOM	1440
SECOND FLOOR	2910
TOTAL	6560
SCHOOL BUILDING	
BASEMENT	1669
FIRST FLOOR	4474
SECOND FLOOR	4481
TOTAL	10624
CARRIAGE HOUSES	
FIRST FLOOR	1802
TOTAL	1802
TOTAL UNITS SF	37199

TOTAL SF	37199
TOTAL RESIDENTIAL SF	37199
TOTAL ALLOWABLE RESIDENTIAL SF IN RH-2	74398 X .5 FAR = 37199

FLOOR AREA CALCULATIONS (Non-Res)

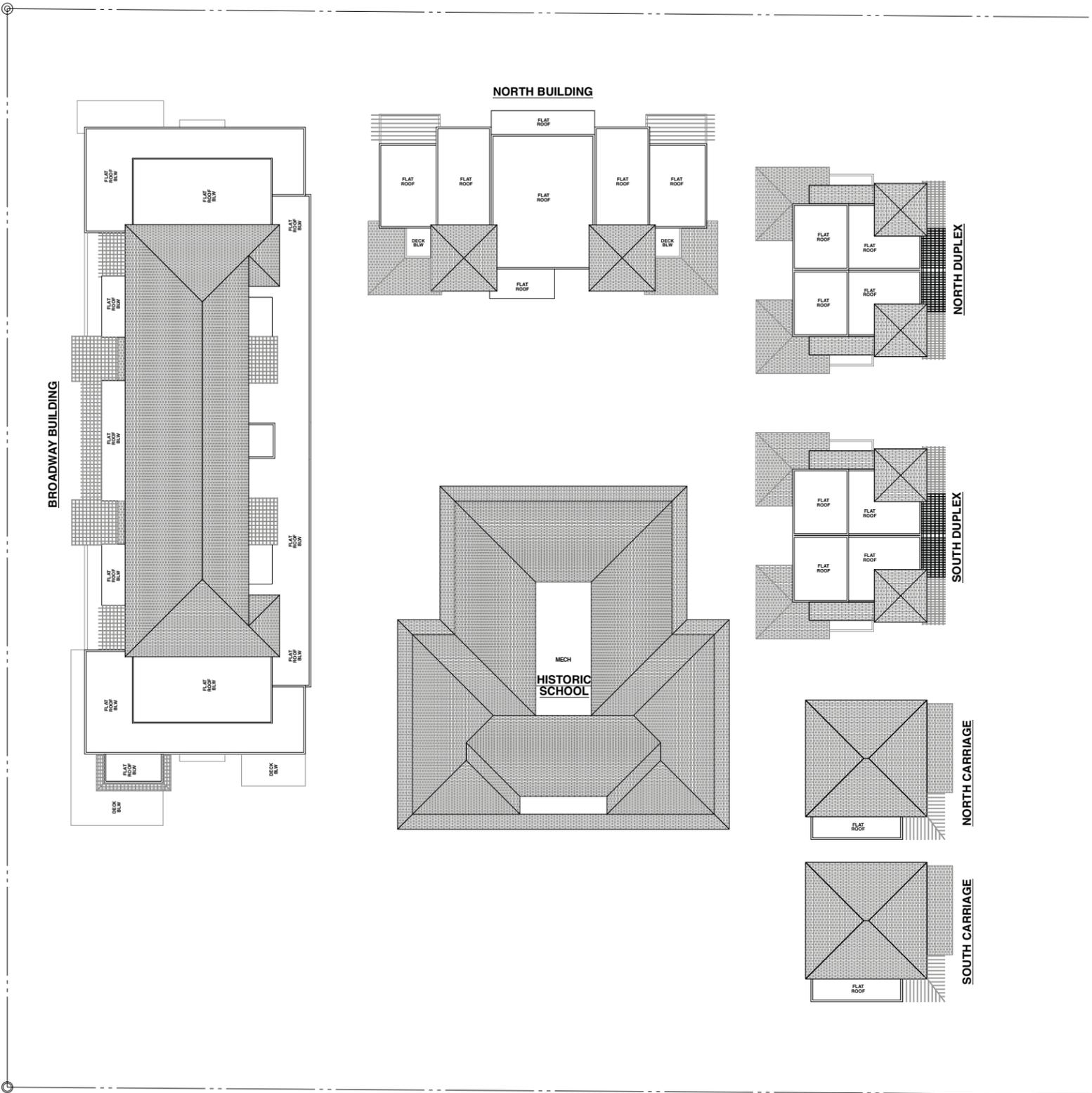
RH-2 ZONE	
BROADWAY BUILDING	
GARAGE	10363
FIRST FLOOR COMMERCIAL	2946
FIRST FLOOR COMMONS	2647
TOTAL	15956
SCHOOL BUILDING	
BASEMENT COMMONS	2217
BASEMENT OTHER (MECHANICAL / STORAGE)	1538
TOTAL	3755
TOTAL SF	19711



THIRD LEVEL PLANS

SCALE: 1/16" = 1'-0"





ROOF PLAN

SCALE: 1/16" = 1'-0"

1





SCHOOL EAST ELEVATION

SCALE: 1/8" = 1'-0"

3



SCHOOL WEST ELEVATION

SCALE: 1/8" = 1'-0"

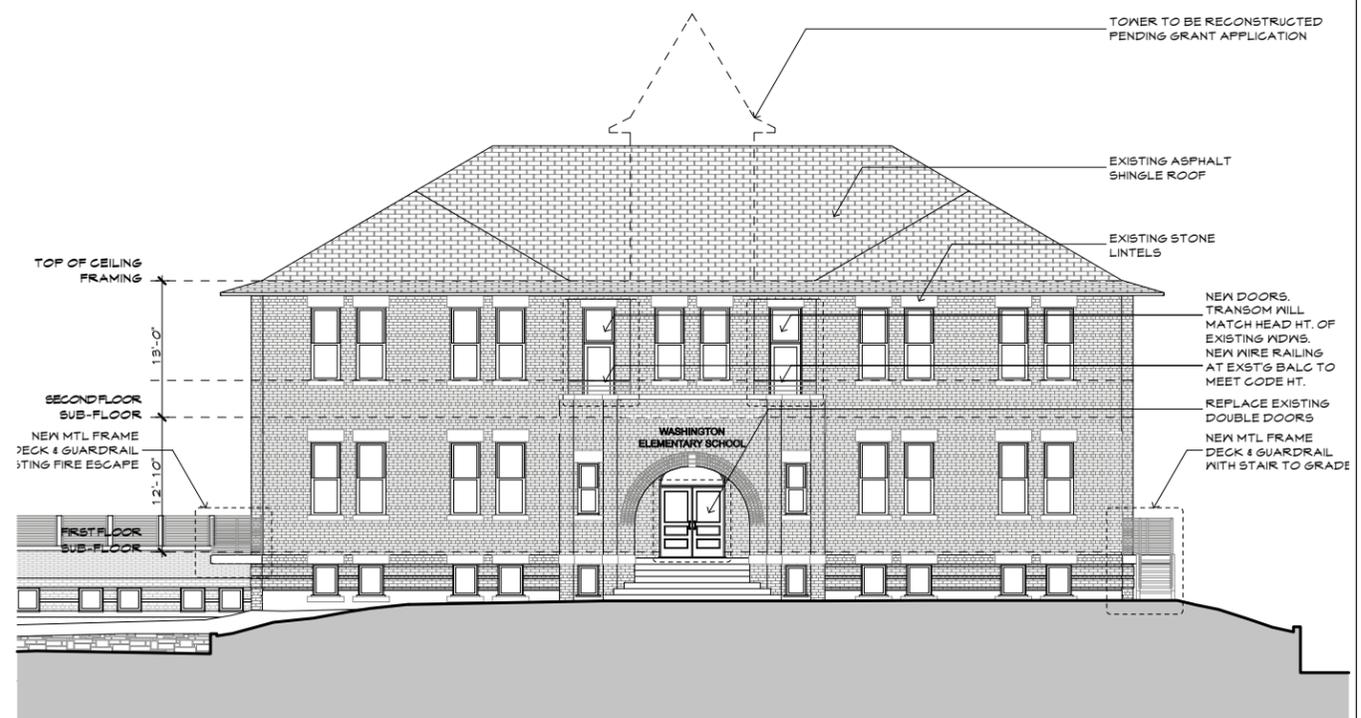
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SCHOOL NORTH ELEVATION

SCALE: 1/8" = 1'-0"

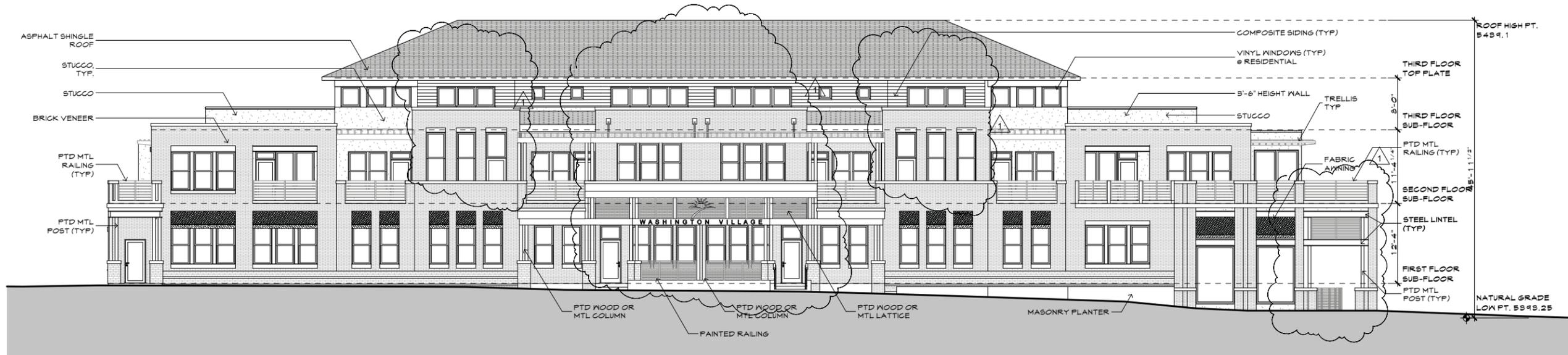
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SCHOOL SOUTH ELEVATION

SCALE: 1/8" = 1'-0"

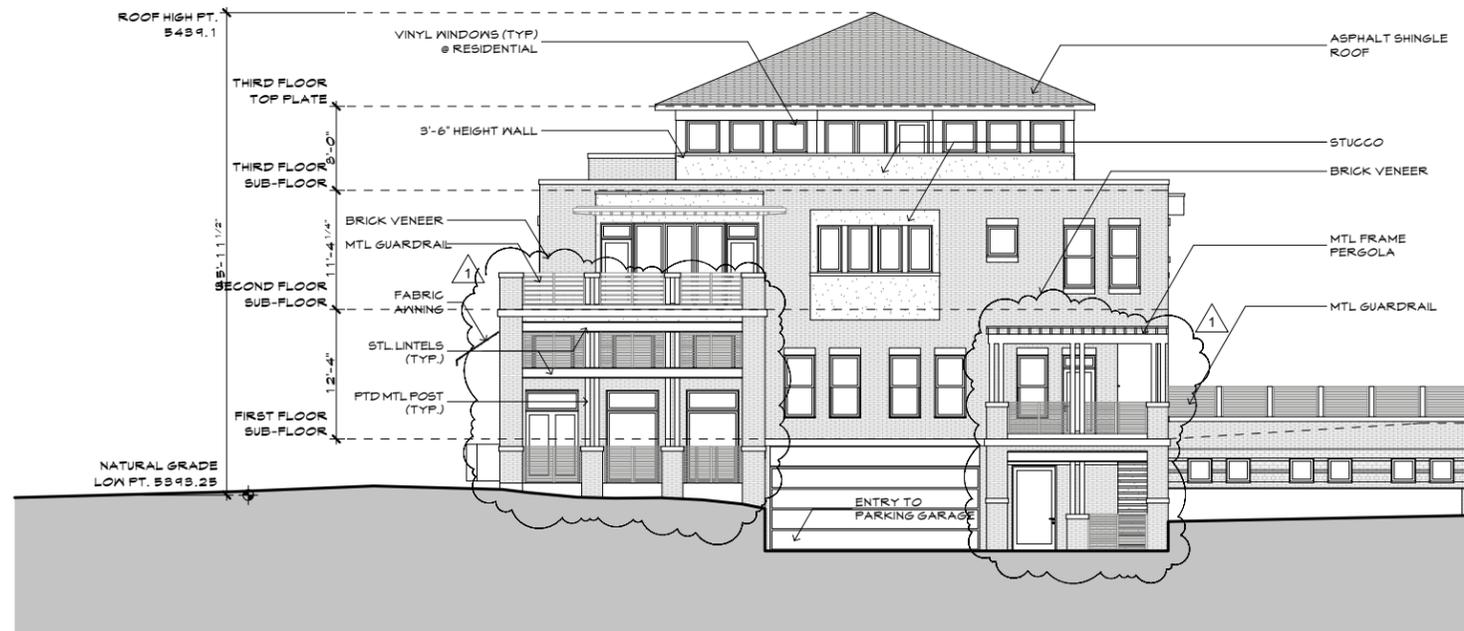
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BROADWAY WEST ELEVATION

SCALE: 1/8" = 1'-0"

2



BROADWAY SOUTH ELEVATION

SCALE: 1/8" = 1'-0"

1



BROADWAY EAST ELEVATION

SCALE: 1/8" = 1'-0" **2**



BROADWAY NORTH ELEVATION

SCALE: 1/8" = 1'-0" **1**



NORTH BLDG WEST ELEVATION

SCALE: 1/8" = 1'-0" **1**



NORTH BLDG SOUTH ELEVATION

SCALE: 1/8" = 1'-0" **2**



NORTH BLDG EAST ELEVATION

SCALE: 1/8" = 1'-0" **3**



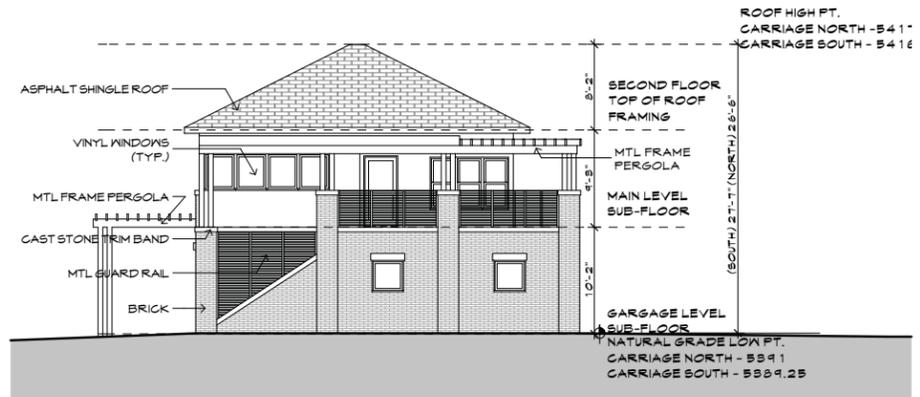
NORTH BLDG NORTH ELEVATION

SCALE: 1/8" = 1'-0"



CARRIAGE WEST ELEVATION

SCALE: 1/8" = 1'-0" **5**



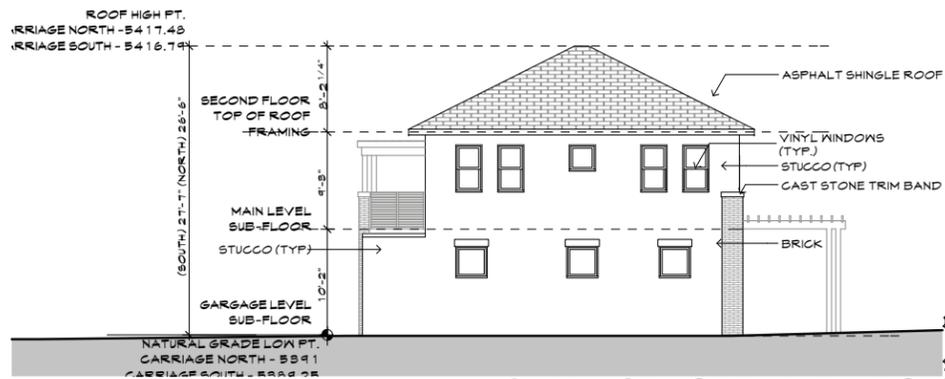
CARRIAGE SOUTH ELEVATION

SCALE: 1/8" = 1'-0" **6**



CARRIAGE EAST ELEVATION

SCALE: 1/8" = 1'-0" **7**



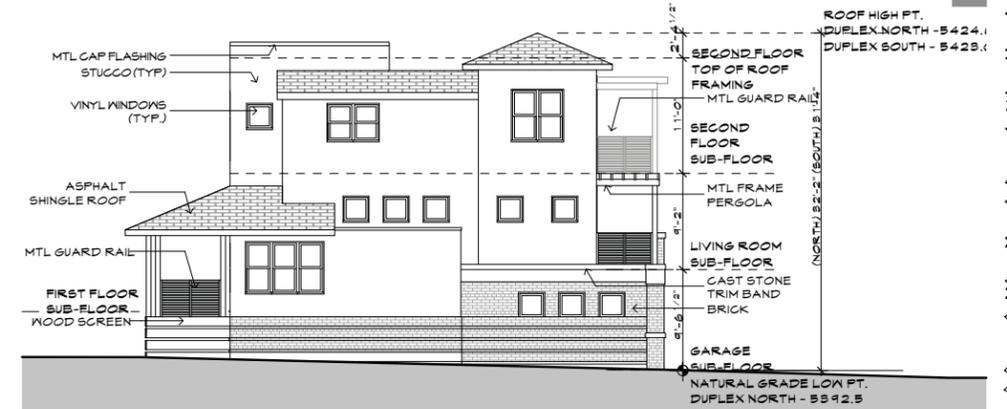
CARRIAGE NORTH ELEVATION

SCALE: 1/8" = 1'-0" **8**



DUPLEX WEST ELEVATION

SCALE: 1/8" = 1'-0" **1**



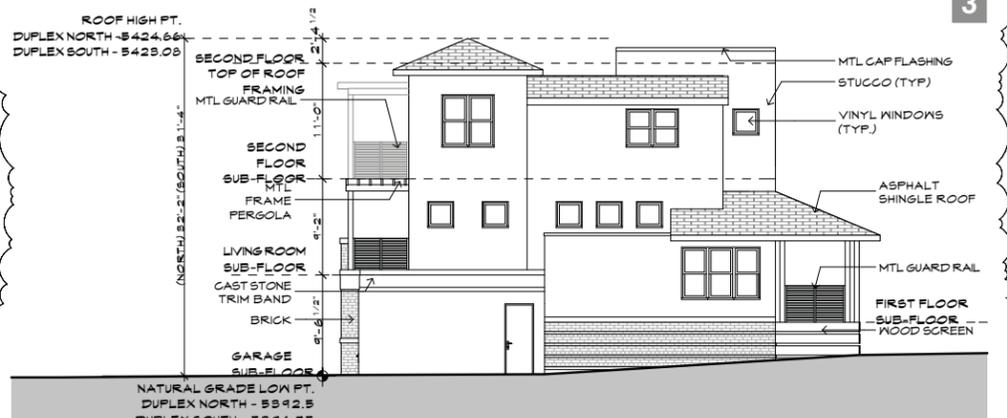
DUPLEX SOUTH ELEVATION

SCALE: 1/8" = 1'-0" **2**



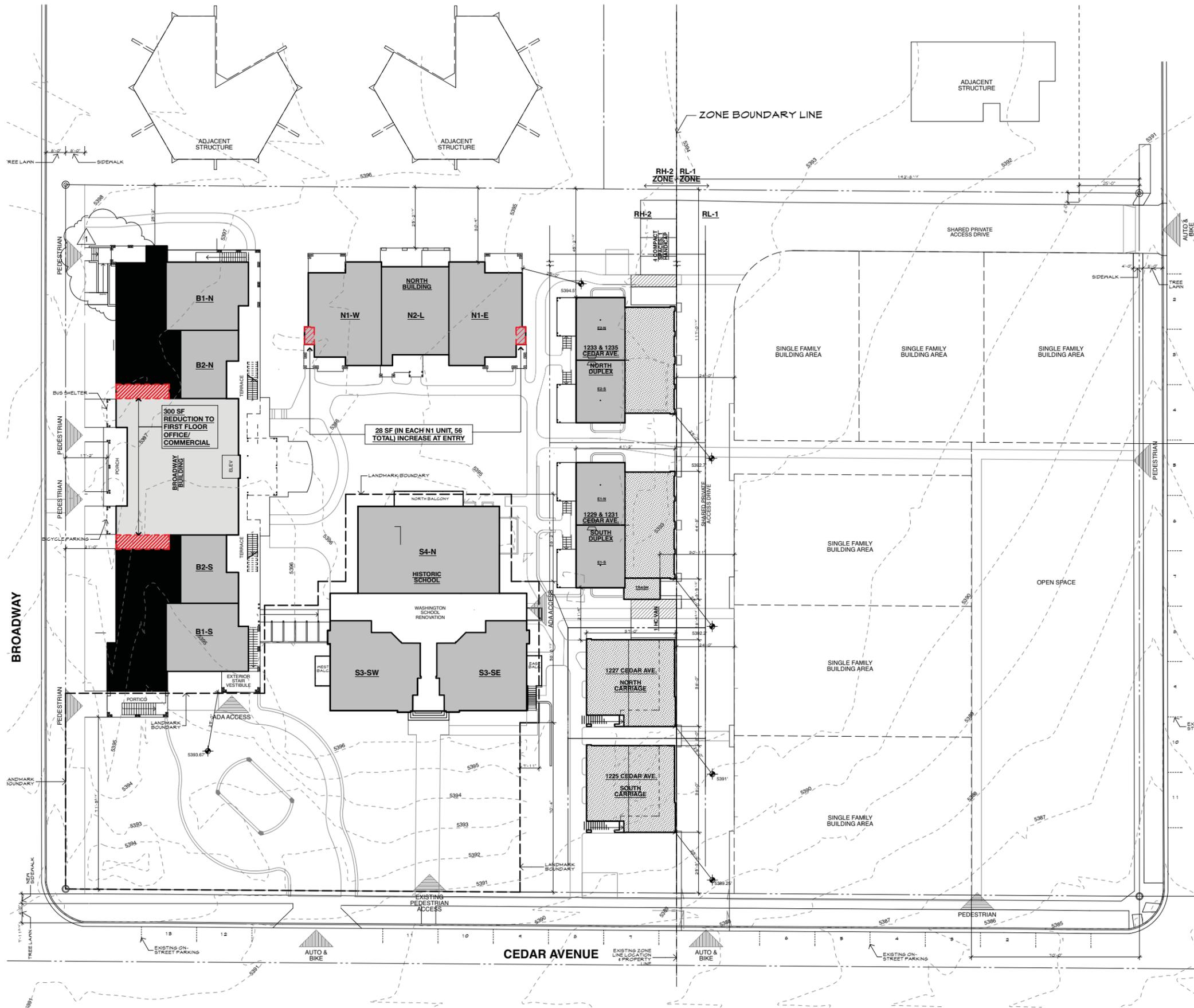
DUPLEX EAST ELEVATION

SCALE: 1/8" = 1'-0" **3**



DUPLEX NORTH ELEVATION

SCALE: 1/8" = 1'-0" **4**



BUILDING PLACEMENT & SETBACKS

NOTE: ALL HEIGHTS ARE MEASURED TO EXISTING GRADE AT A 25' RADIUS FROM THE LOWEST POINT ON THE STRUCTURE (TYPICALLY, THE SOUTH EASTERN CORNER OF THE BUILDING)

RH-2 ZONE		SUBMITTAL	
		9/08	1/13
BROADWAY	MIXED USE STRUCTURE		
SETBACK FOR PORCHES & SINGLE STORY MASSES	17.0'	SAME	
SETBACK FOR TWO STORY BUILDING MASSES	21.0'	SAME	
SETBACK FOR THIRD STORY BUILDING MASSES	26.0'	SAME	
BUILDING HEIGHT (for Broadway Building)	47.5'	41'	
CEDAR AVENUE	CARRIAGE HOUSE		
SETBACK FOR PORCHES	25.0'	SAME	
SETBACK FOR PRINCIPAL BUILDING	25.0'	SAME	
NORTH PROPERTY LINE	NORTH BUILDING & DUPLEX		
SETBACK FOR STOOPS & PORCHES & SINGLE STORY MASSES	25.0'	SAME	
SETBACK FOR TWO STORY BUILDING MASSES	25.0'	SAME	
SETBACK FOR THIRD STORY BUILDING MASSES (for Broadway Building)	26.0'	SAME	
SETBACK FOR ACCESSORY STRUCTURES	0.0'	N/A	
BUILDING HEIGHT (for North Building)	30' max	SAME	
20' PRIVATE ACCESS DRIVE	EAST BUILDINGS		
SETBACK FOR SINGLE STORY MASSES & BALCONIES ABOVE	0.0'	SAME	
SETBACK FOR TWO & THREE STORY BUILDING MASSES	6.0'	N/A	
SETBACK FOR ACCESSORY USES	0.0'	SAME	
BUILDING HEIGHT (for Duplexes)	±31'	N/A	
BUILDING HEIGHT (for Carriage Houses)	±28'	N/A	
EXISTING HISTORIC SCHOOL			
EXISTING BUILDING HEIGHT (excluding tower)	47.0'	SAME	
RL-1 ZONE			
CEDAR AVENUE	SINGLE FAMILY RESIDENCES		
SIDEYARD SETBACK ADJACENT TO STREET	25.0'	SAME	
13TH STREET	SINGLE FAMILY RESIDENCES		
FRONT YARD SETBACK ADJACENT TO STREET FOR PORCHES & ONE STORY MASSES	20.0'	SAME	
FRONT YARD SETBACK ADJACENT TO STREET FOR TWO STORY MASSES	25.0'	SAME	
PRIVATE ACCESS DRIVE	SINGLE FAMILY RESIDENCES & ACCESSORY BLDGS		
DISTANCE TO A ONE OR TWO STORY ACCESSORY STRUCTURE	4.0'	SAME	
DISTANCE TO A ONE STORY PRINCIPAL STRUCTURE ELEMENT	4.0'	SAME	
DISTANCE TO A TWO STORY PRINCIPAL STRUCTURE ELEMENT	10.0'	SAME	
ADJACENT SINGLE FAMILY BUILDING MASSES			
DISTANCE FROM A ONE STORY MASS TO NEIGHBORING BUILDING ELEMENTS	10.0'	SAME	
DISTANCE FROM A TWO STORY MASS TO NEIGHBORING BUILDING ELEMENTS	15.0'	SAME	
DISTANCE BETWEEN PRINCIPAL & ACCESSORY STRUCTURE (under single ownership)	5.0'	SAME	
MAXIMUM BUILDING HEIGHT	35.0'	SAME	

OPEN SPACE CALCULATIONS

RH-2 ZONE TOTAL SQUARE FOOTAGE	74,398
REQUIRED OPEN SPACE FOR BUILDINGS 35'-45' (15% OF SITE AREA)	11,159
REQUIRED OPEN SPACE FOR RH-2 ZONE (600 SF X # OF DWELLING UNITS)	18,000
RH-2 ZONE PROPOSED OPEN SPACE (NOT INCLUDING PRIVATE DECKS OR PEDESTRIAN WAYS)	34,637
RH-2 ZONE PRIVATE OPEN SPACE (DECKS AND PORCHES)	6,445
RH-2 ZONE PEDESTRIAN WAY (EAST SIDE OF BROADWAY BLDG)	1,857
RH-2 ZONE PRIVATE OPEN SPACE ALLOWABLE IN CALCULATIONS (no greater than 18000 X .25, or 25% of req'd open space)	4,500
RH-2 ZONE PEDESTRIAN WAY OPEN SPACE ALLOWABLE IN CALCULATIONS (no greater than 18000 X .25, or 25% of req'd open space)	1,857
RH-2 ZONE TOTAL OPEN SPACE	40,994
PERCENTAGE OF PROPOSED OPEN SPACE FOR RH-2 ZONE	55%

DRAWING KEY

- GARAGE / PARKING / STORAGE SPACE
- COMMERCIAL / OFFICE SPACE
- CO-HOUSING COMMONS SPACE
- RESIDENTIAL SPACE

SITE PLAN

SCALE: 1" = 20'



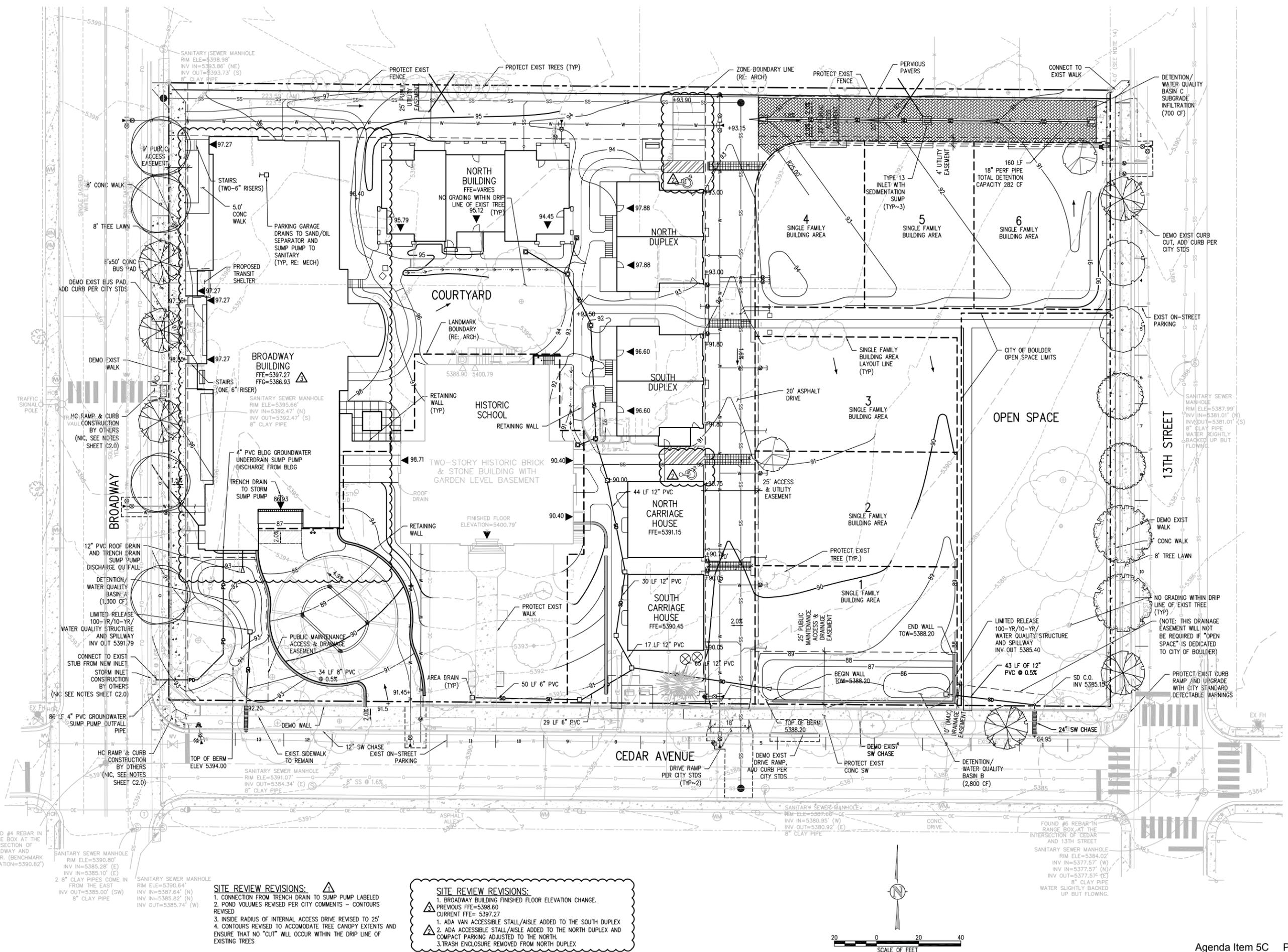
SITE REVIEW	12 SEPT 2008
SUBMITTAL	2008
SITE REVIEW	17 NOV 2008
RESUBMITTAL	2008
SITE REVIEW	6 JAN 2014
AMENDMENT	2014

**PRELIMINARY GRADING AND DRAINAGE PLAN
 NOT FOR CONSTRUCTION**

SITE REVIEW 12 SEPT 2008
 SUBMITTAL 2008
 SITE REVIEW 17 NOV 2008
 RESUBMITTAL 2008

REVISIONS	DATE
17 FEB 09	△
20 DEC 13	△
06 JAN 14	△

C1.0_R



SITE REVIEW REVISIONS: △
 1. CONNECTION FROM TRENCH DRAIN TO SUMP PUMP LABELED
 2. POND VOLUMES REVISED PER CITY COMMENTS - CONTOURS REVISED
 3. INSIDE RADIUS OF INTERNAL ACCESS DRIVE REVISED TO 25'
 4. CONTOURS REVISED TO ACCOMMODATE TREE CANOPY EXTENTS AND ENSURE THAT NO "CUT" WILL OCCUR WITHIN THE DRIP LINE OF EXISTING TREES

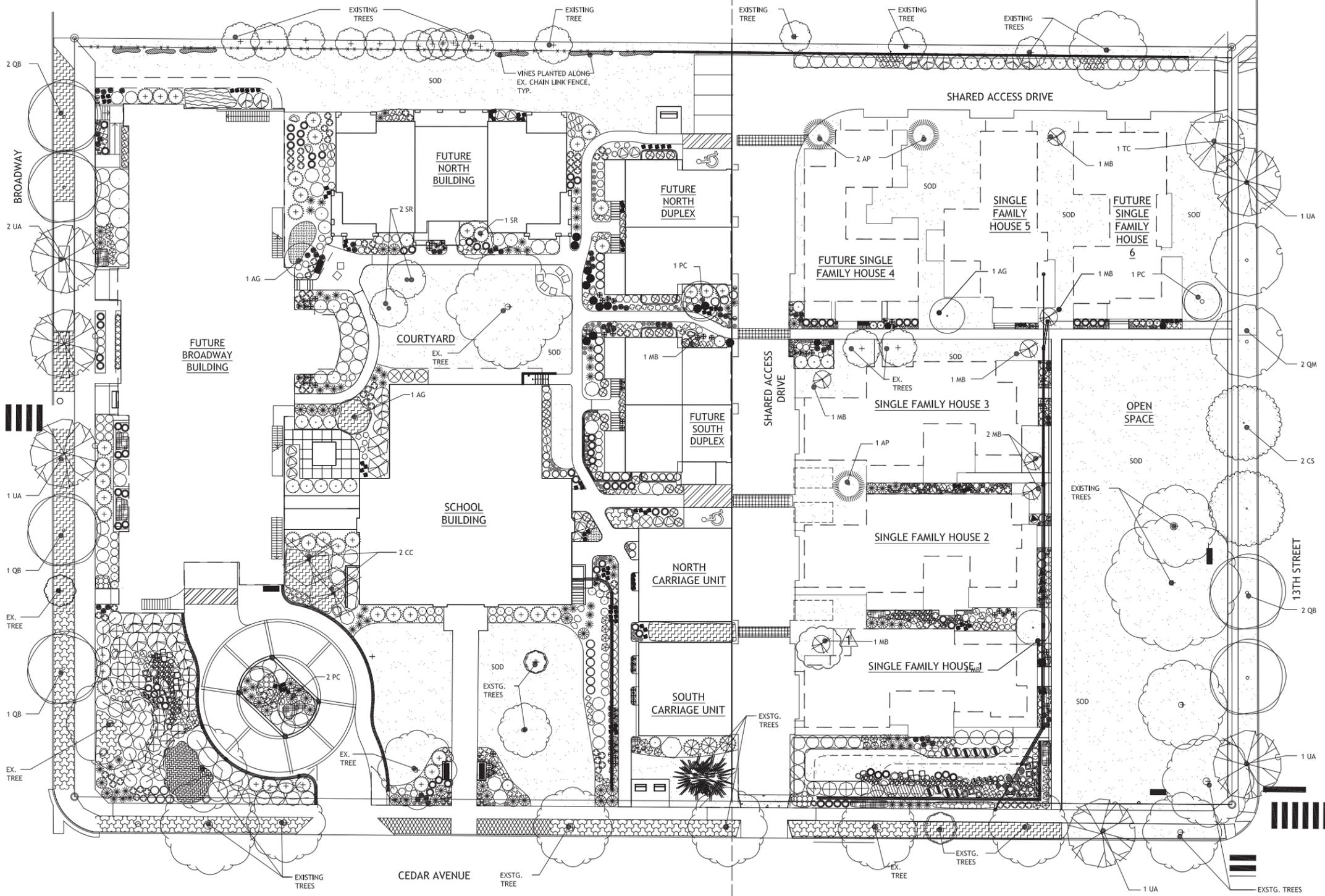
SITE REVIEW REVISIONS: △
 1. BROADWAY BUILDING FINISHED FLOOR ELEVATION CHANGE. PREVIOUS FFE=5398.60 CURRENT FFE= 5397.27
 2. ADA VAN ACCESSIBLE STALL/AISLE ADDED TO THE SOUTH DUPLEX AND COMPACT PARKING ADJUSTED TO THE NORTH.
 3. TRASH ENCLOSURE REMOVED FROM NORTH DUPLEX

FOUND #4 REBAR IN RANGE BOX AT THE INTERSECTION OF BROADWAY AND CEDAR. (BENCHMARK ELEVATION=5390.82)

SANITARY SEWER MANHOLE RIM ELE=5390.80' INV IN=5385.28' (E) INV OUT=5385.10' (E) 2 8" CLAY PIPES COME IN FROM THE EAST INV OUT=5385.00' (SW) 8" CLAY PIPE
 SANITARY SEWER MANHOLE RIM ELE=5390.64' INV IN=5387.64' (W) INV IN=5385.82' (N) INV OUT=5385.74' (W) 8" CLAY PIPE

FOUND #6 REBAR IN RANGE BOX AT THE INTERSECTION OF CEDAR AND 13TH STREET
 SANITARY SEWER MANHOLE RIM ELE=5384.02' INV IN=5377.57' (W) INV IN=5377.57' (N) INV OUT=5377.57' (E) 8" CLAY PIPE WATER SLIGHTLY BACKED UP BUT FLOWING.

(NOTE: THIS DRAINAGE EASEMENT WILL NOT BE REQUIRED IF "OPEN SPACE" IS DEDICATED TO CITY OF BOULDER)



ISSUE RECORD		DATE
PURPOSE		
TECH DOC RESUBMITTAL		12/20/13
TECH DOC SUBMITTAL		08/02/13
NO.	REVISION	DATE
△	REVISIONS	01/06/14
△	REVISIONS	02/10/14

DRAWN	SCR
CHECKED	SCR
DATE	02-10-14
SHEET TITLE	
FINAL SITE & LANDSCAPE PLANS	
LANDSCAPE & TREE PLAN	

303 ARCHITECTURE, INC.

303 ARCHITECTURE, INC.
1942 BROADWAY, SUITE 314
BOULDER, COLORADO 80302
PH: 303-441-6708
FAX: 303-448-5883
Stephanie@303architecture.com

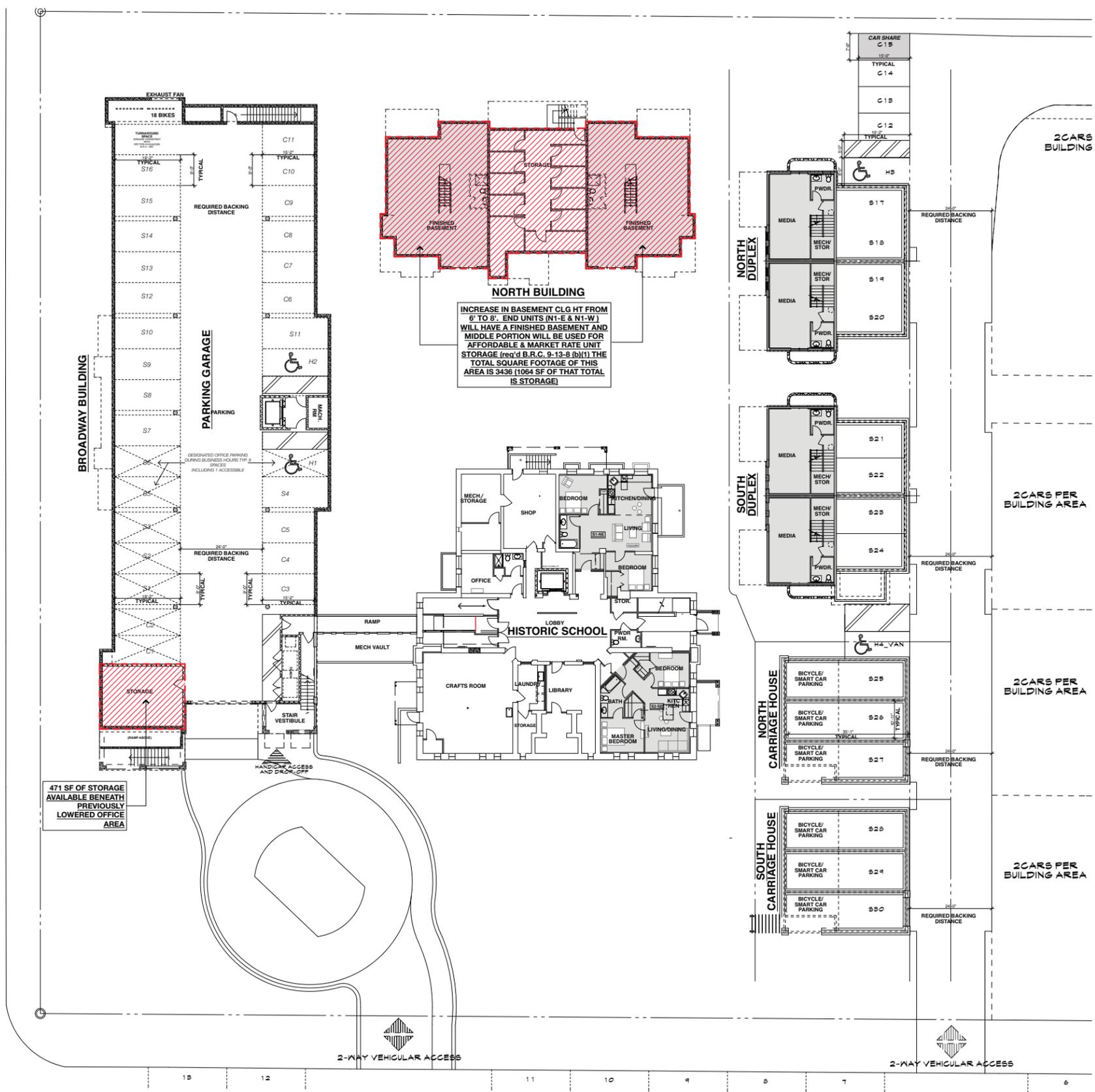
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LP-2_R

REFER TO LANDSCAPE DETAIL SHEETS FOR LANDSCAPE REQUIREMENTS TABLE, PLANTING NOTES AND ADDITIONAL INFORMATION



1 LANDSCAPE & TREE PLAN



LOWER LEVEL PLANS

SCALE: 1/16" = 1'-0" **1**

PREVIOUS SPR APPROVAL

PARKING ANALYSIS BASED ON RH-2 REQUIREMENTS

1 BEDROOM = 1 CAR, 2 BEDROOM = 1.5 CARS, 3 BEDROOM = 2 CARS, 4+ BEDROOM = 3 CARS
PARKING APPROVED UNDER LUR 2008-00083

PRIVATE RESIDENTIAL AREAS (count toward total required spaces)	BEDROOMS PER UNIT	CARS PER UNIT	NUMBER OF UNITS	TOTAL CARS
HISTORIC SCHOOL				
BASEMENT LEVEL				
S1 AFFORDABLE (NORTH EAST)	2	1.5	1	1.5
S2 AFFORDABLE (SOUTH EAST)	2	1.5	1	1.5
MAIN LEVEL				
S3 MARKET FLAT SOUTHWEST AND SOUTHEAST	1	1	2	2
S4 MARKET FLAT NORTH	2	1.5	1	1.5
UPPER LEVEL				
S5 MARKET FLAT SOUTH	2	1.5	1	1.5
S6 MARKET FLAT NORTH	2	1.5	1	1.5
BROADWAY BUILDING				
MAIN LEVEL				
B1 AFFORDABLE CENTRAL UNITS	1	1	2	2
B2 AFFORDABLE END UNITS	2	1.5	2	3
UPPER LEVEL				
B3 MARKET FLAT SOUTH	2	1.5	1	1.5
B4 MARKET FLAT NORTH	2	1.5	1	1.5
B5 MARKET SOUTH TOWNHOUSE	2	1.5	1	1.5
B6 MARKET NORTH TOWNHOUSE	2	1.5	1	1.5
B7 MARKET FLAT WITH HIGH LIVING	1	1	2	2
NORTH BUILDING				
MAIN LEVEL				
N1 MARKET TOWN HOMES EAST AND WEST	2	1.5	2	3
N2 AFFORDABLE MODERATE LOWER	2	1.5	1	1.5
N3 AFFORDABLE MODERATE UPPER	2	1.5	1	1.5
DUPLEX				
E1 MARKET TOWN HOMES	2	1.5	4	6
CARRIAGE				
C1 AFFORDABLE INCL. NORTH	2	1.5	1	1.5
C2 AFFORDABLE MODERATE SOUTH	2	1.5	1	1.5
TOTAL CARS IN RH-2 ZONE				38
OFFICE USE				
OFF-STREET PARKING REQUIRED (typically 1 car/300 sf of office use)	2,946	300	9.82	9
RH-2 ZONE TOTAL OFFICE PARKING REQUIRED				9
TOTAL CARS REQUIRED IN RH-2 ZONE				47

RESIDENTIAL ACCESSIBLE SPACES REQ'D: 27
 NON-RESIDENTIAL ACCESSIBLE SPACES REQ'D: 1

REQUIRED PARKING FOR SITE REVIEW AMENDMENT
 (ADDITIONAL PARKING ALREADY APPROVED THROUGH MINOR MOD.)

RH-2 PARKING REQUIRED UNDER SITE REVIEW AMENDMENT REQUESTS

PRIVATE RESIDENTIAL AREAS (count toward total required spaces)	BEDROOMS PER UNIT	CARS PER UNIT	NUMBER OF UNITS	TOTAL CARS
HISTORIC SCHOOL				
BASEMENT LEVEL				
S1-NE AFFORDABLE (NORTH EAST)	2	1.5	1	1.5
S2-SE AFFORDABLE (SOUTH EAST)	2	1.5	1	1.5
MAIN LEVEL				
S3 MARKET FLAT SOUTHWEST AND SOUTHEAST	1	1	2	2
S4 MARKET FLAT NORTH	3	2	1	2
UPPER LEVEL				
S5-SE MARKET FLAT SOUTH (SOUTHEAST)	1	1	1	1
S5-SW MARKET FLAT SOUTH (SOUTHWEST)	1	1	1	1
S6 MARKET FLAT NORTH	3	2	1	2
BROADWAY BUILDING				
MAIN LEVEL				
B1 AFFORDABLE CENTRAL UNITS	1	1	2	2
B2 AFFORDABLE END UNITS	2	1.5	2	3
SECOND LEVEL				
B3-S MARKET FLAT SOUTH	2	1.5	1	1.5
B3-N MARKET FLAT NORTH	2	1.5	1	1.5
B4 MARKET FLAT (1-BED)	2	1.5	1	1.5
B4 MARKET FLAT (1-BED)	2	1.5	1	1.5
B4 MARKET FLAT (1-BED)	2	1.5	1	1.5
THIRD LEVEL				
B6-N PENTHOUSE FLAT	3	2	1	2
B6-S PENTHOUSE FLAT	3	2	1	2
NORTH BUILDING				
MAIN LEVEL				
N1 MARKET TOWN HOMES EAST AND WEST	4	3	2	6
N2 AFFORDABLE MODERATE LOWER	2	1.5	1	1.5
N3 AFFORDABLE MODERATE UPPER	2	1.5	1	1.5
DUPLEX				
E1 MARKET TOWN HOMES	2	1.5	4	6
CARRIAGE				
C1 AFFORDABLE INCL. NORTH	2	1.5	1	1.5
C2 AFFORDABLE MODERATE SOUTH	2	1.5	1	1.5
TOTAL CARS IN RH-2 ZONE				47
NUMBER OF BEDROOMS (includes potential for owners of N1-E and N1-W to add a bedroom to the unfinished basement)			62	
OFFICE USE				
OFF-STREET PARKING REQUIRED (typically 1 car/300 sf of office use)	2646	300	8.82	8
RH-2 ZONE TOTAL OFFICE PARKING REQUIRED				8
TOTAL CARS REQUIRED IN RH-2 ZONE				55

TOTAL CARS PROVIDED IN RH-2 ZONE: 49

PARKING REDUCTION REQUESTED: 11%

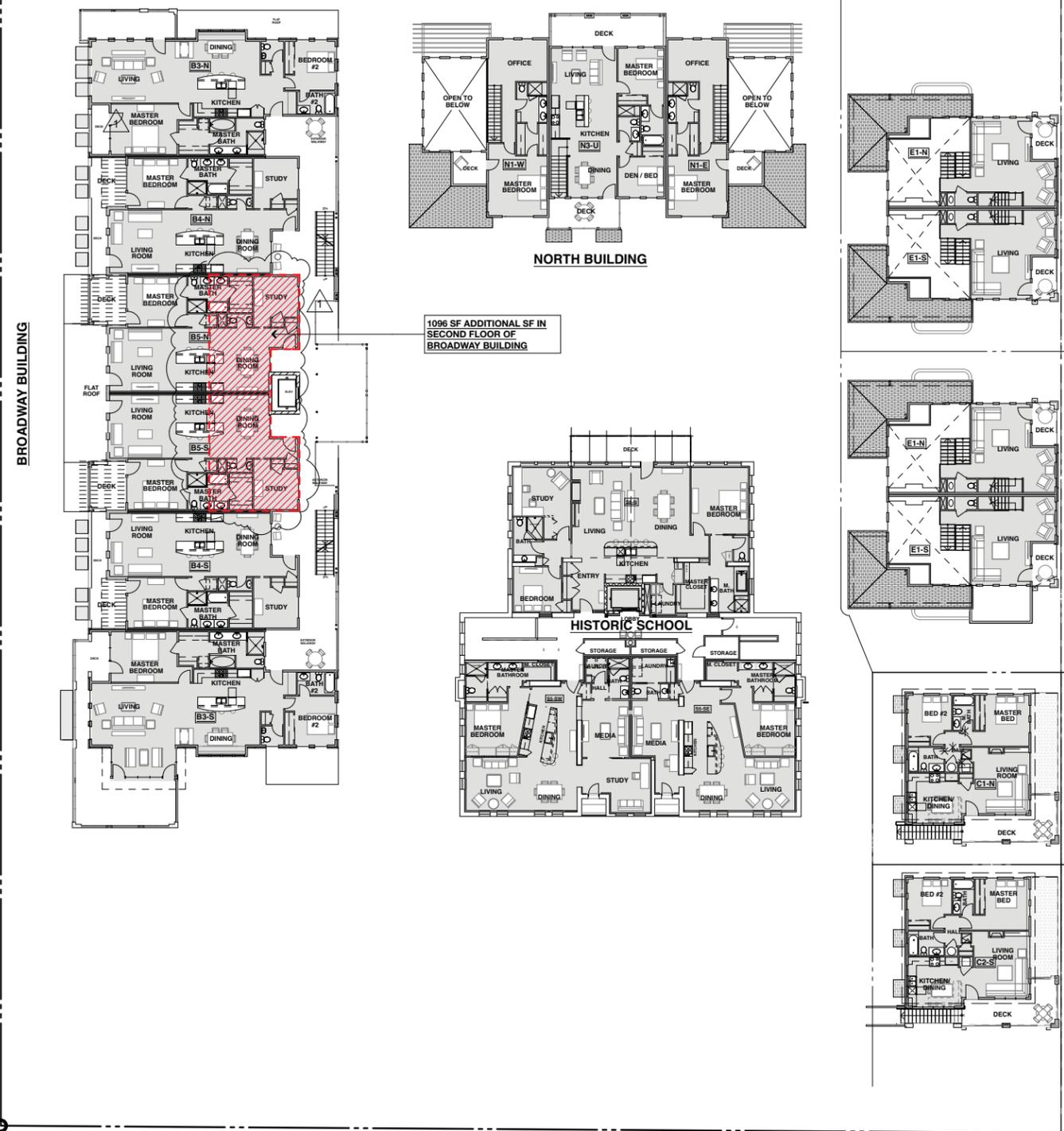
RESIDENTIAL ACCESSIBLE SPACES REQ'D: 30
 NON-RESIDENTIAL ACCESSIBLE SPACES REQ'D: 1
 TOTAL HANDICAP SPACES REQ'D: 4

PARKING SUMMARY

RH-2 ZONE -- ON SITE	
BELOW BUILDING STRUCTURED PARKING	
BROADWAY BUILDING	16
STANDARD	11
COMPACT	2
HANDICAP	2
ON-GRADE PARKING	
COMPACT (NORTH OF NORTHERN DUPLEX)	4
HANDICAP (NORTH OF NORTHERN DUPLEX)	1
HANDICAP VAN (NORTH OF NORTHERN CARRIAGE HOUSE)	1
TUCK-UNDER PARKING	
STANDARD (DUPLEXES)	8
STANDARD (CARRIAGE UNITS)	6
TOTAL STANDARD CARS	30
TOTAL COMPACT CARS	15
TOTAL HANDICAP CARS	4
TOTAL CARS PROVIDED FOR RH-2 ZONE ON-SITE	49
RL-1 ZONE -- ON SITE	
ON-GRADE INTERIOR GARAGE PARKING	
SINGLE FAMILY RESIDENCES	12
(presumed as 2 cars for each single family detached unit)	
TOTAL CARS PROVIDED FOR RL-1 ZONE ON-SITE	12
TOTAL AMOUNT OF PARKING SPACES PROVIDED ON SITE	61
ON STREET PARKING	
CEDAR AVENUE	13
13th STREET	11
TOTAL AMOUNT OF PARKING SPACES AVAILABLE ON STREET	24

***11% PARKING REDUCTION REQUESTED WITH SPR AMENDMENT REQUESTS**

RH-2 ZONE RL-1 ZONE



APPROVED LUR 2008-00083	APPROVED THROUGH MINOR MODIFICATIONS	REQUESTED THROUGH SPR AMENDMENT
<p>RESIDENTIAL FLOOR AREA CALCULATIONS (APPROVED LUR 2008-00083)</p> <p>RH-2 ZONE BROADWAY BUILDING FIRST FLOOR 3050 SECOND FLOOR 7570 THIRD FLOOR 2014 TOTAL 12634</p> <p>NORTH BUILDING FIRST FLOOR 3235 SECOND FLOOR 2344 TOTAL 5579</p> <p>EAST BUILDINGS FIRST FLOOR 2210 LIVING ROOM 1440 SECOND FLOOR 2910 TOTAL 6560</p> <p>SCHOOL BUILDING BASEMENT 1669 FIRST FLOOR 4474 SECOND FLOOR 4481 TOTAL 10624</p> <p>CARRIAGE HOUSES FIRST FLOOR 1802 TOTAL 1802</p> <p>TOTAL UNITS SF 37199 TOTAL SF 37199</p>	<p>RESIDENTIAL FLOOR AREA CALCULATIONS (APPROVED THROUGH MINOR MODIFICATION TO LUR 2008-00083)</p> <p>RH-2 ZONE BROADWAY BUILDING FIRST FLOOR 2998 SECOND FLOOR 6684 THIRD FLOOR 4474 TOTAL 14156</p> <p>NORTH BUILDING FIRST FLOOR 3235 SECOND FLOOR 2344 TOTAL 5579</p> <p>EAST BUILDINGS LOWER LEVEL/BASEMENT 1796 FIRST FLOOR 2912 LIVING ROOM 1788 SECOND FLOOR 3156 TOTAL 8752</p> <p>SCHOOL BUILDING BASEMENT 1806 FIRST FLOOR 4496 SECOND FLOOR 5029 TOTAL 11331</p> <p>CARRIAGE HOUSES FIRST FLOOR 1808 TOTAL 1808</p> <p>TOTAL UNITS SF 41826 TOTAL SF 40919</p>	<p>RESIDENTIAL FLOOR AREA CALCULATIONS (REQUESTED THROUGH SPR AMENDMENT)</p> <p>RH-2 ZONE BROADWAY BUILDING FIRST FLOOR 2998 SECOND FLOOR 7780 THIRD FLOOR 4474 TOTAL 15252</p> <p>NORTH BUILDING FIRST FLOOR 2370 SECOND FLOOR 3291 TOTAL 8005</p> <p>EAST BUILDINGS LOWER LEVEL/BASEMENT 1796 FIRST FLOOR 2912 LIVING ROOM 1788 SECOND FLOOR 3156 TOTAL 8752</p> <p>SCHOOL BUILDING BASEMENT 1806 FIRST FLOOR 4496 SECOND FLOOR 5029 TOTAL 11331</p> <p>CARRIAGE HOUSES FIRST FLOOR 1808 TOTAL 1808</p> <p>TOTAL UNITS SF 45148 TOTAL SF 44441</p> <p><small>*non-contributing additions to residential SF in the existing school (707 SF) are excluded from this SF *REQUESTED SF IS 16% ADDITIONAL SF FROM ALREADY APPROVED RESIDENTIAL SF</small></p>
<p>NON-RESIDENTIAL FLOOR AREA CALCULATIONS (APPROVED LUR 2008-00083)</p> <p>RH-2 ZONE BROADWAY BUILDING GARAGE 10363 FIRST FLOOR COMMERCIAL 2946 FIRST FLOOR COMMONS 2647 15956</p> <p>SCHOOL BUILDING BASEMENT COMMONS 2217 BASEMENT OTHER (MECH / STOR) 1538 3755</p> <p>TOTAL SF 19711</p>	<p>300 SF REDUCTION TO FIRST FLOOR OFFICE/COMMERCIAL</p>	<p>NON-RESIDENTIAL FLOOR AREA CALCULATIONS (REQUESTED THROUGH SPR AMENDMENT)</p> <p>BROADWAY BUILDING GARAGE 10363 FIRST FLOOR COMMERCIAL 2646 FIRST FLOOR COMMONS 2947 15956</p> <p>SCHOOL BUILDING BASEMENT COMMONS 2217 BASEMENT OTHER (MECH / STOR) 1538 3755</p> <p>TOTAL SF 19711</p>
<p>DENSITY (APPROVED LUR 2008-00083)</p> <p>RH-2 ZONE: 3,000 SF OF SITE AREA REQUIRED PER DWELLING UNIT. PLANNING BOARD CAN APPROVE A REDUCTION TO 1,600 SF/UNIT. TOTAL NUMBER OF UNITS 27 AREA OF SITE (RH-2) 74,398 TOTAL SITE AREA PER DWELLING UNIT 2,755</p> <p>*UP TO 46 DWELLING UNITS CAN BE REQUESTED PER SECTION 9-3-3(b), B.R.C. 1981</p> <p>BROADWAY BUILDING GARAGE 475 FIRST FLOOR 8720 SECOND FLOOR 8413 THIRD FLOOR 2013 TOTAL 19623</p> <p><small>*Below grade parking areas are NOT included in the floor area totals per the definition of 'uninhabitable space' (stairs, trash enclosure area, storage, and elevator on the parking level ARE included in these calculations) *exterior elevated above grade corridors necessary for egress ARE included in the floor area calculations</small></p> <p>NORTH BUILDING FIRST FLOOR 3379 SECOND FLOOR 2221 TOTAL 5600</p> <p>EAST BUILDINGS FIRST FLOOR 4234 LIVING ROOM 1440 SECOND FLOOR 2910 TOTAL 8584</p> <p>SCHOOL BUILDING BASEMENT 6142 FIRST FLOOR 6072 SECOND FLOOR 6084 TOTAL 18298</p> <p>CARRIAGE HOUSES GARAGE LEVEL 2452 FIRST FLOOR 1972 TOTAL 4424</p> <p><small>*exterior elevated above grade corridors necessary for egress ARE included in the floor area calculations</small></p> <p>TOTAL SF 56529 TOTAL SF 56529</p>	<p>FINISHED BASEMENT UNDER END UNITS (N1-E & N1-W) AND STORAGE FOR AFFORDABLE UNITS UNDER INTERNAL UNIT (N2)</p>	<p>DENSITY (REQUESTED THROUGH SPR AMENDMENT)</p> <p>RH-2 ZONE: 3,000 SF OF SITE AREA REQUIRED PER DWELLING UNIT. PLANNING BOARD CAN APPROVE A REDUCTION TO 1,600 SF/UNIT. TOTAL NUMBER OF UNITS 30 AREA OF SITE (RH-2) 74,398 TOTAL SITE AREA PER DWELLING UNIT 2,480</p> <p>*UP TO 46 DWELLING UNITS CAN BE REQUESTED PER SECTION 9-3-3(b), B.R.C. 1981</p> <p>BROADWAY BUILDING GARAGE 995 FIRST FLOOR 8632 SECOND FLOOR 8506 THIRD FLOOR 2771 TOTAL 22904</p> <p><small>*Below grade parking areas are NOT included in the floor area totals per the definition of 'uninhabitable space' (stairs, trash enclosure area, storage, and elevator on the parking level ARE included in these calculations) *exterior elevated above grade corridors necessary for egress ARE included in the floor area calculations</small></p> <p>NORTH BUILDING BASEMENT/STOR 3436 FIRST FLOOR 3435 SECOND FLOOR 2221 TOTAL 9092</p> <p>EAST BUILDINGS BASEMENT 2120 FIRST FLOOR 4050 LIVING ROOM 1788 SECOND FLOOR 3156 TOTAL 11114</p> <p>SCHOOL BUILDING BASEMENT 6142 FIRST FLOOR 6072 SECOND FLOOR 6084 TOTAL 18298</p> <p>CARRIAGE HOUSES GARAGE LEVEL 2476 FIRST FLOOR 1948 TOTAL 4424</p> <p><small>*exterior elevated above grade corridors necessary for egress ARE included in the floor area calculations</small></p> <p>TOTAL SF 65832 TOTAL SF 65832</p>

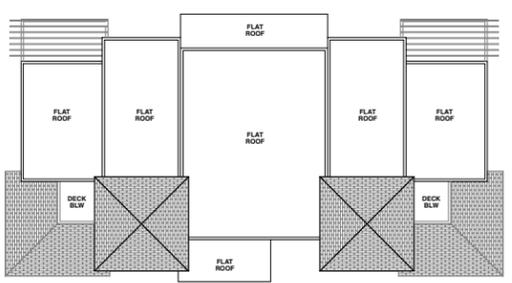
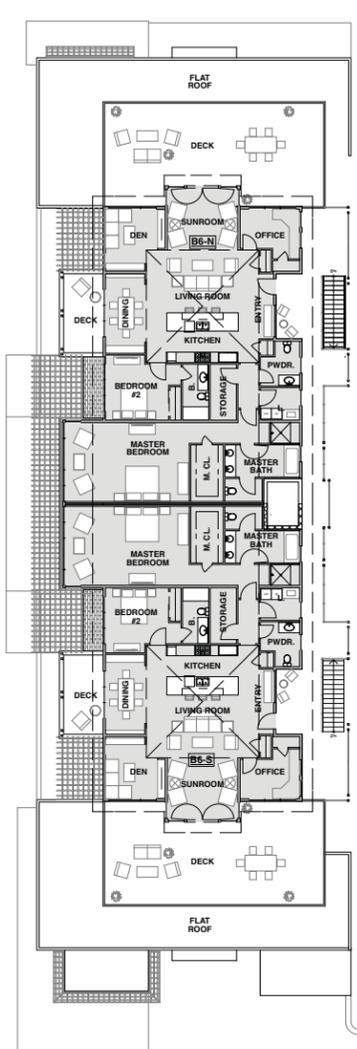
SECOND LEVEL PLANS

SCALE: 1/16" = 1'-0"

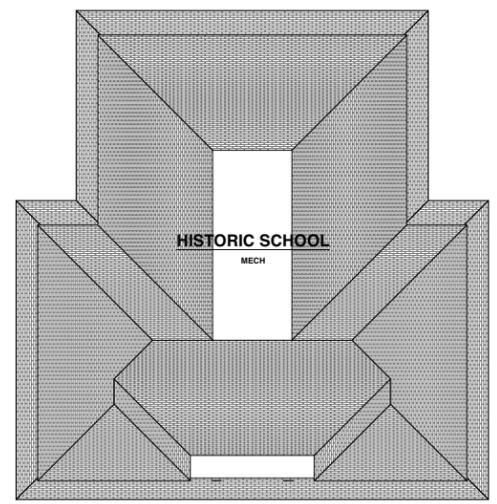
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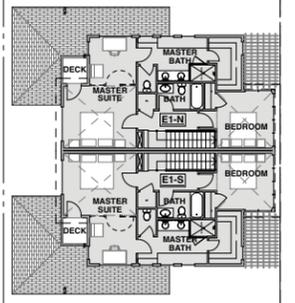
BROADWAY BUILDING



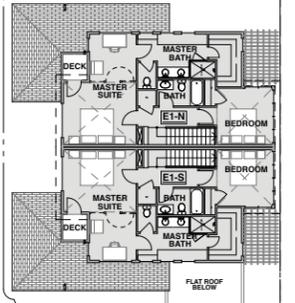
NORTH BUILDING



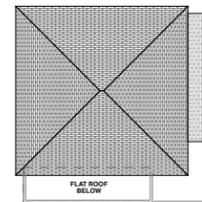
HISTORIC SCHOOL
MECH



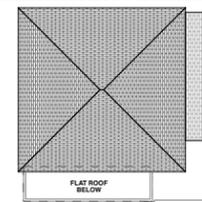
NORTH DUPLEX



SOUTH DUPLEX



NORTH CARRIAGE HOUSE



SOUTH CARRIAGE HOUSE

THIRD LEVEL PLANS

SCALE: 1/16" = 1'-0"

1

PREVIOUS CODE

APPROVED LUR 2008-00083	APPROVED THROUGH MINOR MODIFICATIONS	REQUESTED THROUGH SPR AMENDMENT
RESIDENTIAL FLOOR AREA CALCULATIONS (APPROVED LUR 2008-00083)		
RH-2 ZONE BROADWAY BUILDING		
FIRST FLOOR	3050	2998
SECOND FLOOR	7570	6684
THIRD FLOOR	2014	4474
TOTAL	12634	15252
NORTH BUILDING		
FIRST FLOOR	3235	2370
SECOND FLOOR	2344	3291
TOTAL	5579	8005
EAST BUILDINGS		
FIRST FLOOR	2210	1795
LIVING ROOM	1440	2012
SECOND FLOOR	2910	1788
TOTAL	6560	8752
SCHOOL BUILDING		
BASEMENT	1669	1806
FIRST FLOOR	4474	4496
SECOND FLOOR	4481	5029
TOTAL	10624	11331
CARRIAGE HOUSES		
FIRST FLOOR	1802	1808
TOTAL	1802	1808
TOTAL UNITS SF	37199	41826
TOTAL SF	37199	40919

NON-RESIDENTIAL FLOOR AREA CALCULATIONS (APPROVED LUR 2008-00083)	300 SF REDUCTION TO FIRST FLOOR OFFICE/COMMERCIAL	NON-RESIDENTIAL FLOOR AREA CALCULATIONS (REQUESTED THROUGH SPR AMENDMENT)
RH-2 ZONE BROADWAY BUILDING		
GARAGE	10363	10363
FIRST FLOOR COMMERCIAL	2946	2646
FIRST FLOOR COMMONS	2647	2947
TOTAL	15956	15956
SCHOOL BUILDING		
BASEMENT COMMONS	2217	2217
BASEMENT OTHER (MECH / STOR)	1538	1538
TOTAL	3755	3755
TOTAL SF	19711	19711

DENSITY (APPROVED LUR 2008-00083)	FINISHED BASEMENT UNDER END UNITS (IN E & N1-W) AND STORAGE FOR AFFORDABLE UNITS UNDER INTERNAL UNIT (N2)	DENSITY (REQUESTED THROUGH SPR AMENDMENT)
RH-2 ZONE:		
3,000 SF OF SITE AREA REQUIRED PER DWELLING UNIT. PLANNING BOARD CAN APPROVE A REDUCTION TO 1,600 SF/UNIT.		
TOTAL NUMBER OF UNITS	27	30
AREA OF SITE (RH-2)	74,398	74,398
TOTAL SITE AREA PER DWELLING UNIT	2,755	2,480
*UP TO 46 DWELLING UNITS CAN BE REQUESTED PER SECTION 9-3-3 (b), B.R.C. 1981		
BROADWAY BUILDING		
GARAGE	475	995
FIRST FLOOR	8720	8632
SECOND FLOOR	8413	8506
THIRD FLOOR	2013	4771
TOTAL	19623	22904
*Below grade parking areas are NOT included in the floor area totals per the definition of 'uninhabitable space'. (stairs, trash enclosure area, storage, and elevator on the parking level ARE included in these calculations)		
*exterior elevated above grade corridors necessary for egress ARE included in the floor area calculations		
NORTH BUILDING		
FIRST FLOOR	3379	3436
SECOND FLOOR	2221	3435
TOTAL	5600	9092
EAST BUILDINGS		
FIRST FLOOR	4234	2120
LIVING ROOM	1440	4050
SECOND FLOOR	2910	1788
TOTAL	8584	11114
SCHOOL BUILDING		
BASEMENT	6142	6142
FIRST FLOOR	6072	6072
SECOND FLOOR	6084	6084
TOTAL	18298	18298
CARRIAGE HOUSES		
GARAGE LEVEL	2452	2476
FIRST FLOOR	1972	1948
TOTAL	4424	4424
TOTAL SF	56529	65832
TOTAL SF	56529	65832

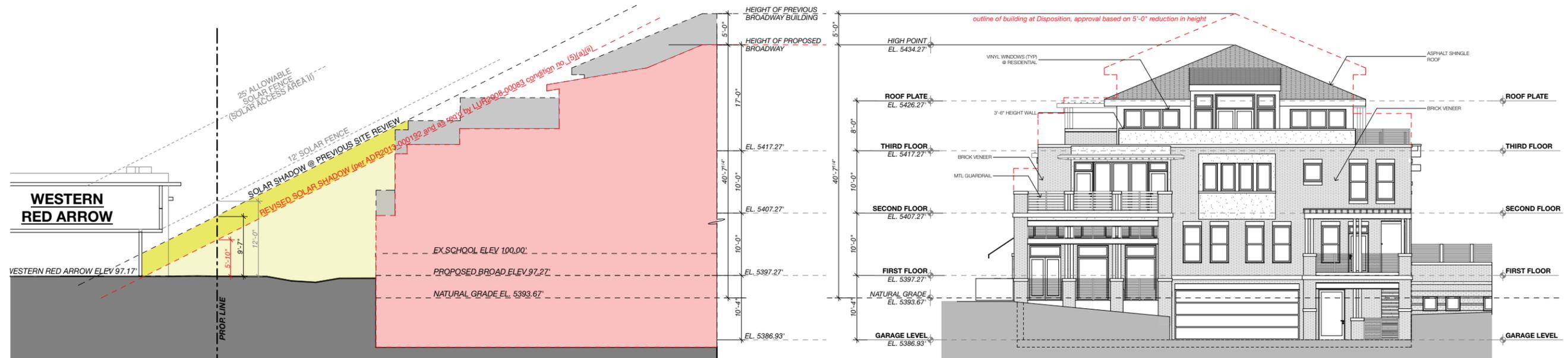
CURRENT CODE



WEST ELEVATION

SCALE: 1/8" = 1'-0"

1



RED ARROW SOLAR BROADWAY

SCALE: 1/8" = 1'-0"

3

SOUTH ELEVATION

SCALE: 1/8" = 1'-0"

2

SITE REVIEW SUBMITTAL	12 SEPT 2008
SITE REVIEW RESUBMITTAL	17 NOV 2008
SITE REVIEW AMENDMENT	6 JAN 2014



EAST ELEVATION

SCALE: 1/8" = 1'-0" **1**



NORTH ELEVATION

SCALE: 1/8" = 1'-0" **2**

SITE REVIEW

(h) Criteria for Review: No site review application shall be approved unless the approving agency finds that:

(1) Boulder Valley Comprehensive Plan:

(A) The proposed site plan is consistent with the land use map and the service area map and, on balance, the policies of the Boulder Valley Comprehensive Plan.

The Washington Village project has been found to be compatible with the policies of the comprehensive plan, including but not limited to, policies related to compact land use pattern, infill development, mixed-use, affordable housing, mixture of housing types, historic preservation, and sensitive infill and development.

(B) The proposed development shall not exceed the maximum density associated with the Boulder Valley Comprehensive Plan residential land use designation. Additionally, if the density of existing residential development within a three-hundred-foot area surrounding the site is at or exceeds the density permitted in the Boulder Valley Comprehensive Plan, then the maximum density permitted on the site shall not exceed the lesser of:

(i) The density permitted in the Boulder Valley Comprehensive Plan, or

The density permitted in the BVCP for the western portion of the site is 14 units or greater. The subject project would be approximately 17.6 dwelling units per acre (du/ac) on that side- an increase from the previous approval which was for 15.8 du/ac. The applicant has requested additional density pursuant to Section 9-8-3(d), B.R.C. 1981. Based on the ability of the project to meet the other aspects of these criteria, the amount of density is appropriate on the Broadway multi-modal corridor. On the eastern portion where low density residential development is intended, the density would be approximately 4.6 dwelling units per acre, which is within the 2 to 6 units per acre range for Low Density Residential.

(ii) The maximum number of units that could be placed on the site without waiving or varying any of the requirements of [chapter 9-8](#), "Intensity Standards," B.R.C. 1981.

(C) The proposed development's success in meeting the broad range of BVCP policies considers the economic feasibility of implementation techniques required to meet other site review criteria.

The development would not be rendered infeasible in meeting the BVCP policies or the Site Review criteria.

(2) Site Design: Projects should preserve and enhance the community's unique sense of place through creative design that respects historic character, relationship to the natural environment, multi-modal transportation connectivity and its physical setting. Projects should utilize site design techniques which are consistent with the purpose of site review in subsection (a) of this section and enhance the quality of the project. In determining whether this subsection is met, the approving agency will consider the following factors:

(A) Open Space: Open space, including, without limitation, parks, recreation areas, and playgrounds:

(i) Useable open space is arranged to be accessible and functional and incorporates quality landscaping, a mixture of sun and shade and places to gather;

The project provides nearly three times the amount of open space as required (i.e., 55% where 20% required) on the RH-2 portion. In addition, 23% of the RL-1 portion is allocated as an open space in process to be dedicated to the city as a pocket park at the corner of Cedar Avenue and 13th Street. The space is set aside as a result of community requests for a publicly accessible open space similar to the function it now serves. All of the open spaces on the property would be accessible and functional and would contain quality landscaping and appropriate plantings to achieve a mix of sun and shade.

(ii) Private open space is provided for each detached residential unit;

With the exception of several units proposed within the historic school building, most units, attached and detached, would have private open space, which exceeds the intent of the criterion and the requirements of the RH-2 zone. This requirement does not require private open space for attached units; nevertheless, most attached units would have the benefit of private open space and those that do not, would benefit from the ample common open space provided on the site.

(iii) The project provides for the preservation of or mitigation of adverse impacts to natural features, including, without limitation, healthy long-lived trees, significant plant communities, ground and surface water, wetlands, riparian areas, drainage areas and species on the federal Endangered Species List, "Species of Special Concern in Boulder County" designated by Boulder County, or prairie dogs (Cynomys ludovicianus), which is a species of local concern, and their habitat;

There are no significant plant communities, wetland or riparian areas that are of environmental concern on the property. There are, however, a good number of mature trees. A previously recorded covenant has required the preservation of matures within 50 feet of 13th Street right-of-way on the east side of the site. The applicant has submitted a tree preservation plan and a report from an arborist assessing the health of all trees. The plan has been reviewed by the city and has been found to be accurate and appropriate in its depiction of trees that are proposed for preservation. For instance, large trees are proposed in the southeastern green space area, along the north property line (those that are healthy), a Maple tree in the courtyard space, and in the southwestern view shed area. To avoid the removal of trees along the north lot line of the development, a condition of approval requires the relocation of a proposed water main into the Cedar Avenue right-of-way.

(iv) The open space provides a relief to the density, both within the project and from surrounding development;

Aside from the Broadway Building's closer location to Broadway where relief to density is provided in the articulated building (as discussed in Section 9-2-14(h)(2)(F)(i), B.R.C. 1981 below) more than in open space, the perimeter of the site on the majority of all other sides would have landscaped setbacks that would exceed code requirements providing a relief from the density to surrounding development. Further, the proposed southeastern open space would also provide such relief to the neighborhood. Within the development, landscape pathways, greenspace in front of the school, and the interior courtyard would provide appropriate relief to the density.

One area along the perimeter that does not appear to be appropriately buffered to surrounding development is the location of the access drive along the existing fence line between the three single-family homes locations to the existing single family home to the north. Although the drive increases the level of separation between the buildings in that area than what could occur, the impacts of vehicular noise and activity along the lot line would not be appropriate relief. A condition of approval has been applied to the project that would require a minimum landscape buffer of no less than 5 feet from the existing fence line to be appropriately planted with bushes and trees to increase compatibility between the properties. Further, a wood fence would be required to replace the existing chain link fence in that location providing additional screening above the new landscape strip and

existing vegetation. This condition would not permit the building sites from reducing the size of the proposed southeastern open space.

(v) Open space designed for active recreational purposes is of a size that it will be functionally useable and located in a safe and convenient proximity to the uses to which it is meant to serve;

Most of the spaces for open space are located in close proximity to residential uses and thus, are of a more passive character. However, the 0.3 acre green space located in the site's southwest corner would be of adequate size to accommodate more active recreational uses. Removal of the stormwater facilities on the green space has been required to make the space functionally useable consistent with this criterion. This space is in process to be dedicated by the applicant to the city as public park.

(vi) The open space provides a buffer to protect sensitive environmental features and natural areas; and

The site is an infill site where there are no sensitive natural areas to be buffered.

(vii) If possible, open space is linked to an area- or city-wide system.

The site is located in a developed location where connections to the sidewalk system are provided along the perimeter of the development. Community Park, three blocks to the west, is easily accessed by walking or biking.

(B) Open Space in Mixed Use Developments (Developments That Contain a Mix of Residential and Nonresidential Uses):

(i) The open space provides for a balance of private and shared areas for the residential uses and common open space that is available for use by both the residential and nonresidential uses that will meet the needs of the anticipated residents, occupants, tenants, and visitors of the property; and

Most residential units have private open space for their use. A majority of the development, as a mixed-use co-housing development, contains ample open space for use of the residents and the greater neighborhood. This results in an appropriate balance for residents and visitors to the property.

(ii) The open space provides active areas and passive areas that will meet the needs of the anticipated residents, occupants, tenants, and visitors of the property and are compatible with the surrounding area or an adopted plan for the area.

As noted above, the project is an infill project that is mostly residential. The project provides more open space than is required and has a greater diversity of private and shared areas than typical projects. However, in line with the discussion in (A)(iii) above, this criterion requires active open spaces open to anticipated residents, occupants, tenants, and visitors of the property. The elimination of the drainage detention area would increase the functionality of the space for more active recreational purposes like small scale football and/or Frisbee and would enable the project to have an appropriate balance of passive and active recreational spaces.

(C) Landscaping:

(i) The project provides for aesthetic enhancement and a variety of plant and hard surface materials, and the selection of materials provides for a variety of colors and contrasts and the preservation or use of local native vegetation where appropriate;

The project includes a large assortment of plantings filling landscape areas, as well as green spaces and hard

surface areas that will be attractive and inviting to residents and visitors.

(ii) Landscape design attempts to avoid, minimize or mitigate impacts on and off site to important native species, healthy, long lived trees, plant communities of special concern, threatened and endangered species and habitat by integrating the existing natural environment into the project;

The project has historically been used as a school and thus, its development is infill and would not impact any native flora or fauna.

(iii) The project provides significant amounts of plant material sized in excess of the landscaping requirements of [sections 9-9-12](#), "Landscaping and Screening Standards," and [9-9-13](#), "Streetscape Design Standards," B.R.C. 1981; and

With the ample amount of open space and variety of plants, the project would exceed the standards of the landscaping regulations.

(iv) The setbacks, yards and useable open space along public rights of way are landscaped to provide attractive streetscapes, to enhance architectural features and to contribute to the development of an attractive site plan.

All three streetscapes of the property would include landscaping and tree plantings that would improve the attractiveness of the site plan. The applicant has agreed to move utilities along Broadway to allow for the installation of street trees per City requirements. New street trees are proposed and several street trees are proposed to remain along both Broadway and Cedar, which will contribute to an attractive streetscape.

(D) Circulation: Circulation, including, without limitation, the transportation system that serves the property, whether public or private and whether constructed by the developer or not:

(i) High speeds are discouraged or a physical separation between streets and the project is provided;

The site is primarily accessed by a shared access drive and a vehicular entry to the Broadway Building. The access drive is narrow (roughly 24 feet wide), lined by garages and residences, crossed by pedestrian crosswalks, and takes a 90 degree turn, and furthermore, the vehicular entry to the Broadway Building is interrupted by a landscape island – all of these aspects will discourage high speed travel.

(ii) Potential conflicts with vehicles are minimized;

The site would be served by three access points which channel vehicles into the site and off of the street system. All single-family residences would be rear loaded and thus, the need for driveways for each unit is eliminated, which reduce potential conflicts between vehicles and pedestrians. Based on the designed pathway system within the project, it is possible to cross through the entire site with only one interface between sidewalks and the proposed shared access drive. The basic site design adequately reduces potential conflict with vehicles.

(iii) Safe and convenient connections are provided that support multi-modal mobility through and between properties, accessible to the public within the project and between the project and the existing and proposed transportation systems, including, without limitation, streets, bikeways, pedestrianways and trails;

The project has various pedestrian access points that connect to the existing sidewalk system. The project is also conveniently located adjacent to an established bike route on 13th Street.

(iv) Alternatives to the automobile are promoted by incorporating site design techniques, land use patterns, and supporting infrastructure that supports and encourages walking, biking, and other alternatives to the single-occupant vehicle;

The co-housing nature of the project is expected to generate less traffic. Beyond that aspect, the project has bicycle parking to encourage use of the bike (considering the nearby bike route) and a car share program to discourage vehicle ownership. Further, a new bus stop will be provided along Broadway encouraging convenient transit usage.

(v) Where practical and beneficial, a significant shift away from single-occupant vehicle use to alternate modes is promoted through the use of travel demand management techniques;

The applicant has agreed to implement TDM strategies to minimize the necessity of automobile use within the development. Such strategies are provided bicycle parking in excess of requirements and participation a vehicle sharing program among other incentives to encourage residents to not own vehicles.

(vi) On-site facilities for external linkage are provided with other modes of transportation, where applicable;

As noted above, the applicant has provided adequate bike storage and a new bus stop to encourage alternative modes of travel.

(vii) The amount of land devoted to the street system is minimized; and

The site is accessed by one shared access drive and one car entry point to the Broadway Building leaving most of the site allocated to buildings and open space. A majority of parking is subterranean.

(viii) The project is designed for the types of traffic expected, including, without limitation, automobiles, bicycles, and pedestrians, and provides safety, separation from living areas, and control of noise and exhaust.

Automobile areas are confined to the access to the Broadway Building and the shared access drive. Otherwise, the majority of the site contains pedestrian paths and living areas, which are appropriately separated from the externalities of automobiles.

(E) Parking:

(i) The project incorporates into the design of parking areas measures to provide safety, convenience, and separation of pedestrian movements from vehicular movements;

The parking areas provided in the development are linear in nature, which minimize the amount pedestrians must interface with automobiles. Where there is interaction, raised crosswalks and convenient access to stairways are provided.

(ii) The design of parking areas makes efficient use of the land and uses the minimum amount of land necessary to meet the parking needs of the project;

Most of the parking areas are proposed in subterranean parking garages under the Broadway Building and in garages under the carriage houses and duplexes. With only six surface parking spaces (potentially 9 per condition), the visual impact and areas dedicated to parking would be minimal.

(iii) Parking areas and lighting are designed to reduce the visual impact on the project, adjacent properties, and adjacent streets; and

As noted above, most of the parking areas are subterranean and would not significantly affect the aesthetics of the site. Lighting would be internal, also minimizing any externalities of parking areas.

(iv) Parking areas utilize landscaping materials to provide shade in excess of the requirements in subsection [9-9-6\(d\)](#), and section [9-9-14](#), "Parking Lot Landscaping Standards," B.R.C. 1981.

Most parking is within buildings; however, the vehicle turnaround and drop off area in the southwest corner of the site would be the most visible area of vehicular activity. The applicant has proposed ample landscaping, including a raised planter, in that area to minimize any adverse aesthetic effects.

(F) Building Design, Livability, and Relationship to the Existing or Proposed Surrounding Area:

(i) The building height, mass, scale, orientation, architecture and configuration are compatible with the existing character of the area or the character established by adopted design guidelines or plans for the area;

This factor is met as follows:

- The height of the majority of the proposed buildings on the site would conform to the 35-foot height limitation for the RH-2 and RL-1 districts. In fact, most of the multi-family buildings would not exceed 30 feet. The Broadway Building would be built at 41 feet (per previous condition of approval).
- The mass of the interior buildings would be appropriate, since they are all well articulated and would not appear imposing, since most are only two stories and those over two stories are of similar stature to large single family homes, which exist in the neighborhood.
- Additional floor area proposed for the development would be within the massing, footprints and basements of buildings and would not contribute to any additional perceivable bulk. While additional floor area is proposed, the massing of buildings would actually decrease as a result of design changes to the Broadway Building. For instance, the Broadway Building has been reduced in height by 5 feet per the previous condition of approval. The massing is less, because the reduced height was achieved by sinking the building and subterranean parking deeper into the ground. This design enables the retention of the hip roof as opposed to what may have been a perceivably taller building with a flat roof.
- Aside from the historic school, the Broadway Building would be the most massive on the site. Its size would be noticeable by virtue of its length along Broadway at 194 feet. This compares to a roughly 170 foot length of the Broadway Brownstones project to the south. The length is not considered out of context since it is mostly two story massing along the streetscape with relief provided by upper floor setbacks. Where the first story is at 17 feet at its closest point to the Broadway lot line (this correlates to setbacks of buildings across the street), the second story would be set back 22 feet (which matches the setbacks of the Broadway Brownstones one block down) and the third floor would be set back 34 feet, which is 9 feet more than the required setback on that side. The entire length of the building is well articulated and will not present any more mass than the Broadway Brownstones building along Broadway. Concentrating massing along Broadway is considered a more practical way of accommodating the density on the site, as the alternative could present greater impacts to the single-family neighborhood on the project's east side.
- The orientation and configuration of buildings on the site are found compatible, in that all would orient to the three streetscapes (a condition is proposed that would require this) and the configuration of the buildings are appropriate to the different contexts on each side of the site. For example, buildings on the RH-2 side have larger footprints similar to other multi-family projects along the high density residential Broadway corridor, whereas moving eastward on the sites, the form and footprints of buildings decrease to match the RL-1 single-family character on the east side.

(ii) The height of buildings is in general proportion to the height of existing buildings and the proposed or projected heights of approved buildings or approved plans or design guidelines for the immediate area;

The heights of the buildings, as noted above, have been found to be compatible with the height of existing for the immediate area and other buildings proposed for the site. The existing Washington School building is over the height limit at nearly 50-feet and would continue to be the most prominent building on the site. The next tallest building would be the Broadway Building, which has been limited to no taller than 41 feet per previous condition of approval. The applicant has already adjusted the design of the building to meet this height limitation while also retraining a hip roof design lending to more consistency in form to adjacent buildings on and around the site. The proposed building at that height will be in general proportion to the height of existing buildings considering the school building the height of Broadway Brownstones and the BHP building at 3120 Broadway, which were both built to a height of 41-feet.

(iii) The orientation of buildings minimizes shadows on and blocking of views from adjacent properties;

- Although there are views of the Flatirons and mountains from the site, they are considered marginal and not significant. Most views are block by existing foliage. If the marginal views were considered of significant value, it would necessitate a severe restriction on building locations on the site – especially portions designated for high density residential. Nevertheless, some views of the mountains to the west would be preserved, since the lowest portions of the Broadway Building roof would not block the views. Therefore, minimization of view blockage from the site is considered appropriate for the context of the project.
- The RH-2 portion of the project is subject to the Solar Access area II standards which do not permit buildings to cast a shadow greater than what a 25-foot fence along the property lines would. The applicant has located the buildings more than double (in some cases over triple) the required distance from the north property line in order to have the buildings not exceed a 12-foot solar fence, which is applied to single family neighborhoods. Sunlight during the day of the lowest sun angle (Dec. 21st) would still be able to reach into dwelling units (i.e., Red Arrow Townhomes) at noon north of the property line. This is a significant minimization of shadowing beyond what the code allows. Furthermore, considering the reduction in height of the Broadway Building, the solar shadow during worst case scenario (Dec. 21st), shadows cast would be equivalent to if a 6 foot fence were installed on the property. This is half the single-family (Solar Access Area I) requirement where RH-2 requirements permit situations where a 25-foot solar fence would be code compliant.

(iv) If the character of the area is identifiable, the project is made compatible by the appropriate use of color, materials, landscaping, signs, and lighting;

To match context, the project must be found compatible with the general character of the neighborhood, as well as the historic school. The historic school is an iconic building, whereas away from Broadway, most of the neighborhood is of a single-family character that contains a variety of architectural styles from the early 1900s up to more current neo-traditional designs. The project is found compatible as follows:

- Most buildings on the site incorporate similar brickwork as the school and similar window and roof designs to match that of the school. However, no incorporation of the school design aspects reaches the level of imitation, which is not preferred.
- The proposed colors are subdued and generally, earth tone and would not compete with the school and/or look out of character with the neighborhood.

- The proposed designs for the single-family homes, as ascertained from the applicant's submitted design guidelines and pictures of example homes, is an appropriate design direction with the use of gable roofs, front porches and general forms that would not be overly massive and would be compatible with the neighborhood. Homes sizes are also be limited by condition to ensure scale compatibility.

(v) Projects are designed to a human scale and promote a safe and vibrant pedestrian experience through the location of building frontages along public streets, plazas, sidewalks and paths, and through the use of building elements, design details and landscape materials that include, without limitation, the location of entrances and windows, and the creation of transparency and activity at the pedestrian level;

- The Broadway Building is designed with notable pedestrian interest with the building situated closer to the street as in similar urban situations, ample fenestration along the street and on upper floors, interesting articulation and wall details, and a large porch entryway to the Broadway Building in the center part of its frontage. The emphasis of two-story massing of the building along Broadway would also be appropriate to the pedestrian scale. This area is also proposed to be well landscaped.
- Visual interest along Cedar Avenue would be provided by the southwest viewshed and open space in front of the school where landscaping and preserved trees would be within the space. Aside from the entryway into the Broadway Building, the entirety of the Cedar Avenue frontage would be landscaped open space.
- A condition of approval would require single family buildings to present attractive street faces along 13th and onto the proposed greenspace consistent with this criterion. The 13th Street streetscape is, otherwise, found consistent with this criterion with the preserved greenspace.

(vi) To the extent practical, the project provides public amenities and planned public facilities;

Following the public process and review of this project, the applicant has provided a 13,160 square foot open space area intended to be a pocket park. It is in process to be dedicated to the city as a public park. This dedication is above and beyond a typical development review project.

(vii) For residential projects, the project assists the community in producing a variety of housing types, such as multifamily, townhouses and detached single family units, as well as mixed lot sizes, number of bedrooms and sizes of units;

The project provides a range of smaller one-bedroom affordable units up to larger market-rate units of two or three bedrooms, as well as single-family residences.

(viii) For residential projects, noise is minimized between units, between buildings, and from either on-site or off-site external sources through spacing, landscaping, and building materials;

The Broadway Building serves as the largest buffer of noise, by blocking vehicular traffic noise from Broadway and created a more sedate internal open space framed by the interior buildings. The duplexes and carriage houses would serve as buffers from interior automotive noise. The construction of the on-site buildings would otherwise follow standard building code practices for minimizing noise between units.

(ix) A lighting plan is provided which augments security, energy conservation, safety, and aesthetics;

Lighting has been evaluated through the Technical Documents process and meets the Outdoor Lighting code and the intent of this criterion.

(x) The project incorporates the natural environment into the design and avoids, minimizes, or mitigates impacts to natural systems;

The site has been developed with a school for over 100 years. The recent development of the site will therefore, not impact any natural systems and incorporates mature trees in its preservation plan.

(xi) Buildings minimize or mitigate energy use; support on-site renewable energy generation and/or energy management systems; construction wastes are minimized; the project mitigates urban heat island effects; and the project reasonably mitigates or minimizes water use and impacts on water quality.

The applicant has committed to doing energy efficient homes and this is demonstrated by Home Energy Rating System (HERS) scores that are close to net zero on the homes that have already been issued building permits. This has been achieved through installation of geothermal energy systems and structural insulated panel wall systems. The applicant also intends to make upgrades to the existing school to make the building more energy efficient.

(xii) Exteriors or buildings present a sense of permanence through the use of authentic materials such as stone, brick, wood, metal or similar products and building material detailing;

The entirety of the project includes a mix of brick, wood and stucco. While stucco is used within the development, brick to match the historic school will be used on almost all buildings in efforts to tie the architecture of the site together with the historic school. The Broadway Building will be almost entirely brick and will provide a high quality and visible presence on Broadway. Most buildings with a public face include brick. In whole, the materials appropriately tie the historic architecture with more contemporary designs and present a sense of permanence.

(xiii) Cut and fill are minimized on the site, the design of buildings conforms to the natural contours of the land, and the site design minimizes erosion, slope instability, landslide, mudflow or subsidence, and minimizes the potential threat to property caused by geological hazards;

The project is on a largely level site, but does require some grading to level out intended open spaces and to create the subterranean parking underneath the Broadway Building. Some contouring is necessary to facilitate appropriate drainage, but is not excessive, nor would it create any impact to natural systems or create any potential geological threat.

(xiv) In the urbanizing areas along the Boulder Valley Comprehensive Plan boundaries between Area II and Area III, the building and site design provide for a well-defined urban edge; and

The site is within Area I and therefore, this criterion is inapplicable.

(xv) In the urbanizing areas located on the major streets shown on the map in Appendix A of this title near the Boulder Valley Comprehensive Plan boundaries between Area II and Area III, the buildings and site design establish a sense of entry and arrival to the City by creating a defined urban edge and a transition between rural and urban areas.

The site is within Area I and therefore, this criterion is inapplicable.

(G) Solar Siting and Construction: For the purpose of ensuring the maximum potential for utilization of solar energy in the City, all applicants for residential site reviews shall place streets, lots, open spaces, and buildings so as to maximize the potential for the use of solar energy in accordance with the following solar siting criteria:

(i) Placement of Open Space and Streets: Open space areas are located wherever practical to protect buildings from shading by other buildings within the development or from buildings on adjacent properties. Topography and other natural features and constraints may justify deviations from this criterion.

The applicant has been working on an optimal layout of buildings considering the location of the existing school, where higher densities are intended, and the required viewshed of no development in the site's southwest corner. Under these conditions, the applicant has positioned the buildings such that shadowing of the on-site open space and onto properties to the north would occur. Nevertheless, the project would conform to the solar regulations and considering the applicant's attempts at minimizing impact on solar access to the north and the above mentioned constraining factors, the placement of open space is considered the most practical.

(ii) Lot Layout and Building Siting: Lots are oriented and buildings are sited in a way which maximizes the solar potential of each principal building. Lots are designed to facilitate siting a structure which is unshaded by other nearby structures. Wherever practical, buildings are sited close to the north lot line to increase yard space to the south for better owner control of shading.

As stated above, buildings are sited in a practical way to deal with a number of constraints on the site, as well as considering these criteria. Buildings are located to the north as much as possible to increase yard space to the south while also ensuring a minimal shadowing impact to Red Arrow Townhomes. Reduced impacts on Red Arrow also are achieved by the additional lowering of the Broadway Building's height. Nevertheless, it is expected that some shading from the historic school upon the north building and courtyard would occur during winter months, but not to an extent found unacceptable. Other buildings on the site, otherwise, are sited such that shading from historic school would not preclude the use of solar systems.

(iii) Building Form: The shapes of buildings are designed to maximize utilization of solar energy. Buildings shall meet the solar access protection and solar siting requirements of [section 9-9-17](#), "Solar Access," B.R.C. 1981.

Most buildings on the site would have gable and hip roof forms and would have surfaces conducive to the installation of solar energy systems.

(iv) Landscaping: The shading effects of proposed landscaping on adjacent buildings are minimized.

Most of the mature trees on the site are concentrated along the south lot line and furthest from the majority of new buildings on the site.

(H) Additional Criteria for Poles Above the Permitted Height: No site review application for a pole above the permitted height will be approved unless the approving agency finds all of the following:

Not applicable to this project.

(i) The light pole is required for nighttime recreation activities which are compatible with the surrounding neighborhood, light or traffic signal pole is required for safety, or the electrical utility pole is required to serve the needs of the City; and

(ii) The pole is at the minimum height appropriate to accomplish the purposes for which the pole was erected and is designed and constructed so as to minimize light and electromagnetic pollution.

(I) Land Use Intensity Modifications:

Not applicable and not requested.

(i) Potential Land Use Intensity Modifications:

- a. The density of a project may be increased in the BR-1 district through a reduction of the lot area requirement or in the Downtown (DT), BR-2, or MU-3 districts through a reduction in the open space requirements.
- b. The open space requirements in all Downtown (DT) districts may be reduced by up to one hundred percent.
- c. The open space per lot requirements for the total amount of open space required on the lot in the BR-2 district may be reduced by up to fifty percent.
- d. Land use intensity may be increased up to twenty-five percent in the BR-1 district through a reduction of the lot area requirement.

(ii) Additional Criteria for Land Use Intensity Modifications: A land use intensity increase will be permitted up to the maximum amount set forth below if the approving agency finds that the criteria in paragraph (h)(1) through subparagraph (h)(2)(H) of this section and following criteria have been met:

- a. **Open Space Needs Met:** The needs of the project's occupants and visitors for high quality and functional useable open space can be met adequately;
- b. **Character of Project and Area:** The open space reduction does not adversely affect the character of the development or the character of the surrounding area; and
- c. **Open Space and Lot Area Reductions:** The specific percentage reduction in open space or lot area requested by the applicant is justified by any one or combination of the following site design features not to exceed the maximum reduction set forth above:
 1. Close proximity to a public mall or park for which the development is specially assessed or to which the project contributes funding of capital improvements beyond that required by the parks and recreation component of the development excise tax set forth in [chapter 3-8](#), "Development Excise Tax," B.R.C. 1981: maximum one hundred percent reduction in all Downtown (DT) districts and ten percent in the BR-1 district;
 2. Architectural treatment that results in reducing the apparent bulk and mass of the structure or structures and site planning which increases the openness of the site: maximum five percent reduction;
 3. A common park, recreation, or playground area functionally useable and accessible by the development's occupants for active recreational purposes and sized for the number of inhabitants of the development, maximum five percent reduction; or developed facilities within the project designed to meet the active recreational needs of the occupants: maximum five percent reduction;
 4. Permanent dedication of the development to use by a unique residential population whose needs for conventional open space are reduced: maximum five percent reduction;
 5. The reduction in open space is part of a development with a mix of residential and nonresidential uses within a BR-2 zoning district that, due to the ratio of residential to

nonresidential uses and because of the size, type, and mix of dwelling units, the need for open space is reduced: maximum fifteen percent reduction; and

6. The reduction in open space is part of a development with a mix of residential and nonresidential uses within a BR-2 zoning district that provides high quality urban design elements that will meet the needs of anticipated residents, occupants, tenants, and visitors of the property or will accommodate public gatherings, important activities, or events in the life of the community and its people, that may include, without limitation, recreational or cultural amenities, intimate spaces that foster social interaction, street furniture, landscaping, and hard surface treatments for the open space: maximum twenty-five percent reduction.

(J) Additional Criteria for Floor Area Ratio Increase for Buildings in the BR-1 District:

Not applicable and not requested.

(i) **Process:** For buildings in the BR-1 district, the floor area ratio ("FAR") permitted under table 8-2, [section 9-8-2](#), "Floor Area Ratio Requirements," B.R.C. 1981, may be increased by the city manager under the criteria set forth in this subparagraph.

(ii) **Maximum FAR Increase:** The maximum FAR increase allowed for buildings thirty-five feet and over in height in the BR-1 district shall be from 2:1 to 4:1.

(iii) **Criteria for the BR-1 District:** The FAR may be increased in the BR-1 district to the extent allowed in subparagraph (h)(2)(J)(ii) of this section if the approving agency finds that the following criteria are met:

- a. Site and building design provide open space exceeding the required useable open space by at least ten percent: an increase in FAR not to exceed 0.25:1.
- b. Site and building design provide private outdoor space for each office unit equal to at least ten percent of the lot area for buildings twenty-five feet and under and at least twenty percent of the lot area for buildings above twenty-five feet: an increase in FAR not to exceed 0.25:1.
- c. Site and building design provide a street front facade and an alley facade at a pedestrian scale, including, without limitation, features such as awnings and windows, well-defined building entrances, and other building details: an increase in FAR not to exceed 0.25:1.
- d. For a building containing residential and nonresidential uses in which neither use comprises less than twenty-five percent of the total square footage: an increase in FAR not to exceed 1:1.
- e. The unused portion of the allowed FAR of historic buildings designated as landmarks under [chapter 9-11](#), "Historic Preservation," B.R.C. 1981, may be transferred to other sites in the same zoning district. However, the increase in FAR of a proposed building to which FAR is transferred under this subparagraph may not exceed an increase of 0.5:1.
- f. For a building which provides one full level of parking below grade, an increase in FAR not to exceed 0.5:1 may be granted.

(K) Additional Criteria for Parking Reductions: The off-street parking requirements of [section 9-9-6](#), "Parking Standards," B.R.C. 1981, may be modified as follows:

- (i) **Process:** The city manager may grant a parking reduction not to exceed fifty percent of the required parking. The planning board or city council may grant a reduction exceeding fifty percent.

When the project was originally approved, the previous RH-2 parking standards applied whereby one parking space was required for the first 800 square feet of a unit and one additional parking space was required for every 300 square feet (or portion thereof) afterwards. This was one of the highest parking requirements in the city and was generally meant to apply to RH-2 areas in proximity to the university where there were instances of students doubling up within rooms. This restrictive standard often resulted in requests for parking reductions. For the Washington Village II project, a parking reduction of over 50 percent was required and was approved in 2009.

This parking standard was amended in 2012 to be more synonymous with other RH zones with calculations based on bedroom counts. The current requirement is outlined below:

1-bedroom units	1 parking space
2-bedroom units	1.5 parking spaces
3-bedroom units	2 parking spaces
4 or more bedroom units	3 parking spaces

When applied to the Washington Village project, the following number of residential spaces is required:

Unit type	Unit count	Requirement	Total
1-bedroom units	6 units	1 parking space	6 spaces
2-bedroom units	18 units	1.5 parking spaces	27 spaces
3-bedroom units	6 units	2 parking spaces	12 spaces
Total Residential parking	30 units	NA	45 spaces
<i>Extra spaces for additional future bedrooms</i>	NA	NA	2 spaces
Total Commercial parking	NA	1 space per 300 sf	8 spaces
Total required spaces	30 units & 2,650 sf of commercial	See above	55 spaces
Total spaces provided			49 spaces

The current requirement is for a total of 55 parking spaces on the RH-2 side. Previous requirements required 105 spaces for both residential and non-residential. The applicant is proposing 49 on-site parking spaces. This amounts to an 11 percent parking reduction.

(ii) Criteria: Upon submission of documentation by the applicant of how the project meets the following criteria, the approving agency may approve proposed modifications to the parking requirements of [section 9-9-6](#), "Parking Standards," B.R.C. 1981 (see tables 9-1, 9-2, 9-3 and 9-4), if it finds that:

a. For residential uses, the probable number of motor vehicles to be owned by occupants of and visitors to dwellings in the project will be adequately accommodated;

The submitted parking study dated March 13, 2014 indicates that peak residential need during the peak time (i.e., night time) would be 42 parking spaces where 49 spaces would be available. During the day, the peak need would be 35 parking spaces, where 41 spaces would be available to residential (8 spaces would be allotted to commercial until 4pm by condition of approval). Based on these conclusions, the

probable number of motor vehicles to be owned by occupants of and visitors will be adequately accommodated.

In addition, the applicant is required to employ TDM strategies to encourage residents to own less or have no cars. The TDM prepared by the applicant has been reviewed and found to be appropriate in its provision of on-site bicycle parking (which exceeds requirements), a bike pool and car share program. The TDM also indicates that incentives, such as the provision of transit passes, would be allotted to residents that do not have a car as an incentive to free up parking spaces on the site and to reduce vehicles trips from the site, which is the principal goal of TDM strategies.

b. The parking needs of any nonresidential uses will be adequately accommodated through on-street parking or off-street parking;

Eight (8) off-street parking spaces would be allotted to the non-residential uses on the site. They would be restricted to non-residential uses for the majority of the day (8am to 4pm). Eight spaces meet the required number of spaces for the proposed office use. Further, office spaces would be required to close at 6pm to ensure spaces are available during times of peak need for residential.

c. A mix of residential with either office or retail uses is proposed, and the parking needs of all uses will be accommodated through shared parking;

The submitted parking study prepared by Transportation Consultants, Inc. and dated March 13, 2014 indicates a peak need of 47 parking spaces for residential and office uses on the site. With 49 on-site parking spaces available and considering the conditions of approval referenced below, the parking needs of both uses, which would rely on shared parking, would be met on the site.

-A condition of approval requires that the 8 spaces allotted to office uses during daytime hours be open for residential use after 4pm.

-A condition of approval restricts the office use to 8am and 6pm, thus eliminating any office need between 10pm and 8am.

d. If joint use of common parking areas is proposed, varying time periods of use will accommodate proposed parking needs; and

Parking for office uses would be reserved during the hours of 8am and 4pm. After 4pm, the parking spaces would be available for residential uses, which enable the project to better meet the peak demand during evening hours. Office spaces would be required to close at 6pm to ensure spaces are available during times of peak need for residential.

e. If the number of off-street parking spaces is reduced because of the nature of the occupancy, the applicant provides assurances that the nature of the occupancy will not change.

Should the occupancy of the development change such that its occupancy would require more parking than a standard residential development, a reconsideration of the Site Review and parking reduction would be required. The applicant is aware of this condition.

(L) Additional Criteria for Off-Site Parking: The parking required under [section 9-9-6](#), "Parking Standards," B.R.C. 1981, may be located on a separate lot if the following conditions are met:

Not applicable and not requested.

(i) The lots are held in common ownership;

(ii) The separate lot is in the same zoning district and located within three hundred feet of the lot that it serves; and

(iii) The property used for off-site parking under this subparagraph continues under common ownership or control.

USE REVIEW

The responses to the following criteria reflect the previously approved criteria as part of application LUR2008-00083 and have been updated to reflect the numbers associated with the reduction of 300 square feet of office space.

(e) Criteria for Review: No use review application will be approved unless the approving agency finds all of the following:

(1) Consistency With Zoning and Nonconformity: The use is consistent with the purpose of the zoning district as set forth in [section 9-5-2](#), "Zoning Districts," B.R.C. 1981, except in the case of a nonconforming use;

The RH-2 zoning districts are high density residential areas primarily used for a variety of types of attached residential units, including, without limitation, apartment buildings, and where complementary uses may be allowed. The proposed project includes 30 attached residential units concentrated along Broadway as intended by the zoning code and the Boulder Valley Comprehensive Plan for development along multi-modal corridors. Commercial uses (i.e., professional and technical offices) and community facilities have been found complementary, as there are examples of office along Broadway in similar or greater square footages and also since the site will accommodate parking needed for those uses.

(2) Rationale: The use either:

(A) Provides direct service or convenience to or reduces adverse impacts to the surrounding uses or neighborhood;

The proposed uses would be concentrated on the high density portion of the site nearest to Broadway where higher intensities are expected to occur and where policies encourage mixed-use. By locating the commercial uses at ground level on that side, they serve as a buffer to the residential uses on the interior of the site from the noise and traffic associated with Broadway. The project itself, in how it is arranged, is done to reflect a transition of the higher intensities of the RH (High Density Residential) district down to the lower intensities of the RL (Low Density) portion of the site where the density and scale decrease. The commercial uses would encourage more pedestrian activity on this northern stretch of Broadway, but would be appropriately buffered from the single-family character to the east.

(B) Provides a compatible transition between higher intensity and lower intensity uses;

(C) Is necessary to foster a specific city policy, as expressed in the Boulder Valley Comprehensive Plan, including, without limitation, historic preservation, moderate income housing, residential and nonresidential mixed uses in appropriate locations, and group living arrangements for special populations;
or

(D) Is an existing legal nonconforming use or a change thereto that is permitted under subsection (f) of this section;

(3) Compatibility: The location, size, design, and operating characteristics of the proposed development or change to an existing development are such that the use will be reasonably compatible with and have minimal negative impact on the use of nearby properties or for residential uses in industrial zoning districts, the proposed development reasonably mitigates the potential negative impacts from nearby properties;

As noted above, the office use would be concentrated on a portion of the site expected to have a greater intensity of use and scale, but would be appropriately buffered from the single-family character on the east side of the site. The size of the office use is comparable to several office buildings that exist on this stretch of Broadway. Some examples are the North Broadway Building at the corner of Elder Avenue and Broadway that is entirely commercial with 6,745 square feet and 3093 Broadway, which is also entirely commercial with 3,799 square feet. Another

mixed-use example is the Newland Court project at 3011 Broadway that contains condominiums and a 4,200 square foot office building in an old Victorian building. All of these projects have compliant parking for commercial uses. The subject proposal is for 2,654 square feet of commercial uses. The applicant has targeted professional and technical offices that would generate a lower incidence of customers coming to the site. By having compliant parking and being concentrated directly on Broadway, the use reasonably mitigates the potential negative impacts on neighboring properties. Therefore, the proposed office use is found to be compatible with the immediate neighborhood. Based on the square footage above, eight (8) commercial spaces would be required for the uses. This would be supplied on site per the updated condition of approval.

(4) Infrastructure: As compared to development permitted under [section 9-6-1](#), "Schedule of Permitted Land Uses," B.R.C. 1981, in the zone, or as compared to the existing level of impact of a nonconforming use, the proposed development will not significantly adversely affect the infrastructure of the surrounding area, including, without limitation, water, wastewater, and storm drainage utilities and streets;

There is no evidence that the introduction of commercial uses and residential common facilities on the site would create an adverse impact to City infrastructure above what would be permitted by-right on the property or as compared to other commercial uses that already exist along Broadway.

(5) Character of Area: The use will not change the predominant character of the surrounding area or the character established by adopted design guidelines or plans for the area; and

The predominant character of this portion of Broadway is largely residential. However, there are a number of commercial establishments that are comparable in size and location to the subject proposal. This project would introduce a new development that incorporates a majority of residential with a smaller non-residential component, which is in line with the emerging mixed-use, more urban corridor occurring along Broadway.

(6) Conversion of Dwelling Units to Nonresidential Uses: There shall be a presumption against approving the conversion of dwelling units in the residential zoning districts to nonresidential uses that are allowed pursuant to a use review, or through the change of one nonconforming use to another nonconforming use. The presumption against such a conversion may be overcome by a finding that the use to be approved serves another compelling social, human services, governmental or recreational need in the community, including, without limitation, a use for a daycare center, park, religious assembly, social service use, benevolent organization use, art or craft studio space, museum or an educational use.

The project will result in 36 new dwelling units on the Washington Village site. No conversions from residential to non-residential would occur.

LSC TRANSPORTATION CONSULTANTS, INC.

1889 York Street
Denver, CO 80206
(303) 333-1105
FAX (303) 333-1107
E-mail: lsc@lscdenver.com



March 13, 2014

Mr. Adrian Sopher
Sopher Architects, LLC
3008 Folsom Street
Boulder, CO 80304

Re: Washington Village
Updated Parking Analysis
Boulder, CO
(LSC #130950)

Dear Mr. Sopher:

At your request, we have prepared this updated parking analysis for the non-single-family detached land uses for the Washington Village development in Boulder, Colorado. The proposed site plan includes 49 on-site parking spaces for the 30 multi-family residential units and the 2,700 square feet of office space. Eight of these spaces will be reserved for office space during business hours. The purpose of this analysis is to evaluate the parking demands of these two land uses and to determine whether the parking supply is adequate.

Shared Parking in a Mixed-Use Development

There are many transportation benefits of a mixed-use development, including reduction of vehicle-trips due to multi-purpose trips and encouragement of walking due to putting various uses in close proximity to each other. Another benefit is a reduction in parking due to peak parking demand for different land uses occurring at different times of day. For example, residential uses have peak parking demand during evening and early morning hours while office uses have peak demands during mid-day.

The report *Parking Generation, 4th Edition*, 2010, published by the Institute of Transportation Engineers, includes information on peak parking demand for various land uses as well as percentages of each demand during various times of day for selected uses. Based on this information, Table 1 was compiled to calculate the estimated parking demand for the Washington Village development. This table displays the peak demand for each use based on the parking demand rates included in *Parking Generation*, along with the percentage of peak demand for night, mid-day, and evening time periods. Copies of the applicable parking generation data sheets are attached. For example, the planned 30 multi-family residential dwelling units, with a peak demand of 1.38 spaces per dwelling unit, will have a demand of 42 spaces at night, 27 during the mid-day, and 33 in the evening. The office uses will have a demand of five spaces at night, eight during the mid-day, and two in the evening.

Note that assumptions were made about the approximately 13,000 square feet of open space park to be provided on the southeast corner of the site. It was assumed that this neighborhood park will be served by non-vehicle trips such as walking or bicycle trips.

Totaling up the parking demand for each use for each time period yields a maximum shared demand of 47 parking spaces during the overnight period. This is a reduction of three spaces compared with the total peak demand of both land uses. The Parking Plan for the site shows that 49 parking spaces will be provided on-site for these two land uses.

Summary and Conclusions

The following conclusions can be made regarding the parking requirements of the Washington Village development:

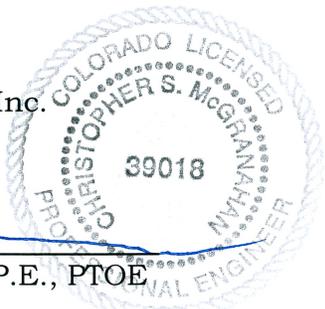
1. The development will contain a mix of residential, office, and open space which will have varying parking demands during different times of the day. The maximum parking demand for the multi-family residential and office land uses will occur during the overnight hours when a total of 47 parked vehicles can be expected.
2. The Parking Plan for Washington Village includes 49 on-site parking spaces which is enough to satisfy the project peak demand for the 30 multi-family residential units and the 2,700 square feet of office space.

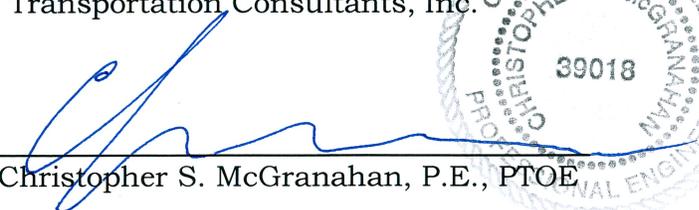
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Please call us if we can be of further assistance.

Respectfully submitted,

LSC Transportation Consultants, Inc.



By: 
Christopher S. McGranahan, P.E., PTOE

CSM/wc

3-13-14

Enclosures: Table 1
Parking Generation datasheets

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**Table 1
Shared Parking Analysis
Washington Village
March, 2013; LSC #130950
Suburban Assumptions**

ITE Land Use Category	Land Use Description	Area	Demand Units	Peak (1) Weekday Rate	Peak Demand	Percent of Minimum Required (1)	Percent of Minimum Required (1)	Percent of Minimum Required (1)	Parking Demand		
						10 PM - 7 AM	8 AM - 5 PM	6 PM - 9 PM	10 PM - 7 AM	8 AM - 5 PM	6 PM - 9 PM
230	Residential - MF		30 DU	1.38	42	100%	64%	77%	42	27	33
701	Office	2.7 KSF ⁽²⁾	2.7 KSF	2.84	8	59%	100%	25%	5	8	2
		2.7 KSF		Total	50			Total	47	35	35
Maximum Shared Demand										47	
Reduction: Peak - Shared Demand										3	

Notes:

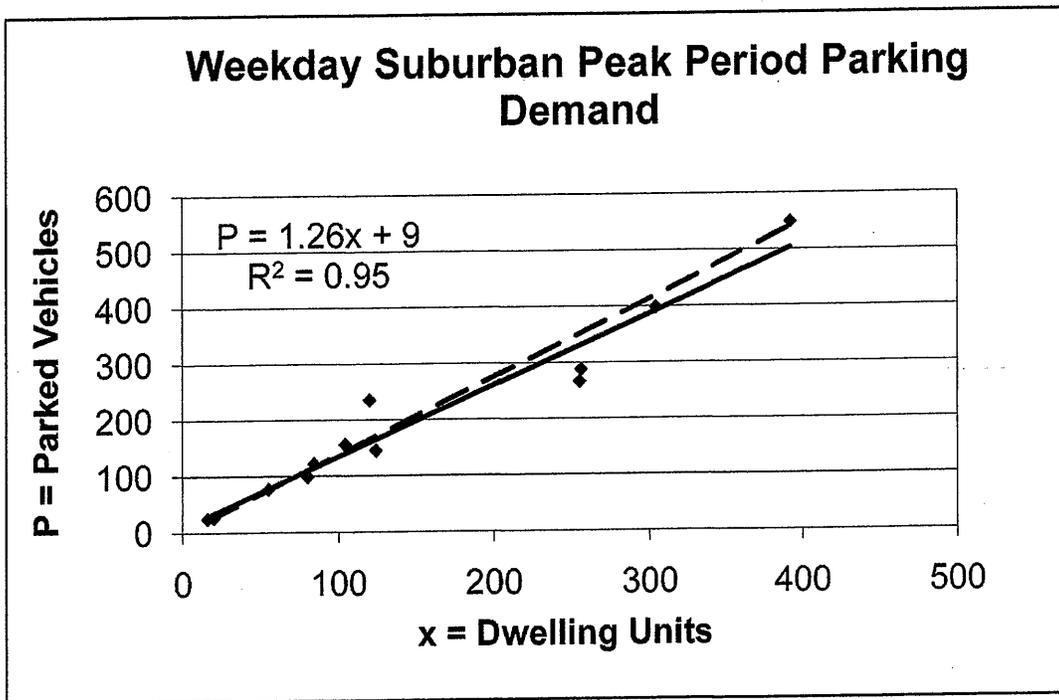
(1) Source: *Parking Generation*, 4th Edition, 2010, Institute of Transportation Engineers.

(2) KSF = 1,000 square feet

Land Use: 230 Residential Condominium/Townhouse

**Average Peak Period Parking Demand vs. Dwelling Units
On a Weekday
Location: Suburban**

Statistic	Peak Period Demand
Peak Period	11:00 p.m.–6:00 a.m.
Number of Study Sites	12
Average Size of Study Sites	151 dwelling units
Average Peak Period Parking Demand	1.38 vehicles per dwelling unit
Standard Deviation	0.24
Coefficient of Variation	17%
Range	1.04–1.96 vehicles per dwelling unit
85th Percentile	1.52 vehicles per dwelling unit
33rd Percentile	1.28 vehicles per dwelling unit



◆ Actual Data Points

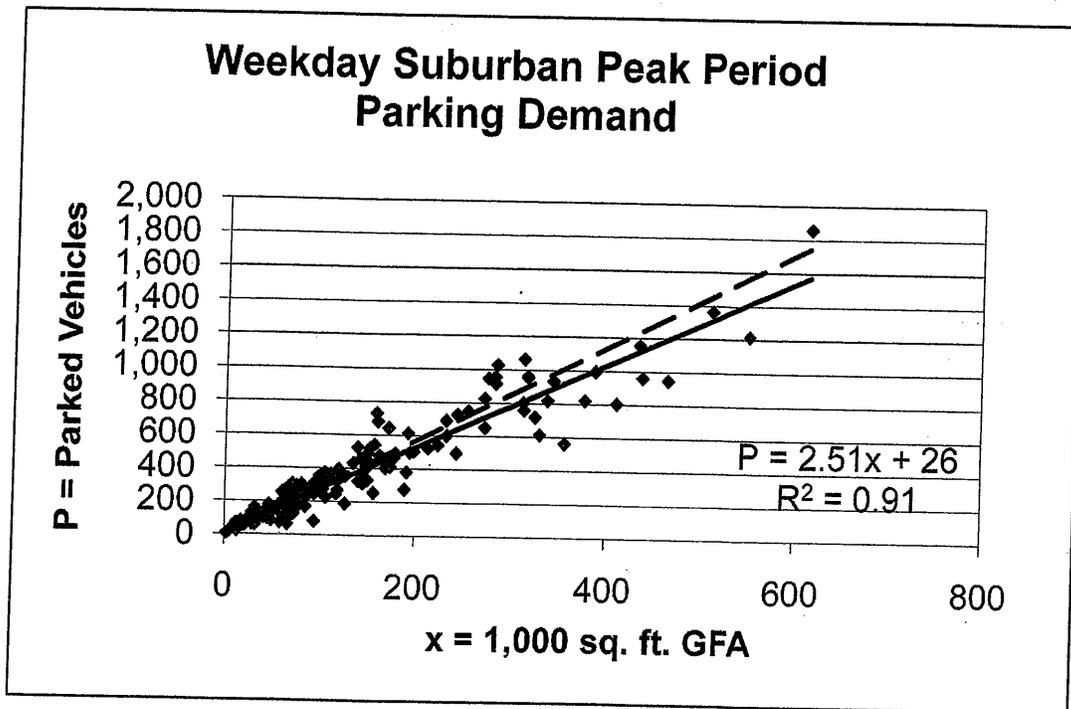
— Fitted Curve

---- Average Rate

Land Use: 701 Office Building

Average Peak Period Parking Demand vs. 1,000 sq. ft. GFA
On a: Weekday
Location: Suburban

Statistic	Peak Period Demand
Peak Period	9:00 a.m.–4:00 p.m.
Number of Study Sites	176
Average Size of Study Sites	136,000 sq. ft. GFA
Average Peak Period Parking Demand	2.84 vehicles per 1,000 sq. ft. GFA
Standard Deviation	0.73
Coefficient of Variation	26%
95% Confidence Interval	2.73–2.94 vehicles per 1,000 sq. ft. GFA
Range	0.86–5.58 vehicles per 1,000 sq. ft. GFA
85th Percentile	3.45 vehicles per 1,000 sq. ft. GFA
33rd Percentile	2.56 vehicles per 1,000 sq. ft. GFA



◆ Actual Data Points — Fitted Curve - - - - Average Rate

Land Use: 221

Low/Mid-Rise Apartment

Based on Vehicles per Dwelling Unit (Suburban)	Weekday	
Hour Beginning	Percent of Peak Period	Number of Data Points*
12:00–4:00 a.m.	100	14
5:00 a.m.	96	14
6:00 a.m.	92	14
7:00 a.m.	74	1
8:00 a.m.	64	1
9:00 a.m.	–	0
10:00 a.m.	–	0
11:00 a.m.	–	0
12:00 p.m.	–	0
1:00 p.m.	–	0
2:00 p.m.	–	0
3:00 p.m.	–	0
4:00 p.m.	44	1
5:00 p.m.	59	1
6:00 p.m.	69	1
7:00 p.m.	66	9
8:00 p.m.	75	9
9:00 p.m.	77	10
10:00 p.m.	92	14
11:00 p.m.	94	14

* Subset of database

Parking studies of apartments should attempt to obtain information on occupancy rate and on the mix of apartment sizes (in other words, number of bedrooms per apartment and number of units in the complex). Future parking studies should also indicate the number of levels contained in the apartment building.

Additional Data

- Apartment occupancy can affect parking demand ratio. In the United States, successful apartment complexes commonly have a vacancy rate between 5 and 10 percent.¹

Study Sites/Years

Canada:

Central City, Not Downtown:
Brooks, AB (1998)

Puerto Rico:

Central City, Not Downtown:
Mayaguez, PR (2007)

¹ Rental and Homeowner Vacancy Rates for the United States: 1960 and 1965 to 2009, U.S. Census Bureau. <http://www.census.gov/hhes/www/housing/hvs/qtr309/q309tab1.html>

Land Use: 701 Office Building

As noted, peak parking demand rates were different between sites located in suburban settings and those located in urban settings for the independent variable 1,000 sq. ft. GFA. The individual site surveys did not enable a quantitative explanation of the factors that caused the difference. One potential explanation may relate to differences in the availability of alternative modes (for example, transit, bike and pedestrian) available at the urban sites. Of the studies with data on transit availability and presence of a transportation demand management (TDM) program, the suburban sites reported about 55 percent with available transit services and 20 percent with TDM programs. The urban sites reported almost 100 percent with available transit and 63 percent with TDM programs of some form.

Weekend parking demand data were available at two study sites. At one site, the Saturday peak demand was less than 10 percent of peak weekday demand at the same site. At the other site, the Saturday and Sunday demand approached 90 percent of the weekday peak demand for the same site. It was not possible to derive reliable weekend parking demand rates due to lack of information on the nature of work conducted during the weekend at the two sites.

The following table presents the time-of-day distributions of parking demand variation for suburban and urban sites. The only sites included in the table data were those that submitted at least four consecutive hours of parking demand observations. (*Note: the majority of the parking demand data in the overall database consisted of one or two hourly observations.*)

Based on Vehicles per 1,000 sq. ft. GFA	Weekday Suburban		Weekday Urban	
	Percent of Peak Period	Number of Data Points*	Percent of Peak Period	Number of Data Points*
Hour Beginning				
12:00-4:00 a.m.	—	0	—	0
5:00 a.m.	—	0	—	0
6:00 a.m.	—	0	—	0
7:00 a.m.	59	1	19	2
8:00 a.m.	79	10	64	4
9:00 a.m.	95	12	91	5
10:00 a.m.	100	12	99	5
11:00 a.m.	98	12	99	5
12:00 p.m.	90	12	98	5
1:00 p.m.	77	7	96	5
2:00 p.m.	84	7	100	5
3:00 p.m.	81	6	99	5
4:00 p.m.	72	6	90	5
5:00 p.m.	46	6	58	3
6:00 p.m.	25	1	—	0
7:00 p.m.	—	0	—	0
8:00 p.m.	—	0	—	0
9:00 p.m.	—	0	—	0
10:00 p.m.	—	0	—	0
11:00 p.m.	—	0	—	0

* Subset of database

1889 York Street
Denver, CO 80206
(303) 333-1105
FAX (303) 333-1107
E-mail: lsc@lscdenver.com



March 13, 2014

Mr. Adrian Sopher
Sopher Architects, LLC
3008 Folsom Street
Boulder, CO 80304

Re: Washington Village
Traffic Study Supplemental
Memorandum
Boulder, CO
(LSC #130950)

Dear Mr. Sopher:

Per your request, we have completed this supplemental memorandum for the Washington Village development in Boulder, Colorado. The purpose of this memorandum is to compare trip generation from the currently proposed land use with the trip generation from the previously approved land use from the September 9, 2008 *Washington Village Traffic Impact Analysis (TIA)* by LSC. Figure 1 shows the vicinity map.

TRIP GENERATION

The currently proposed plan for the Washington Village development increases the residential portion from 27 condominium/townhomes to 30 condominium/townhomes and decreases the office square footage from 3,000 square feet to 2,700 square feet. The number of single-family detached homes remains the same. Table 1 shows the estimated trip generation potential from the 2008 LSC analysis (7th edition of the *ITE Trip Generation Manual*, 2003) as well as for the currently proposed land use based on the trip generation rates from the 9th edition of the *ITE Trip Generation Manual*, 2012. Table 1 shows the proposed change in land use is expected to generate eleven additional weekday trips, one additional morning peak-hour trip, and one less afternoon peak-hour trip than the land use approved in the previous TIA.

To be consistent with the previous TIA, an alternative travel modes reduction of five percent was assumed. Participation in the ECO Pass program and the site's location along Broadway Street, a strong transit corridor, will likely result in an alternate travel modes reduction of greater than five percent - perhaps 15 to 20 percent. For this reason, the estimated trip generation in Table 1 and Figure 3 should be considered a conservative estimate.

TRIP DISTRIBUTION AND ASSIGNMENT

Figure 2 shows the estimated directional distribution of site-generated traffic. Figure 3 shows the updated assignment of site-generated traffic.

PROJECTED LEVELS OF SERVICE

Table 2 from the previous TIA is attached and shows some movements operating at Level of Service "E" and/or "F" for future conditions. The primary reason for these poor levels of service is unsignalized control - the hourly traffic volume is very low for the movements expected to fail. The Broadway Street/Cedar Avenue intersection is unlikely to be signalized in the future because during the peak-hours drivers can use the local street grid to access Broadway Street via the signalized Balsam Avenue intersection one block south of Cedar Avenue. The change in the proposed land use will have a negligible effect on levels of service at the intersections in the vicinity of the site so no additional analysis is recommended.

SUMMARY

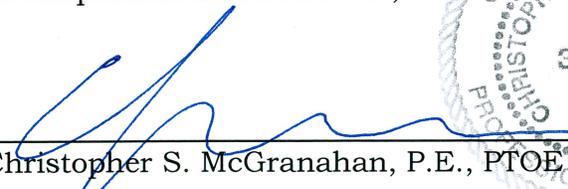
1. The minor change in land use from what was assumed in the 2008 TIA is expected to increase the daily trip generation by eleven trips and be negligible to the morning and afternoon peak-hour trips. This change in land use will have a negligible effect on levels of service so no additional analysis is recommended.
2. The unsignalized intersection of Broadway Street/Cedar Avenue is expected to operate poorly for the low volume side road approaches during the peak-hours due to heavy through traffic on Broadway Street. During peak-hours some drivers may choose to use the local street grid to alternatively access Broadway Street at Balsam Avenue.
3. The existing street network is adequate to accommodate the additional site traffic.

* * *

We trust this information will assist you in planning for the Washington Village development.

Respectfully submitted,

LSC Transportation Consultants, Inc.

By: 
Christopher S. McGranahan, P.E., PTOE



CSM/wc

3-13-14

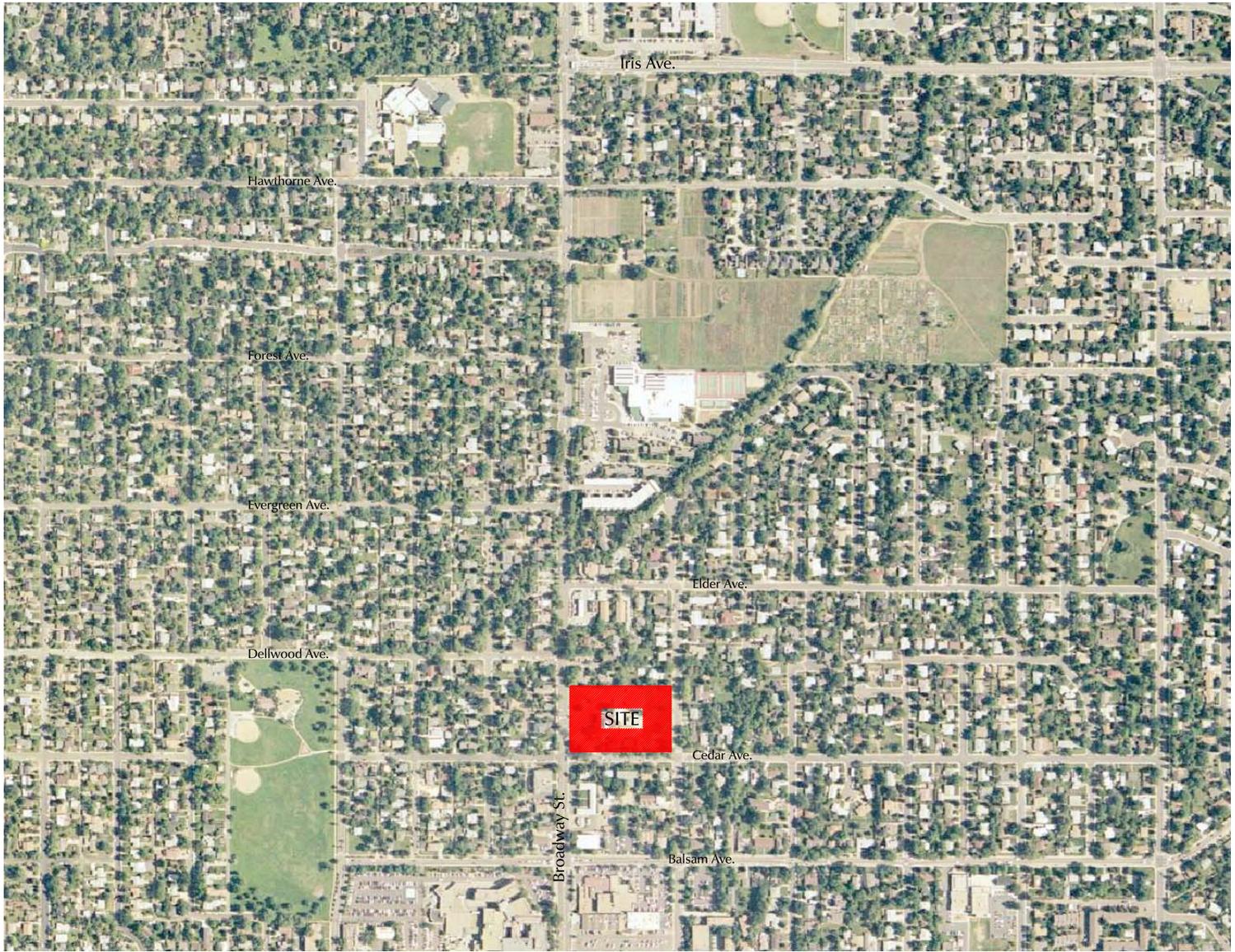
Enclosures: Table 1
Figures 1 through 3
Table 2 from the September 9, 2008 Washington Village (TIA) by LSC

**Table 1
Trip Generation Comparison
Washington Village
Boulder, Colorado
(LSC #130950; March, 2014)**

Trip Generating Category	Quantity	Trip Generation Rates ⁽¹⁾					Vehicle-Trips Generated					Transit/ Bicycle/ Walk Reduction	Non-Vehicle-Trips					Net External Trips				
		Average Weekday	AM Peak Hour In	PM Peak Hour Out	AM Peak Hour In	PM Peak Hour Out	Average Weekday	AM Peak Hour In	PM Peak - Hour Out	AM Peak Hour In	PM Peak - Hour Out		Average Weekday	AM Peak Hour In	PM Peak - Hour Out	AM Peak Hour In	PM Peak - Hour Out	Average Weekday	AM Peak Hour In	PM Peak - Hour Out	AM Peak Hour In	PM Peak - Hour Out
Previously Approved Land Use (Washington Village TIA, September 9, 2008 by LSC)																						
Single-Family ⁽²⁾	6 DU ⁽³⁾	9.57	0.19	0.56	0.64	0.37	57	1	3	4	2	5%	3	0	0	0	0	54	1	3	4	2
Multi-Family ⁽⁴⁾	27 DU	5.86	0.07	0.37	0.35	0.17	158	2	10	9	5	5%	8	0	1	0	0	150	2	9	9	5
General Office ⁽⁵⁾	3 KSF ⁽⁶⁾	16.52	2.04	0.29	0.38	1.86	50	6	1	1	6	5%	3	0	0	0	0	47	6	1	1	6
Total							265	9	14	14	13		14	0	1	0	0	251	9	13	14	13
Currently Proposed Land Use																						
Single-Family	6 DU	9.52	0.19	0.56	0.63	0.37	57	1	3	4	2	5%	3	0	0	0	0	54	1	3	4	2
Multi-Family	30 DU	5.81	0.07	0.37	0.35	0.17	174	2	11	10	5	5%	9	0	1	1	0	165	2	10	9	5
General Office	2.7 KSF	16.55	2.06	0.28	0.38	1.86	45	6	1	1	5	5%	2	0	0	0	0	43	6	1	1	5
Total							276	9	15	15	12		14	0	1	1	0	262	9	14	14	12
Difference							11	0	1	1	-1		0	0	0	1	0	11	0	1	0	-1

Notes:

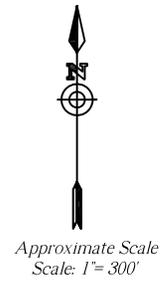
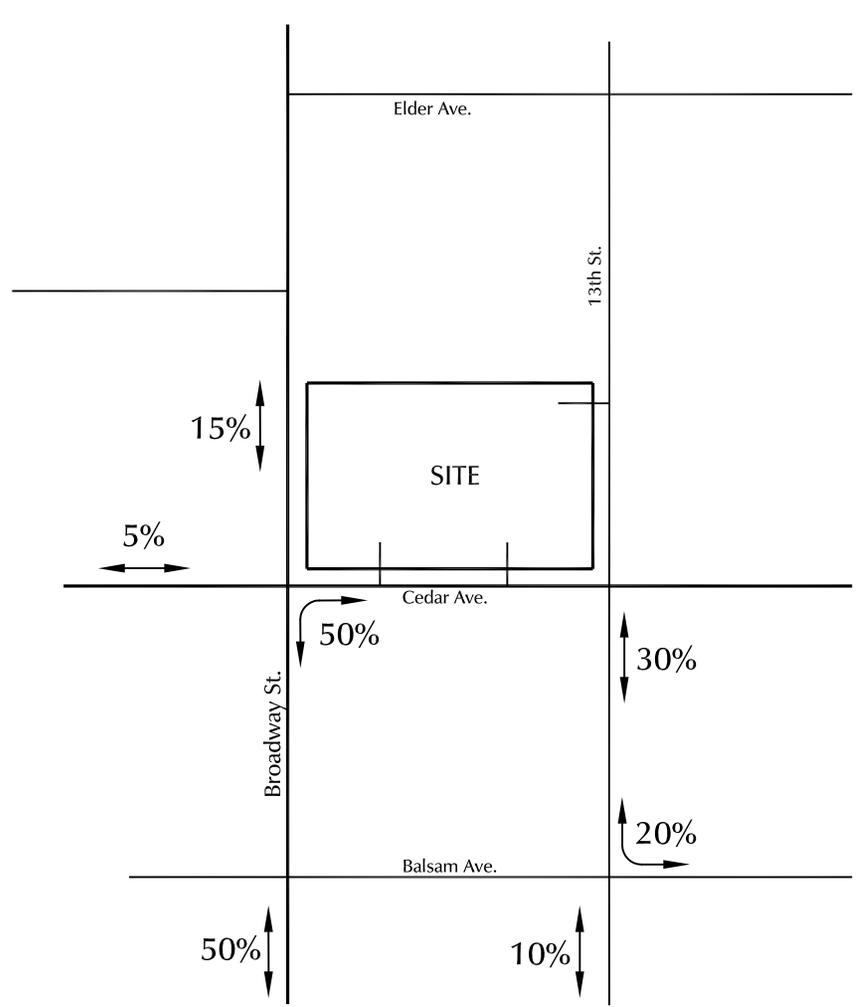
- (1) Source: *Trip Generation*, Institute of Transportation Engineers, 7th Edition, 2003 (used in 2008 TIA) and *Trip Generation*, ITE, 9th Edition, 2012.
- (2) Land Use No.210, Single Family Detached Housing
- (3) Dwelling Units
- (4) Land Use No. 230, Residential Condominium/Townhomes
- (5) Land Use No. 710, General Office (Increased average ITE rates by 50% due to relatively small floor area)
- (6) KSF = 1,000 square feet



Approximate Scale
Scale: 1" = 700'

Figure 1
**Vicinity
Map**

Washington Village (LSC #130950)

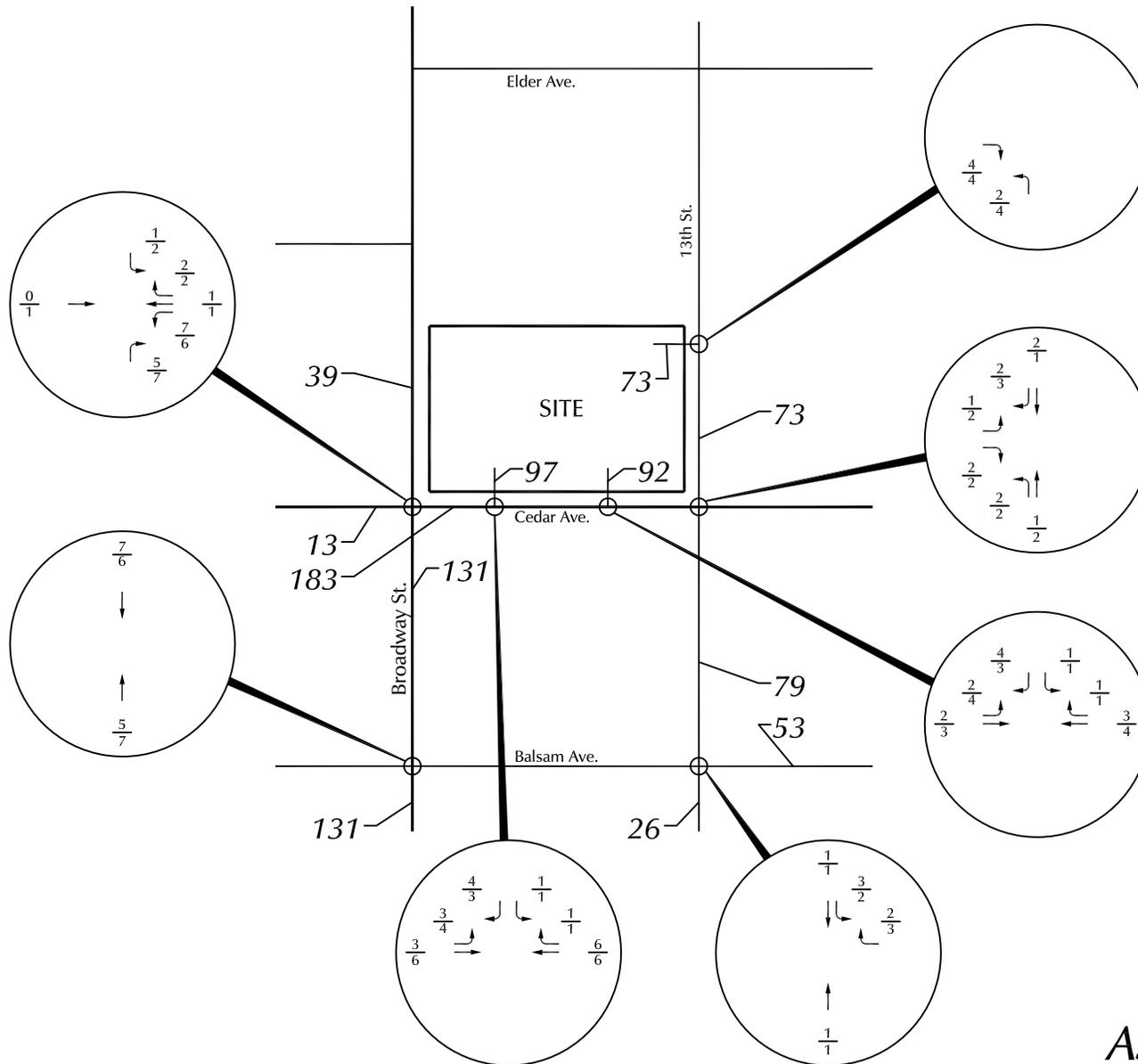


LEGEND:

← 65% → = Percent Directional Distribution

Figure 2
Directional Distribution of Site-Generated Traffic

Washington Village (LSC #130950)



Approximate Scale
Scale: 1" = 300'

Figure 3
**Assignment of
Site-Generated Traffic**
Washington Village (LSC #130950)

LEGEND:
 $\frac{26}{35}$ = AM Peak Hour Traffic
 $\frac{35}{35}$ = PM Peak Hour Traffic
 400 = Average Daily Traffic

**Table 2
Intersection Level of Service
Washington Village
Boulder, Colorado
(LSC #061711; September, 2008)**

Traffic Control	Intersection Location	Year 2010		Year 2010		Year 2030		Year 2030	
		Background Traffic		Background plus Site-Generated Traffic		Background Traffic		Background plus Site-Generated Traffic	
		Level of Service	Level of Service	Level of Service	Level of Service	Level of Service	Level of Service	Level of Service	Level of Service
		AM	PM	AM	PM	AM	PM	AM	PM
Unsignalized	<u>Broadway/Cedar Avenue</u>								
	Eastbound Approach	B	B	B	E	C	B	C	F
	Westbound Approach	C	C	E	D	D	E	E	F
	Northbound Left	A	A	A	A	A	A	A	A
	Southbound Left	A	A	A	A	A	A	A	A
	Critical Approach Delay(sec /veh)	21.6	17.0	35.9	41.3	25.5	39.3	41.6	>100
Unsignalized	<u>Cedar Avenue/West Site Access</u>								
	Eastbound Approach	-	-	A	A	-	-	A	A
	Westbound Approach	-	-	A	A	-	-	A	A
	Southbound Approach	-	-	A	A	-	-	A	A
	Critical Approach Delay(sec /veh)	-	-	8.7	8.7	-	-	8.7	8.8
Unsignalized	<u>Cedar Avenue/East Site Access</u>								
	Eastbound Approach	-	-	A	A	-	-	A	A
	Westbound Approach	-	-	A	A	-	-	A	A
	Southbound Approach	-	-	A	A	-	-	A	A
	Critical Approach Delay(sec /veh)	-	-	8.6	8.7	-	-	8.7	8.7
Signalized	<u>Broadway/Balsam Avenue</u>								
	Eastbound Left	C	C	C	C	C	D	C	D
	Eastbound Shared Through/Right	C	C	C	C	C	C	C	C
	Westbound Left	C	C	C	C	C	C	C	C
	Westbound Shared Through/Right	C	C	C	C	D	D	D	D
	Northbound Left	C	C	C	C	B	C	B	C
	Northbound Shared Through/Right	B	C	B	C	B	C	B	C
	Southbound Left	B	D	B	D	B	C	B	C
	Southbound Shared Through/Right	C	C	C	C	C	C	C	C
	Average Intersection Delay (sec/veh)	21.5	28.5	21.5	28.6	21.3	32.3	21.3	32.5
	Entire Intersection Level of Service	C	C	C	C	C	C	C	C
Unsignalized	<u>Balsam Avenue/13th Street</u>								
	Eastbound Approach	-	B	-	B	-	C	-	C
	Westbound Approach	-	C	-	C	-	C	-	C
	Northbound Approach	-	B	-	B	-	B	-	B
	Southbound Approach	-	A	-	B	-	B	-	B
	Critical Approach Delay(sec /veh)	-	15.0	-	15.4	-	18.3	-	19.1
Unsignalized	<u>Cedar Avenue/13th Street</u>								
	Eastbound Approach	A	A	A	A	A	A	A	A
	Westbound Approach	A	A	A	A	A	A	A	A
	Northbound Approach	A	A	A	A	A	A	A	A
	Southbound Approach	A	A	A	A	A	A	A	A
	Critical Approach Delay(sec /veh)	7.3	7.4	7.3	7.5	7.4	7.5	7.4	7.6
Unsignalized	<u>13th Street/Site Access</u>								
	Eastbound Approach	-	-	A	A	-	-	A	A
	Northbound Approach	-	-	A	A	-	-	A	A
	Southbound Approach	-	-	A	A	-	-	A	A
	Critical Approach Delay(sec /veh)	-	-	8.5	8.5	-	-	8.5	8.6

1-20-14

CITY OF BOULDER
 PLANNING & DEVELOPMENT SERVICES

RE: PROPOSED AMENDMENT TO WASHINGTON VILLAGE PROJECT

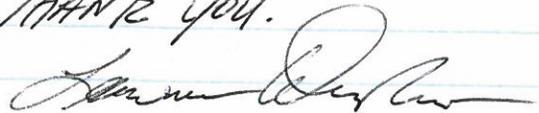
ATTN: KARL GUILER

I AM WRITING TO EXPRESS MY OPPOSITION TO INCREASING THE NUMBER OF DWELLING UNITS IN THE WASHINGTON VILLAGE PROJECT. WE ALL WENT THROUGH RATHER EXHAUSTIVE DISCUSSION & NEGOTIATION RESULTING IN WHAT IS CURRENTLY APPROVED FOR THE SITE. CONCERNS OVER DENSITY AND PARKING WERE AT THE CENTER OF SUCH DISCUSSIONS.

IT SHOULD BE CLEAR THAT PARKING ON CEDAR AVE. & 13TH ST. IS ALREADY HEAVY. UNLESS WASHINGTON VILLAGE CAN ADD AT LEAST THREE ADDITIONAL PARKING SPACES TO ITS OFF STREET PARKING, THE IMPENDING PARKING PROBLEM WOULD ONLY BE WORSENERED FOR ITS RESIDENTS & NEIGHBORS BY SUCH AN AMENDMENT.

AT OUR HOUSE, WE ALREADY DEAL WITH PEOPLE ALL TOO OFTEN PARKING IN FRONT OF OUR DRIVEWAY. WASHINGTON VILLAGE IS ENTITLED TO PARKING ALONG THE STREET CURB OF ITS OWN PERIMETER, BUT I WANT THAT ACCESS AT MY OWN HOUSE PRESERVED AS WELL FOR OUR FAMILY & VISITORS.

THANK YOU.



LAWRENCE DUCKRO

2920 13TH ST. Agenda Item 50 Page 89 of 103
 BOULDER, CO 80304-3102

Guiler, Karl

From: Stan Kyed [stankyed@gmail.com]
Sent: Wednesday, January 22, 2014 5:00 PM
To: Guiler, Karl
Cc: Holly Kyed; John Kyed
Subject: Washington Village Site Review

Follow Up Flag: Follow up
Flag Status: Flagged

Karl:

I hope this note finds you well. I suddenly realized today is the deadline for comments on the Washington Village Site review comments that will be considered by staff. Though late in the day on the 22nd (the deadline), I hope they will be included.

To: Planning Board and City Staff

From: Stan, Holly, and John Kyed, 2945 13th St. Boulder, 80304

I am writing on behalf of my family about the Washington Village Site Review Amendment. We own the property on the north side of the site, and along with the Red Arrow Apartments are among the most impacted by the development. I have been involved with the development process since the original community review panel some years ago and am quite familiar with all the issues. As I write this I am looking out my window at a quite dense, ugly site that doesn't really fit the neighborhood. Might as well be living in New Jersey.

Despite that and the horrible impact the slow and invasive construction has had on our family, I am not necessarily opposed to the request by the developer, subject to the concerns I outline below:

- 1. History of Project:** We as a community arrived at the current site plan through a long and painful process. This project has a perhaps unprecedented history. I am very concerned that after council made a decision concerning the site, and after all the community activism. that we are again at the site review level. No one is happy with the outcome, but we reached this after a lengthy process. Many of us in the neighborhood are resigned to what will happen. Why reopen old wounds? That said, I am pleased that Karl Guiller continues as the case manager and trust him to educate city staff and the Planning Board on the history and significance of this project. Then again, I really don't see why we are going into this again because council gave their mandate and we have all resigned to live with it.
- 2. Community Benefit:** This project concept centered around trade-offs concerning community benefit. I never saw the benefit of multi-level live/work housing that was proposed for the Broadway building. It was not a fit for the neighborhood and promised to bring more traffic and parking issues. If, as I understand, the proposal creates "flat" apartments, this will help attract the aging population that doesn't really want to climb the two or three flights of stairs the rest of the development is offering. Even that is a small, small, benefit compared to what was originally envisioned. Maybe one of the three units should be affordable? The developer will bring up the benefits of co-housing and sustainable living, and of course the alleged park at 13th and Cedar, which the city seems reluctant to accept.
- 3. Red Arrow Apartments:** The developer seems to be saying that the reconfiguration of the Broadway building will be of benefit to the Red Arrow. I need to understand that and I think we need to hear clearly from

the residents. Many have moved on and may not understand, The discussion needs to go beyond the solar ordinance for me to be convinced.

4. **PARKING:** If we add 3 units we need to add parking. No discussion about how the parking ordinance is unreasonable and intended for the Hill.

5. **Conflict of Interest:** Adrian Sopher, former Planning Board member is listed as the applicant. He is a professional with an impressive history and has been an important public servant. I do hope Planning Board will fully disclose relationships and conflicts of interest. Mr. Sopher has the right to come forth with the request--the board needs to just be transparent, as I know they will.

6. **Pace of Construction:** Another site review provides another opportunity for delaying construction, once again. Let's get it done folks. This has gone on long enough. My household calls it the "two board a day construction company." Can't we increase the pace a bit? And get the promised fence in sooner than later (the fence on our property line was part of council's condition for approval). This thing has gone on long enough.

7. **Don't Make a Decision Until You Visit:** The roof on the first duplex went up the other day meaning we can no longer see the Flagstaff Star. Once the other duplex is finished we will have no view of the historic school building. I encourage policy makers to come walk the actual constructed site and see what has been approved. Then, ask yourself can we handle three more units? It won't impact the view, but it will add one more impact to our already torn neighborhood. As I previously noted it feels like Jersey!

I'll close with that. As I've said, I can probably live with the proposal, though I'm not happy. I just don't like trying to tweak what council approved and what we all worked for.

Sincerely,

Stan, Holly, and John Kyed
2945 13th St
Boulder, 80304

Guiler, Karl

From: Kathy Icenogle [kathy.icenogle@gmail.com]
Sent: Wednesday, January 29, 2014 5:41 PM
To: boulderplanningboard
Cc: Guiler, Karl; 'Jim Leach'; asopher@sopherarchitects.com; 'Kathy Icenogle'
Subject: In SUPPORT of WHDC's proposed amendment to Washington Village plan for Broadway Building

Dear Planning Board,

My husband and I were the first residents of Washington Village at 2905 13th Street. I am writing to express our full support for the amendment that Wonderland Hill Dev. Corp. (WHDC) has proposed for the Broadway Building in Washington Village cohousing development.

We have enjoyed living in the home that Jim Leach built for us for over a year now, and have been getting to know many of the neighbors, most of whom express a great deal of appreciation for the project. We are blessed to live in our new home, and are very much looking forward to having the other members of Washington Village move into their new homes, so we can start doing the things we have dreamed about doing as a community – the sooner, the better.

Positive Impact on Washington Village Cohousing Community

- The proposed amendment will allow WHDC to sell some of the market rate units at a lower price, which opens the community up to a broader spectrum of the market. There seem to be a lot more people who want a more urban, less car-dependent lifestyle who can afford something in the \$700,000's who would not consider a unit in the \$800,000 range. We value diversity, and this proposed amendment would help us get a little more economic diversity within the community.
- Everyone wants to see the project done ASAP. We understand that a certain number of units need to be pre-sold before Wonderland can move forward on that construction. Many of us, particularly those who have had an opportunity to talk with prospective buyers, believe that changing the configuration of units to provide main level master bedrooms in all units, along with the lower price on the middle units will make it easier to sell the remaining units in the Broadway. So, approving this amendment will help the project get done sooner.
- We are pleased to see that the building design retains its aesthetic resemblance to the School Building roofline, while almost eliminating the shadow it casts on our neighbors in the Red Arrow complex. We have friends in that complex and appreciate the benefit of the lower building height for them.

Parking

I am aware of the concerns that have been raised regarding the impact that the development will have on parking in the neighborhood. After a year living on the corner of Cedar and 13th, I have had an opportunity to study the neighborhood parking situation. I am offering the following thoughts about the unique aspects of this development and our location for the Board to consider in determining how much parking this development really needs.

- **People who live in WV don't need a lot of cars.** A huge part of what attracted my husband and I to Washington Village – and a huge selling point of the WV location in general - is the fact that we do not need a car to get around. We can walk, or ride our bikes on Boulder's many bike-friendly paths and streets, or take the bus to most of the places we need to get to. Community members are even further encouraged to reduce our dependence on cars by the fact that the HOA provides us with EcoPasses. We don't need a second car, and put very few miles on the one car we own.
- Even without being co-resident, many of our WV community members already offer to provide transportation or carpool with other community members when we can. Sharing rides will be even easier when we live in the same location. **The supportive nature of our community further reduces the dependency of community members on having cars.**

- The Broadway and North Building units are relatively distant from the available street parking. Part of the reason that Washington Village residents are so concerned about having their parking needs met within the property is because **WV residents don't want to park out on the street.**
- **The parking provisions in the amended plan should be adequate to meet the needs of WV residents.** With the adjustments that have been made to the plans, the proposed amendment to the Broadway plan provides at least one parking space for each of the 20 units that do not have a private garage, with 3 additional spaces that can be assigned to residents who have a second car, as well as the three deferred spaces that could still be used, if needed. That leaves 14 units that have only a single parking space. Currently, 13 of the 19 units that have been sold or reserved will be occupied by owners who have only one car, which suggests that the likelihood of having a high percentage of Washington Village unit owners who only need one parking space (now and in the future) is pretty high.
- **I question how serious the street parking problem is.** In that time, my friends and I have always been able to find parking along Cedar or 13th Street whenever we've needed it, even with half the spaces taken up by construction workers. I find it hard to believe that, when the construction is done, the impact of WV residents on street parking in this neighborhood will be any more than the current impact of the construction workers, which is not that much of a problem. In my experience, sufficient street parking IS available.
- **Residents are not the source of parking congestion.** Most of the residents in this neighborhood have private parking spaces available to them in driveways or garages. The businesses in the area provide parking for their customers. The street parking **IS** heavily utilized - particularly during weekdays. However, other than the construction workers (who won't be here after the project is done), most of the people I see parking on Cedar and 13th Street are "regulars" who commute here from somewhere else and use our streets for parking while they are at work. I believe **the availability of "free parking" in this area for downtown workers puts a lot more pressure on street parking availability** than the residents of Washington Village ever will. If some WV residents do end up using some parking space on the streets, I would say that people who actually live (and pay property taxes) on the high property values in this area should have at least as much right to use the available street parking as non-residents.
- I understand that, technically, the number of rooms counted as "bedrooms" determines parking requirements. Many of the units in WV have rooms marked as "study" that are counted as bedrooms because someone **could** use them as a bedroom. But you need to consider the likelihood of that happening. Just because a room could be used as a bedroom doesn't mean it ever will. In this development, at this price point, in this location, **those "bonus" rooms are way more likely to be used as they are labeled on the plans, than as bedrooms.** Many of our prospective buyers require an home office, because they are either retired and hang out on their computers a lot, or they are professionals who want to work at home instead of commuting, or they want do part-time jobs at home for businesses that do not provide office space. As a Planning Board, if you want to advocate for a live/work non-commuting lifestyle, I encourage you to give more credence to a "study" being as much of a necessity in a modern home as a dining room, and factor that in your calculation of parking requirements.
- The four "Elm" townhomes, which expect are likely to attract families or dual-income couples, have two car garages dedicated to those units.
- **WV units are not likely to get the kind of high-density occupancy that you typically associate with a 3-4 bedroom home.** The development has been attracting primarily individuals, couples, or small families. So far, we have 11 units with one occupant, 7 with two, and 1 family of three. Given today's college costs, there are very few large families who are willing or able to invest in homes at this price point. **Our bedrooms are also not likely to get filled up with renters,** as has become the case for many of the "single-family" units in the area, because cohousing owners generally occupy their unit as their primary residence, so they can fully participate in the community. If Washington Village were to abandon its charter as a cohousing community, these units might get higher density occupancy as rental units, but I encourage the Board to bet on us being a successful cohousing community well into the future.

Thank you for your time and attention to our project,

Kathy Icenogle

Guiler, Karl

From: Kathy Icenogle [kathy.icenogle@gmail.com]
Sent: Wednesday, January 29, 2014 5:43 PM
To: boulderplanningboard
Cc: Guiler, Karl; Jim Leach; asopher@sopherarchitects.com; Kathy Icenogle
Subject: Rebuttle to Barbara Turner's comments re: Washington Village Amendment

Dear Planning Board

My name is Kathy Icenogle. My husband and I have been members of the Washington Village Cohousing Community since 2011.

I read the letter that Barbara Turner wrote to the Board (attached below for reference) and would like to respond to a number of her comments. I recognize her right to oppose the amendment, but I am very concerned about the many misrepresentations of the facts that Barbara used in voicing her opposition. I would not want to have the proposed amendment questioned or denied on the basis of Barbara's misinformed claims.

Barbara has chosen to be absent from most of our community meetings, which would have allowed her to be better informed. (She doesn't drive at night, but has rides available to her.) She chooses not to communicate well and directly with individuals and Wonderland to get her concerns clarified. If she had chosen to express her concerns to Wonderland, or to another community member, those concerns might have been addressed in a more constructive manner. Unfortunately, it seems that Barbara is often unwilling to accept information that does not support what she wants to believe. She often speaks and acts primarily on behalf of her individual needs, and rarely shows any consideration for what benefits the community at large.

1. "Jim Leach will not guarantee me a parking place in the Broadway garage." Jim has publicly stated on many occasions that every resident will have a parking space on the property, and the plans provide evidence that he can deliver on that promise. Barbara has a unit reserved in the Broadway Building, and as she said, she is 80 years old. There is no reason for Barbara to think that the space assigned to her unit would not be in the Broadway garage. She has expressed concern about her parking space before and she has been assured of a parking space. Therefore, her anxiety about being "relegated to parking on the street" is unfounded. We unfortunately cannot convince her of that.
2. "... requesting an additional 3 units in the Broadway Building **with no additional provision for parking.**" I believe Jim ***has*** made the necessary adjustments in the Site Plan to provide additional parking spaces for three additional residential units. When Jim presented his proposed amendment for input at a Community Meeting, he explained that the proposal reduces the size of the commercial space to allow two of the commercial spaces in the Broadway garage to be assigned to residential units. He also eliminated the third trash enclosure to provide an additional space off the common driveway. (I did some analysis on our trash requirements and determined that we don't need the third trash enclosure, so it is reasonable to use that space for parking.) All of the other spaces that were called out for residential parking in the original plan are still in the amended plan.
3. "Members of the community are actively engaged in discussing putting a commercial kitchen in the community space in the Broadway Building to be rented out..." The idea of renting the kitchen/dining space came up as a possible means of bringing revenue into the community. (We are looking for alternatives to special assessments for funding improvements the community might want to pursue after the project is done.) It is nothing more than that - an **idea** proposed by a community member, which some of us were choosing to research so we could give it more informed consideration at our common space workshop. (In our normal process, research would have led us to the RH-2 restriction that Karl Guiler pointed to, and we have also learned that it would require significant architectural changes that are not reasonable to pursue.)

After seeing a reference to the kitchen rental idea in an email, Barbara reacted, saying: "My bedroom wall is on the other side of the kitchen and I don't want groups renting the kitchen. I oppose the concept and will do everything in my power to prevent it." Members assured her it was simply an idea people were looking into, so

it could be discussed by the whole community at a workshop we are planning. Barbara apparently has no patience for our community decision making process. Two days after learning about the idea, Barbara chose to raise the fact that renting the kitchen was up for discussion in her letter to the Planning Board, as a reason for opposing the addition of 3 units to the project. It doesn't even have any relevance to the changes being proposed.

4. "...to make money to finish the common areas in the school building." Barbara has confused the kitchen discussion with a different issue. There has been no talk of renting the kitchen to pay for any part of the development. Also, the School Building will be finished long before ground will even be broken on the Broadway. So the idea of using kitchen rental income to pay for any part of the School Building project would not make sense. (Again, if Barbara participated more fully, she would be more tuned in to our discussions.)
5. "Jim Leach says he has run out of money to finish those areas." Jim has said no such thing. What Jim ***has*** told us is that the budget he originally allocated for finishing the School Building common space is tight, but he assured the Community **in writing** that "...even if the costs run over the allowance we intend to finish the common areas appropriately and consistent with the quality of the living units. It is important to all of us to have the school building looking complete and attractive when it is finished, and we will find a way to make sure that happens."
6. "Yet he has been taking a lavish draw for over 5 years. His original requirement of the community was for \$20,000 per month. He raised private funds to pay for that draw, but those borrowed funds will have to be repaid. He says the bank has cut him back to \$6500 per month..." The only thing accurate in this collection of statements is the fact that Wonderland is drawing \$6500/month from the School Building budget for project management and marketing expenses, and that Jim is working with some private investors to supplement the bank loans he needs to get the project built. Not a penny of project funding is going into Jim Leach's pocket. Barbara's claim that Jim has been "taking a lavish draw" from the project is just the vindictive voice of someone who is impatient for her unit to get built, but has done nothing but cause problems within the community and impede the progress of the project with threats of law suits and making ridiculous demands that Jim has been good enough to try and accommodate.
7. "...we have no way to verify that figure until he opens his books to the community when 75% of the units are sold." Barbara is confusing the HOA accounts (which we are reviewing monthly and expect to have transitioned to the community when the project is 75-80% occupied) with the project construction accounts. The construction accounts are managed by a professional and very competent accountant who has no vested interest in Wonderland's profits. The accounts are regularly overseen by Mile High Bank, as the primary investor. We have no reason to think that the Washington Village project finances are being handled in anything but an appropriate and ethical manner. Therefore, there is no reason to consume their time and energy by demanding that Jim "open his books" for members of the community to review. Barbara is the only one issuing such a request. I believe it is in community's best interests to allow Wonderland to focus on getting the project done.

As the Board is well aware, circumstances beyond Wonderland's control have dragged the Washington Village project out. Many of the early investors would have been left with nothing if it wasn't for Jim Leach's experience, integrity, hard work, and diligence in keeping the best interests of the community in mind. One might think that Barbara, as one of those early investors, might be more appreciative of that fact. In my opinion, Barbara's interests would be better served if the Board were to approve the amendment, because it will help facilitate the sales that are needed to get financing for the building that Barbara is so anxious to move into.

Regards,
Kathy Icenogle
2905 13th Street, Boulder

From: "BARBARA-TURNER" <barbara-turner@comcast.net>
To: "Boulder Planning" <Board@bouldercolorado.gov>
Sent: Sunday, January 26, 2014 12:16:48 PM
Subject: Opposition to application of Jim Leach to add three units at Washington Village

Guiler, Karl

From: Linda Spiegler [spiegler@colorado.edu]
Sent: Wednesday, January 29, 2014 9:46 PM
To: boulderplanningboard
Cc: Leach, Jim; Guiler, Karl
Subject: Re: Washington Village Amendment

I am a member of the Washington Village cohousing community and have been since 2008. I will be moving into the Broadway Building when it is constructed, and am eagerly looking forward to that time.

Very simply, I am writing in support of both of Kathy Icenogle's letters of January 29, 2014 to the Planning Board. I believe it is important for you to know that other members of our community share the views Kathy has expressed. None of us in Washington Village expects a perfect process, or complete agreement among our members. But I can tell you that the spirit of Washington Village is positive and constructive. Naturally it is distressing when one member (in this case Barbara Turner, who has also written to you) misrepresents the facts, creating obstacles rather than bridges to understanding and moving forward.

I hope you will focus on facts and help Washington Village move quickly toward completion with the carefully considered amendment submitted to the Planning Board.

Sincerely,
Linda Spiegler
P.O. Box 4211
Boulder, CO 80306
<http://www.lcsarts.com>

"Compassion is the foundation of everything positive, everything good. If you carry the power of compassion to the marketplace [buy cruelty-free products] and the dinner table [eat a plant-based diet], you can make your life really count." Rue McClanahan

Guiler, Karl

From: Joan Brody [joanbrody@hotmail.com]
Sent: Saturday, February 22, 2014 4:37 PM
To: Guiler, Karl; phyllis savage; Joan Brody
Subject: IMPORTANT MESSAGE FROM JOAN BRODY AT MEETING THIS MONDAY

Follow Up Flag: Follow up
Flag Status: Flagged

To Karl,

Will you please speak for me at Washington School meeting this coming Monday evening? I will not be able to attend this meeting. I would appreciate if you would read the following into the record at the meeting:

I live at Unit 9, in the West building of Red Arrow Condominiums which I have owned since 2006, and therefore, I am directly impacted by the Washington School Development.

I am a retired, deaf senior living on a very limited income. As you know, the Red Arrow apartments have very large windows. In order to conserve energy, I have invested substantial money to replace my gas heater and to install triple pane windows throughout my apartment. Nevertheless, I am heavily dependent on solar energy to light and heat my apartment, especially during winter months. I am very concerned about the impact of the Washington Hill Development on my home and safety.

WASHINGTON HILL DEVELOPMENT HAS A MUCH LARGER SHADOW IMPACT ON RED ARROW CONDOMINIUMS THAN IS SHOWN ON WONDERLAND'S PRE-CONSTRUCTION DIAGRAMS : The shadow lines shown on the solar impact diagrams previously submitted to the City by Wonderland Hill Development are not credible as the buildings already constructed clearly demonstrate. This is not surprising as the developer has previously made faulty calculations in its favor which I pointed out to the City many times in the past. I would encourage representatives of the City to visit the site any sunny day at noon to witness that the shadow lines thrown by the already constructed building far exceed those shown on the diagrams previously submitted by Wonderland Hill to the City. It is apparent from the preconstruction diagrams that my home, Unit 9, is the most negatively impacted of all the units at Red Arrow, particularly by the proposed West Building along Broadway. While I now receive wonderful light and warmth through my windows. I will be thrust into cold and darkness throughout the afternoon hours in winter months if the West Building of Washington Village is permitted to be constructed in accordance with the plans. It is not clear from the plans what the impact would be in

other seasons throughout the year. I urge the City to require sufficient changes to the West Building to avoid this outcome.

WASHINGTON HILL DEVELOPMENT SHOULD NOT BE ALLOWED TO ADD ADDITIONAL UNITS AND RESIDENTIAL SPACE IN THE WEST BUILDING: Wonderland Development has come to the City multiple times to request additional development rights. Already Washington Village is crowded with high density units. This will have a negative impact on the quality of life and safety of Red Arrow residents. I urge to City not to approve this additional change to the plans for the West Building at Washington Hill Development.

WONDERLAND HILL DEVELOPMENT SHOULD BE REQUIRED TO CONSTRUCT A FENCE ALONG THE NORTH BORDER OF THEIR PROPERTY ADJACENT TO RED ARROW CONDOMINIUMS: The Washington Hill Development will cause a great deal of foot traffic along the border with Red Arrow. This will negatively impact the privacy and safety of Red Arrow residents. I understand the developer previously made verbal commitments to City representatives that they would construct a privacy/security fence along the border of the two properties. However that commitment was never formalized. I urge the City to require Wonderland Hill Development to construct and maintain, at its expense, such a permanent fence of materials and height that will provide privacy and security.

I, therefore, sincerely request that the City of Boulder deny approval for the plans of Wonderland Hill Development for Washington Village, unless and until the negative impact on my home can be eliminated by sufficiently lower the height of the the West (Broadway) Building and moving it further south, by denying the request of Wonderland Hill Development to add additional units to the West Building and requiring the construction and maintenance of a privacy/security fence.

JOAN BRODY - RED ARROW UNIT 9

Guiler, Karl

From: Fred Rubin [sail4sun@msn.com]
Sent: Wednesday, February 26, 2014 3:57 PM
To: Guiler, Karl
Subject: Washington Village

Follow Up Flag: Follow up
Flag Status: Flagged

Hi Karl,

Setting aside that one of the originally promised "community benefits" of Washington Village (senior housing as you may recall) has fallen by the wayside, I have two major questions about the current amendment request.

1. I would like to see a more detailed parking analysis of the project. It seems like they already received a reduction in the requirement, and I would be opposed to any additional reduction without "proof", not wishful thinking, that the residents will be using fewer vehicles.

As I have seen on the "Hill", these reductions are being handed out like Halloween candy, but no one ever follows up so see if the reductions were warranted, and there is NO enforcement of proposed parking mitigation plans after a project is completed.

As the effects of an insufficient amount of parking will take their toll on the neighborhood, I believe that we should see hard evidence that it is justified, not just the usual patten about how the residents will be expected to use the bus, bicycle, and walking.

2. I am very concerned that there seems to be no real time limit on this project being completed. I have read 9-2-11 and 12, (assuming those are the relevant code sections) and it is not at all clear to me that these extensions are justified per those requirements. Is another extension being requested at this time? If so, I think if the time limit that the code imposes is to mean anything, this needs to be the last one. In response to this question, I would like to see a timeline since the project was approved showing the various extensions and the reasons for them.

Thank you.

Fred Rubin

"For were the impulses of conscience clear, uniform and irresistibly obeyed, man would need no other law-giver; but that not being the case, he finds it necessary to surrender up a part of his property to furnish means for the protection of the rest; and this he is induced to do by the same prudence which in every other case advises him, out of two evils to choose the least." Thomas Paine

It will be of little avail to the people that the laws are made by men of their own choice if the laws be so voluminous that they cannot be read, or so incoherent that they cannot be understood; if they be repealed or revised before they are promulgated, or

undergo such incessant changes that no man, who knows what the law is today, can guess what it will be tomorrow. James Madison

Guiler, Karl

From: Jane Blair [divamomjane@gmail.com]
Sent: Monday, March 10, 2014 5:08 PM
To: Guiler, Karl
Subject: Re: Public Notice for Washington Village

dear karl,

thanks so much for this update. to be honest, this is probably the 10th notice we have received about how the washington village developer wants to "change" his previously approved plans. as a resident in the neighborhood, i am not amused. we already stated our concerns about it being too dense, creating too much traffic, etc...throughout the several years that the design was being created, and then approved....now, we still have to continuously be involved or they are going to undo all those negotiations! i would appreciate it if you could pass this letter on to anyone who might be able to stop this madness. it is really quite unreasonable for them to take up so much of everyone's time. as a parent, it reminds me of how my kids sometime act when they want something unreasonable....and i have to hold a firm boundary so they do not run amok. in this instance, the city needs to hold a firm boundary with the developer, so that the citizens' needs are met.

again, i thank you for considering my opinion,

jane blair
2975 jefferson st.
boulder, co 80304

On Mon, Mar 10, 2014 at 3:17 PM, Guiler, Karl <GuilerK@bouldercolorado.gov> wrote:

Please see the attached notice regarding the Washington Village project. Let me know if you have any questions.

Best,

Karl

Karl Guiler, AICP

Senior Planner/Code Amendment Specialist

City of Boulder Department of Community Planning & Sustainability

Planning & Development Services Center

1739 Broadway, 3rd Floor

Boulder, CO 80306-0791

Phone: 303.441.4236

Fax: 303.441.3241

Email: guilerk@bouldercolorado.gov

Guiler, Karl

From: Tommy Lorden [tlorden@gmail.com]
Sent: Wednesday, March 12, 2014 3:51 PM
To: Guiler, Karl
Subject: Re: Public Notice for Washington Village

Thanks, Karl!

We will try to attend, and as a resident with four kids...all of us whom are walking or riding on 13th St every day, we would strongly object to any additional cars using that area. As you know, it's already pretty crazy with employees of the hospital parking there, and being that it's a designated bike route (without any striping)....it can be touch and go through there at times now.

The rules were presumably developed for good reason, and we would just object to any variance that had the affect of putting any more traffic and/or parking onto 13th St.

Thanks!

Tommy & Katie Lorden
3015 Jefferson St
Boulder, CO

On Mon, Mar 10, 2014 at 3:17 PM, Guiler, Karl <GuilerK@bouldercolorado.gov> wrote:

Please see the attached notice regarding the Washington Village project. Let me know if you have any questions.

Best,

Karl

Karl Guiler, AICP

Senior Planner/Code Amendment Specialist

City of Boulder Department of Community Planning & Sustainability

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