

MEMORANDUM
May 7th, 2014

TO: Landmarks Board

FROM: Lesli Ellis, Comprehensive Planning Manager
Debra Kalish, Senior Assistant City Attorney
James Hewat, Senior Historic Preservation Planner
Marcy Cameron, Historic Preservation Planner

SUBJECT: Public hearing and consideration of an application for a Landmark Alteration Certificate to construct a new two-car, 950 sq. ft. detached garage with an Owners Accessory Unit above at 420 Spruce St. in the Mapleton Hill Historic District, per section 9-11-18 of the Boulder Revised Code (HIS2014-00081). Applicant: Juana Gomez Owner: Luis and Julia Garza.

STATISTICS:

- | | | |
|----|-------------------------|-----------------------------|
| 1. | Site: | 420 Spruce St. |
| 2. | Zoning: | RMX-1 (Residential Mixed-1) |
| 3. | Owner: | Luis and Julia Garza |
| 4. | Site Area: | 8,924 sq. ft. |
| 5. | Proposed Garage: | 950 sq. ft. |
| 6. | Proposed Garage Height: | 24'6 |

STAFF RECOMMENDATION:

Staff recommends that, provided the listed conditions are met, the applicant's proposal to construct a new accessory building is appropriate as it generally meets the standards for the approval of Landmark Alteration Certificates found in Subsections 9-11-18(b) and (c), B.R.C. 1981. Staff recommends the Landmarks Board adopt the following motion:

I move that the Landmarks Board adopt the staff memorandum dated May 7th, 2014 as the findings of the Board and approve a Landmarks Alteration Certificate for the construction of a garage at 420 Spruce St. as shown on plans dated 03.24.2014, subject to the following conditions:

CONDITIONS OF APPROVAL:

1. The applicant shall be responsible for ensuring that the development will be constructed in compliance with the application dated 03.24.2014 on file in the City of Boulder Community Planning and Sustainability Department, except as modified by these conditions of approval.
2. Prior building permit application and final issuance of the Landmark Alteration Certificate, the applicant shall submit revised plans for the proposed garage showing a reduction in height of the building so that it reads as a one-and-one half story building consistent with the *General Design Guidelines for Boulder's Historic Districts and Individual Landmarks* and the *Mapleton Hill Design Guidelines*.
3. Prior to submitting a building permit application and final issuance of the Landmark Alteration Certificate, the applicant shall submit the following: final details showing door and window details, roofing materials, railings, stairs, decking, siding, paving and proposed colors. These design details shall be reviewed and approved by the Landmarks design review committee, prior to the issuance of a building permit. The applicant shall demonstrate that the design details are in compliance with the intent of this approval and the *General Design Guidelines* and the *Mapleton Hill Historic District Design Guidelines*.

Staff finds the proposed installation consistent with Section 9-11-18, Boulder Revised Code (B.R.C.) 1981, the *Mapleton Hill Historic District Design Guidelines* and the *General Design Guidelines for Boulder's Historic Districts and Individual Landmarks*.

SUMMARY

- On Nov. 3, 2013, the Landmarks Board approved an application to demolish the non-contributing c.1923 house and construct a new house at 420 Spruce. The site is presently under construction.
- Because the application calls for new free-standing construction of more than 340 sq. ft., review by the full Landmarks Board in a quasi-judicial hearing is required pursuant to Section 9-11-14(b) of the Boulder Revised Code 1981.
- The application calls for the removal of a c.1960s carport, which is not considered to be contributing to the historic character of Mapleton Hill Historic District.
- In terms of scale, proportion and style, staff finds the proposed design is consistent with Section 7, New Accessory Buildings of the *General Design Guidelines*, Section U of the *Mapleton Hill Design Guidelines* and Section 9-11-18(b)(1)-(b)(4) and (c) of the Boulder Revised Code 1981.

- Staff considers that the height of the building should be reduced and as proposed, the design does not meet the *General Design Guidelines* or the *Mapleton Hill Design Guidelines* or Section 9-11-18(b)(1)-(b)(4) and (c) of the Boulder Revised Code 1981.
- Staff finds that if the height is reduced, the proposed new construction will be consistent with the criteria for a Landmark Alteration Certificate as per 9-11-18(a) & (b)(1)-(4) B.R.C. 1981, the *General Design Guidelines*, and the *Mapleton Hill Historic District Design Guidelines*.
- Staff’s recommendation to approve the construction of a new garage is based upon the understanding that the details to be provided pursuant to the conditions of approval will be reviewed and approved by the Landmarks design review committee (Ldrc) prior to the issuance of a Landmark Alteration Certificate.

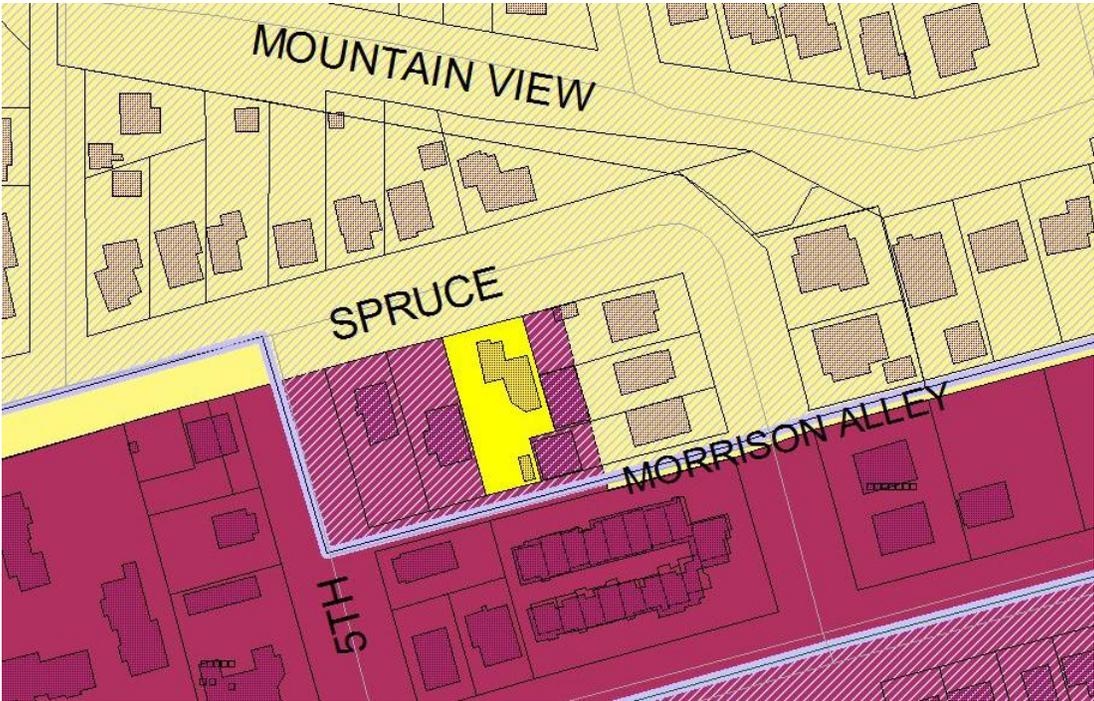


Figure 1. Location Map, 420 Spruce St. Mapleton Hill Historic District outlined in purple; maroon indicates RMX-1 zoning district; Pale yellow indicates RL-1 zoning district. 420 Spruce highlighted in neon yellow.

PROPERTY DESCRIPTION:

The property at 420 Spruce Street is part of the Mapleton Terrace addition to the city, which was platted in 1890 by W.H. Thompson, Harold D. Thompson, and Isaac C. Dennett. The Landmarks Board approved the demolition of the c.1923 house in 2013, finding that alterations to the house had diminished the historic architectural integrity of the building. In December 2013, the applicants received a Landmark Alteration Certificate for the construction of a new house.

The property is located at the southwest corner of the Mapleton Hill Historic District, and is in the Residential-Mixed 1 (RM-X 1) zoning district, whereas the majority of the historic district is in the Residential-Low 1 (RL-1) zoning district. Morrison Alley marks the southern boundary of the district. As such, the alleyscape on the 400 block between 4th and 8th Streets includes an eclectic mix of buildings in both size and age. A wide, one-story, gable-roof garage is located directly to the east of the proposed site, and the property to the west does not have an accessory building. Cedar, stockade fences line the north side of the alley and a 1990s condo development (located outside of the district), lines the south side of the alley and has both garage and pedestrian doors.



Figure 2. 420 Spruce St., 2013.



Figure 3. Rendering of proposed house at 420 Spruce St., 2013

The one and one-half story, 3,300 sq. ft. multi gable-roof house (approved by the Landmarks Board in 2013 and currently under construction), is roughly square in plan and built upon the existing 1990 foundation. In form, the house is designed in a neo-traditional vocabulary inspired by Edwardian Vernacular houses found in the area. At its highest point the house is shown to be 27'6" above grade. The first story of the building will be stuccoed and the upper gables sheathed in wood shingles.

PROPOSED NEW CONSTRUCTION:

The applicant now proposes to construct a two story, 950 sq. ft. garage to shelter two cars and provide space for an Owner's Accessory Unit above. The building is shown to be roughly square in plan and measure 24'6 ft. in height.



Figure 4. Rendering of proposed garage, south elevation.



Figure 5. Rendering of proposed garage, west and south elevations.

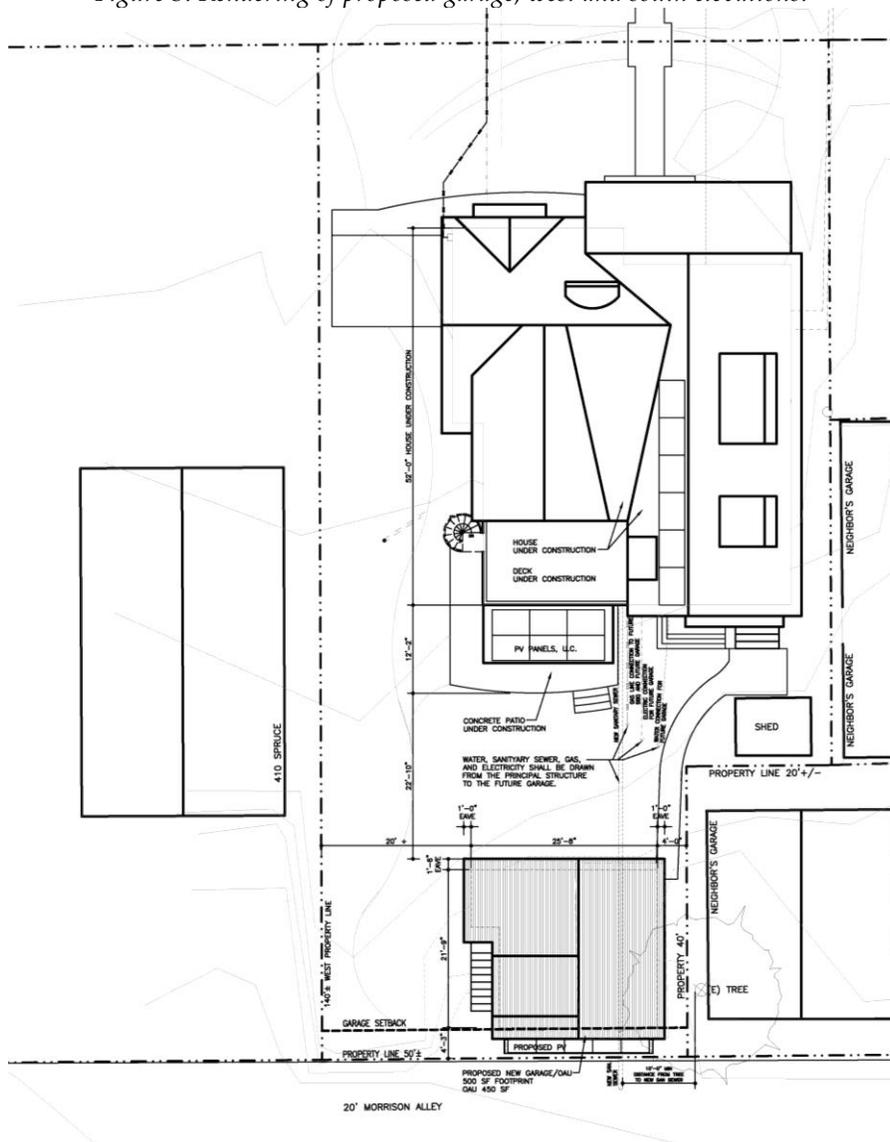


Figure 6. Proposed site plan.

In plan, the proposed garage measures approximately 21'9 by 21'9 and is shown to be located at the southeast corner of the lot, with alley access to the south. A rear deck, approved under a separate Landmark Alteration Certificate, is to be located at the rear of the primary house. Approximately 22' of landscaped area would separate the proposed garage and rear deck on the house.

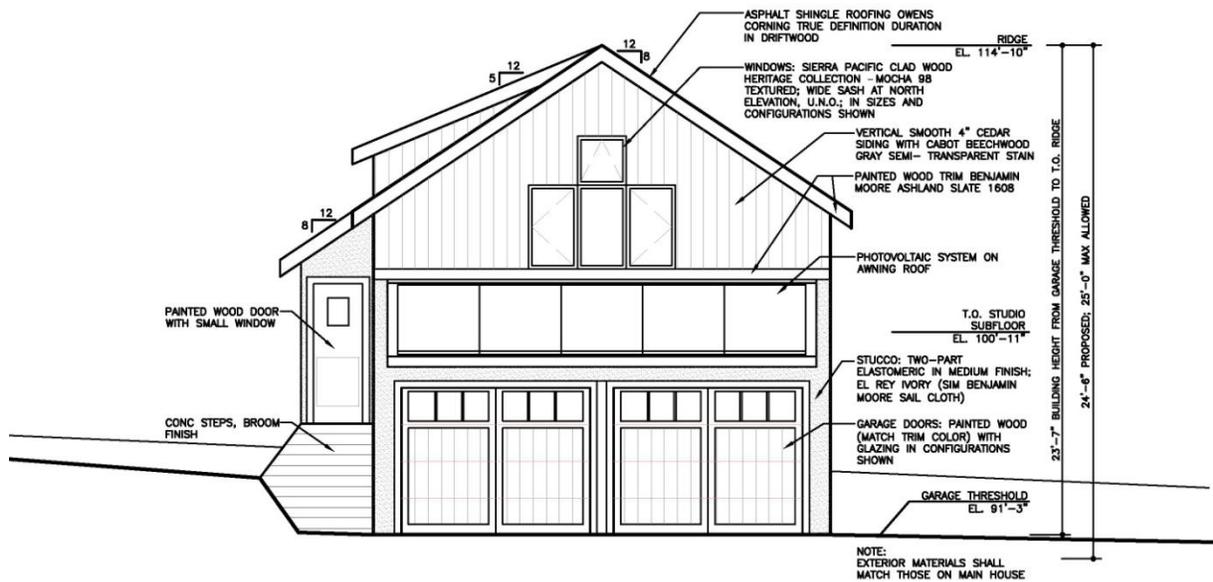


Figure 7. Proposed south elevation

The garage is proposed to have a two-story, front gable form with a small, shed roof form projecting from the west side. The garage is shown to measure 24'6 in height. The lower portion of the building is proposed to be clad in stucco to match the house, with vertical, smooth cedar siding with a gray semi-transparent stain above. Plans show windows to be Sierra Pacific clad wood and the garage to be painted to match the house, with ivory colored stucco, slate blue wood trim, cedar shingles with an opaque stain, and clad wood windows in "Mocha."

The proposed south elevation features two painted wood garage doors with a traditional carriage door configuration. Five photovoltaic panels are shown on a shed roof that projects over the garage doors. A wood door with a small, square light is located on the west side of the building and is accessed by a staircase that follows the grade of the site. Four windows, arranged in a symmetrical pattern, are shown at the gable end, and the shed dormer on the west elevation is visible from the south.

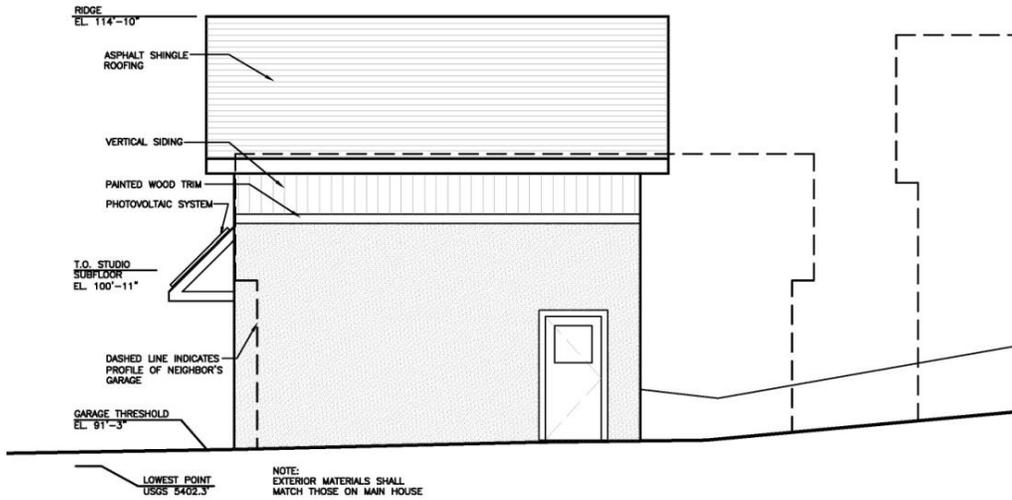


Figure 8. Proposed east elevation

The proposed east elevation of the building is shown to be minimally fenestrated, with a single door at the north end. Vertical wood siding is located under the eaves of the roof.

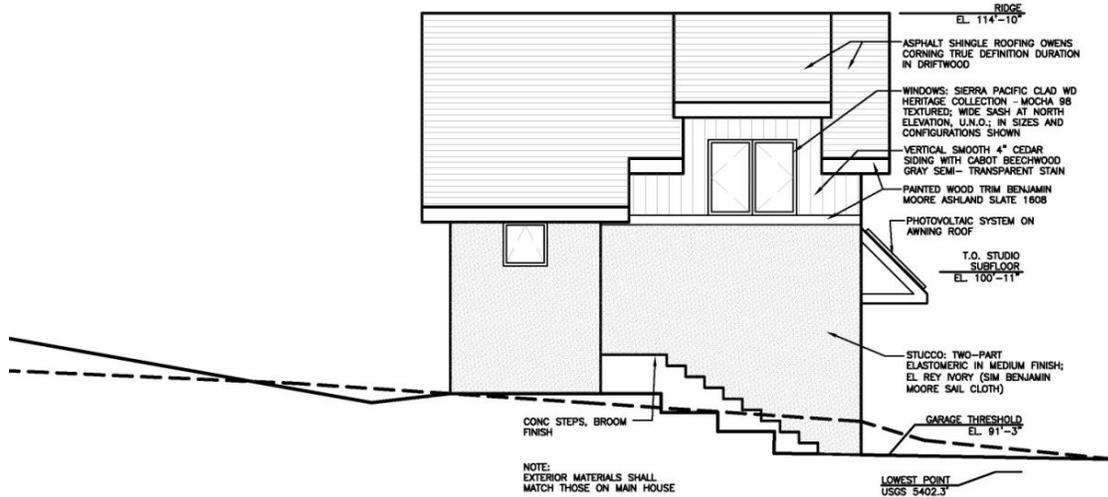


Figure 9. Proposed west elevation

A shed-roof wall dormer is located on the west elevation of the proposed garage. Two windows are located at the dormer and a single casement window is located at the north end of the elevation. A concrete stair that follows the grade of the side will also be visible from the west.

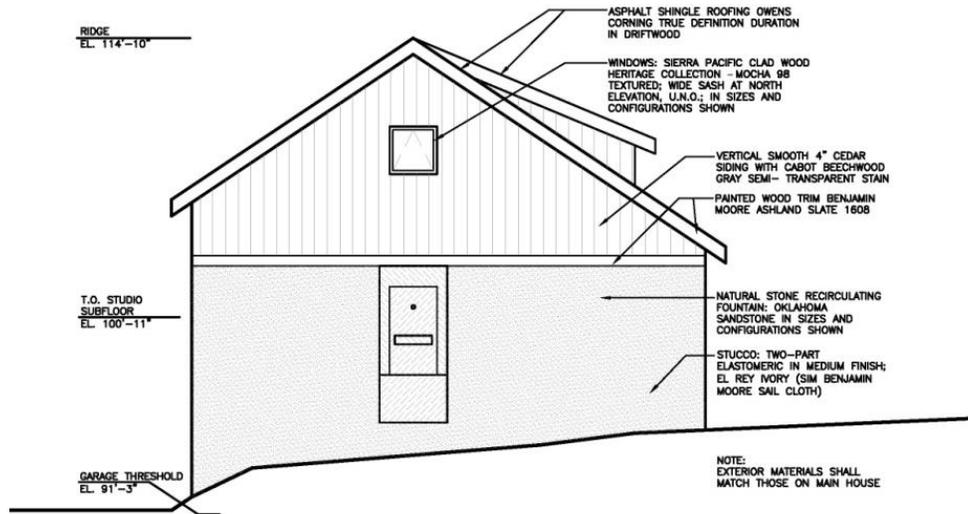


Figure 10. Proposed north elevation (interior lot)

The proposed north (garden) elevation features a single casement window on the gable end and a stone recirculating fountain is show mounted to the wall.

Plans call for the removal of a c.1960s carport that is currently located at the south end of the lot. The *General Design Guidelines* and *Mapleton Hill Historic District Design Guidelines* discourage carports, and its construction lies outside of the district’s period of significance. Staff considers that the removal of this non-historic feature will not detract from the historic character of the district.

CRITERIA FOR THE BOARD’S DECISION

Subsection 9-11-18(b) and (c), B.R.C. 1981, sets forth the standards the Landmarks Board must apply when reviewing a request for a Landmark Alteration Certificate.

(b) Neither the Landmarks Board nor the City Council shall approve a Landmark Alteration Certificate unless it meets the following conditions:

- (1) The proposed work preserves, enhances, or restores and does not damage or destroy the exterior architectural features of the landmark or the subject property within an historic district;
- (2) The proposed work does not adversely affect the special character or special historic, architectural, or aesthetic interest or value of the landmark and its site or the district;
- (3) The architectural style, arrangement, texture, color, arrangement of color, and materials used on existing and proposed constructions are compatible with the character of the existing landmark and its site or the historic

district;

- (4) With respect to a proposal to demolish a building in an historic district, the proposed new construction to replace the building meets the requirements of paragraphs (b)(2) and (3) above.

- (c) In determining whether to approve a landmark alteration certificate, the Landmarks Board shall consider the economic feasibility of alternatives, incorporation of energy-efficient design, and enhanced access for the disabled.

ANALYSIS

1. *Does the proposed application preserve, enhance, or restore, and not damage or destroy the exterior architectural features of the landmark or the subject property within a historic district?*

The house currently under construction is considered to be non-contributing to Mapleton Hill Historic District, as it will have been constructed outside of the period of significance. Staff finds that, provided the listed conditions are met, the demolition of the non-contributing c.1960 carport and construction of the proposed house will not damage or destroy contributing properties in the streetscape and will be generally compatible and consistent with the *General Design Guidelines* and the *Mapleton Hill Historic District Guidelines* (see Design Guidelines Analysis section).

2. *Does the proposed application adversely affect the special character or special historic, architectural, or aesthetic interest or value of the district?*

The staff finds that, provided the listed conditions are met, the proposed application will not adversely affect the special character or special historic, architectural, or aesthetic interest or value of the district because the proposed new garage will be generally compatible with the *General Design Guidelines* and the *Mapleton Hill Historic District Guidelines* in terms of mass, scale, height, design and color (see Design Guidelines Analysis section).

3. *Is the architectural style, arrangement, texture, color, arrangement of color, and materials used on existing and proposed structures compatible with the character of the historic district?*

Staff considers the proposed design of the two-car garage to be generally compatible with the architectural style, arrangement, texture, color, arrangement of color, and materials used on the proposed building and will be generally

compatible with the character of the historic district (see Design Guidelines Analysis section).

4. Does the proposal to demolish the building within the Mapleton Hill Historic District and the proposed new construction to replace the proposed demolished building meet the requirements of paragraphs 9-11-18(b)(2), 9-11-18(b)(3) and (4) of this section?

Staff finds that the application to demolish the non-contributing carport and construct a new garage meets the requirements of paragraphs 9-11-18(b)(2), 9-11-18(b)(3) and 9-11-18(b)(4) because, provided the listed conditions are met, the construction of a new garage will establish compatible features on the alleyscape. With the stated conditions, the application is generally compatible and consistent with the *General Design Guidelines* and the *Mapleton Hill Historic District Guidelines* (see Design Guidelines Analysis section).

Once modified as suggested in the Conditions of Approval, the proposal will be consistent in terms of site planning, mass, scale, materials and architectural details and will not detract from the Mapleton Hill Historic District.

DESIGN GUIDELINES

The Historic Preservation Ordinance sets forth the standards the Landmarks Board must apply when reviewing a request for a Landmark Alteration Certificate and the board has adopted the *General Design Guidelines* to help interpret the ordinance. The following is an analysis of the submitted proposal with respect to relevant guidelines. It is important to emphasize that design guidelines are intended to be used as an aid to appropriate design, and not as a checklist of items for compliance.

The following is an analysis of the proposal's compliance with the applicable design guidelines:

GENERAL DESIGN GUIDELINES: GARAGES & OTHER ACCESSORY BUILDINGS

2.3	Site Design: Alleys
	<p>The alleys in historic districts were traditionally used for secondary access to the houses, for deliveries, and as storage places for horses and buggies, and later, for cars. A view of the backyards from the alleys was maintained. While today's alleys have evolved into use as pedestrian paths for jogging, bicycling and dog walking, they still contribute to the historic character of the neighborhood. They are typically minimally paved.</p> <p>Along the alleys are historic accessory buildings of various shapes and sizes including barns, chicken coops, sheds and small garages. This variety contributes to the general feeling of human scale in the alleys.</p>

	Guidelines	Analysis	Conforms?
.1	<i>Maintain alley access for parking and retain the character of alleys as clearly secondary access to properties.</i>	Rear parking is maintained by the proposal.	Yes
.2	<i>Retain and preserve the variety and character found in the existing historic accessory buildings along the alleys.</i>	The 1960s carport proposed for demolition does not contribute to the special character of the historic district.	Yes
.3	<i>The use of historically proportioned materials for building new accessory buildings contributes to the human scale of the alleys. For example, narrower lap siding and smaller brick are appropriate.</i>	New garage shown to be clad in stucco and horizontal lap siding, similar to the house that is currently under construction	Yes
.4	<i>Buildings that were constructed after the period of significance but are still more than 50 years old and contribute to the variety and character of the alleyway should be retained.</i>	The 1960s carport does not contribute to the variety and character of the alleyway. Carports are discouraged by the design guidelines.	Yes
.5	<i>Maintain adequate spacing between accessory building so that the view of the main house is not obscured, and the alley does not evolve into a tunnel-like passage.</i>	The location of the proposed garage would partially obscure the view of the non-contributing house, but its location would maintain adequate spacing along the alley.	Yes

7.0	Garages & Other Accessory Structures
	<p><i>Accessory structures include barns, sheds, garages and outbuildings. Originally accessory structures were used for storage of equipment, animals, or carriages. Generally, these structures have been adapted for the storage of cars. In most cases, accessory building were located to the rear of the lot and accessed by alleys. They were subordinate in size and detailing to the primary house. Over time they have emerged as important elements of many lots and alleys in the district. Efforts should be made to protect the eclectic character of alleys.</i></p> <p><i>Both additions to existing accessory buildings and new accessory building will be evaluated in terms of how they affect the historic character of the individual site and the district as a whole. In the past, larger accessory structures have been allowed than may be appropriate today.</i></p>

7.1	Existing Historic Accessory Buildings
	<i>A primary concern of the Landmarks Board in reviewing proposed changes in historic districts is the protection of existing historic accessory structures and the character of the site and district.</i>

	GUIDELINES:	ANALYSIS:	CONFORMS
.1	<i>Retain and preserve garages and</i>	Existing carport was constructed in	Yes

	<i>accessory buildings that contribute to the overall character of the site or district.</i>	the 1960s and is not considered contributing to the historic district.	
.2	<i>Retain and preserve the character-defining materials, features, and architectural details of historic garages and accessory buildings, including roofs, exterior materials, windows and doors.</i>	Existing carport was constructed in the 1960s and is not considered contributing to the historic district.	Yes
7.2	New Accessory Buildings		
<i>New accessory buildings should follow the character and pattern of historic accessory buildings. While they should take design cues from the primary buildings, they must be subordinate in size, massing, and detailing. Alley buildings should maintain a scale that is pleasant to walk along and comfortable for pedestrians.</i>			
Location and Orientation			
.1	<i>It is inappropriate to introduce a new garage or accessory building if doing so will detract from the overall historic character of the principal building, and the site, or if it will require removal of a significant historic building element or site feature, such as a mature tree.</i>	As the primary house is considered to be non-contributing to the character of the historic district, the construction of a new garage will not impact the character of the principal building. Its construction will not require the removal of a significant historic site feature.	Yes
.2	<i>New garages and accessory buildings should generally be located at the rear of the lot, respecting the traditional relationship of such buildings to the primary structure and the site.</i>	The new garage would be located at the southeast corner (rear) of the lot.	Yes
.3	<i>Maintain adequate spacing between accessory buildings so alleys do not evolve into tunnel-like passageways.</i>	Accessory building setback approximately 4'3 from the south property line and 4' from the west property line. The proposed garage will be located adjacent to the garage at 2029 5 th St., but due to the perpendicular configuration of the lots, adequate spacing will be maintained.	Yes
.4	<i>Preserve a backyard area between the house and the accessory buildings, maintaining the general proportion of built mass to open space found within the area.</i>	Proposed garage shown to be located 4'3 from the alley, 4' from the east property line and approximately 22' from the deck at the rear of the house. This will allow for adequate back yard space.	Yes
Mass and Scale			
.5	<i>New accessory buildings should take design cues from the primary building on the property, but be subordinate to it</i>	Proposed design relates to non-contributing primary building currently under construction in	Maybe

	<i>in terms of size and massing.</i>	terms of proportion and style, however, the 24'6 height is much greater than the typical height of accessory buildings in the historic district. Consider lowering 3–5' in height to 1 & ½ story building and reduction of mass and scale of garage to be more in keeping with historic accessory buildings in Mapleton Hill.	
.6	<i>New garages for single-family residences should generally be one story tall and shelter no more than two cars. In some cases, a two-car garage may be inappropriate.</i>	Proposed two-car garage measures 24'6 in height and is two-stories tall. The property is located at the southwest corner of the historic district, and is in the RMX-1 zoning district. This portion of the alley has an eclectic mix of building types and sizes, and is bordered on the south by a 1990s condominium building. Staff considers that a two car garage in this location will not detract from the character of the historic district. However, the building should be reduced in height to be more in character with accessory buildings within the historic district. Resolve at Ldrc.	Maybe
.7	<i>Roof form and pitch should be complementary to the primary structure.</i>	Roof form is complementary to the non-contributing main house.	Yes
Materials and Detailing			
.8	<i>Accessory structures should be simpler in design and detail than the primary building.</i>	As shown, garage is complimentary to the main house.	Yes
.9	<i>Materials for new garages and accessory structures should be compatible with those found on the primary structure and in the district. Vinyl siding and prefabricated structures are inappropriate.</i>	Proposed materials (stucco, wood siding, clad windows in new construction) will be compatible with the character of the primary house and with the historic district.	Yes
.10	<i>Windows, like all elements of accessory structures, should be simpler in detailing and smaller in scale than similar elements on primary structures.</i>	Proposed design of windows on north and west elevations appear to be compatible in terms of window type, size and detailing with similar elements on the primary building. Contemporary window pattern	Maybe

		shown on south (alley-facing) elevation relates to the non-contributing house but is unusual in the historic district. A simplified window pattern may be more appropriate. Resolve at Ldrc.	
.11	<i>If consistent with the architectural style and appropriately sized and located, dormers may be an appropriate way to increase storage space in garages.</i>	Shed dormer on west elevation shown to be appropriately sized and located, and subordinate to the main roof form.	Yes
.12	<i>Garage doors should be consistent with the historic scale and materials of traditional accessory structures. Wood is the most appropriate material and two smaller doors may be more appropriate than one large door.</i>	Garage doors proposed to be painted wood; appear to be consistent in terms of scale and materials. Review final details at Ldrc.	Yes
.13	<i>It is inappropriate to introduce features or details to a garage or an accessory building in an attempt to create a false historical appearance.</i>	Proposed design does not attempt to recreate a false historic appearance.	Yes
.14	<i>Carports are inappropriate in districts where their form has no historic precedent.</i>	Carport proposed for removal.	Yes

8.2 ENERGY EFFICIENCY			
	Guideline	Analysis	Conforms?
.4	<i>It is not appropriate to install solar collectors in locations that compromise prominent roofs. The installation of solar collectors may be appropriate provided it does not detract from the historic character of the property, landmark or historic district.</i>	Solar panels proposed at shed roof on south elevation of the accessory building. This location on a new accessory building will not detract from the character of the historic district.	Yes

Mapleton Hill Historic District Guidelines

The following section is an analysis of the proposal relative to Section VI of the *Mapleton Hill Historic District Design Guidelines*. Only those guidelines that further the analysis of the proposed project are included and those that reflect what has been evaluated in the previous section are not repeated.

B	SITE
	<i>Traditional settlement patterns generally placed houses in the center of a site, with garages, carriage houses, etc. and parking at the rear...</i>

	Guideline	Analysis	Conforms?
.1	<i>Accessory buildings such as sheds and garages, and driveways should be located at the rear of the lot as is traditional. Adding them between existing buildings interrupts the rhythm and spacing.</i>	New garage shown to be located at the southeast corner (rear) of the lot.	Yes
2.	<i>Accessory buildings should generally be small in scale and mass and simply detailed. They are clearly secondary in importance to the primary house.</i>	At two stories, the proposed garage is larger in scale and mass than accessory buildings found in the district. This section of the alley has a more varied scale due to its location at the edge of the district and RMX-1 zoning district, and staff considers a one-and-a-half story garage would be appropriate, where in other areas of Mapleton Hill an upper-story would be out of place. Resolve at Ldrc.	No

D ALLEYS, EASEMENTS AND ACCESSWAYS			
	<i>Alleys are a strong visual element of the district, and have much variety of scale and detail. They play an important part in the development patterns that give the more visible areas their character. Alleys provide access to rear parking and garages. They have a varied edge quality, with building both on the property lines and set back. The size and quality of these accessory building varies considerably. Careful consideration should be given to changes in traditional use.</i>		
	Guideline	Analysis	Conforms?
1.	<i>The use of alleys to provide access to the rear of properties should be preserved</i>	Access to rear of property preserved.	Yes
2.	<i>Efforts should be made to protect the variety of shape, size, and alignment of buildings along the alleys. Alleys should maintain a human scale and be sensitive to pedestrians.</i>	Proposal will preserve variety found on immediate alleyscape. Two-story form may not maintain human scale of the alley. Reducing height will reduce overall mass and scale of the building. Resolve at Ldrc.	Maybe

3.	<i>Building such as garages, sheds, etc. which contribute to this variety should be retained in their original form whenever possible.</i>	The existing carport is not considered contributing to the historic district; staff finds its removal appropriate.	Yes
4.	<i>Efforts should be made to maintain character of the alleys in the district</i>	Morrison Alley marks the southern boundary of the Mapleton Hill Historic District, and is the boundary between the Residential-Low and higher density Residential Mixed zoning districts. Proposal will preserve variety found in immediate alleyscape.	Yes
10.	<i>Lighting in alleys should be low wattage and focused downward.</i>	Lighting not shown on plans. If light fixtures are proposed, review at Ldrc.	Maybe

P	GARAGES, CARPORTS AND ACCESSORY STRUCTURES		
	<i>A variety of accessory buildings has been adapted for use as garages in the Mapleton Hill Historic District. Whether carriage houses or sheds, these structures have certain similarities. They are plain and utilitarian and are located at the rear of the property on the alley. Materials and building elements are varied.</i>		
	Guideline	Analysis	Conforms?
1.	<i>Free-standing carports are extremely difficult to fit into the district since their form has no historic precedent. Other solutions for sheltering vehicles should be sought.</i>	Carport proposed for removal.	Yes
3.	<i>If a new building is to be constructed, design ideas might be found in existing historic accessory buildings located nearby</i>	This section of the alley has an eclectic variety of accessory buildings. Design references one-and-a-half story accessory buildings along the 700 block of Morrison Alley.	Yes
4.	<i>The new building should be secondary in nature to the main house and smaller in scale.</i>	Proposed design will be secondary to main house in terms of scale. Building height of 24'6 is greater than typically found in the historic district. Reducing height will reduce overall mass and scale of the building. Resolve at Ldrc.	Maybe

5.	<i>Accessory buildings should be small in scale and mass, and constructed in a manner which is complimentary to the character of the house and alley. They are clearly secondary in importance to the primary structure. Typically, prefabricated sheds are discouraged.</i>	Height of proposed accessory building should be reduced 3-5' to be smaller in scale and mass. Stylistically, the house is complementary to the house and alley, and is secondary to the house. . Reducing height will reduce overall mass and scale of the building. Resolve at Ldrc.	Maybe
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The property at 420 Spruce St. is uniquely situated in the southwest corner of the Mapleton Hill Historic District, in a small area that is in the RMX-1 zoning district. Morrison Alley, located south of the property, marks the boundary between the Mapleton Hill Historic District and the denser, urban character of the downtown area. Due to this location and the non-contributing character of the existing house (currently under construction), staff finds the proposed construction of a two-car garage appropriate. Staff considers that the proposed construction of a new garage is generally consistent with the design guidelines for site design, orientation, architectural design and detailing, but considers that the height should be reduced 3-5' so that the building is less massive and maintains a one-and-a-half story form more characteristic of historic accessory buildings in this section of the historic district..

Revisions to the design should be reviewed and approved by the Landmarks design review committee (Ldrc).

FINDINGS

Subject to the conditions stated in the recommended motion, staff recommends that the Landmarks Board approve the application and adopt the following findings:

This decision is consistent with the purposes and standards of the Historic Preservation Ordinance, in that:

1. The proposed new construction will not adversely affect the special character of the Mapleton Hill District. (9-11-18(b)(1), B.R.C. 1981).
2. The proposed new construction will not adversely affect the special character or special historic, architectural, or aesthetic interest or value of the property or the historic district. (9-11-18(b)(2).
3. The proposed new construction will generally comply with Sections 2, 7, and 8 of

the *General Design Guidelines* and Sections B (Site), D (Alleys) and P (Garages) of the *Mapleton Hill Historic District Guidelines*, adopted by the Landmarks Board as Administrative Regulations, and Section 9-11-18(b)(3) of the Boulder Revised Code 1981.

ATTACHMENTS:

- A: Photographs
- B: Plans and Elevations

Attachment A: Photographs



Photo 1. View of existing carport and garage at 2029 5th St, 2014. .



Photo 2. View of existing carport and garage at 2029 5th St., 2014.



Photo 3. View facing west, existing carport and garage at 2029 5th St. visible, 2014.

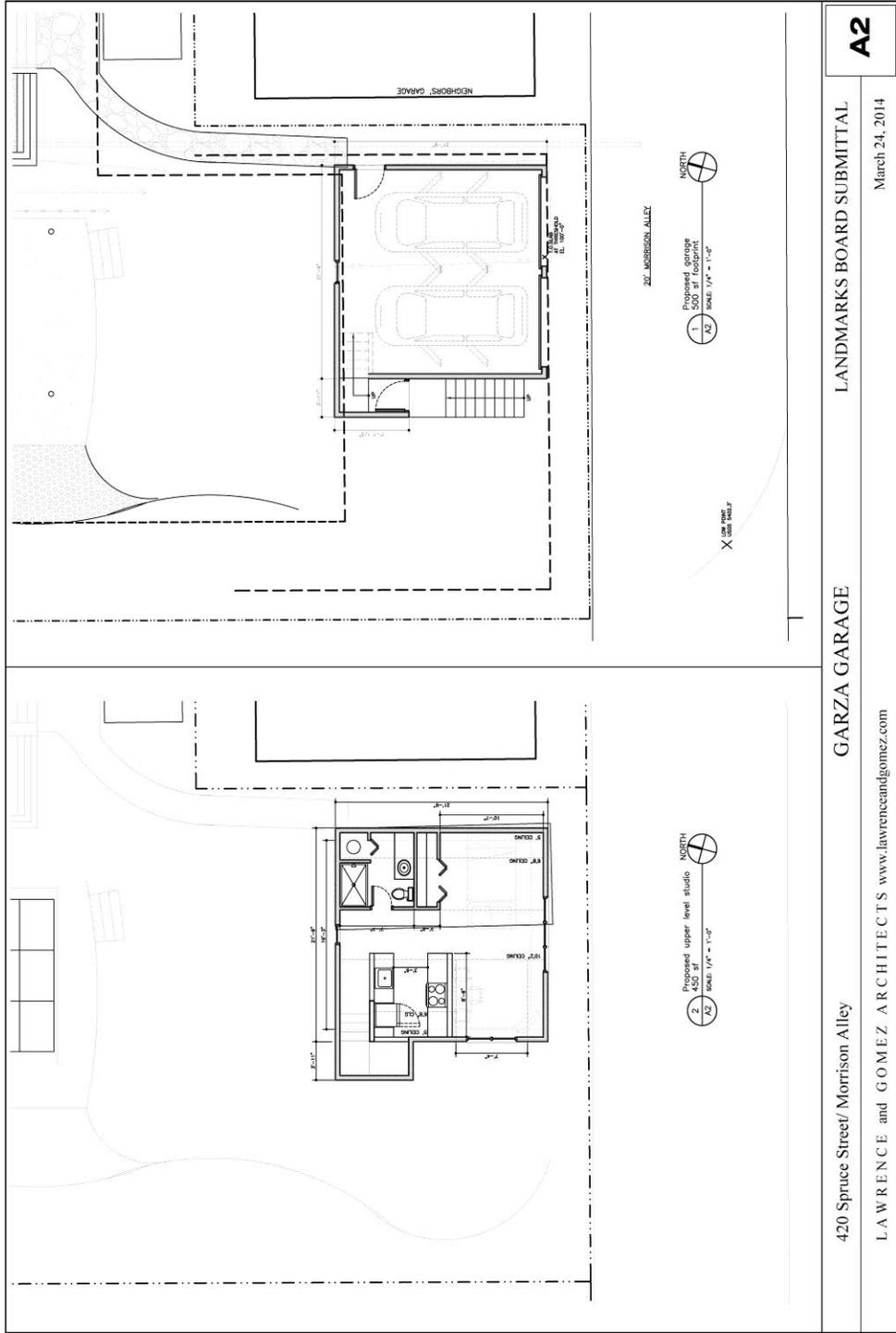


Photo 4. View facing north showing proposed location of new garage, 2014.



Photo 5. View facing east down Morrison Alley, 2014.

Proposed Floor Plans



Existing Site Plan (partial)

Renderings

RENDERINGS OF PROPOSED GARAGE AT 420 Spruce Street / Morrison Alley
March 24, 2014



Proposed detached garage seen on along Morrison Alley. The house is currently under construction. The neighbors are rendered in light blue. North is to the left.

RENDERINGS OF PROPOSED GARAGE AT 420 Spruce Street / Morrison Alley
March 24, 2014



View of proposed garage from the southeast

RENDERINGS OF PROPOSED GARAGE AT 420 Spruce Street / Morrison Alley
March 24, 2014



South from Morrison Alley

RENDERINGS OF PROPOSED GARAGE AT 420 Spruce Street / Morrison Alley
March 24, 2014



View looking west of proposed garage roof above neighbor's roof from upper level of neighbor to the east, per Google Earth