

MEMORANDUM

July 2, 2014

TO: Landmarks Board

FROM: Lesli Ellis, Comprehensive Planning Manager
James Hewat, Senior Historic Preservation Planner
Marcy Cameron, Historic Preservation Planner
Angela Smelker, Historic Preservation Intern

SUBJECT: Public hearing and consideration of an application for a Landmark Alteration Certificate to construct a 925 sq. ft., one-story, four-car garage at the northeast corner of the property at 1507 Pine Street, an individually landmarked property, per section 9-11-18 of the Boulder Revised Code 1981 (HIS2013-00219).

STATISTICS:

1. Site: 1507 Pine St.
2. Designation: Individual Landmark
3. Historic Name(s): Temple-Bowron House
4. Date of Construction: 1883
5. Zoning: RMX-1 (Residential-Mixed 1)
6. Lot size: 13,962 sq. ft.
7. Proposed Garage: 925 sq. ft.
8. Applicant: Kristin Lewis
9. Owner: Susan Dawson

STAFF RECOMMENDATION:

It is staff's opinion that if the applicant complies with the conditions listed below, the proposed construction of a new free-standing garage will be generally consistent with the conditions specified in Section 9-11-18, B.R.C. 1981, and the *General Design Guidelines*. Staff recommends that the Landmarks Board adopt the following motion:

I move that the Landmarks Board adopt the staff memorandum dated July 2, 2014, in matter 5C (HIS2014-00174), as the findings of the board and approve the construction of a new, free-standing garage as shown on plans dated May 29, 2014, finding that the plans generally meets the standards for issuance of a Landmark Alteration Certificate in Chapter 9-11-18, B.R.C. 1981, subject to the following conditions:

CONDITIONS OF APPROVAL:

1. The applicant shall be responsible for constructing the garage in compliance with the approved plans dated May 29, 2014, except as modified by these conditions of approval.
2. Prior to submitting a building permit application and final issuance of the Landmark Alteration Certificate, the applicant shall submit the following: final details showing door and window details, roofing materials, wall materials, proposed colors, hardscaping and fencing, including the proposed trash enclosure. These design details shall be reviewed and approved by the Landmarks design review committee, prior to the issuance of a building permit. The applicant shall demonstrate that the design details are in compliance with the intent of this approval and the *General Design Guidelines*.

SUMMARY:

- Because this application calls for free-standing construction of more than 340 sq. ft., review by the full Landmarks Board in a quasi-judicial hearing is required, pursuant to Section 9-11-14(b) of the historic preservation ordinance.
- In 2009, the Landmarks Board reviewed an application for the construction of a two-story, 5,050 sq. ft. rear and side addition to the building. The application was denied.
- In May 2014, the Landmarks design review committee (Ldrc) conceptually reviewed plans for rehabilitation of the house, including restoration of the front porch and construction of an elevator tower at the rear of the house. The Ldrc was generally supportive of the mass, scale and orientation of the proposed addition and of the property owner's plans to restore the house and return the property to its historic residential use.
- Staff finds the proposed new construction to be generally consistent with the criteria for a Landmark Alteration Certificate, pursuant to Section 9-11-18(a) and (b)(1)-(4), B.R.C. 1981, and the *General Design Guidelines*.
- Staff's recommendation to approve the proposed alteration is based upon the understanding that the stated conditions will be reviewed and approved by the Ldrc prior to the issuance of a Landmark Alteration Certificate.

PROPERTY DESCRIPTION:

Located at the southwest corner of Pine and 15th Streets, the 13,962 sq. ft. lot contains a two and one-half story Italianate house constructed for local businessman and early regent of the University of Colorado, Edwin J. Temple. The prominent location and generous size of the lot affords the Temple-Bowron house high visibility in the potential Whittier Historic District in which it is located, especially when approached from the north, south, and west.



Figure 1: 1507 Pine St. Tax Assessor Card photograph c.1929, showing original hipped roof. Photograph Courtesy the Carnegie Branch Library for Local History.

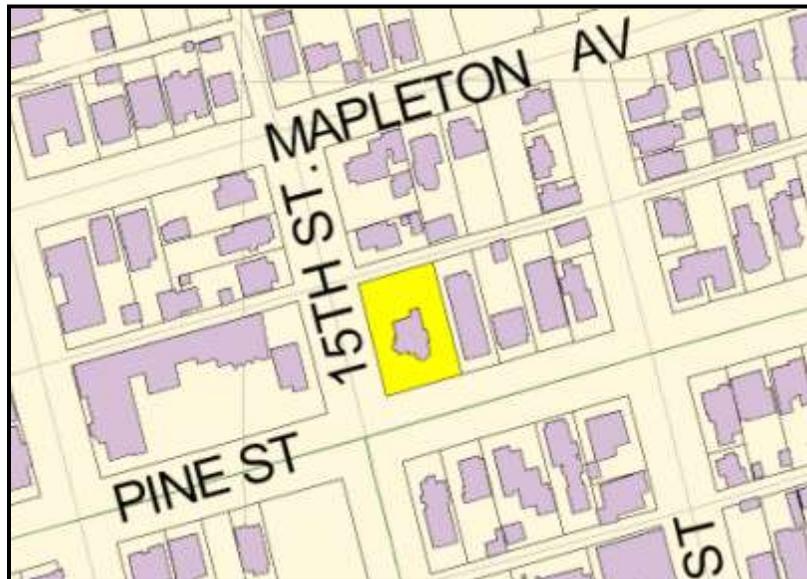


Figure 2: Location Map, 1507 Pine St.

Temple was a native of Youngstown, Ohio, and came to Colorado with his parents when he was ten years old.¹ He engaged in the milling and merchandising businesses in Black Hawk before settling in Boulder in 1881. He was the vice-president of the Pawnee Cattle Company of Colorado and was involved in ranches in Routt County and in New Mexico. He was a director and manager of the Boulder Electric Light Company and served several terms as a member of the University of Colorado Board of Regents and the Boulder School Board. He also was a member of the City Council and a director of National State Bank. Edwin was married to Nina M. Smith, daughter of pioneer Nelson K. Smith.

The Temples resided in the house from its construction in 1883 until Edwin's death in 1929. The property was then purchased by Mrs. Bertha Shuey in 1929 and converted into apartments. In 1941, the building housed six apartments and by 1948 the building housed nine apartments. In 1947, the house was significantly remodeled, including the addition of a third story (altering the hipped roof to a mansard roof) and removal of the tower.



*Figure 3. 1507 Pine St., 1974.
Third story addition constructed in 1947, original porches removed.*

In 1971, the property was purchased by Robert and Prissy Bowron. The Bowrons undertook renovation efforts, including replacement of a small porch on the west elevation and reconstruction of the tower. In 1977, the property was designated

*Barker.*¹ 76 Homes of Boulder, p43.

as the city's 12th individual landmark. In 1981, the house went through special Use Review to allow offices on the first and second floors, with third story apartment use. The current owner intends to convert the property back to its historic residential use.

Alterations to the house include the removal and subsequent reconstruction of the tower, third story addition that changed the roof profile from a hipped roof to a mansard roof, and removal of rear and side porches. Analysis of the Sanborn Fire Insurance Maps and building permit records show that the one-story rear addition was constructed between 1895 and 1900. Also in that period, the bay window and porch at the southwest corner of the house appear to have been altered. The footprint of the house has not changed since 1900.

Historically, accessory buildings were located at the rear of the lot. Prior to 1890, a one story, frame accessory building was located along the northern property line. By 1895, it has been removed and two accessory buildings were constructed in its place (See Figures 4 and 5). A two-story accessory building was located at the northeast corner of the lot, with a smaller, one-story building located to the west. The buildings remained until 1940, when they were demolished and a one-story, four-car garage was constructed in its place. The Tax Assessor Card indicates the four-car garage measured 21 ft. wide, 40 ft. long and was 10 ft. high. It was constructed of stone, and had a cement floor and a flat tin roof. It was oriented north-south, with access from the west side. As a condition of Use Review included by the Landmarks Board in 1981, the applicant was required to either "completely restore or demolish" the garage. The owners chose to demolish the building.

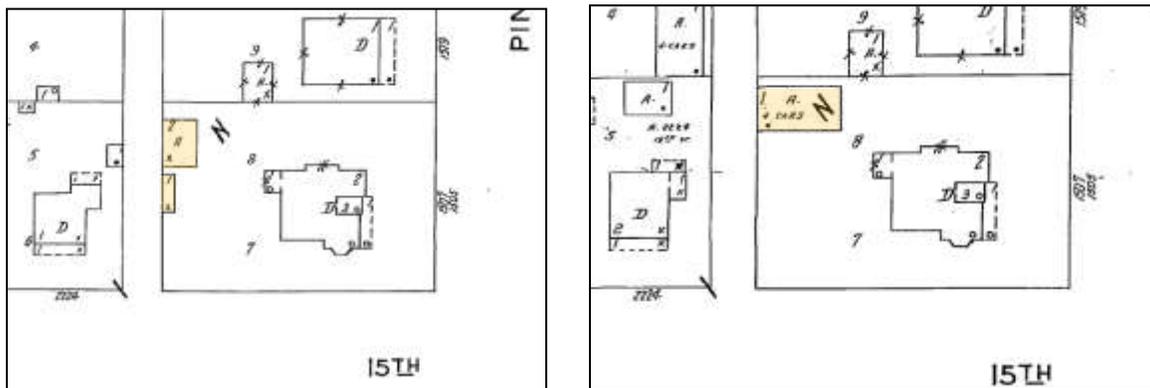


Figure 4. Sanborn Fire Insurance Maps, 1922 (L) and 1931-1960 (R).
Accessory buildings located along the alley (highlighted).



Figure 5. View from Lover's Hill showing accessory buildings at rear of lot, c.1898



Figure 6. 1507 Pine St., southwest corner, 2014.



Figure 7. 1507 Pine St., location of proposed garage from northwest corner of property, 2014.



Figure 8. 1507 Pine St., location of proposed garage, 2014.



Figure 9. 1507 Pine St., fence at north east corner of property, 2014.



Figure 10. 1507 Pine St., south elevation, 2014.



Figure 11: 1507 Pine St., facing east with alley in view on left, 2014.

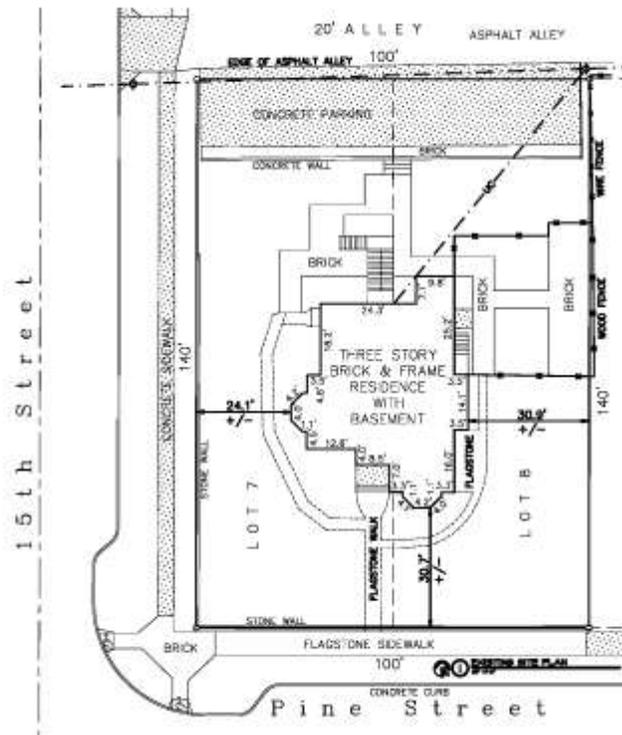


Figure 12: Existing Site Plan

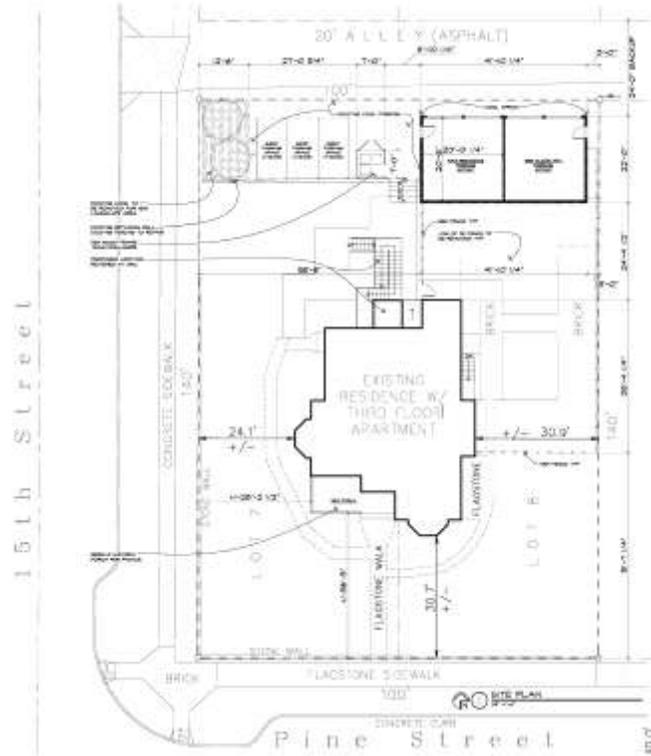


Figure 13: Proposed Site Plan

PROPOSED CONSTRUCTION OF A GARAGE

A 925 sq. ft., one-story garage is proposed at the northeast corner of the lot. The garage is to measure 22' by 41'10" and is accessed off the alley on the north side. The building is to measure 12' to the top of the parapet. The interior head height would measure 9'.

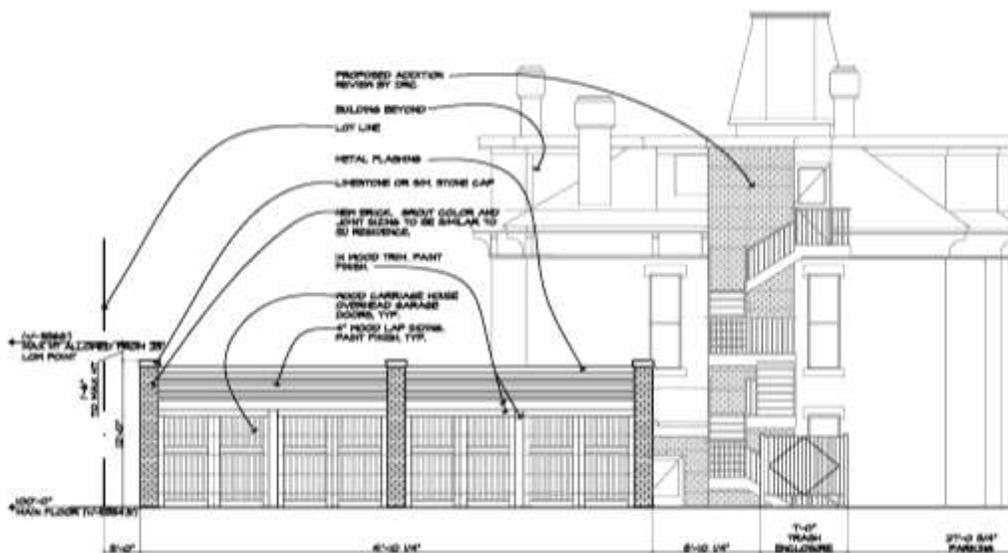


Figure 14: Proposed North Elevation (façade)

The north elevation, facing the garage, is shown to have two carriage-style overhead doors. Brick piers are to be located on the ends and in the middle. The parapet above the garage doors would be clad in 4 inch wood lap siding.

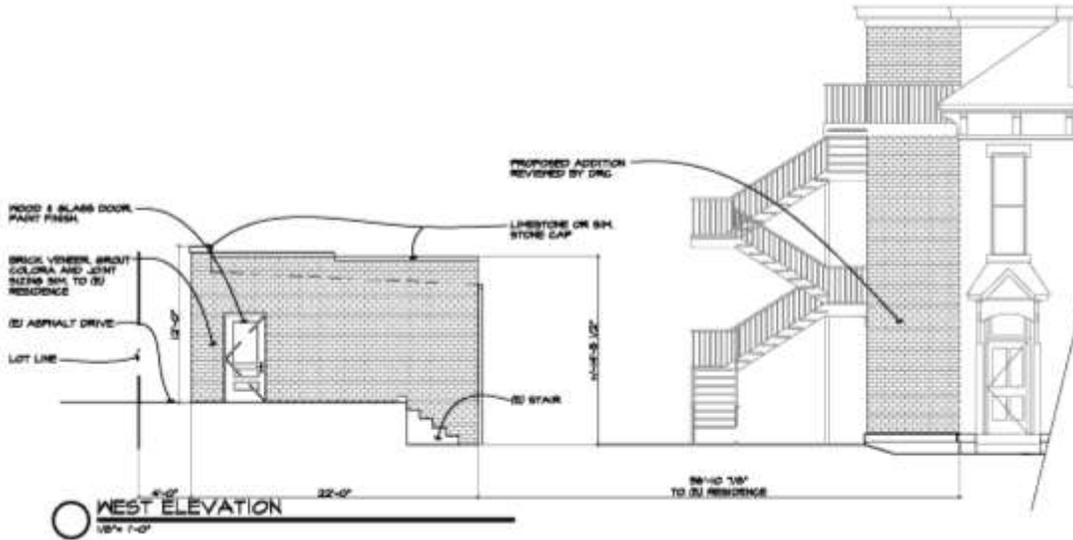


Figure 15: Proposed West Elevation

The west elevation, facing 15th St., is shown to be simply detailed, with a half-light wood pedestrian door and stairs from the existing raised parking area to the lawn below.

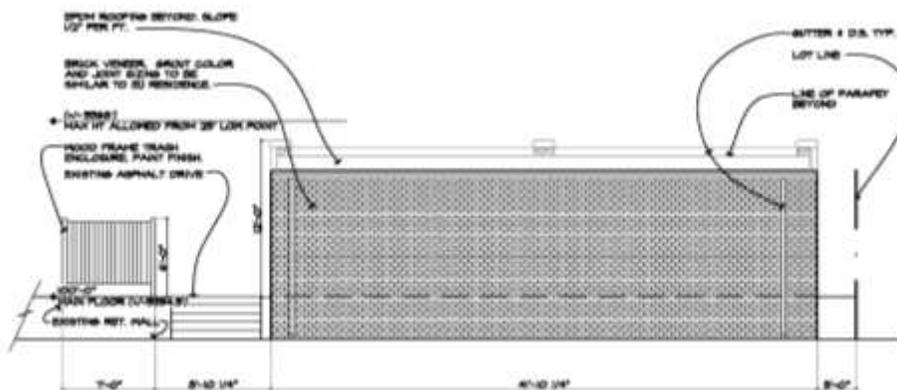


Figure 16: Proposed South Elevation

The south elevation, facing the interior of the lot, does not feature any fenestration or ornamental detailing.

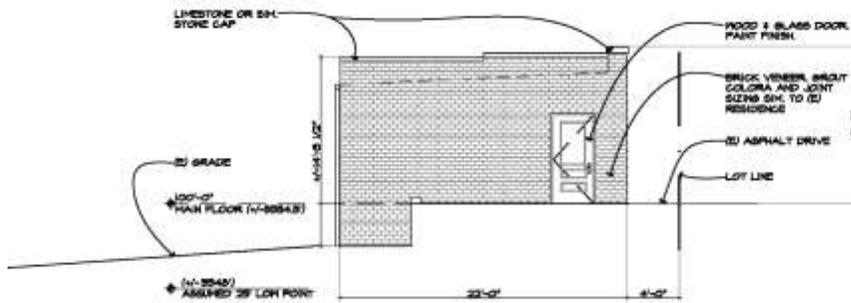


Figure 17: Proposed East Elevation

The east elevation mirrors the west elevation, and is shown to be simply detailed, with a half-light wood pedestrian door.

A fence is proposed to enclose the area between the house and garage. A fence currently surrounds the brick patio in this location. On the west side, the proposed fence would extend from the southwest corner of the garage to the rear of the house, and extend approximately 90' along the east property line, continuing west and connecting to the house approximately 50' from Pine Street. Details regarding the height and material of the fence were not included in this application.

A trash enclosure is shown along the alley. The structure would be 7' by 7' and measure 6' tall. The wood enclosure would be painted.

CRITERIA FOR THE BOARD'S DECISION

Subsection 9-11-18(b) and (c), B.R.C. 1981, sets forth the standards the Landmarks Board must apply when reviewing a request for a Landmark Alteration Certificate.

(b) Neither the Landmarks Board nor the City Council shall approve a Landmark Alteration Certificate unless it meets the following conditions:

- (1) The proposed work preserves, enhances, or restores and does not damage or destroy the exterior architectural features of the landmark or the subject property within an historic district;
- (2) The proposed work does not adversely affect the special character or special historic, architectural, or aesthetic interest or value of the landmark and its site or the district;
- (3) The architectural style, arrangement, texture, color, arrangement of

- color, and materials used on existing and proposed constructions are compatible with the character of the existing landmark and its site or the historic district;
- (4) With respect to a proposal to demolish a building in an historic district, the proposed new construction to replace the building meets the requirements of paragraphs (b)(2) and (3) above.
- (c) In determining whether to approve a landmark alteration certificate, the Landmarks Board shall consider the economic feasibility of alternatives, incorporation of energy-efficient design, and enhanced access for the disabled.

ANALYSIS

1. Does the proposed application preserve, enhance, or restore, and not damage or destroy the exterior architectural features of the landmark or the subject property within a historic district?

Staff considers that while the construction of a four car garage is uncommon on designated properties, the proposed garage is appropriate due to the property's location in the Residential-Mixed 1 zoning district and the precedent of a historic garage on the property, which measured 21' wide, 40' long and 10' high. The proposed garage is shown to be 22' wide, 41'10" long and 12' to the top of the parapet. Staff considers that proposed construction of a garage will not damage or destroy the exterior architectural features of the designated property.

2. Does the proposed application adversely affect the special character or special historical, architectural, or aesthetic interest or value of the district?

The staff finds that the proposed construction of a four-car garage will not adversely affect the special character or special historic, architectural, or aesthetic interest or value of the landmark property as it will be generally compatible with the *General Design Guidelines* in terms of mass, scale, height, design and color (see *Design Guidelines Analysis* section). Additional details regarding the height, design and material of the proposed fence and trash enclosure should be reviewed by the Landmarks design review committee.

3. Is the architectural style, arrangement, texture, color, arrangement of color, and materials used on existing and proposed structures compatible with the character of the historic district?

Staff finds that the architectural style, arrangement, texture, color, arrangement of color, and materials used on the proposed building and will be generally

compatible with the character of the historic district in terms of mass, scale, height, and design (see Design Guidelines Analysis section). The design references the four-car garage that was on the property from 1940 until 1981 in terms of size, scale and use of masonry.

4. Does the proposal to demolish the building and the proposed new construction to replace the proposed demolished building meet the requirements of paragraphs 9-11-18(b)(2), 9-11-18(b)(3) and 9-11-18(b)(4) of this section?

Not applicable.

DESIGN GUIDELINES ANALYSIS:

The Historic Preservation Ordinance sets forth the standards the Landmarks Board must apply when reviewing a request for a Landmark Alteration Certificate. The Board has adopted the *General Design Guidelines* to help interpret the historic preservation ordinance. The following is an analysis of the proposed new construction with respect to relevant guidelines. Design guidelines are intended to be used as an aid to appropriate design and not as a checklist of items for compliance.

The following is an analysis of the proposal’s compliance with the appropriate sections of the *General Design Guidelines*.

GENERAL DESIGN GUIDELINES

SITE DESIGN 2.0

2.1 Building Alignment, Orientation and Spacing			
	Guideline	Analysis	Meets Guideline?
.1	<i>Located structures within the range of alignments seen traditionally in the area, maintaining tradition setbacks at the front, side and rear of the property.</i>	Proposed garage maintains traditional rear yard setback.	Yes
.2	<i>In neighborhoods with alleys, garages should be located at the rear of the lot and accessed from the alley</i>	Proposed garage located at rear of lot and accessed by the alley.	Yes
.3	<i>Preserve a backyard area between the house and the garage, maintaining the general proportion of built mass to open space found within the area</i>	Proposed garage references location, size and scale of garage located on the property from 1940 until 1981. Due to large lot and orientation of garage along alley, proportion of built mass to open space will be maintained and backyard area will be preserved.	Yes

2.3 Alleys			
	Guideline	Analysis	Meets Guideline?
.1	<i>Maintain alley access for parking and retain the character of alleys as clearly secondary access to properties.</i>	Alley access is maintained.	Yes
.3	<i>The use of historically proportioned materials for building new accessory buildings contributes to the human scale of the alleys.</i>	Proposed use of brick and lap siding appropriate.	Yes
.5	<i>Maintain adequate spacing between accessory buildings so that the view of the main house is not obscured, and the alley does not evolve into a tunnel-like passage.</i>	View of main house will be maintained, most critically from the northwest corner of the property (15 th St.).	Yes
2.4 Parking and Driveways			
	Guideline	Analysis	Meets Guideline?
.1	<i>Maintain the traditional pattern of parking at the rear of the lot</i>	Parking at rear of lot maintained.	Yes
.2	<i>Access to parking should be from the alleys whenever possible</i>	Parking at rear of lot maintained.	Yes
.3	<i>The visual impact of parking for multi-family and commercial uses should be minimized. Common approaches include separating parking into small clusters, screening with buildings or landscaping, and the use of small accessory buildings.</i>	Proposed garage references four-car garage that was historically on the property (1940-1981). Building proposed at existing parking area.	Maybe
2.6 Fences			
<i>The appearance of the house from the sidewalk, street and alley contributes to an area's character. Historically, fences were not common in Boulder. Where they existed, they were very open, low, and used to delineate space rather than create walled-off privacy areas. Rear and side yard fences were built low enough so neighbors could talk to each other over them. The fences could be easily seen through and were built of woven wire (not chain-link), wrought iron, or painted or opaque stained wood pickets. Elaborate wrought iron and cast iron fences were typically found only on lots with large or grand homes.</i>			
	Guideline	Analysis	Meets Guideline?
.1	<i>Retain and preserve historic fences that contribute to the historic character of the site or district whenever possible. Repair deteriorated fence components rather than replace them.</i>	Existing fence believed to have been constructed in the 1980s; additional information is needed.	Maybe
	<i>New fencing should reflect the character of historic fences in height, openness, materials, and finish.</i>	Details regarding height, openness, materials and finish should be reviewed by the Landmarks design review committee (Ldrc)	Maybe

	<p><i>Where appropriate, fences should be no more than 36 inches high. This low height should be maintained along the side yard as far as necessary to maintain an unobstructed view of the building's main architectural features, at least to the front elevation of the house and/or porch. At that point, the fence may become gradually higher and less open.</i></p>	<p>Details regarding height, openness, materials and finish should be reviewed by the Landmarks design review committee (Ldrc)</p>	<p>Maybe</p>
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PROPOSED NEW CONSTRUCTION

<p>7.0</p>	<p>Garages & Other Accessory Structures</p>
	<p><i>Accessory buildings include barns, sheds, garages and outbuildings. Originally accessory structures were used for storage of equipment, animals, or carriages. Generally, these structures have been adapted for the storage of cars. In most cases, accessory building were located to the rear of the lot and accessed by alleys. They were subordinate in size and detailing to the primary house. Over time they have emerged as important elements of many lots and alleys in the district. Efforts should be made to protect the eclectic character of alleys.</i></p> <p><i>Both additions to existing accessory buildings and new accessory building will be evaluated in terms of how they affect the historic character of the individual site and the district as a whole. In the past, larger accessory structures have been allowed than may be appropriate today.</i></p>

<p>7.2</p>	<p>New Accessory Buildings</p>
	<p><i>New accessory buildings should follow the character and pattern of historic accessory structures. While they should be take design clues from the primary structure, they must be subordinate to the primary structure in size, massing and detailing. Alley buildings should maintain a scale that is pleasant to walk along and comfortable for pedestrians.</i></p>

	<p>Guideline</p>	<p>Analysis</p>	<p>Meets Guideline?</p>
<p>.1</p>	<p><i>It is inappropriate to introduce a new garage or accessory building if doing so will detract from the overall historic character of the principal building and the site, or if it will require the removal of a significant historic building element or site feature, such as a mature tree.</i></p>	<p>The proposed construction reflects the historic relationship of accessory buildings located on the site. The design references the 4-car garage that was previously on the site in terms of location, size, scale and materials. The construction will not require the removal of an existing historic building element or site feature.</p>	<p>Yes</p>
<p>.2</p>	<p><i>New garages and accessory buildings should generally be located at the rear of the lot, respecting the traditional</i></p>	<p>The building location at northeast corner of lot is consistent with historic patterns of accessory buildings on the</p>	<p>Yes</p>

	<i>relationship of such buildings to the primary structure and the site.</i>	property.	
.3	<i>Maintain adequate spacing between accessory buildings so alleys do not evolve into tunnel-like passageways.</i>	Property is not located within a historic district; proposed garage located at northeast corner and will maintain open area to west of garage.	Yes
.4	<i>Preserve a backyard area between the house and the accessory buildings, maintaining the general proportion of built mass to open space found within the area.</i>	Proposed garage extends approximately 8' from the existing paved parking area. The design references the historic relationship of the house and accessory buildings at the northeast corner of the lot. The 1940 four-car garage was oriented perpendicular to the proposed garage, however, the current orientation allows for greater spacing between the house and garage.	Yes
.5	<i>New accessory structures should take design cues from the primary structure on the site, but be subordinate to it in terms of size and massing.</i>	The new one-story building is generally compatible with the three-story primary building in terms of size and massing.	Yes
.6	<i>New garages for single-family residences should generally be one story tall and shelter no more than two cars. In some cases, a two-car garage may be inappropriate.</i>	Proposed one-story garage references historic four-car garage that was on the property when the house was divided into multiple residential units. The applicant plans to convert the house from its current office to its historic residential use.	Maybe
.7	<i>Roof form and pitch should be complimentary to the primary structure.</i>	The flat roof form reduces the profile of the proposed building and references the roof line of the garage that was on the site until 1981.	Yes
.8	<i>Accessory structures should be simpler in design and detail than the primary building.</i>	The proposed garage is simpler than house in scale and detail.	Yes
.9	<i>Materials for new garages and accessory structures should be compatible with those found on the primary structure and in the district. Vinyl siding and prefabricated structures are inappropriate.</i>	Materials as proposed, including brick, wood lap siding and wood doors are generally appropriate. Provide material/color details to Ldrc for review and approval.	Yes

.10	<i>Windows, like all elements of accessory buildings, should be simpler in detailing and smaller in scale than similar elements on primary structures.</i>	Windows are not proposed on the garage.	N/A
.11	<i>If consistent with the architectural style and appropriately sized and located, dormers may be an appropriate way to increase storage space in garages.</i>	Dormers are not proposed.	N/A
.12	<i>Garage doors should be consistent with the historic scale and materials of traditional accessory structures. Wood is the most appropriate material and two smaller doors may be more appropriate than one large door.</i>	Proposed overhead carriage doors appear to be consistent in terms of scale and materials. Review final details at Ldrc.	Maybe
.13	<i>It is inappropriate to introduce features or details to a garage or an accessory building in an attempt to create a false historical appearance.</i>	Proposed design does not attempt to create a false historic appearance.	Yes
.14	<i>Carports are inappropriate in districts where their form has no historic precedent.</i>	Carport not proposed.	N/A

8.3 Mechanical and Utility Facilities			
	Guideline	Analysis	Meets Guideline?
.5	<i>Minimize the visual impacts of trash storage and service areas by screening them from the street.</i>	Trash enclosure (7'x7'x6') proposed at west side of proposed garage. Details should be reviewed by the Ldrc.	Maybe

Staff considers the proposed construction of a four-car garage to be generally consistent with the Historic Preservation Ordinance and Sections 2.0, Site Design and 7.2, New Accessory Buildings, of the *General Design Guidelines* in terms of site design, orientation, materials and detailing. Details should be reviewed and approved by the Landmarks design review committee (Ldrc).

The construction of a four-car garage is rare for individually designated properties and those located within a historic district. However, the proposed design references the mass, size, scale and materiality of the garage that was on the property from 1940 until 1981. Prior to the construction of the four-car garage

in 1940, two accessory buildings were located at the same location. The proposed four-car garage reflects the historic relationship between the house and accessory building and the ratio of built mass to open space on the lot. The one-story height makes the building clearly secondary to the three-story main house. Simple in detail, the garage does not seek to create a false sense of history. Overall, the design of the proposed garage is compatible with the historic character of the individual landmark.

Staff considers the design for the trash enclosure at the rear of the lot and for the fence between the house and garage to be generally appropriate, but that additional details regarding height, openness, materials and finish be reviewed by the Ldrc.

PUBLIC COMMENT

Staff has received no public comment regarding this case.

FINDINGS:

Provided the conditions outlined in the staff recommendation are met, staff recommends that the Landmarks Board approve the application and adopt the following findings:

1. The proposed new construction meets the standards in 9-11-18 of the Boulder Revised Code 1981.
2. The proposed garage will not have an adverse effect on the value of the landmark property, as it will be generally compatible in terms of mass, scale, or orientation with other buildings in the district.
3. In terms of mass, scale, and orientation the proposed new garage will be generally consistent with Section 9-11-18, B.R.C. 1981, and Section 7 of the *General Design Guidelines*.

ATTACHMENTS:

- A: Tax Assessors Card
- B: Photographs
- C: Application
- D: Plans

Memo to the Landmarks Board
Re: Landmark Alteration Certificate for 1507 Pine St.



Attachment B: Photographs



Photo 1. 1507 Pine St., view of south (front) elevation, 2014.



Photo 2. 1507 Pine St., view of east elevation, 2014.



Photo 3. 1507 Pine St., view of west elevation, 2014.



Photo 4. 1507 Pine St., view of north elevation, 2014.

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Photo 5. 1507 Pine St., view of parking area at north side of building, 2014.



Photo 6. 1507 Pine St., view of parking area from northwest corner, 2014.

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Re: Landmark Alteration Certificate for 1507 Pine St.



Photo 7. 1507 Pine St., view of entrance into lawn on north side of building, 2014.



Photo 8. 1507 Pine St., view of lawn at northeast corner of property, 2014.

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Photo 9. 1507 Pine St., fence on east side of property, 2014.



Photo 10. 1507 Pine St., view of adjacent accessory building, 2014.

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Photo 11. 1507 Pine St., close-up of adjacent accessory building, 2014.



Photo 12. 1507 Pine St., view from southwest corner of property, 2014.

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Photo 13. 1507 Pine St., view looking north up 15th St, 2014.

Attachment C: Application



The property at 1507 Pine Street is an individual landmark named the Temple-Bowron House. The Temple-Bowron House is located in the Wittier neighborhood. It is not located in any historic district. The house has a very commanding presence on the NE corner of the intersection of 15th and Pine streets. The property is zoned RMX-1. There are currently two residential apartment units on the third floor and the first and second floors have multiple business offices. The applicant currently has an offer on the property contingent on Landmark Board comments and inspection reports as to the condition of the building. If the applicant purchases the property, they propose to return the home to a two floor single owner occupied residence with a single third floor residential unit. This proposed configuration is allowed by-right. In addition to returning the home to its original residential usage, the applicant proposes to reconstruct the original front porch based on photographic evidence. Proposed changes to the house for residential usage have already been reviewed by the DRC. Given the expense involved to rehabilitate the house and the location, it will be important to provide two secured, covered parking spaces for each unit.

In 1981 as a condition to convert the first two stories to office space, a four car garage was removed from the Northeast corner of the site. The former 4 car garage was accessed from the alley and from a curb cut on 15th street. The doors faced west, with the parking taking up much of the northern portion of the yard. See the 1981 existing drawing before the garage was torn down:



1926 14th St. #200
Boulder CO 80502
Phone 303 448 5747
Fax 303 447 2843
klarch@indra.com



When the offices were remodeled, a new parking area was provided at the northern lot line of 1507 Pine Street. This parking area remains as the current configuration. It is built-up three feet from the grade adjacent to the residence to be flush with the alley. The current proposal is to build a one-story brick 4-car garage at the same northeast corner similar to the previous garage. The new garage will be accessed from the alley with the doors facing North utilizing the current parking configuration. It is situated as far north to the lot line as possible, while still providing the 24' required backup. The proposed structure is brick veneer and frame construction with carriage house style overhead doors. The 4-car garage will serve both the two-floor single family residence and the third floor rental unit. Three other guest compact parking spaces will remain at the northern parking area and 12.5' of existing parking pad at the east edge of the lot will be removed and new landscaping will be provided to act as a buffer along Pine Street.

General Design Guidelines:

Site Design:

1. The proposed garage retains a similar location as the previous 4-car garage building, in the NE corner of the lot. It is just under 42' wide and 22' deep. The new building will sit about 4' back from the surveyed edge of alley and this setback will be consistent with other accessory buildings located on this alley.
2. The garage will be located approximately 24' from the back of the house. This spacing is consistent with the rest of the homes on the block.
3. The existing conc. Parking pad will remain for guest parking. A 12.5' wide swatch of existing pavement will be removed to provide a landscape buffer along Pine Street.

Parking & Driveways:

1. The parking will continue to be located at the northern edge of the lot.
2. The proposed garage will maintain alley access.
3. The existing conc. Parking will remain per plan. The new garage will be set back 4' from the lot line with a concrete apron.

New Accessory Structures:

1. The proposed garage maintains the historic character of the existing property. The proposed location of the new four-car garage is situated in the same corner of the lot as the previously torn down structure. The proposed garage maintains alley access and is to be constructed out of brick veneer and lap siding which are consistent with other one-story historic garages that serve multi-family residences around boulder. See attached examples.
2. The new garage is to be located at the rear of the lot. It has been placed so as to allow for the minimal back-up required by the city.

1928 14th Bl. #200
Boulder CO 80502
Phone 303-449-5747
Fax 303-447-2949
klaroh@indra.com



3. The south wall of the new garage is located approximately 5' further to the south than the existing parking area retaining wall. There will be 24' spacing between the accessory building and residence. A minimal 20'x20' parking area will be provided for each unit (4 spaces total).
4. The garage is pushed as far north as possible to maintain a similar relationship of building and parking as currently exists on the property.
5. The proposed garage is subordinate to the main residence in mass, height and scale. The location and materials are to be brick and lap siding. The proposed garage also maintains a similar setback to other buildings located on the alley.
6. The proposed garage is a one story, flat roof four-car garage. See attached photos for historic precedent.
7. The proposed roof is flat with a minimal slope to allow for proper drainage. There is a parapet that will disguise this flat roof from the alley. The proposed parapet is approximately 18" below the maximum allowable garage building height based on the 20' height limitation for accessory buildings. It is in keeping with the other historic roof pitches for multi-family residences around Boulder.
8. The proposed garage is brick veneer construction with lap siding accents and EPDM roofing.
9. The proposed brick veneer is appropriate for this individual landmark and will complement the existing brick structure.
10. The proposed structure has simple access doors on the east and west elevations.
11. The proposed design maintains the simplicity of form found in existing historic structures located throughout Boulder.
12. The garage door is a carriage door style, will be wood and is scaled similar to other garage doors in historic districts around town.

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Historic Garage Examples:



Broadway & Maxwell



Broadway & Maxwell

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Memo to the Landmarks Board
Re: Landmark Alteration Certificate for 1507 Pine St.

kristin lewis
Architects



Mapleton (Between Broadway and 9th)



5th and Marine

1928 14th St. #200
Boulder CO 80302
Phone 303 449.5747
Fax 303 447.2643
klarch@indra.com

Memo to the Landmarks Board
 Re: Landmark Alteration Certificate for 1507 Pine St.

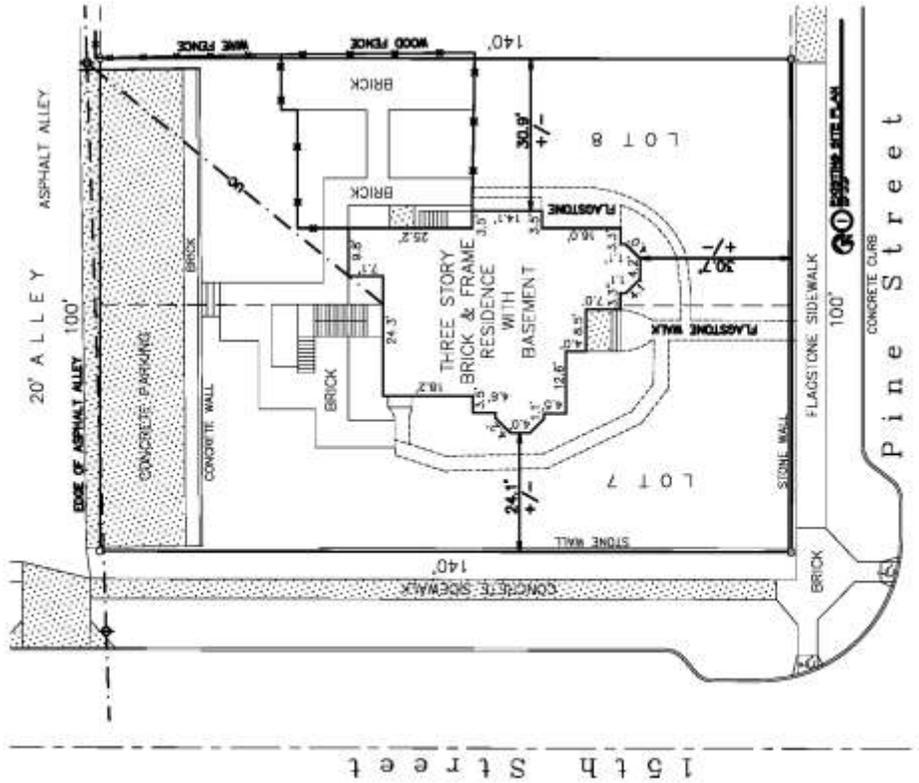
Attachment D: Plans

DATE	BY	PROJECT	NO.	SCALE	DATE	BY	PROJECT	NO.	SCALE	DATE	BY	PROJECT	NO.	SCALE

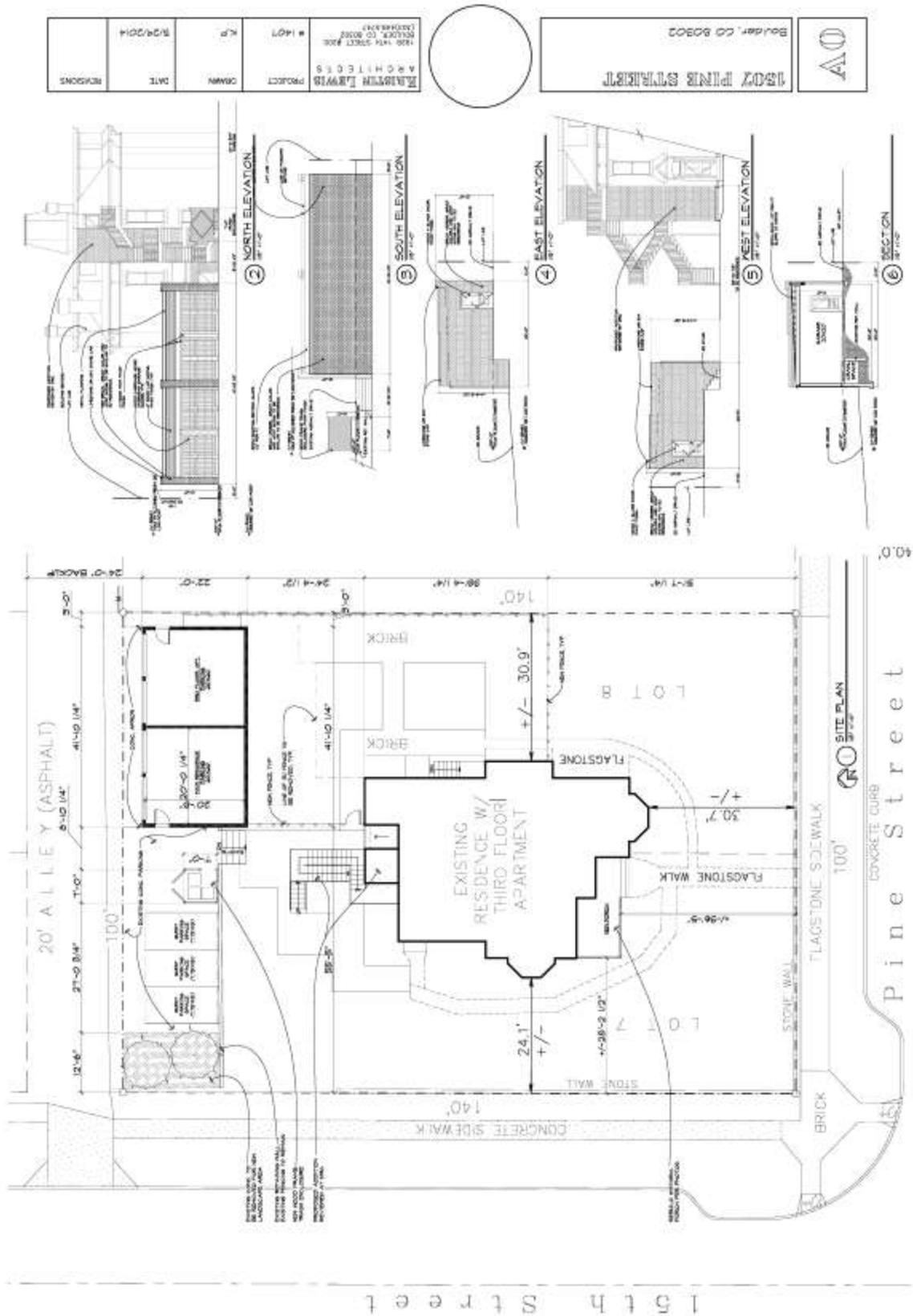
1507 PINE STREET

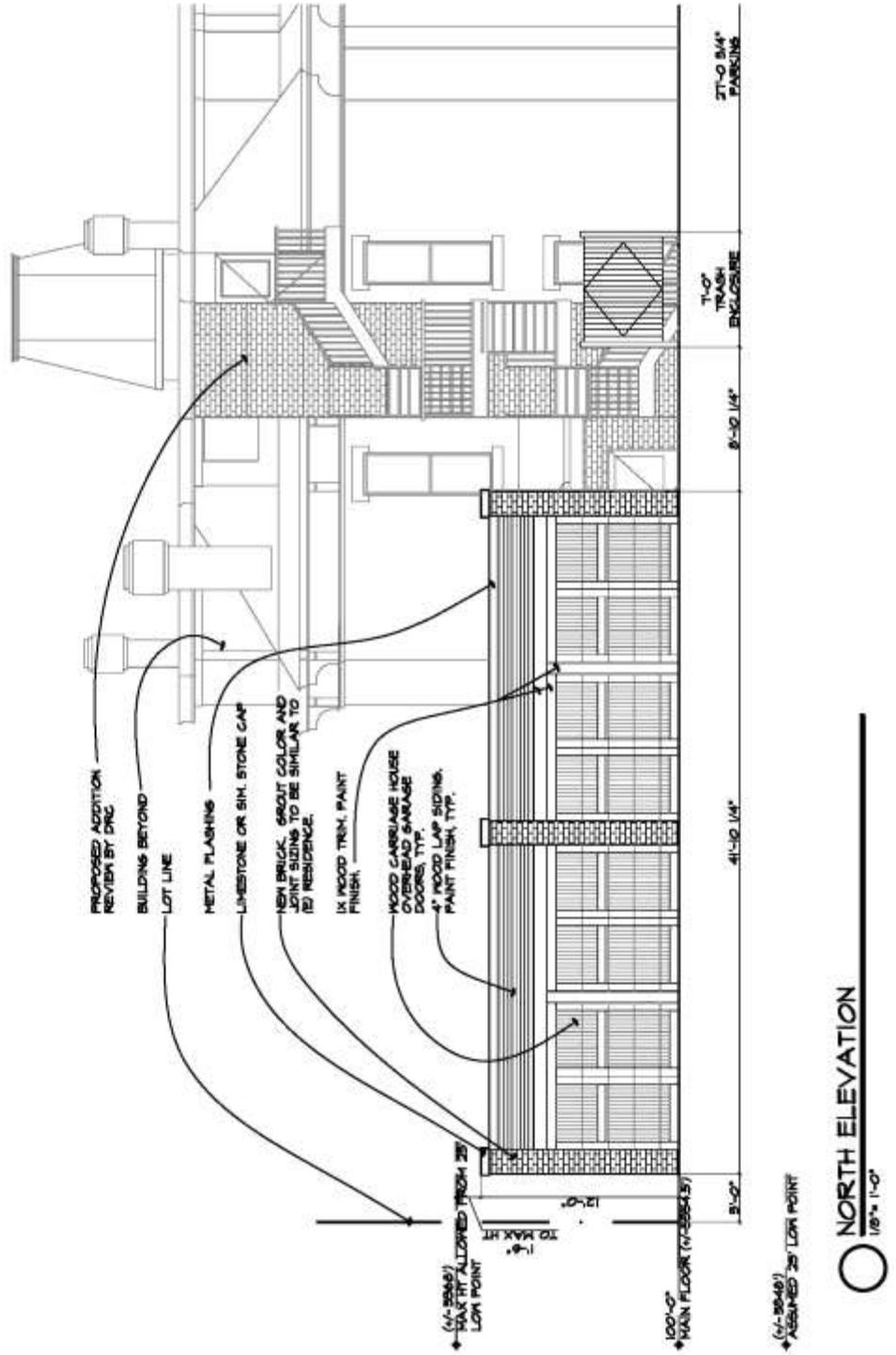
1507 PINE STREET

A0

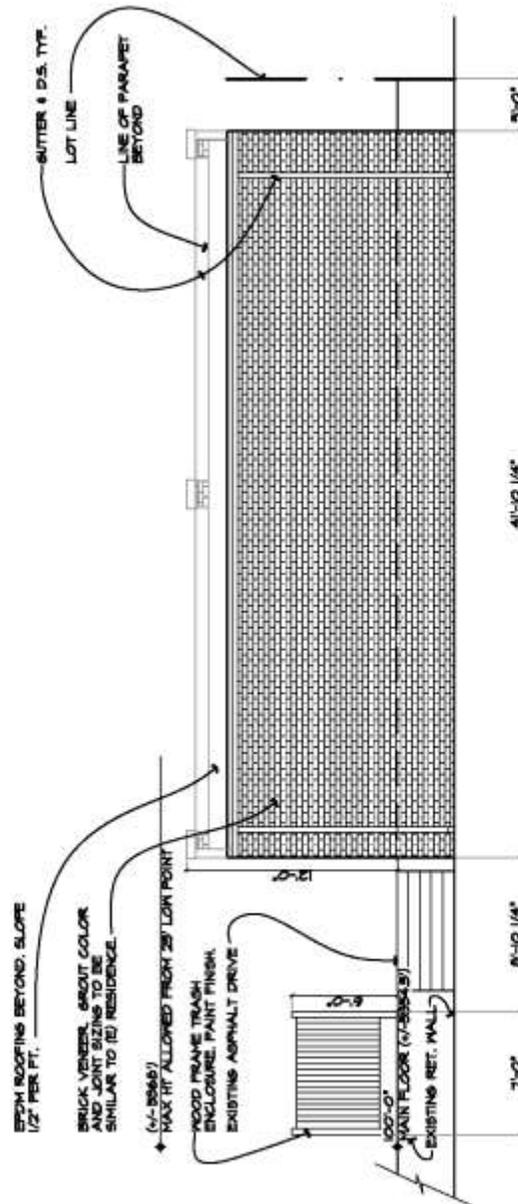


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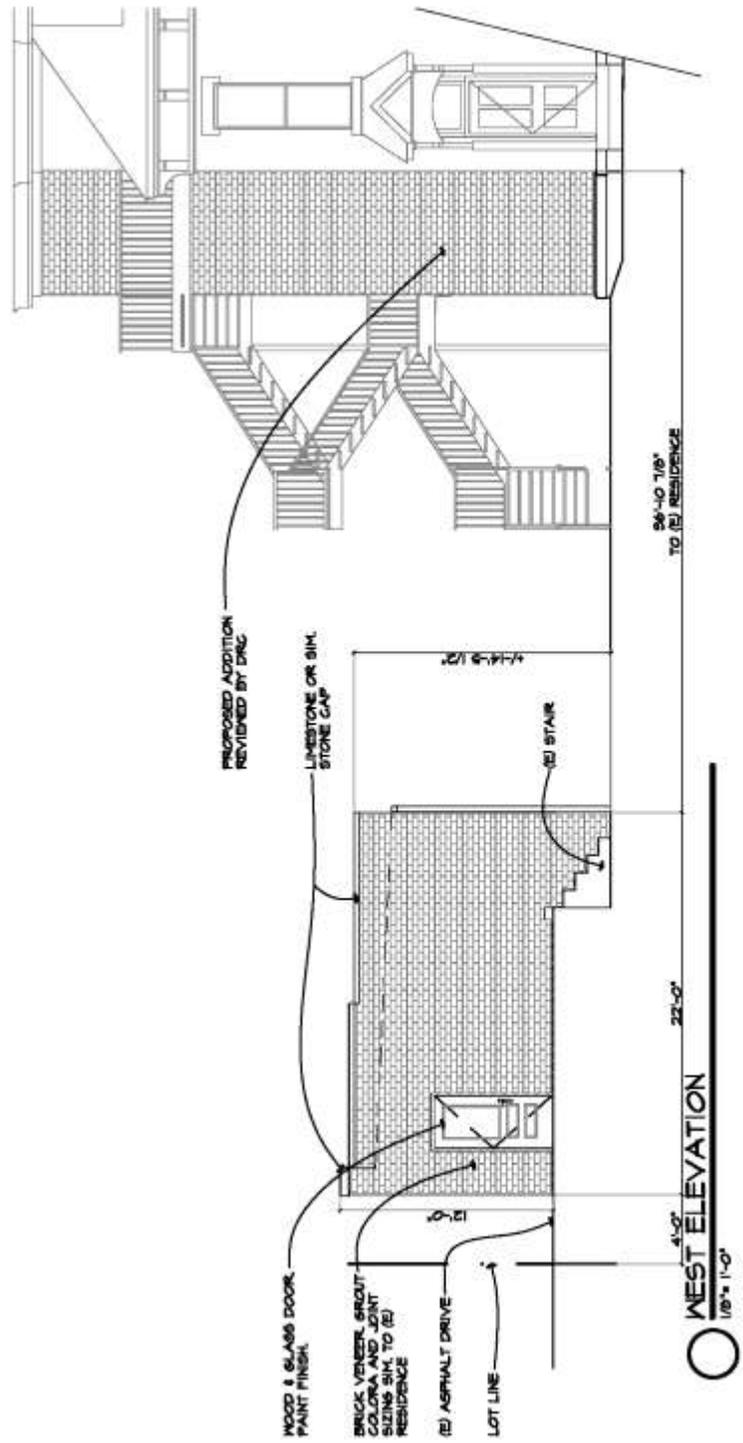


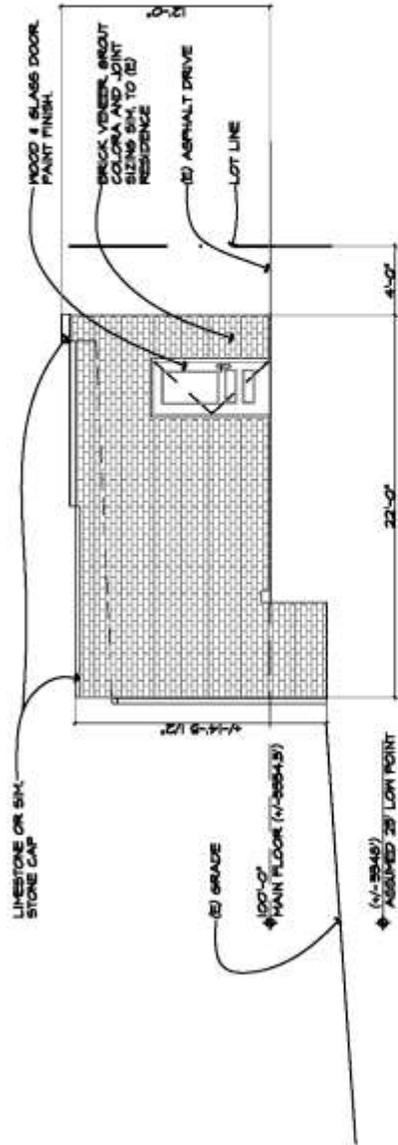


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SOUTH ELEVATION
1/8" = 1'-0"





EAST ELEVATION
1/8" = 1'-0"