

**M E M O R A N D U M**  
**July 2<sup>nd</sup>, 2014**

**TO:** Landmarks Board

**FROM:** Lesli Ellis, Comprehensive Planning Manager  
Debra Kalish, Senior Assistant City Attorney  
James Hewat, Senior Historic Preservation Planner  
Marcy Cameron, Historic Preservation Planner  
Angela Smelker, Historic Preservation Intern

**SUBJECT:** Public hearing and consideration of an application for a Landmark Alteration Certificate to construct a one-story, 425 sq. ft., two-car garage, remove a portion of the contributing garage, and construct a fence along the north, south, east and property lines at 730 Pine St. in the Mapleton Hill Historic District, per section 9-11-18 of the Boulder Revised Code 1981 (HIS2014-00087).

**STATISTICS:**

- |    |                              |                               |
|----|------------------------------|-------------------------------|
| 1. | Site:                        | 730 Pine St.                  |
| 2. | Zoning:                      | RL-1 (Residential Low-1)      |
| 3. | Owner:                       | Douglas and Jennifer Campbell |
| 4. | Site Area:                   | 7,173 sq. ft.                 |
| 6. | Existing Accessory Building: | Approximately 225 sq. ft.     |
| 7. | Proposed Garage:             | 425 sq. ft.                   |
| 8. | Proposed Garage Height:      | 15', 10"                      |

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**STAFF RECOMMENDATION:**

The proposal to construct a new accessory building, remove a portion of the contributing garage, and construct a fence along the north, south, east and west property lines is appropriate, as it generally meets the standards as set out in Subsection 9-11-18(b) and (c), B.R.C. 1981 if it complies with the Conditions of Approval. Staff recommends the Landmarks Board adopt the following motion and conditions:

*I move that the Landmarks Board adopt the staff memorandum dated July 2<sup>nd</sup>, 2014, as the findings of the Board and approve the application for a Landmarks Alteration Certificate for the removal of a portion of the contributing accessory building, construction of a garage and construction of fences along the south and west property lines at 730 Pine St. as shown on plans dated 05.27.2014, subject*

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*to the following conditions:*

**CONDITIONS OF APPROVAL:**

1. The applicant shall be responsible for ensuring that the development will be constructed in compliance with the application dated 05.27.2014 on file in the City of Boulder Community Planning and Sustainability Department, except as modified by these conditions of approval.
2. Prior building permit application and final issuance of the Landmark Alteration Certificate, the applicant shall submit revised plans for the proposed fence so that the height of the fence around the front of yard be no more than 30" and of a very open character in a manner consistent with *General Design Guidelines for Boulder's Historic Districts and Individual Landmarks* and the *Mapleton Hill Design Guidelines*.
3. Prior to submitting a building permit application and final issuance of the Landmark Alteration Certificate, the applicant shall submit the following: final details showing door and window details, roofing materials, siding, paving and proposed colors. These design details shall be reviewed and approved by the Landmarks design review committee, prior to the issuance of a building permit. The applicant shall demonstrate that the design details are in compliance with the intent of this approval and the *General Design Guidelines* and the *Mapleton Hill Historic District Design Guidelines*.

This recommendation is based upon staff's opinion that, if constructed in accordance with the conditions of approval, the proposed installation will be consistent with Section 9-11-18, B.R.C. 1981, the *Mapleton Hill Historic District Design Guidelines* and the *General Design Guidelines for Boulder's Historic Districts and Individual Landmarks*.

**SUMMARY:**

- On March 5<sup>th</sup>, 2014, the Landmarks Board reviewed an application to demolish the contributing garage on the property and construct a new two-car garage at 730 Pine St. After a majority of board members indicated they would not support the request, the applicant withdrew the application at the meeting.
- On May 7<sup>th</sup>, 2014, the Landmarks Board reviewed a revised proposal to retain the existing garage and construct a new, two-story garage. After a majority of board members indicated they would not support the request, the applicant withdrew the application to redesign the project reducing the height and mass of the proposed accessory building per the Board's comments.

- Because the application calls for new free-standing construction of more than 340 sq. ft., review by the full Landmarks Board in a quasi-judicial hearing is required pursuant to Section 9-11-14(b), B.R.C. 1981.
- The current application proposes to retain the contributing accessory building, which was constructed in 1941, within the period of significance of the Mapleton Hill Historic District (1865-1946).
- The contributing accessory building has not been significantly altered over the years and was recommended as contributing to the Mapleton Historic District in the 2005 Accessory Building Survey. For this reason, staff considers the accessory building a contributing resource to the Mapleton Hill Historic District in that it was constructed within the period of significance, has not been significantly altered, and adds to the architectural diversity of the immediate streetscape and to the Mapleton Hill Historic District as a whole.
- Staff recommends that Landmarks Board approve the application with the listed conditions.



*Figure 1. Location Map, 730 Pine St.*

**PROPERTY DESCRIPTION:**

The property at 730 Pine St. is located on the south side of Pine St. between 7<sup>th</sup> and 8<sup>th</sup> streets. An alley runs along the east and south sides of the property, an unusual condition for Mapleton Hill. The original Minimal-Traditional house (see *Figure 2*) was constructed in 1941 and featured a side gable roof, projecting front gable, six-over-six double-hung windows and wide board siding. The house appears to have remained relatively unchanged until 1992, when a hipped-roof, second-story and one-story rear addition were constructed. A Landmark Alteration Certificate indicates the remodel was reviewed and approved by the Historic Preservation program. At the time, the house would not have been considered contributing given the Mapleton Hill Historic District's period of significance ran from 1865-1930 at that time. In 2001, the districts period of significance was extended to 1946, the end of World War II.



*Figure 2. 730 Pine St., Tax Assessor Photograph, c. 1949*



*Figure 3. 730 Pine St., Main House, 2013*

A 225 sq. ft. accessory building, also constructed in 1941, is located along the east property line, behind the main house and prominently situated on the east and west alleys. The one-story, front-gabled building features wide board wood lap-siding, shallow eaves, and narrow wood trim. A garage door opening is located on the north elevation taking access off of the side alley, and a small shed addition with a pedestrian door is located at the southwest corner. The 2005 Accessory Building survey form identifies the building as being constructed around 1941, in good condition and as contributing to the Mapleton Hill Historic District. The replacement of the garage door appears to have been the only alteration to the building. *See Attachment A: Historic Building Inventory Form.*



*Figure 4. 730 Pine St. Accessory Building, northeast corner, 2014.*



*Figure 5. 730 Pine St. Accessory Building, southeast corner, 2014*



*Figure 6. 730 Pine St. Accessory Building, southwest corner, 2014*



*Figure 7. 730 Pine St. Accessory Building, west wall, 2014*

The General Design Guidelines define **contributing buildings** as “those buildings built during the district’s period of significance that exist in comparatively original condition, or that have been appropriately restored, and clearly contribute to the historic significance of the district. Such buildings may have compatible additions.”

**Non-contributing buildings** are defined as “those buildings built during the district’s period of significance that has been altered to such an extent that historic information is not interpretable and restoration is not possible. This includes buildings erected outside the period of significance that are not individually significant.”

Although the garage at 730 Pine St. is a modest Minimal-Traditional building that no longer relates stylistically to the main house on the property, staff considers that due to its 1941 date of construction, relative lack of exterior changes, and prominence on the alley-scape, it should be considered contributing to the historic character of the Mapleton Hill Historic District.

**PROPOSED DEMOLITION AND NEW CONSTRUCTION:**

The applicant proposes to rehabilitate the existing garage and construct a new one and one-half story, 715 sq. ft. garage to shelter two cars.



*Figure 8. Rendering of proposed garage (left) and garage (right)*

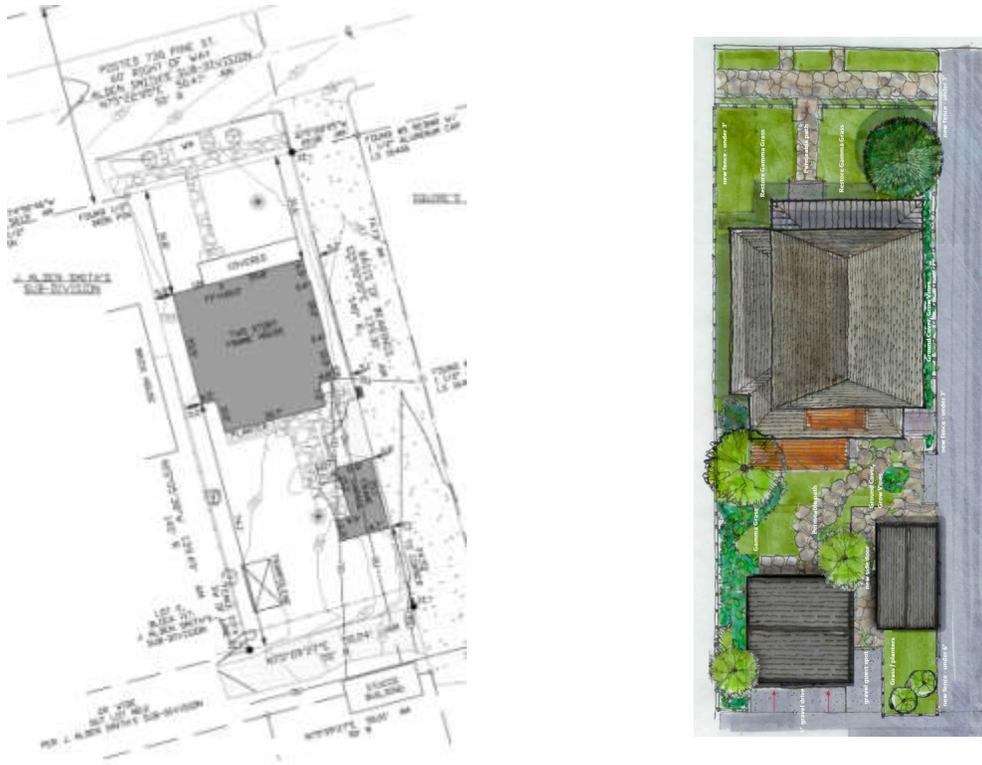


Figure 9. Existing (l) and proposed (r) site plans.

In plan, the proposed garage measures 22 ft. 6 in. by 22 ft. and is shown to be located toward the rear of the property, with alley access to the south. It is positioned approximately 37 ft. from the rear of the primary house, 5 ft. 3 in. from the west property line, 22 ft. from the east property line (alley side) and 5 ft. 4 in. from the south property line (alley side). Plans show a free-standing trellis to be located between the contributing garage and new accessory building. A rear deck, approved under a separate Landmark Alteration Certificate, is to be located at the rear of the primary house. Approximately 20 ft. of landscaped area would separate the proposed garage and rear deck on the house.

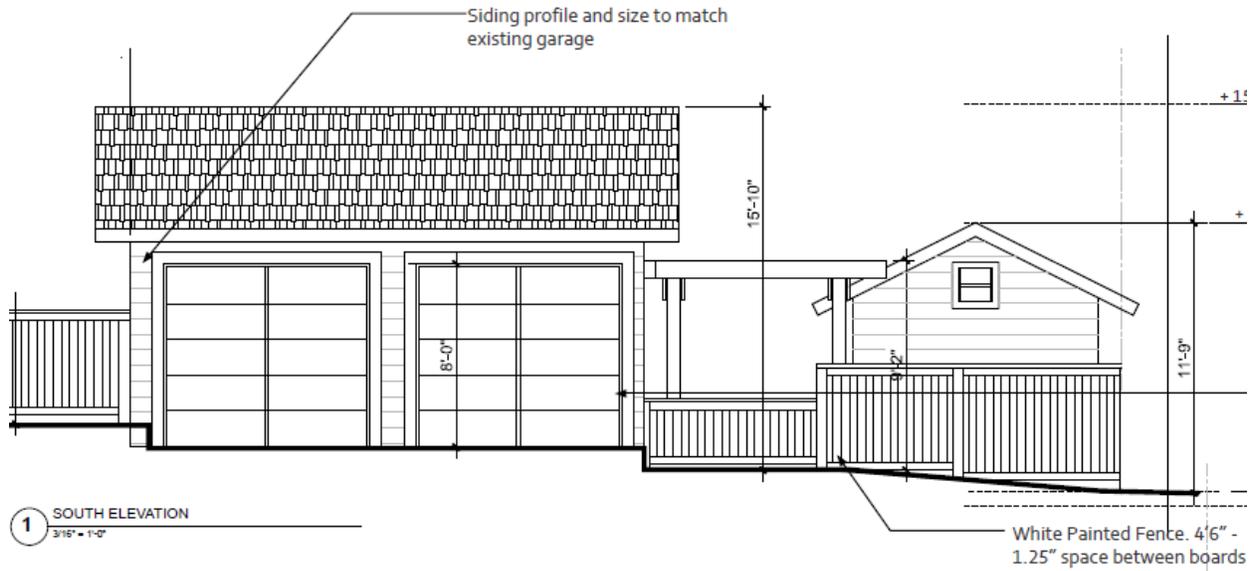


Figure 10. Proposed south elevation

The garage is proposed to have simple a one- story side gable form with the south elevation featuring two multi-panel garage doors.

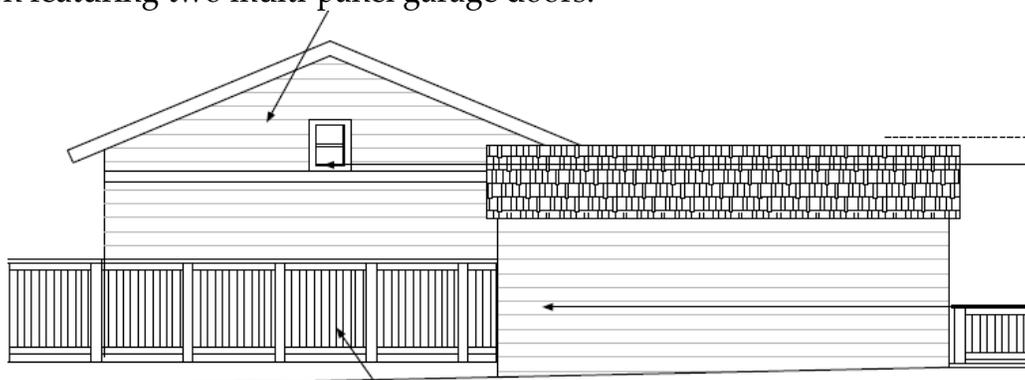


Figure 11. Proposed east elevation (facing alley)

The proposed east elevation of the building features a double-hung window on the upper level.

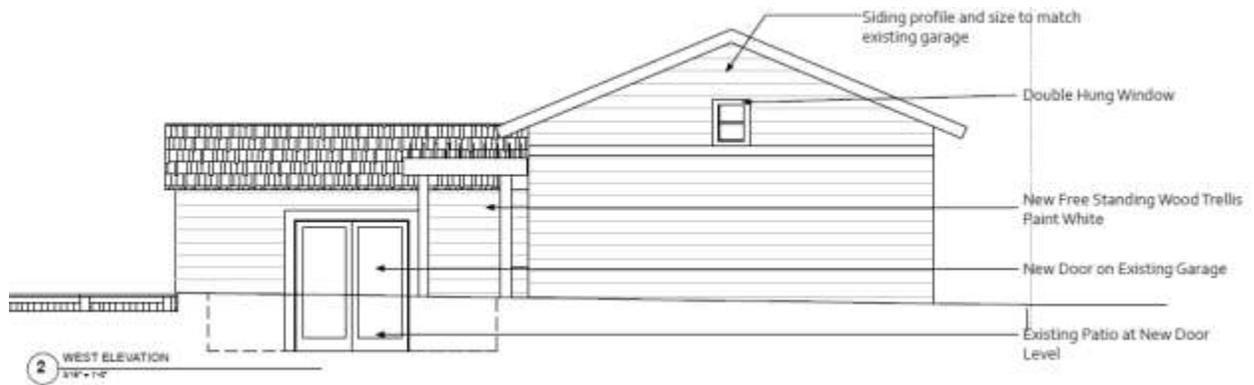


Figure 12. Proposed west elevation

The proposed west elevation is shown to also feature a double-hung window on the gable matching that on the east face of the garage. The building is shown to be sided with wide clapboard siding similar to that on the existing contributing garage.

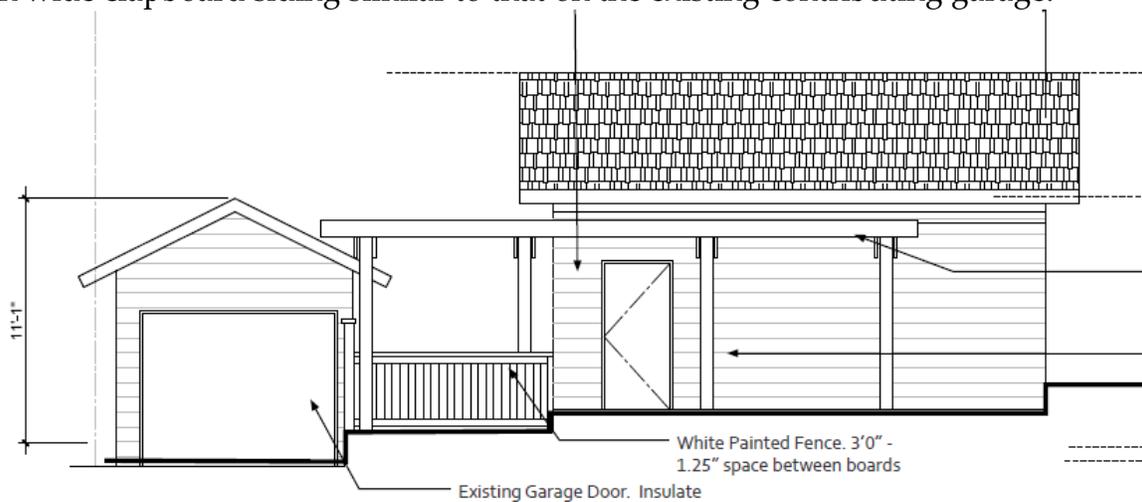


Figure 13. Proposed north elevation (interior lot)

The proposed north elevation, facing the rear of the main house, features a pedestrian door while a three bay pergola is shown to be located between the contributing garage and proposed new garage.



Figure 14. Portion of contributing garage proposed for removal (hatched line)

Plans also call for the removal of a small shed-roof portion at the southwest corner of the contributing garage. This appendage appears to be original to the construction of the garage, but it is located on a secondary (or possibly tertiary) elevation of the building. A new opening with two new doors is shown on the west wall of the garage. A small two-light casement window is proposed at the south gable end (secondary elevation) of the garage.



Figure 15. Looking southwest along 700 block of Pine St. (730 Pine at left)

A wooden fence is proposed along the rear south edge and east side of the property. It is shown to range in height from 3 ft. to 6 ft. with top and bottom rails, no spacing between boards, and painted white. A second fence is proposed along the front portion of the east, north and west property line enclosing the front yard area. It would replace an existing wooden picket fence, which rests on a concrete retaining wall.



*Figure 16. Existing concrete wall and wood fence at east side of property*

Landscape plans include path of sandstone pavers between the house and garage and paving around the north, east, south sides of the garage, and a portion of the west elevation (see *Figure 8.*)

### **CRITERIA FOR THE BOARD'S DECISION**

Subsection 9-11-18(b) and (c), B.R.C. 1981, sets forth the standards the Landmarks Board must apply when reviewing a request for a Landmark Alteration Certificate.

(b) Neither the Landmarks Board nor the City Council shall approve a Landmark Alteration Certificate unless it meets the following conditions:

- (1) The proposed work preserves, enhances, or restores and does not damage or destroy the exterior architectural features of the landmark or the subject property within an historic district;
- (2) The proposed work does not adversely affect the special character or

special historic, architectural, or aesthetic interest or value of the landmark and its site or the district;

- (3) The architectural style, arrangement, texture, color, arrangement of color, and materials used on existing and proposed constructions are compatible with the character of the existing landmark and its site or the historic district;
- (4) With respect to a proposal to demolish a building in an historic district, the proposed new construction to replace the building meets the requirements of paragraphs (b)(2) and (3) above.

(c) In determining whether to approve a landmark alteration certificate, the Landmarks Board shall consider the economic feasibility of alternatives, incorporation of energy-efficient design, and enhanced access for the disabled.

### **ANALYSIS**

1. *Does the proposed application preserve, enhance, or restore, and not damage or destroy the exterior architectural features of the landmark or the subject property within a historic district?*

While the garage at 730 Pine Street is a modest Minimal-Traditional building that no longer relates stylistically to the main house on the property, staff considers that because of its 1941 date of construction, relative lack of exterior changes, and prominence to the alley-scape, it should be considered contributing to the historic character of the Mapleton Hill Historic District. Staff considers that proposed alterations to this building, including the removal of the non character defining appendage at the southwest corner, will preserve the historic character of the building and the immediate alley-scape.

2. *Does the proposed application adversely affect the special character or special historic, architectural, or aesthetic interest or value of the district?*

Staff considers that the alteration of the contributing building, construction of a two-car garage and construction of front, rear and side fences will not affect the special historic character of the district, though the front fence should not exceed 30" in height and should be constructed to be as open as possible (see Design Guidelines Analysis section).

3. *Is the architectural style, arrangement, texture, color, arrangement of color, and materials used on existing and proposed structures compatible with the character of the historic district?*

Staff considers the proposed one-story, two-car garage, pergola and proposed fencing to be generally compatible with the architectural style, arrangement, texture, color, arrangement of color, and materials used on the proposed building and will be generally compatible with the character of the historic district (see Design Guidelines Analysis section).

4. *Does the proposal to demolish the building within the Mapleton Hill Historic District and the proposed new construction to replace the proposed demolished building meet the requirements of paragraphs 9-11-18(b)(2), 9-11-18(b)(3) and (4) of this section?*

Contributing garage will be preserved.

### DESIGN GUIDELINES

The Historic Preservation Ordinance sets forth the standards the Landmarks Board must apply when reviewing a request for a Landmark Alteration Certificate and the board has adopted the *General Design Guidelines* to help interpret the ordinance. The following is an analysis of the submitted proposal with respect to relevant guidelines. It is important to emphasize that design guidelines are intended to be used as an aid to appropriate design, and not as a checklist of items for compliance.

The following is an analysis of the proposal’s compliance with the applicable design guidelines:

### GENERAL DESIGN GUIDELINES FOR GARAGES & OTHER ACCESSORY BUILDINGS.

2.3	<b>Site Design: Alleys</b>		
	<p>The alleys in historic districts were traditionally used for secondary access to the houses, for deliveries, and as storage places for horses and buggies, and later, for cars. A view of the backyards from the alleys was maintained. While today’s alleys have evolved into use as pedestrian paths for jogging, bicycling and dog walking, they still contribute to the historic character of the neighborhood. They are typically minimally paved.</p> <p>Along the alleys are historic accessory buildings of various shapes and sizes including barns, chicken coops, sheds and small garages. This variety contributes to the general feeling of human scale in the alleys.</p>		
	Guidelines	Analysis	Conforms?
.1	<i>Maintain alley access for parking and retain the character of alleys as clearly secondary access to properties.</i>	Rear parking is maintained by the proposal.	<b>Yes</b>
.2	<i>Retain and preserve the variety and character found in the existing historic</i>	Retention of the contributing garage will maintain the variety of	<b>Yes</b>

	<i>accessory buildings along the alleys.</i>	historic buildings in the 700 block of Pine Street alleys.	
.3	<i>The use of historically proportioned materials for building new accessory buildings contributes to the human scale of the alleys. For example, narrower lap siding and smaller brick are appropriate.</i>	New garage shown to be clad in wide board siding, similar wood siding on historic garage.	<b>Yes</b>
.4	<i>Buildings that were constructed after the period of significance but are still more than 50 years old and contribute to the variety and character of the alleyway should be retained.</i>	N/A	<b>N/A</b>
.5	<i>Maintain adequate spacing between accessory building so that the view of the main house is not obscured, and the alley does not evolve into a tunnel-like passage.</i>	The location of the proposed garage would obscure the view of the non-contributing house, but its setbacks would maintain adequate spacing along the alley.	<b>Yes</b>

<b>7.0</b>	<b>Garages &amp; Other Accessory Structures</b>
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	<p><i>Accessory buildings include barns, sheds, garages and outbuildings. Originally accessory structures were used for storage of equipment, animals, or carriages. Generally, these structures have been adapted for the storage of cars. In most cases, accessory building were located to the rear of the lot and accessed by alleys. They were subordinate in size and detailing to the primary house. Over time they have emerged as important elements of many lots and alleys in the district. Efforts should be made to protect the eclectic character of alleys.</i></p> <p><i>Both additions to existing accessory buildings and new accessory building will be evaluated in terms of how they affect the historic character of the individual site and the district as a whole. In the past, larger accessory structures have been allowed than may be appropriate today.</i></p>
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<b>7.1</b>	<b>Existing Historic Accessory Buildings</b>
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	<i>A primary concern of the Landmarks Board in reviewing proposed changes in historic districts is the protection of existing historic accessory structures and the character of the site and district.</i>
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	GUIDELINES:	ANALYSIS:	CONFORMS
.1	<i>Retain and preserve garages and accessory buildings that contribute to the overall character of the site or district.</i>	The existing accessory building was constructed in 1941, within the period of significance for the Mapleton Hill Historic District, and has not been significantly altered.	<b>Yes</b>
.2	<i>Retain and preserve the character-defining materials, features, and architectural details of historic garages</i>	The existing garage remains largely intact from its original construction and retains its original materials	<b>Yes</b>

	<i>and accessory buildings, including roofs, exterior materials, windows and doors.</i>	with the exception of the garage door. Staff considers the proposed appendage at the southwest corner to be non-character defining and its removal appropriate. Likewise, the addition of new doors in this location and a small gable-end window is appropriate and consistent with Section 3.7.7 of these guidelines.	
<b>7.2</b>	<b>New Accessory Buildings</b>		
<i>New accessory buildings should follow the character and pattern of historic accessory buildings. While they should take design cues from the primary buildings, they must be subordinate in size, massing, and detailing. Alley buildings should maintain a scale that is pleasant to walk along and comfortable for pedestrians.</i>			
<b>Location and Orientation</b>			
.1	<i>It is inappropriate to introduce a new garage or accessory building if doing so will detract from the overall historic character of the principal building, and the site, or if it will require removal of a significant historic building element or site feature, such as a mature tree.</i>	As the primary house is considered non-contributing to the character of the historic district, the construction of a new garage will not impact the character of the principal building.	<b>Yes</b>
.2	<i>New garages and accessory buildings should generally be located at the rear of the lot, respecting the traditional relationship of such buildings to the primary structure and the site.</i>	As proposed, the new garage would be located 5 ft. 3 in. from the alley; setback of accessory buildings along this portion of the alley varies greatly. Applicant should confirm that adequate backing distance is proposed.	<b>Yes</b>
.3	<i>Maintain adequate spacing between accessory buildings so alleys do not evolve into tunnel-like passageways.</i>	Accessory building setback approximately 5 ft. 3 in. from the south property line and 8 ft. west of the contributing garage proposed location will not result in a tunnel-like passageway given the setback of the building from the south property line.	<b>Yes</b>
.4	<i>Preserve a backyard area between the house and the accessory buildings, maintaining the general proportion of built mass to open space found within the area.</i>	Proposed garage shown to be located 5 ft. 3 in. from the alley, 6 ft. from the east property line and approximately 36 ft. from the south wall of the main house. This will allow for adequate back yard space.	<b>Yes</b>
<b>Mass and Scale</b>			
.5	<i>New accessory buildings should take</i>	Proposed design relates to non-	<b>Yes</b>

	<i>design cues from the primary building on the property, but be subordinate to it in terms of size and massing.</i>	contributing primary building; size and massing are appropriate.	
.6	<i>New garages for single-family residences should generally be one story tall and shelter no more than two cars. In some cases, a two-car garage may be inappropriate.</i>	Proposed two-car garage is one-story tall. Massing proportionate to built mass and open space on property.	<b>Yes</b>
.7	<i>Roof form and pitch should be complementary to the primary structure.</i>	Roof form is complementary to the non-contributing main house.	<b>Yes</b>
<b>Materials and Detailing</b>			
.8	<i>Accessory structures should be simpler in design and detail than the primary building.</i>	As shown, garage is simpler than main house in design, material, and detailing.	<b>Yes</b>
.9	<i>Materials for new garages and accessory structures should be compatible with those found on the primary structure and in the district. Vinyl siding and prefabricated structures are inappropriate.</i>	Proposed materials (wood siding, windows, and doors) will be compatible with character of historic district. Consider using wide board wood siding (see 2.3.3 above).	<b>Yes</b>
.10	<i>Windows, like all elements of accessory structures, should be simpler in detailing and smaller in scale than similar elements on primary structures.</i>	Proposed design of windows on east and west elevations appear to be compatible in terms of window type, size and detailing with similar elements on the primary building.	<b>Yes</b>
.11	<i>If consistent with the architectural style and appropriately sized and located, dormers may be an appropriate way to increase storage space in garages.</i>	N/A	<b>N/A</b>
.12	<i>Garage doors should be consistent with the historic scale and materials of traditional accessory structures. Wood is the most appropriate material and two smaller doors may be more appropriate than one large door.</i>	Garage doors proposed to be insulated steel; appear to be consistent in terms of scale and materials. Review final details at Ldrc.	<b>Maybe</b>
.13	<i>It is inappropriate to introduce features or details to a garage or an accessory building in an attempt to create a false historical appearance.</i>	Proposed design does not attempt to recreate a false historic appearance.	<b>Yes</b>
.14	<i>Carports are inappropriate in districts where their form has no historic precedent.</i>	Carport not proposed.	<b>N/A</b>

### Mapleton Hill Historic District Guidelines

The following section is an analysis of the proposal relative to Section VI of the *Mapleton Hill Historic District Design Guidelines*. Only those guidelines that further the analysis of the proposed project are included and those that reflect what has been evaluated in the previous section are not repeated.

<b>B SITE</b>			
	<i>Traditional settlement patterns generally placed houses in the center of a site, with garages, carriage houses, etc. and parking at the rear...</i>		
	Guideline	Analysis	Conforms?
.1	<i>Accessory buildings such as sheds and garages, and driveways should be located at the rear of the lot as is traditional. Adding them between existing buildings interrupts the rhythm and spacing.</i>	Accessory building setback approximately 5 ft. 3 in. from the south property line and 8 ft. west of the contributing garage proposed location will not result in a tunnel-like passageway given the setback of the building from the south property line.	<b>Yes</b>
2.	<i>Accessory buildings should generally be small in scale and mass and simply detailed. They are clearly secondary in importance to the primary house.</i>	At one-story, the proposed two-car garage is appropriate.	<b>Yes</b>

<b>D ALLEYS, EASEMENTS AND ACCESSWAYS</b>			
	<i>Alleys are a strong visual element of the district, and have much variety of scale and detail. They play an important part in the development patterns that give the more visible areas their character. Alleys provide access to rear parking and garages. They have a varied edge quality, with building both on the property lines and set back. The size and quality of these accessory building varies considerably. Careful consideration should be given to changes in traditional use.</i>		
	Guideline	Analysis	Conforms?
1.	<i>The use of alleys to provide access to the rear of properties should be preserved</i>	Access to rear of property preserved.	<b>Yes</b>
2.	<i>Efforts should be made to protect the variety of shape, size, and alignment of buildings along the alleys. Alleys should maintain a human scale and be sensitive to pedestrians.</i>	Proposal will preserve variety found on immediate alley-scape.	<b>Yes</b>

3.	<i>Building such as garages, sheds, etc. which contribute to this variety should be retained in their original form whenever possible.</i>	Existing accessory was built in 1941, within the period of significance of the Mapleton Hill Historic District, and has been largely unaltered since its construction. 2005 Accessory building survey determined building to be contributing to the character of the Mapleton Hill Historic District. Proposed removal of southeast appendage, new door at west elevation, and small gable window on south (secondary) gable appropriate. Review details at Ldrc.	<b>Yes</b>
5.	<i>Efforts should be made to maintain character of the alleys in the district</i>	Proposal will preserve variety found on immediate alley scape.	<b>Yes</b>
9.	<i>Dumpsters should be screened from alley view by landscaping or a permanent enclosure.</i>	Trash enclosure shown at east elevation of proposed garage.	<b>Yes</b>

<b>O.</b>	<b>FENCES</b>		
<i>Traditionally, the appearance of a house has been more important than privacy from the streets, so fences were open, for example, made of wrought iron or wood pickets. Solid wood fences are not traditional and were not used at the fronts of houses, and the present-day addition of such a fence interrupts the strong visual element created by uniform building alignment.</i>			
.1	<i>Low fences are encouraged.</i>	Fence around front yard should not exceed 30 in. height. Rear and side fence along alley to measure up to 6 ft. in height. Review details at Ldrc.	<b>Maybe</b>
.2	<i>Although not typically found within front yards, if used, a durable material in an open design should be used for front fences. Painted iron or steel, or painted wood pickets are appropriate and might be used in conjunction with low masonry walls. There are types of wire fencing which are historic and would be encouraged. Low shrub hedges are also appropriate. Vertical board, stockade, chain link fences and heavy brick posts are generally inappropriate.</i>		<b>Maybe</b>

.3	<p><i>Fences without spaces between slats can alter the character of a building site and of the streetscape and alleyscape because the historic architectural elements that contribute to the pattern of spacing, setbacks, scale, details and materials of the historic district are blocked from view.</i></p> <p><i>a. Solid or tight fences are not appropriate</i></p> <p><i>b. Every effort should be made to allow visual penetration in the design of fences visible from the street or alley. The visual impact of solid wood fencing at the rear of a lot is that the alley becomes a visual tunnel, and much of the irregularity and variation that make the essential character of an alley are changed.</i></p>	<p>Spacing between slats shown as 1.5 in. on 5 ft. 6 in. fence proposed along rear of property (south, east and west property lines). Review details at Ldrc.</p>	<p><b>Maybe</b></p>
.4	<p><i>Fences on the rear portion of corner lots should have some degree of spacing along the public right-of-way unless the fence is set back far enough to avoid a fortress effect.</i></p>	<p>Spacing between slats shown 1.5 in. on 5 ft. 6 in. fence proposed along rear of property (south, east and west property lines). Review details at Ldrc.</p>	<p><b>Maybe</b></p>
.5	<p><i>Fences across the front of a house should be low (36" or less). When connecting fencing to a taller side or rear yard fence, a section which gradually increases in height should be included.</i></p>	<p>Transition between heights of horizontal slat fence and vertical fence includes stepped portion.</p>	<p><b>Yes</b></p>
.6	<p><i>Raw wood (unfinished or unpainted) fences are inappropriate in the historic district. Fences should be either painted or coated with an opaque stain.</i></p>	<p>Rear, vertical fence shown to be painted white. Verify design and finish of front yard fence at Ldrc.</p>	<p><b>Maybe</b></p>
.7	<p><i>The finish side of the fence should face toward the street or sidewalk.</i></p>	<p>Finish side of the fence to face toward alley.</p>	<p><b>Yes</b></p>
.8	<p><i>Fences should have a regular pattern.</i></p>	<p>Fences shown to have a regular pattern.</p>	<p><b>Yes</b></p>

<b>P</b>	<b>GARAGES, CARPORTS AND ACCESSORY STRUCTURES</b>		
	<p><i>A variety of accessory buildings has been adapted for use as garages in the Mapleton Hill Historic District. Whether carriage houses or sheds, these structures have certain similarities. They are plain and utilitarian and are located at the rear of the property on the alley. Materials and building elements are varied.</i></p>		

	Guideline	Analysis	Conforms?
1.	<i>Free-standing carports are extremely difficult to fit into the district since their form has no historic precedent. Other solutions for sheltering vehicles should be sought.</i>	Carport not proposed.	N/A
2.	<i>The most visually appropriate carports take the form of a shed roof addition to another building with a low knee wall giving definition to its form.</i>	Carport not proposed.	N/A
3.	<i>If a new building is to be constructed, design ideas might be found in existing historic accessory buildings located nearby</i>	This section of the alley has an eclectic variety of accessory buildings.	Yes
4.	<i>The new building should be secondary in nature to the main house and smaller in scale.</i>	Proposed design will be secondary to main house in terms of height and simplicity. Review details at Ldrc.	Yes
5.	<i>Accessory buildings should be small in scale and mass, and constructed in a manner which is complimentary to the character of the house and alley. They are clearly secondary in importance to the primary structure. Typically, prefabricated sheds are discouraged.</i>	Proposed mass and scale of one story two-car garage appropriate.	Yes

Staff considers the proposed new garage, rehabilitation of the contributing garage and construction is generally consistent with the design guidelines for site design, orientation, materials and detailing. Details should be reviewed and approved by the Landmarks design review committee (Ldrc).

Staff considers the design for the fence along the rear and side of the property is consistent with Mapleton Hill Historic District Design Guidelines, O.2, O.3 and O.2. Detailed plans for an open front fence no higher than 30 in. should be reviewed by the Ldrc.

**FINDINGS**

Subject to the conditions stated in the recommended motion, staff recommends that the Landmarks Board approve the application and adopt the following findings:

This decision is consistent with the purposes and standards of the Historic Preservation Ordinance, in that:

1. The rehabilitation of the contributing garage will preserve the architectural features of the subject property within the Mapleton Hill Historic District, construction of a garage and fencing will not adversely affect the special character of the Mapleton Hill District. §9-11-18(b)(1), B.R.C. 1981.
2. The proposed new construction will not adversely affect the special character or special historic, architectural, or aesthetic interest or value of the property or the historic district. § 9-11-18(b)(2), B.R.C. 1981.
3. The installation of rear and side fences will generally comply with Section 2.6.3, Fences, of the *General Design Guidelines* and Section A.3, Site Design; K.5, Materials; and O, Fences of the *Mapleton Hill Historic District Guidelines*, and Section 9-11-18(b)(3) of the Boulder Revised Code 1981.

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**ATTACHMENTS:**

- A: Cultural Resource Re-evaluation Form: Accessory Building Survey
- B: Tax Assessor Card
- C: Photographs
- D: Plans and Elevations
- E: Applicant's Submittal

**Attachment A: Cultural Resource Re-evaluation Form: Accessory Building Survey**

Address: **730 PINE ST**  
**Boulder, Colorado**

COLORADO CULTURAL RESOURCE SURVEY

**Cultural Resource Re-evaluation Form: Accessory Building Survey**

1. Resource Number: **5BL10067**

2. Temp. Resource Number: **BUILDING A**

3. Attachments:

(Check as many as apply)

- Photographs ✓
- Site sketch map
- U.S.G.S. map photocopy
- Other
- Other

4. Official determination:

**OAHP USE ONLY**

- Determined Eligible
- Determined Not Eligible
- Need Data
- Nominated
- Listed
- Contributing to N.R. District
- Not Contributing to N.R. District

5. Resource Name of Primary Building

6. Purpose of this current site visit: **Resurvey**

7. Previous Recordings: **Front Range Research Assoc.**

8a. Description of Accessory Building:

**Two accessory buildings on lot.**

**(Building A): gable roofed garage with composition roofing, weatherboard siding, and an overhead garage door of vertical boards.**

**(Building B): "non-contributing" accessory building (greenhouse) on lot is less than fifty years old.**

Outbuilding Type:

**Garage**

Outbuilding Material:

**Wood Frame**

Outbuilding Covering

**Other**

Outbuilding Roof Material

**Asphalt**

8b. Date of Construction: **ca. 1941**

8c. Date of Construction Source:

**Historic Assessor's Card, Carnegie Library: 1941 note, 21x15 garage exists.**

9. Condition: **Good**

10a. Changes to Location or Size Information:

10b. UTM Coordinates:

11. Current Ownership ROOSEVELT JAMES A & MARY DUVALL  
730 PINE ST  
BOULDER  
CO  
80302

12. Other Changes, Additions or Observations:

13. Eligibility Assessment:

Individual

District

National Register: N/A

National Register: Contributing

Local Landmark: N/A

Local: Contributing

Locally Designated Property: NO

14. Management Recommendations: N/A

15. Photograph Types and Numbers:

Type: B&W

Roll No: 13

Frame No: 34,36

16. Artifact and Field Documentation Storage Location N/A

17. Report Title: Accessory Building Survey

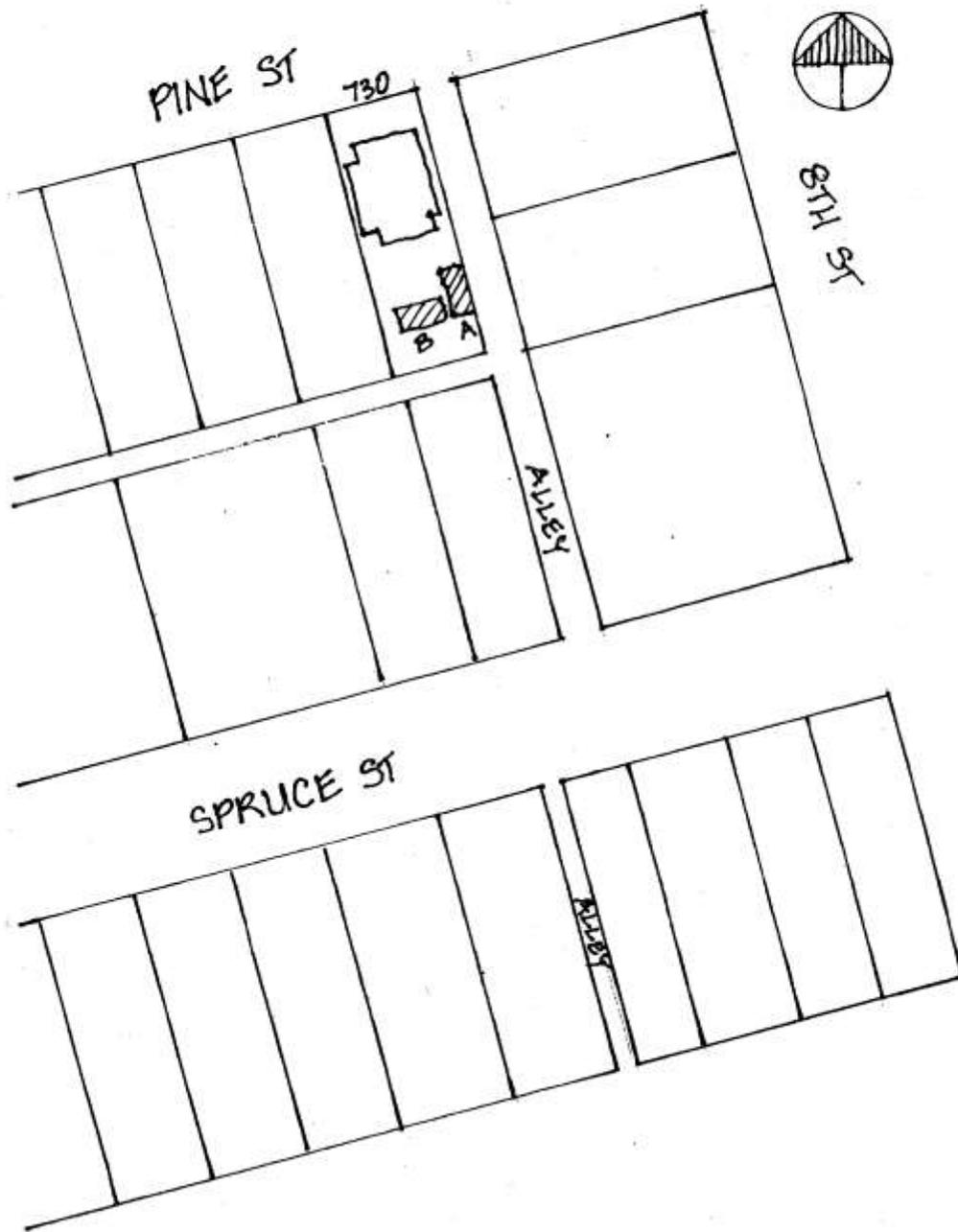
18. Recorder(s): Kathryn Howes Barth, AIA; Lara Ramsey

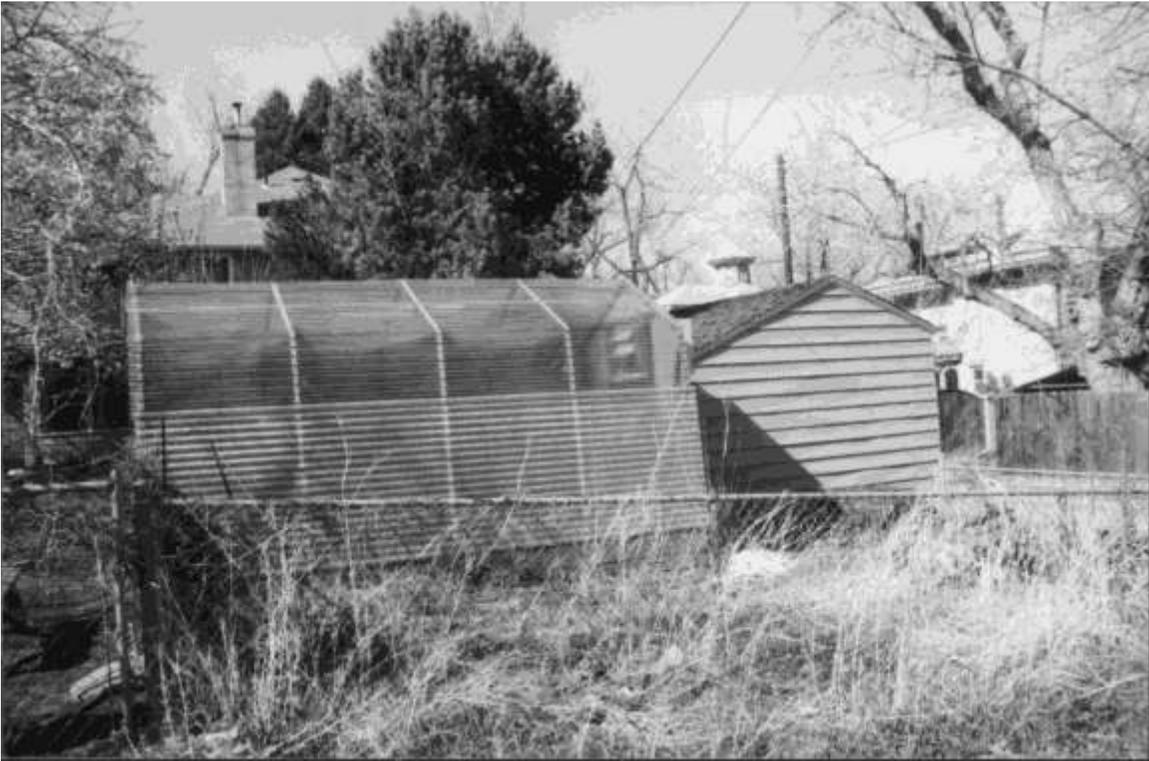
19. Date(s): Apr.2005

20. Recorder Affiliation: Kathryn Howes Barth, AIA; Ramsey Planning and Preservation

730 PINE ST

SITE PLAN





Attachment B: Tax Assessor Card, c.1949



Appraised 11/19/49 19

LOTS 1 BLOCK 117 ADDITION 1

HOUSE No. 117 STREET Bl 117 CITY Bl 117

OWNER J.A. Smith

Year Constructed 1917 Est. Life in Years 30



ESTIMATE OF VALUATION	
BLDG. PART A	BLDG. PART B GARAGE
No. Cubic Feet.....	.....
Cost per cu. ft. ....	.....
Total Cost .....	.....
Porches .....	.....
Garage.....	.....
Extras.....	.....
TOTAL.....	.....
% Obsolescence .....	.....
% Physical Dep. ....	.....
Net After Deducting Depreciation .....	.....
% Utility Dep. ....	.....
PRESENT VALUE.....	.....

DESCRIPTION	AMOUNT		ANNUAL ASSESSMENT		TOTAL
	1948	1949	1948	1949	
Building Permit					
Original Cost, Improvements Only					
Additions and Betterments					
Owner's Estimate of Present Value					
Private Appraisal					
Insurance					
Mortgage					
Monthly Rental					
Advertised for Sale					
Transferred in 19					

CLASS OF BUILDING		HEIGHT		ROOF		LIGHT		DESCRIPTION				
Check	No. of Stories	Check	CONSTRUCTION	Check	Electricity	Check	ROOMS	STORIES			Give Numbers	
1- Single Residence	✓	✓	Wood Shingle	✓	Gas	✓	Living Room	1	2	3	Attic	
2- Duplex			Composition Shingle		Oil		Dining Room					
3- Bungalow, Apt. Crt.			Tar and Gravel				Dinette					
4- Flat or Terrace			Sheet Iron				Kitchen					
5- Apartment House			Copper				Breakfast Nook					
6- Hotel			Concrete Tile				Bed Room					
7- Store Building			Clay Tile				Bath Room					
8- Office Building			Slate				Toilet Room					
9- Hospital or Sanitarium			Asbestos Shingle				Shower Room					
10- Bank Building			Tin				Sleeping Porch					
11- Theatre			Insulated				Sun Room					
12- Warehouse			STYLE				Den					
13- Warehouse			Gable				Storage Room					
14- Factory			Hip				Office					
15- Public Garage			Flat				Halls					
16- Private Garage			Gambrel									
17- Service Station			Mansard									
18- Hot House or Gr. House			Leanto									
19- Poultry House												
20- Barns or Sheds												
CONSTRUCTION		EXTERIOR		PLUMBING		FINISH						
Frame	✓	Common Brick	Old Style	✓	Water	✓	Unfinished					
Brick		Pressed Brick	Modern		No. Bath Tubs		Plastered, Plain					
Tile		Wire Cut Brick	No. Toilet		No. Shower Baths		Plastered, Ornament					
Stone		Glazed Brick	No. Lavatories		No. Toilets		Papered					
Concrete Plain or Block		Wood Siding	No. Urinals		No. Laundry Tubs		Painted or Tinted					
Concrete, Reinforced		Wood Shingles	No. Sinks		No. Sinks		Softwood Floor					
Steel Frame		Cement Stucco	No. Sanitary Closets		Sanitary Closets		Hardwood Floor					
Insulated and W. S.		Stone	Ceas Pool		Ceas Pool		Hardwood Finish					
CHARACTER OF CONST.		HEATING		FUEL		MISCELLANEOUS						
Chap		Stove	Stove		Coal		Tile					
Medium		Hot Air	Hot Air		Oil		Marble or Onyx					
Good		Hot Water	Hot Water		Gas		Wall Board					
Fire Resisting		Steam	Steam		Electricity		Sheetrock					
Non-Fire Resisting		No. Fireplaces	No. Fireplaces				Celotex					
STATE OF REPAIRS		OUTSIDE TRIM				REMARKS						
Bad		Wood	Wood				Wainscoting					
Fair		Terra Cotta	Terra Cotta				Metal Ceiling					
Good		Stone	Stone									
New		Galv. Iron	Galv. Iron									
		Concrete	Concrete									

**Attachment C: Photographs**



*Photo 1. 730 Pine St. Accessory Building, facing southwest, 2014*



*Photo 2. 730 Pine St., West Elevation, 2014*



*Photo 3. 730 Pine St. Accessory Building, East Elevation, 2014*



*Photo 4. 730 Pine St. facing northwest, 2014*



*Photo 5. East-West Alley, facing west, 2014*



*Photo 6. East-West Alley, facing East, 2014*



*Photo 7. North-South Alley, facing north, 2014*

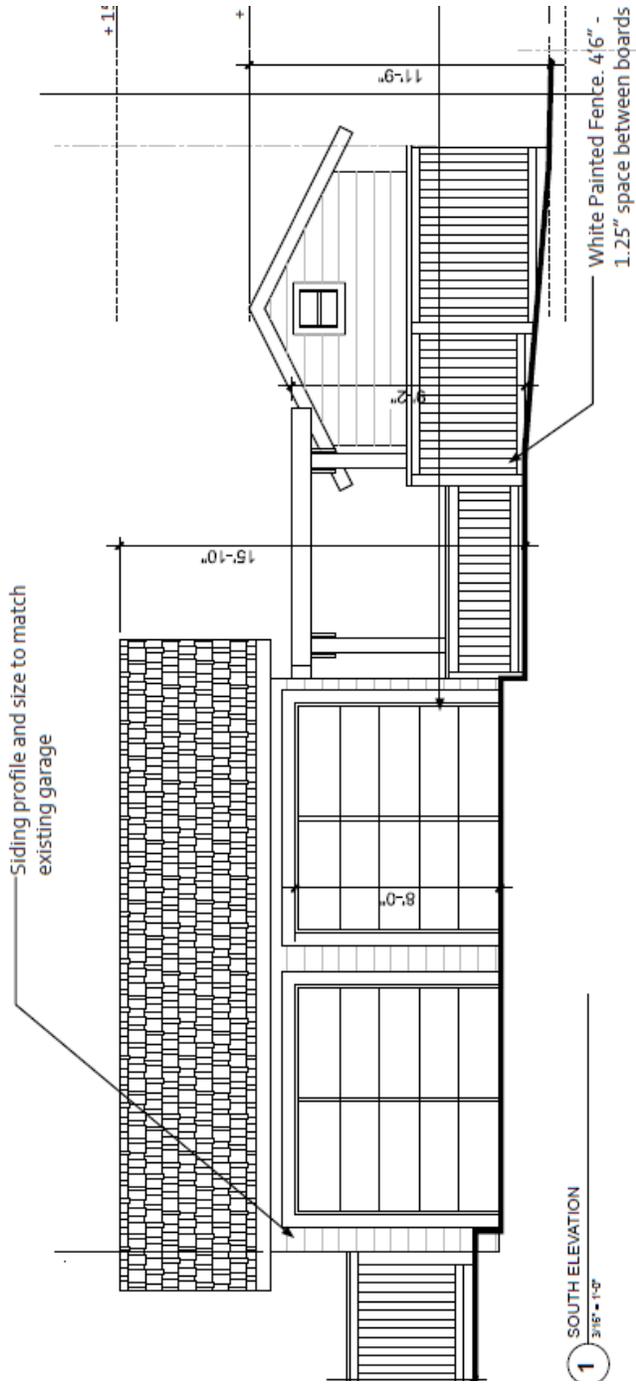


*Photo 8. Accessory Buildings along 700 block of Pine St. Alley (east-west alley), 2014.*

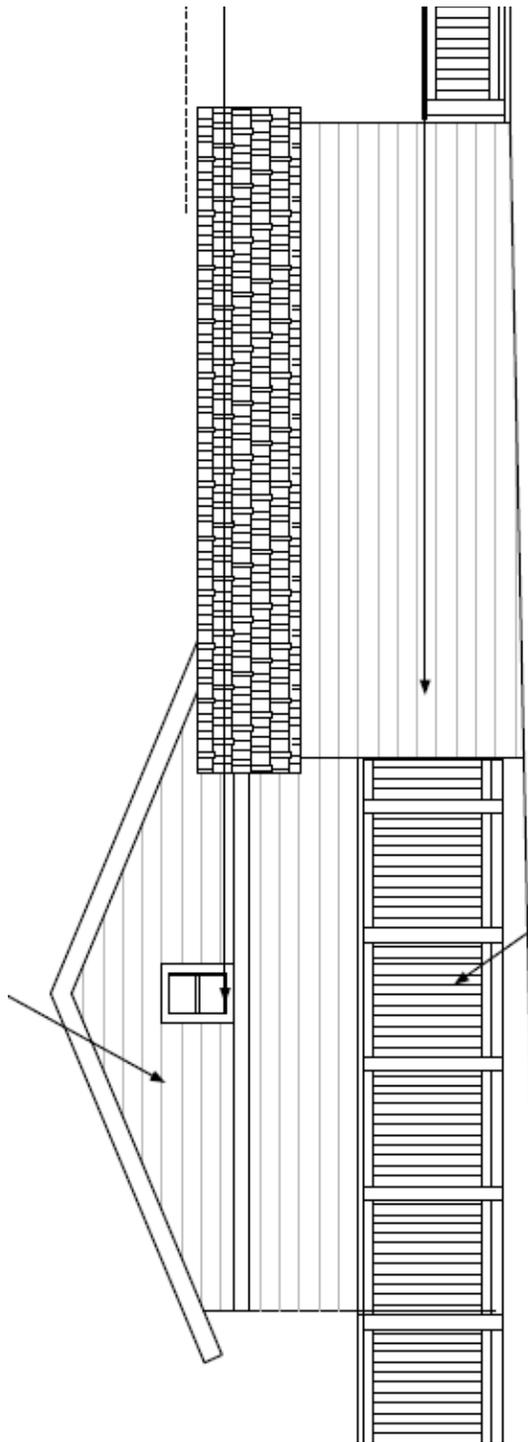
**Attachment D: Plans and Elevations**



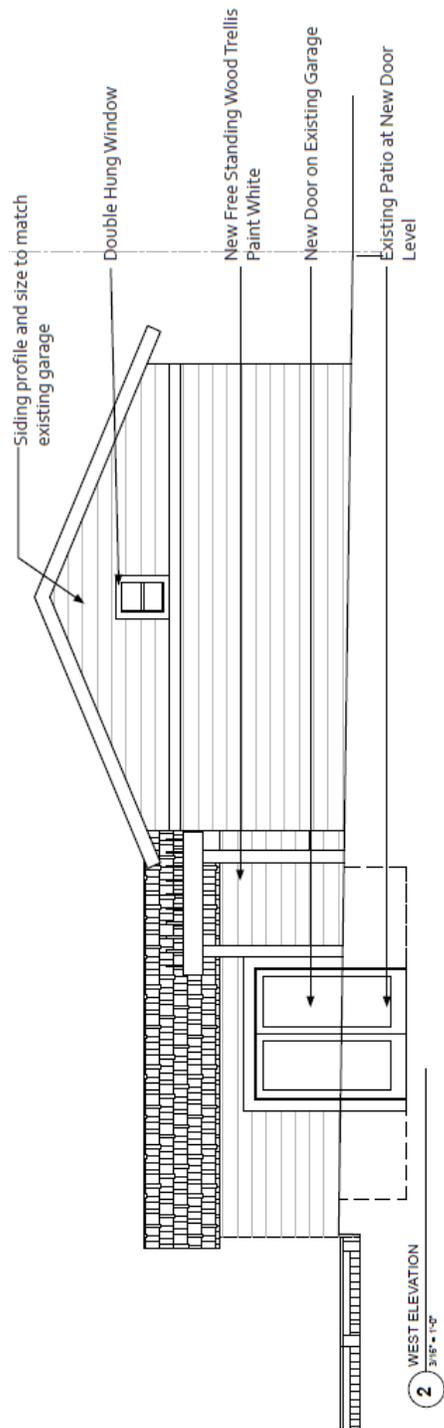
Proposed Site Plan



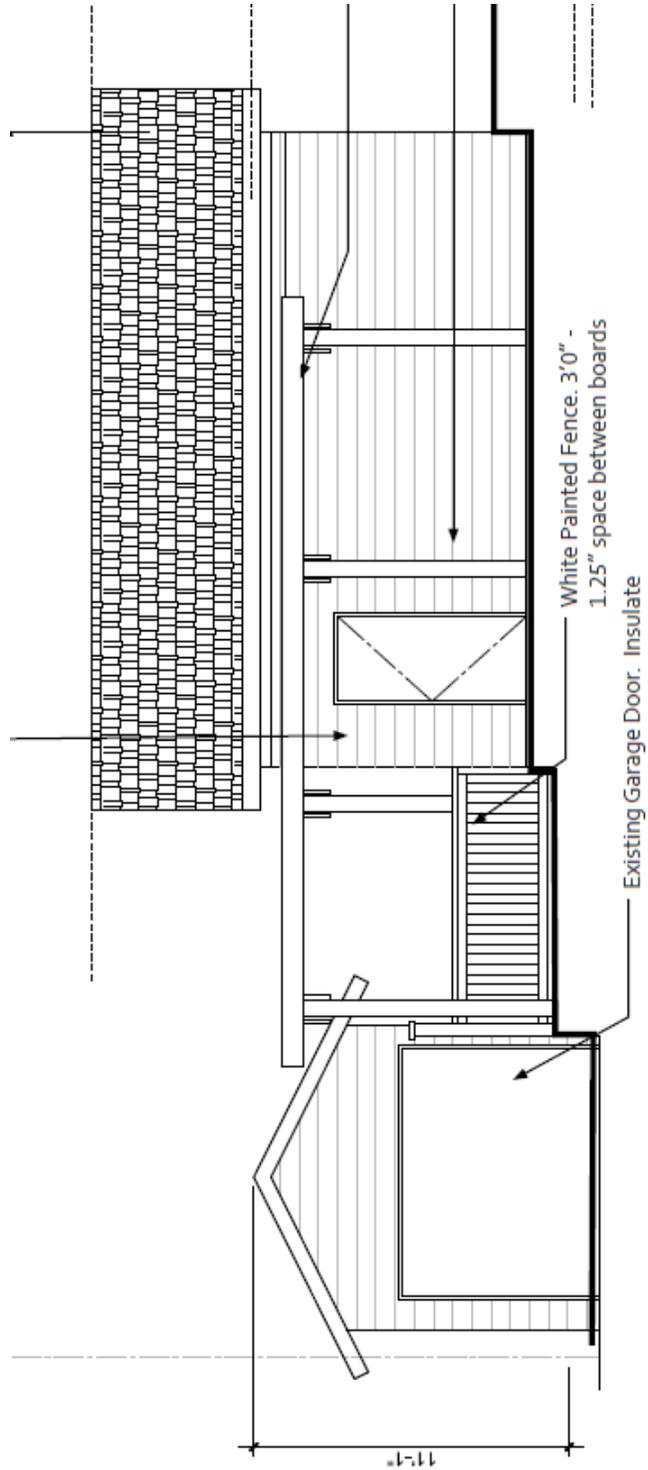
Proposed South Elevation



Proposed East Elevation



Proposed West Elevation



Proposed North Elevation

Renderings