



CITY OF BOULDER
PLANNING BOARD MEETING AGENDA
DATE: September 2, 2015
TIME: 5 p.m.
PLACE: 1777 Broadway, Council Chambers

1. CALL TO ORDER

2. APPROVAL OF MINUTES

3. PUBLIC PARTICIPATION

4. DISCUSSION OF DISPOSITIONS, PLANNING BOARD CALL-UPS/CONTINUATIONS

5. PUBLIC HEARING ITEMS

A. SITE AND USE REVIEWS: Applications under case no.'s LUR2015-00010 and LUR2015-00011, are for the proposed redevelopment of the 10.9 acre former Sutherlands Lumber site including 3390 Valmont Rd.; and 3085, 3155, 3195 Bluff St. within the northern portion of Boulder Junction to create a new mixed use, mixed income neighborhood comprised of seven distinct areas:

- **Markt:** an 55,340 square foot, four story commercial mixed use building with an approximately 7,832 square foot brewpub with a 3,202 square foot tap room restaurant and a 4,630 square foot brewery production area as well as three micro restaurants on the ground floor along with upper story office;
- **Ciclo:** a four story mixed use, 57,901 square foot building with the ground floor housing Community Cycles and with 32 permanently affordable apartments above;
- **Railyards:** an approximately 70,155 square foot, four story commercial mixed use building with ground floor retail including an approximately 2,500 square foot restaurant on the north end of the building and a 3,500 square foot restaurant on the south end of the building both with outdoor dining; and upper story office;
- **Timber Lofts:** an approximately 167,288 square, foot four-story apartment building with 121 apartments along with eight townhomes and ground floor office and retail;
- **Meredith House:** a four story, 15 unit residential condominium building of 20,754 square feet; and
- **S'PARK_west** with 45 units of permanently affordable attached residential, and
- **S'PARK_west** with 24 market rate townhomes.
- The proposed project includes parks, below grade parking, new transportation connections per the TVAP connections plan, a woonerf (shared pedestrian street), and a public plaza in anticipation of the future rail stop.
- The applicant intends to pursue Vested Rights per section 9-2-19, B.R.C. 1981
- Use Reviews are for three restaurants with outdoor seating greater than 300 square feet within 500 feet of a residential area.

6. MATTERS FROM THE PLANNING BOARD, PLANNING DIRECTOR, AND CITY ATTORNEY

7. DEBRIEF MEETING/CALENDAR CHECK

8. ADJOURNMENT

**CITY OF BOULDER PLANNING BOARD
MEETING GUIDELINES**

CALL TO ORDER

The Board must have a quorum (four members present) before the meeting can be called to order.

AGENDA

The Board may rearrange the order of the Agenda or delete items for good cause. The Board may not add items requiring public notice.

PUBLIC PARTICIPATION

The public is welcome to address the Board (3 minutes* maximum per speaker) during the Public Participation portion of the meeting regarding any item not scheduled for a public hearing. The only items scheduled for a public hearing are those listed under the category PUBLIC HEARING ITEMS on the Agenda. Any exhibits introduced into the record at this time must be provided in quantities of ten (10) to the Board Secretary for distribution to the Board and admission into the record.

DISCUSSION AND STUDY SESSION ITEMS

Discussion and study session items do not require motions of approval or recommendation.

PUBLIC HEARING ITEMS

A Public Hearing item requires a motion and a vote. The general format for hearing of an action item is as follows:

1. Presentations

- a. Staff presentation (10 minutes maximum*)
- b. Applicant presentation (10 minute maximum*). Any exhibits introduced into the record at this time must be provided in quantities of ten (10) to the Board Secretary for distribution to the Board and admission into the record.
- c. Planning Board questioning of staff or applicant for information only.

2. Public Hearing

Each speaker will be allowed an oral presentation (3 minutes maximum*). All speakers wishing to pool their time must be present, and time allotted will be determined by the Chair. No pooled time presentation will be permitted to exceed ten minutes total.

- Time remaining is presented by a Green blinking light that means one minute remains, a Yellow light means 30 seconds remain, and a Red light and beep means time has expired.
- Speakers should introduce themselves, giving name and address. If officially representing a group, homeowners' association, etc., please state that for the record as well.
- Speakers are requested not to repeat items addressed by previous speakers other than to express points of agreement or disagreement. Refrain from reading long documents, and summarize comments wherever possible. Long documents may be submitted and will become a part of the official record.
- Speakers should address the Land Use Regulation criteria and, if possible, reference the rules that the Board uses to decide a case.
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- Citizens can send a letter to the Planning staff at 1739 Broadway, Boulder, CO 80302, two weeks before the Planning Board meeting, to be included in the Board packet. Correspondence received after this time will be distributed at the Board meeting.

3. Board Action

- d. Board motion. Motions may take any number of forms. With regard to a specific development proposal, the motion generally is to either approve the project (with or without conditions), to deny it, or to continue the matter to a date certain (generally in order to obtain additional information).
- e. Board discussion. This is undertaken entirely by members of the Board. The applicant, members of the public or city staff participate only if called upon by the Chair.
- f. Board action (the vote). An affirmative vote of at least four members of the Board is required to pass a motion approving any action. If the vote taken results in either a tie, a vote of three to two, or a vote of three to one in favor of approval, the applicant shall be automatically allowed a rehearing upon requesting the same in writing within seven days.

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CITY OF BOULDER
PLANNING BOARD MEETING AGENDA
DATE: September 3, 2015
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PLACE: 1777 Broadway, Council Chambers

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CITY OF BOULDER
PLANNING BOARD AGENDA ITEM
MEETING DATE: September 2, 2015

AGENDA TITLE: SITE AND USE REVIEWS: Applications under case no.'s LUR2015-00010 and LUR2015-00011, are for the proposed redevelopment of the 10.9 acre former Sutherlands Lumber site including 3390 Valmont Rd.; and 3085, 3155, 3195 Bluff St. within the northern portion of Boulder Junction to create a new mixed use, mixed income neighborhood comprised of seven distinct areas:

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Applicant: Scott Holton, Element Properties

Property Owners:

- Sutherland Bldg. Material Shopping Centers, Inc. a Delaware Corporation
- 1240 Cedar LLC, a Colorado Limited Liability Company
- 3155 Element, LLC, a Colorado Limited Liability Company
- 3195 Bluff Element, LLC a Colorado Limited Liability Company

REQUESTING DEPARTMENT:

Community Planning & Sustainability

David Driskell, Executive Director

Susan Richstone, Deputy Director

Charles Ferro, Land Use Review Manager

Elaine McLaughlin, Senior Planner

OBJECTIVE:

1. Hear applicant and staff presentations.
2. Hold public hearing.
3. Planning Board action to approve, approve with conditions, or deny the Site and Use Review applications.

SUMMARY:

Proposal: The proposal includes redevelopment of the site as a new mixed use neighborhood to include residential, retail, office and restaurants.

Project Name: S'PARK

Location: 3390 Valmont Rd.; and 3085, 3155, 3195 Bluff St.

Size of Tract: 10.9 acres
Zoning: Mixed Use – 4 and Residential High - 6
Comprehensive Plan: Mixed Use Business and High Density Residential

Key Issues: Staff is recommending four key issues for the Planning Board's discussion and analysis :

1. Does the proposed project, including the requested modifications to height, number of stories, and setbacks meet the Site Review criteria of Land Use Code section 9-2-14(h), B.R.C. 1981?
2. Is the proposed urban design and planning for the overall plan and the individual areas consistent with the Transit Village Area Plan (TVAP) intent and design guidelines?
3. Is the proposed change to the Transit Village Area Plan Connections Plan for connections 9, 10, 12 and 13 consistent with the requirements of such changes under TVAP?
4. Does the Use Review for the Brewpub and the two small restaurants all meet the Use Review criteria of the Land Use Code section 9-2-15(e), B.R.C. 1981?

I. INTRODUCTION

This proposed project was reviewed as a Concept Plan, first on March 6, 2014 for the area now planned as "S'PARK_west" and on Sept. 4, 2014 for the area encompassing the majority of the area planned as "S'PARK." At the time of the two separate S'PARK Concept Plan reviews, the Planning Board and staff recommended the applicant return with a comprehensive Site Plan that included both areas, as is currently under consideration. The minutes and audio of the March 2014 hearing is [here](#) and the Sept. 2014 is [here](#). A consistency analysis of the plans with Concept Plan recommendations is provided within [Attachment F](#). Once an application for Site Review was submitted for the comprehensive S'PARK plan, the Boulder Design Advisory Board (BDAB) reviewed the project in three separate meetings, on April 8 and July 15, 2015. The minutes of the BDAB meetings are provide in [Attachment E](#).

In April 2015, City Council approved ordinance no. 8028 to limit the eligibility of buildings that could exceed the by-right height limits through the existing Site Review process to specific areas and situations. The approved ordinance allows the consideration of height modifications through site review only in those areas with a clearly defined, approved vision for future development, including Boulder Junction. The intent in including Boulder Junction properties was to reinforce the community's vision of an urban form with higher intensity and taller buildings only in select, transit-rich areas, and areas which had been vetted and approved through a planning process such as the Transit Village Area Plan.

As shown in Figure 1, the 10.9 acre site is located in the northern area of Boulder Junction where redevelopment is overseen by the vision, goals, and guidelines of the Transit Village Area Plan (TVAP). The area plan was the result of a nearly seven year public planning process from which the city envisioned the following:

"The Transit Village area will evolve into a lively, mixed-use, pedestrian-oriented place where people will live, work, shop and access regional transit. It will become a new neighborhood as well as an attractive destination for the larger city, with regional transit and public spaces that will benefit the entire Boulder community."

The proposed project establishes a northern "bookend" to the redevelopment of the area within Boulder Junction. Initial redevelopment over the past five years has established some of the initial vision of TVAP. The RTD bus rapid transit facility was recently completed and bus service just began for the 'HX' and the 'S' bus lines. The Flatiron Flyer bus rapid transit service will begin in January. This critical infrastructure lays the foundation to implement the envisioned transit oriented development at Boulder Junction. As was noted in TVAP, critical to the success of a transit oriented development is a certain level of density or intensity which is mutually dependent upon regional and local transit.

Figure 1:

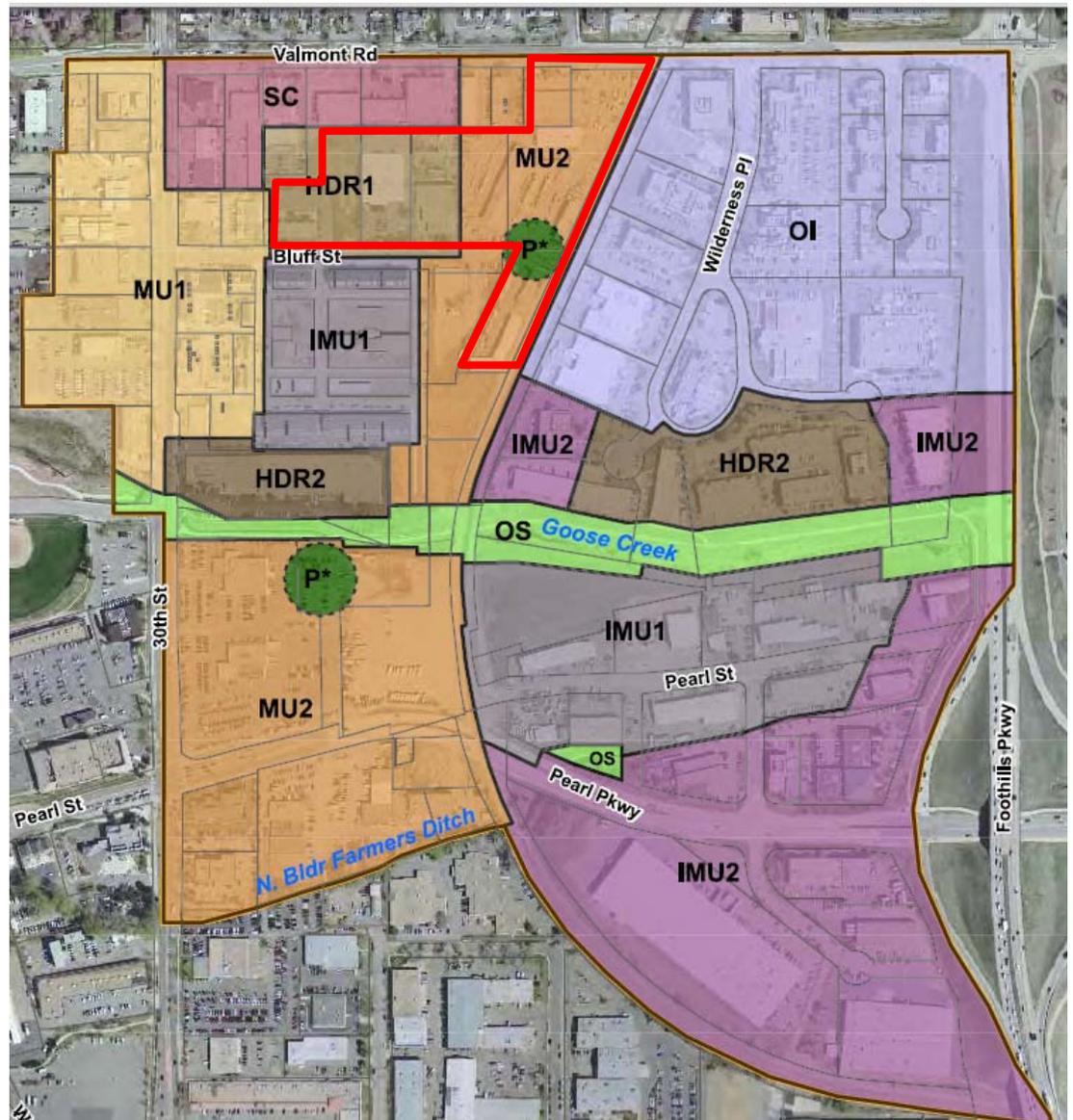
Site Location within
Boulder Junction

(Transit Village Area Plan
Land Use Map)

Legend

Plan Land Use

-  High Density Residential 1
-  High Density Residential 2
-  Office Industrial
-  Industrial Mixed Use 1
-  Industrial Mixed Use 2
-  Mixed Use 1
-  Mixed Use 2
-  Service Commercial
-  Park / Public Plaza
-  Greenway / Open Space



Built Context. Redevelopment in Boulder Junction, and near the project site in particular, began with Steelyards in the early 2000s. Built as a mixed use neighborhood, Steelyards predates the adoption of TVAP but helped to provide a precedent urban character that would inform the vision of TVAP. After TVAP was adopted in 2007, the first redevelopment built was the Solana Apartments in 2012 that includes 319 apartment units along with two retail uses and a fitness facility on the ground floor of buildings facing the new street of Junction Place and the recently completed Pearl Parkway, a multi-way boulevard. Currently nearing completion across Pearl Parkway from Solana Apartments is Depot Square, a mixed use development that includes an RTD below grade bus transit facility, a 150 room Hyatt Hotel, a four story parking structure that is “wrapped” on three sides by 71 apartments that will be rented to qualifying residents as permanently affordable units. Also nearing completion is the restoration of the historic Boulder Jaycees Depot building in Depot Square that will house a new restaurant. A new public plaza surrounds the depot and transitions into the recently constructed shared street of Junction Place. The city recently completed the new Goose Creek bridge that connects the redeveloped areas to the northern area of TVAP. A new pocket park is current in the design phase.

Also nearing completion and just north of the Goose Creek Bridge is a 17-unit attached residential condominium building, Nickel Flats. A plan for 100,000 square feet of office and retail, east of Nickel Flats and across Junction Place, is referred to as Boulder Commons and was recently approved by the Planning Board subject to City Council call-up. These new additions to Boulder Junction can be seen in Figure 2.

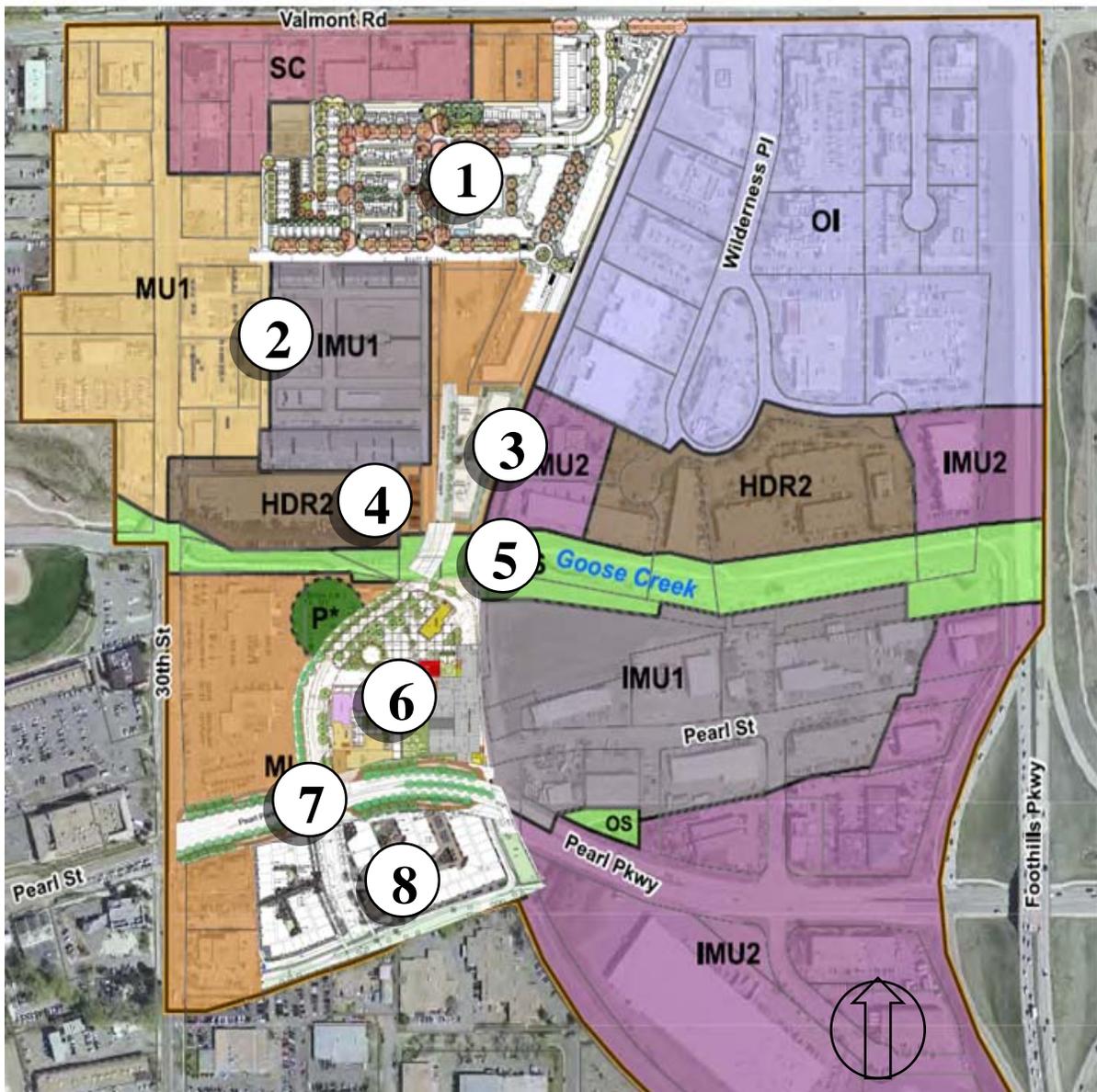


Figure 2:
Boulder Junction Build Out to Date:
Constructed, Approved, and
Proposed Projects

- 1) S'PARK and S'PARK_west
- 2) Steelyards
- 3) Boulder Commons
- 4) Nickel Flats
- 5) Goose Creek Bridge
- 6) Depot Square and Junction Place Shared Street
- 7) Pearl Parkway Multi-way Boulevard

Existing Site and Surroundings. The site itself encompasses several properties, with the largest property operating as the Sutherland Lumber Company for approximately 40 years. Over the last several years, the main sales building was repurposed as a shared office space with several tenants. The other smaller properties that make up the site have been occupied by various industrial and office buildings. Figure 4a, b and c illustrate portions of the existing site. Much of the site was paved over the years and thus is denuded of vegetation with the exception of some existing trees located on the south and east end of the site. There are prominent views toward the Flatirons from the site. The applicant has provided images of the site within the Site Review plans in [Attachment G](#).



**Figures 4a, b, c:
Photos of Portions of the Existing Site**

Directly west of the site are office and service industrial buildings along both Bluff Street and Valmont Road. To the south of the site across Bluff Street is Steelyards mixed use neighborhood. At the southeast intersection of Junction Place and Bluff Street is a branch of Air Gas Co., a supplier of industrial, medical and specialty gas and accessories, safety products, and welding supplies. That site had been included as a part of the Concept Plan evaluated for the proposed project but was removed as a part of the Site Review.

Across Valmont Road to the north are the Hilltop Townhomes and a child care center; further to the west on Valmont is the Orchard Grove Mobile Home Park and further to the north is the San Juan del Centro apartments. To the east, across the railroad tracks from the site are various office and industrial buildings, particularly those located within the Wilderness Place Office Park.



Figure 5:
Photos of Surrounding Context

Site Zoning. There are two zoning districts that were relatively recently adopted for this area of Boulder Junction intended to implement the land uses: Mixed Use – 4 (MU-4) and Residential High – 6 (RH-6). The zoning map for the area is shown in Figure 6.

Each zoning district is defined under the Land Use Code Section 9-5-2, B.R.C. 1981. For the two zoning districts the definitions are as follows:

MU-4: Mixed use – 4, residential areas generally intended for residential uses with neighborhood-serving retail and office uses; and where complementary uses may be allowed. It is anticipated that development will occur in a pedestrian-oriented pattern, with buildings built up to the street.

Residential - High 6: High density residential urban areas that are predominately townhouses in close proximity to either a primary destination or a transit center and where complementary uses may be allowed.

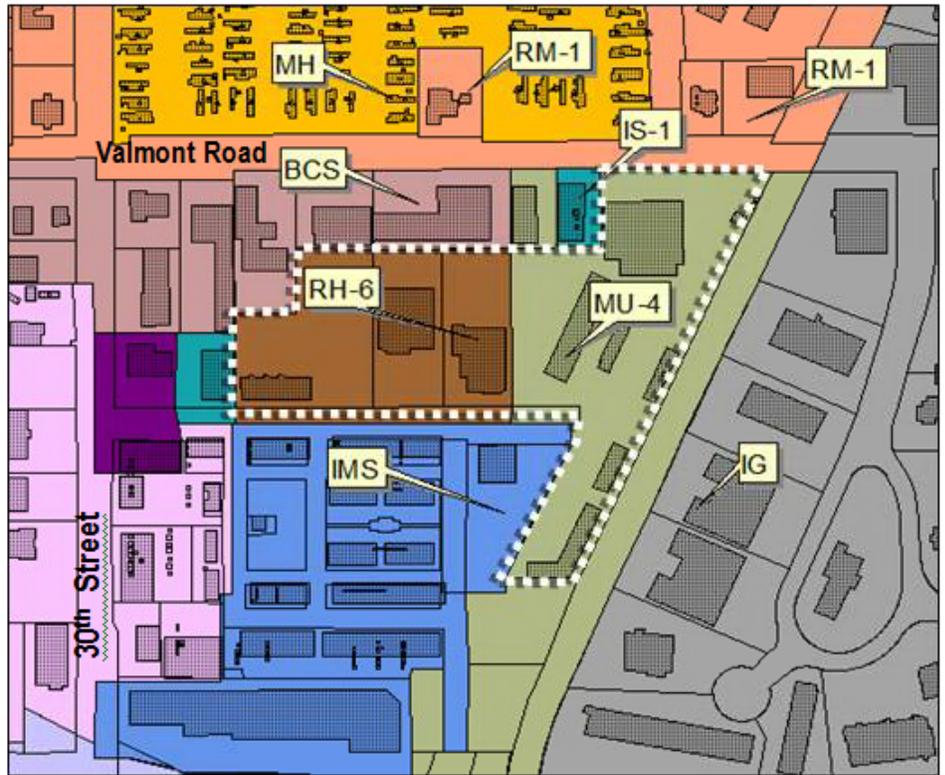


Figure 6: Zoning Map

The MU-4 zone is a relatively new district established to help implement the vision of the TVAP Mixed Use 2 Land Use. The zoning district permits up to a 2.0 FAR by-right. Portions of the zoning district were amended in 2013 to allow additional uses and greater variety in the zoning district. Added were Commercial Kitchen and Catering; Small Manufacturing uses less than 15,000 square feet; and Wholesale businesses. Also added were Live-Work units as an allowed use. A zero lot line front yard setback is permitted by-right in MU-4 with a maximum by-right height of three stories or 38 feet, which can only be modified through Site Review. The stated intent for RH-6 in the land use code (section 9-5-2, B.R.C. 1981) is for “predominately townhouses.” The intensity in the zoning district is based on provision of 1,800 square feet of lot area and 600 square feet of useable open space per dwelling unit, although there are no specific form and bulk standards that would establish a townhome unit configuration.

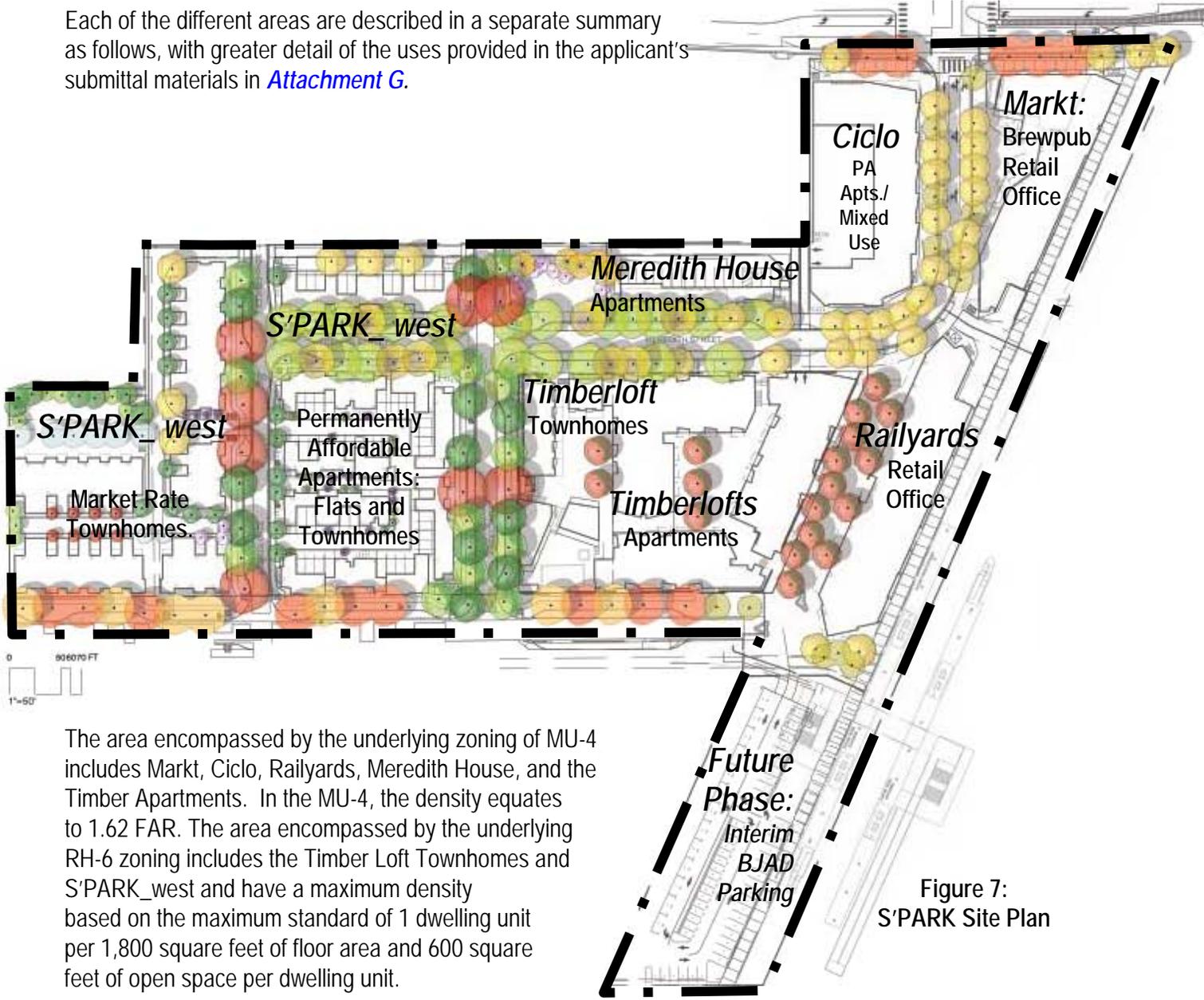
Form Based Code Pilot Program. As part of the Design Excellence Initiative, the city is piloting a Form-Based Code (FBC) in Boulder Junction, defined as the area within the adopted Transit Village Area Plan. Because the pilot process was initiated well after planning for S’PARK and S’PARK_west had begun, the findings for S’PARK application approvals must be based on the existing land use code regulations of the Site Review criteria and the TVAP intent, goals, and guidelines. However, the applicant voluntarily met on several occasions with the consultants and staff for the FBC to help refine project plans. The consultants provided information that helped to inform material use and building form, including what had been extensive use of CMU block and what had also been previous designs that over utilized arcades along building frontages, that echoed both staff and BDAB comments about materials. The applicant also took information from the consultants and reassessed some of the proportionality of the buildings to better meet some of the guiding principles of forms that utilize the harmonious proportions of the “golden mean.” In addition, the applicant included a “design excellence” sheet in the individual project plans that address key FBC concepts of materiality and building form. These are provided in the project plans in [Attachment G](#).

III. PROJECT DESCRIPTION SUMMARY

The proposed mixed use neighborhood of S’PARK is planned with seven distinct areas shown in Figure 7:

- Markt: a 55,340 square foot four story commercial mixed use building
- Ciclo: a 57,901 square foot, three story mixed use building Community Cycles non-profit organization on the ground level and with 32 permanently affordable apartments above;
- Railyards: a 70,155 square foot, four story commercial mixed use building with ground floor retail and upper story office;
- Meredith House: a 20,754 square foot, four story residential condominium building;
- Timber Lofts: a 115,000, four story mixed use building with apartments and townhomes
- S’PARK_west (3085 Bluff): 24 townhomes, and
- S’PARK_west (3155 Bluff): 45 permanently affordable townhomes and apartments.

Each of the different areas are described in a separate summary as follows, with greater detail of the uses provided in the applicant's submittal materials in [Attachment G](#).



The area encompassed by the underlying zoning of MU-4 includes Markt, Ciclo, Railyards, Meredith House, and the Timber Apartments. In the MU-4, the density equates to 1.62 FAR. The area encompassed by the underlying RH-6 zoning includes the Timber Loft Townhomes and S’PARK_west and have a maximum density based on the maximum standard of 1 dwelling unit per 1,800 square feet of floor area and 600 square feet of open space per dwelling unit.

Figure 7:
S’PARK Site Plan

Permanently Affordable Housing. S'PARK is planned to include two, 100 percent permanently affordable projects: Ciclo and Spark_west affordable housing (at 3155 Bluff Street) for a total of 56 affordable units on-site. Ciclo is proposed as affordable rental apartments to meet the inclusionary requirement for the market rate rental apartments at Timber Lofts. Spark_west affordable housing (3155 Bluff Street) is a city-funded affordable rental project. The inclusionary housing requirement for the remaining 39 market rate, for-sale units (planned at both the Meredith House and the S'PARK_west townhomes) is proposed to be met with cash-in-lieu.

Summary of the Planned Buildings. The site is planned with non-residential uses along the rail line to the east, higher density residential uses at the core, and lower density townhomes and apartments to the west and south. The area within the Mu-4 zoning district includes five buildings: Markt, Ciclo, Railyards, Timber Lofts, and Meredith. Together, the buildings have a total floor are of 374,438 square feet on 231,207 square foot (or 5.31 acres) for a total FAR of 1.62 where 2.0 FAR is the standard.

Markt. Planned as a non-residential building aligning the railroad tracks it includes a mixture of office, retail and restaurants. The Markt building is proposed to be 55,340 square feet and four to five stories in height. The ground floor and a mezzanine level on the northern portion of the building is planned as an approximately 7,832 square foot brewpub consisting of a 3,202 square foot tap room restaurant with 182 indoor seats and a 4,630 square foot brewing production area along with outdoor seating area of 50 seats. The brewpub is planned to be operated from 7:00 a.m. until 11:00 p.m. with the morning hours for staff operations of the brewing process. The remaining ground floor area is planned as three "micro-restaurants" ranging in size from 866 to 1,326 square feet with a total of 56 interior seats and 34 outdoor seats (refer to Management Plan in [Attachment B](#)); along with an approximately 500 square foot bike storage room. A total of 56 long term bike spaces are planned along with an additional 30 short term bike parking spaces are provided.

The building is designed as a contemporary building in form, and with the use of exterior materials that recall the former industrial lumber yard. Proposed finish materials include two shades of silver-grey cedar siding, galvanized standing seam metal roof, weathered galvanized metal panels, steel columns and vertically score-jointed stack bond concrete masonry units along the railroad tracks.



Railyards. The other building aligning the tracks is planned as a 70,155 square foot non-residential building, with the ground floor intended to house retail and restaurants and three stories of office above. Two restaurants are planned, both facing the woonerf shared street: one on the north end of the building and one on the south end. The restaurant planned on the south end is proposed to be 3,500 square feet with 120 interior seats and 30 outdoor seats. The north end restaurant is planned to be 2,500 square feet in size with 82 interior seats and 30 exterior seats. Both restaurants intend to operate from 8:00 a.m. to 11:00 p.m. (refer to Management Plans in [Attachment G](#)).

The Railyards building is intended to frame the east side of a planned woonerf "shared street" with ground floor uses intended to activate the woonerf space. There are 32 long term bike parking spaces and 28 short term bike spaces proposed. While the building is planned to be a part of the Boulder Junction Access District where no parking is required for non-residential uses, there are 69 parking spaces planned on a surface parking lot to the south of the site to serve S'PARK and the Boulder Junction Access District (BJAD). The site can be adapted in the future through a site review amendment if a wrapped, structured parking is later proposed.



The building is planned with a more traditional configuration and fenestration, and evocative of an historic industrial warehouse building. The materials include a stacked bond, ground face CMU block along with grey tone rainscreen panels and aluminum composite panels.

Ciclo. Planned along the western side of Junction Place is a 50,667 square foot mixed use building of three stories. A mezzanine space in between the first and second floors is planned with the total height planned to be a maximum of 45 feet. The ground floor is proposed as the offices of Community Cycles that functions as retail/workshop and educational space. There is also a small lobby area for the 32 permanently affordable residential units planned on the two upper stories. There's a second story roof top open space deck that covers on-site parking behind the building. There's also flex gallery space and an art display space on the deck. The building is planned with simple uniformity and includes groundfloor storefront spaces for Community Cycles; and a two story (mezzanine) lobby and gallery space on the south at intersection of Junction Place and the planned woonerf. Materials include stacked bond buff colored brick; powder coated corrugated metal siding along with silver cedar planks. Corten steel is utilized as an accent and the finish of the building is intended to be a contemporary use of industrial-inspired materials.



Meredith House. West of Ciclo is a 15-unit residential condominium building of approximately 20,750 square feet. The ground floor is planned for lobby, office and meeting space. The upper three floors have five condominium units each. Fifteen parking spaces are tucked under the back of the building. The building is contemporary in character and finish materials. Materials include blue rainscreen panels, contemporary window patterns,



and small areas of CMU, perforated metal railing panels on the exterior decks, and accents of Douglas Fir on soffits and as a canted structural timber element near the main entry. A roll-up door accesses the meeting space and opens onto the Meredith Park that will serve residents of the units as well as other community members. Planned are 30 long term bike spaces and 22 short term bike spaces.

Timber Lofts Apartments. Located centrally on the plan, the Timber Lofts is proposed as a 167,288 square foot mixed use, four story building with ground floor retail and office along with 121 apartment units that include 28 efficiency living units, 56 one-bedroom units, 31 two-bedroom units and six three-bedroom units. Two levels of parking for 121 motor vehicles are proposed below grade, unbundled from the residential units and offered as a part of the Boulder Junction Access District (BJAD). There are 209 long term bike parking spaces and 74 short term spaces proposed. There is an interior resident courtyard space with planters of varying height, a pool and fitness area, and light wells into the parking below. The apartments that front the street are accessed into the units by stoops along the street. The building is contemporary with finish materials predominately including buff colored brick, v-ribbed metal cladding, fiberglass window and stucco accents at the inset of balconies.



Timber Lofts Townhomes. As a part of the Timber Lofts building, there are eight 3-story townhomes planned on the west and northwest corner. Designed as a contemporary interpretation of brownstone townhouses, the units open to the street with stoops and planters. The townhomes are finished with standing seam metal cladding with metal spandrel panel between and accoya wood siding in a cedar finish at the ground level. These units are located within the RH-6 zoning district and are therefore required to meet the density standards of 1 unit per 1,800 square feet of lot area.



S'PARK_west Permanently Affordable Residential. Across the extension of Junction Place from the Timber Commons townhomes are 45 permanently affordable attached residential units. The residential units consist of 21 townhomes, three of which are three-bedroom and 18 of which are two bedroom units. There are also eight two-bedroom flats. Also proposed is a central community house. The ground entrances all face the street and have porches or stoops to help foster a sense of community. The project is funded through a series of funding mechanisms involving various agencies including city affordable housing funds. The buildings themselves are intended to be in keeping with the contemporary character established throughout the entire S'PARK neighborhood with finish materials that include vertically oriented, powdercoated red ribbed metal siding; dark grey and buff colored, vertically stacked brick; horizontally oriented, powdercoated bronze metal siding; accents of stucco and clear stained cedar.



S'PARK_west Townhomes. S'PARK_west includes 24 market rate townhomes as part of the proposed mixed-income neighborhood on the far western end of the proposed development. The buildings have the appearance of contemporary brownstone townhouses and are predominately comprised of brick, powdercoated metal siding, fiber cement board siding, and vertical wood screen accents on front porches and on upper story decks. Throughout S'PARK_west, there are 90 long term bike parking spaces and 72 short term spaces.



Requests for Modifications through the Site Review Process. The proposal is within the by-right FAR and density standards and no modifications are planned to density. The applicant has requested several modifications to the MU-4 and RH-6 Form and Bulk standards of the Land Use Code. As indicated in Section 9-2-14(a) B.R.C. 1981, the purpose of the Site Review process is to allow flexibility and encourage innovation in the development process. As a part of the Site Review process, development standards can be modified if the Site Review criteria can be satisfied refer to [Attachment A](#) for staff's analysis of the Site Review Criteria.

For MU-4 Buildings:

- **Minimum front yard setback from a street for 3rd story & above:**
Request to allow for zero setbacks for the third story and above where 20 feet is the standard by-right setback for all of the MU-4 zoned buildings: Markt, Ciclo, Railyards, Meredith House, and Timber Lofts.
- **Maximum front yard setbacks on corner lots where 10 feet is the maximum:**
Request to allow for greater front yard setbacks along Valmont Road for Ciclo building of 22'-3"; Timber Lofts building of 12'-9"; and Markt building of 16'-5".
- **Maximum side yard setbacks adjacent to streets where 10 feet is the maximum:**
Request to allow for greater front yard setbacks along 34th Street/Junction Place of 16'-7", Timber Lofts building of 11'-11"; and Markt building of 13'-9".
- **Maximum number of stories:**
Request to allow all of the MU-4 zoned buildings: Markt, Ciclo, Railyards, Meredith House, and Timber Loft to be four and five stories in height where three stories is the by-right standard.
- **Maximum principal building height:**
Request to allow the buildings to exceed the by-right height maximum of 38 feet:
Ciclo: 44'-6"; Markt: 49'-1"; Railyards: 50'-0"; Timber Lofts: 49'-8"; Meredith House: 47'-2"
- **Maximum floor area of any principal building:**
Request to allow the buildings to be greater than the standard by-right maximum of 15,000 square feet
Ciclo: 57,901 square feet; Markt: 55,340 square feet; Railyards: 70,155 square feet; Timber Lofts: 167,228 square feet; Meredith House.

The proposed project includes several requests for modifications to the RH-6 Form and Bulk standards of the Land Use Code. These modifications would be considered through the Site Review process, and are listed below:

For MU-4 Buildings:

- Minimum front and side setbacks:

Request to allow for reduced setbacks to establish an urban form.

3085 Bluff: Front on Bluff: 0 lot line where 15 feet is standard; Front on Meredith Street: 11 feet where 15 feet is standard; Side facing a street: 3'-8" where 10 feet is minimum.

3155 Bluff: Front on Bluff: 0 lot line

KEY ISSUES: Staff has identified the following key issues to help guide the board's review and discussion of the proposal.

Key Issue 1: Does the proposed project, including the requested modifications to height, number of stories, and setbacks meet the Site Review criteria of Land Use Code section 9-2-14(h), B.R.C. 1981?

The project was found to be consistent with the Site Review Criteria of section 9-2-14(h), B.R.C. 1981 in that the proposed project will provide a new mixed use and mixed income neighborhood with pedestrian amenities and buildings of high caliber design and materials. As a part of a consistency analysis with Site Review criteria, the project is found to be consistent with a significant number of BVCP policies (the BVCP policies can be found in entirety [here](#)). A consistency analysis of the proposed project with the site review criteria is provided in [Attachment A](#). As noted in the BVCP,

"Many of the key policies in the Boulder Valley Comprehensive Plan stem from long-standing community values and represent a clear vision of our community"

In that regard, the BVCP notes the city's commitment to environmental, economic, and social sustainability for a welcoming and inclusive community where there is a culture of creativity and innovation and where *"compact, contiguous development and infill supports evolution to a more sustainable form."* Among the most relevant BVCP policies that the proposed project is found to be consistent with are the following:

- 1.02 Principles of Environmental Sustainability
- 1.03 Principles of Economic Sustainability
- 1.04 Principles of Social Sustainability
- 2.01 Unique Community Identity
- 2.03 Compact Development Pattern
- 2.09 Neighborhoods as Building Blocks
- 2.14 Mix of Complementary Land Uses
- 2.16 Mixed Use and Higher Density Development
- 2.17 Variety of Activity Centers
- 2.21 Commitment to a Walkable and Accessible City
- 2.22 Improve Mobility Grid
- 2.30 Sensitive Infill and Redevelopment
- 2.32 Physical Design for People
- 2.37 Enhanced Design for Private Sector Projects
- 4.05 Energy-Efficient Building Design
- 7.01 Local Solutions to Affordable Housing
- 7.02 Permanently Affordable Housing
- 7.04 Strengthening Community Housing Partnerships
- 7.06 Mixture of Housing Types
- 7.09 Housing for a Full Range of Households

Key Issue 2: Is the proposed urban design and planning for the overall plan and the individual areas consistent with the Transit Village Area Plan (TVAP) intent and design guidelines?

The urban design and planning for the application was found to be substantially consistent with the Transit Village Area Plan. The following is an analysis of the consistency with the intent of the goals of the Transit Village Area Plan.

Attachment E provides a consistency analysis of the proposed project with the TVAP design guidelines which is organized in a matrix format.

Transit Village Area Plan. TVAP is one of four adopted area plans within the Comprehensive Plan, where on pages 67 to 69 the plan describes the purpose of Area Plans as a *"means to provide direction for specific geographic areas, and bridge the gap between the broad policies of the Comprehensive Plan and site specific project review"* and to address *"appropriate character, scale and mix of uses and if regulatory changes are needed to ensure or encourage appropriate development."*

In the BVCP, the purpose of TVAP is noted as follows:

"To describe the city's vision for the future of the 160-acre Transit Village area and guide the long term development of the area. The area is defined as within walking distance to the future FasTracks transit services – commuter rail, bus rapid transit, and regional bus services."

Proposed Project Area under the TVAP Land Use Plan. Redevelopment on the project site is guided by two land use designations under TVAP: MU2 (Mixed Use 2) Land Use (which aligns with Mixed Use Business under the BVCP) for the area encompassed on the east side of the project site nearest the railroad tracks and HDR1 (High Density Residential -1) Land Use (which aligns with High Density Residential under the BVCP) for the areas toward the west and near the adjacent Steelyards neighborhood. TVAP includes land use prototypes as guides to understanding the intent and anticipated building forms and uses typically associated with each land use category.

MU2 Land Use: The MU2 land use prototype is shown in **Figure 8** on the following page. As proposed, the building forms, uses, density and massing of the various buildings within the proposed project appear to be consistent with the analogs provide in TVAP. The buildings proposed within the MU2 area: Markt; Cyclo; Railyards; Meredith House, and Timber Lofts are proposed to be three to five stories, consistent with the MU2 land use and as shown in Figures 9a through 9e.

Mixed Use -2-	1.5 - 2.0 Floor Area*
	
<p>Three- to four-story mixed-use buildings. Predominant use may be business or residential. Mostly structured or first-floor parking; may have some surface parking.</p>	
<p>Figure 8: Excerpt from TVAP: Intent of MU2 land use</p>	



9a: Markt



9b: Railyards



9c: Ciclo



9d: Timber Lofts



9e: Meredith House

Figure 9 a through 9e: Buildings within S'PARK MU2 land use district

The area encompassed on the west side of the project site is within TVAP Land Use "HDR-1" or "High Density Residential – 1". The HDR-1 land use prototype is shown in Figure 10. The SPARK_west project located within the HDR1 land use area of TVAP consists of two and three story townhomes and flats as shown below in Figure 11a thru 11c. Staff finds that the proposed S'PARK_west buildings are consistent with the HDR1 intent of the massing and scale.



Urban townhomes and garden apartments with individual garages, surface parking lots, or underground parking. Mainly two to three stories.

Figure 10: Excerpt from TVAP: Intent of HDR1 land use



Figure 11a: Townhomes



11b. Townhomes



11c. Flats and Townhomes

Character Districts in TVAP. To further express the intent of the various areas within TVAP, the Boulder Junction area was divided into eight "character districts" with the plan shown in Figure 12. The intent in the districts was primarily based on future land use and to "promote a particular urban design character" for each area. There are two predominate "character districts" within the site: the "Rail Plaza District" and the "Steelyards District." As noted on page 23 of TVAP, the Rail Plaza District ultimately will, "host the Boulder stop on the new commuter rail service to Denver and Longmont." While the timing of the rail is unknown, the intent is to lay the urban design groundwork for the eventual location of a rail stop in Boulder.

The Rail Plaza district was defined in TVAP on page 27 as, "The district will evolve into a high-density, commercial and residential mixed use area, with three- to five-story buildings." Staff finds that the buildings within the Rail Plaza district: Markt, Railyards, Ciclo, Meredith House, and Timber Lofts all meet the intent of the Rail Yards character district.

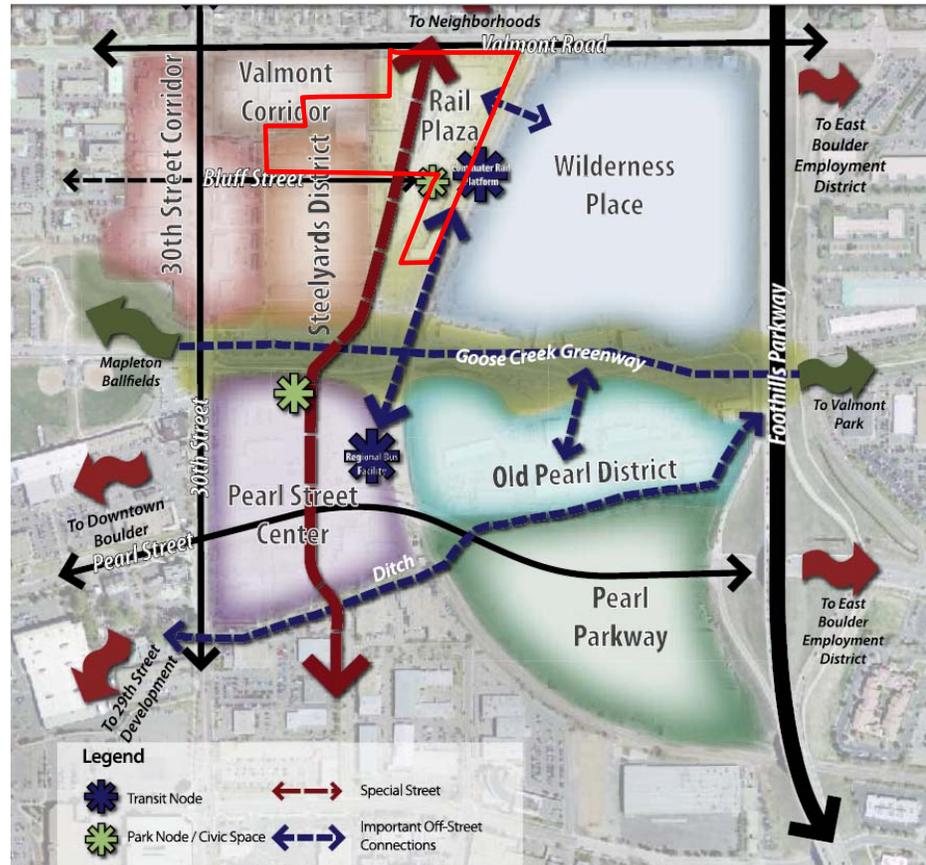


Figure 12: Character District Vision Plan of TVAP

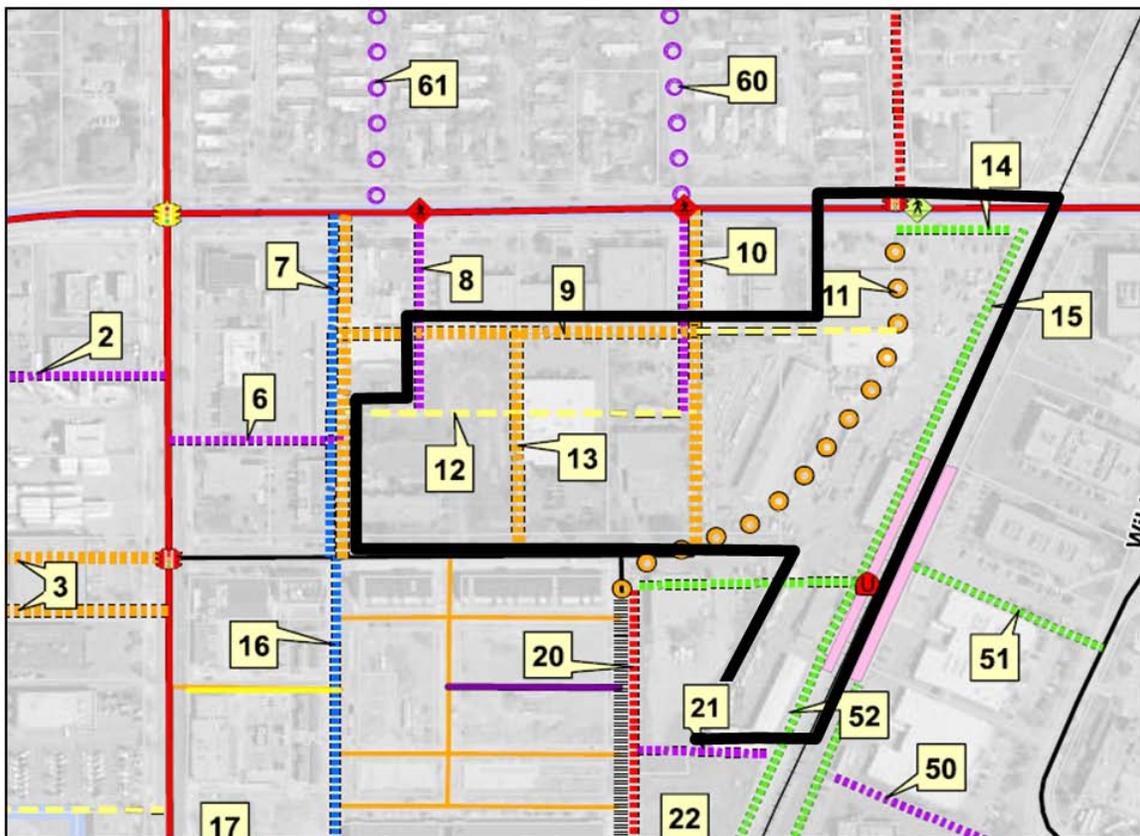
The Steelyards District is acknowledged as being essentially built out on the southern portion of the area, but that for the areas within the planned S'PARK_west neighborhood, on page 32 of TVAP it is noted, "The industrial uses on the north side of Bluff Street will transition to high-density residential, such as urban townhouses." Staff finds that the buildings within the Steelyards district: the S'PARK_west townhomes and flats meet the character district intent.

Key Issue 3: Is the proposed change to the Transit Village Area Plan Connections Plan for connections no. 9, 10, 12 and 13 consistent with the requirements of such changes under TVAP?

TVAP Connections Plan. A connections plan was adopted along with the land use plan for TVAP that includes a number of motor vehicle, bike and pedestrian connections through the site, as delineated in Figure 13. Equally as important as Land Use, the connections plan is intended to,

Create walkable streets in a fine grain grid pattern, providing for walking, biking and possible car free zones. Provide multimodal connections within the area to adjacent neighborhoods and to key nearby destinations and activity areas.

The full text of the connections plan is found beginning on page 56 of TVAP, provided [here](#). As shown on the connections plan, there is an expectation to extend Junction Place through the site, although the alignment is considered, "flexible." Similarly, a multi-use path is shown along the west side of the tracks to access the rail platform. A below grade crossing is shown at the rail platform, that would be constructed by RTD at the time of the implementation of FASTracks rail service.



- | | |
|--|--|
| 8: Pedestrian connection | 15: Multi-use path along west side of railroad tracks |
| 10: Local Road and pedestrian connection | 20: On-Street Bike lanes: Bike connection between Bluff Street and Goose Creek |
| 11: Local Connection with <u>flexible alignment</u> (Junction Place) | 21: Pedestrian Connection to Rail platform |
| 14: Multi-use path | 22: Multi-use path along west side of railroad tracks |

Figure 13: TVAP Connections Plan

The TVAP connections plan was established to break up the existing “superblocks” where no connectivity exists today. There is an assumed level of interpretation within the connections plan, as noted on page 59 of TVAP, Appendix 3, Connections Explanation and Rationale,

“The purpose of this appendix is to provide a detailed explanation and rationale for each connection on the Transportation Connections Plan. It will be used to help interpret the Connections Plan for capital improvement planning and review of individual development review applications.”

Modifications to TVAP require Planning Board approval and, if connections are consolidated or eliminated, City Council approval as well. Staff finds the proposed modifications, specified below, to meet the intent of TVAP. Given the goals of connectivity and specific intent for each connection, staff reviewed an earlier iteration of the proposed connections with the applicant and recommended revisions to be more in keeping with the original intent of each connection. The applicant revised their plan accordingly and staff finds the proposed requests to change the connections plan to meet the intent of the connections plan.

The proposed modifications to the TVAP connections plan are illustrated in a comparison in Figures 14a and 14b with the connections plan and the proposed connections respectively. For the proposed project, and the streets intended to be dedicated per the Connections Plan (numbers 9, 10, 12, and 13), the applicant is proposing the following:

Proposed Changes to the Connections Plan:

- Connection 9 becomes connection 12: an alley to serve Valmont Road properties,
- Connection 12 becomes connection 9: a local east west roadway,
- Connection 10 is consolidated with Junction Place from Bluff Street to Meredith Street and is relocated to the west, and
- Move local connection 13 approximately 50 feet to the west to ultimately connect to Valmont Road.

Note that connection 8 is planned to be implemented through the site as shown in TVAP. As shown in the comparison of proposed Connections Plan (Figure 14a) to the existing connections plan in Figure 14 b, the applicant does not need connection 7 to serve the project and is therefore does not need to construct the connection which today. At this point connection 7 wouldn't connect to Valmont Road due to existing buildings to the north that are unlikely to redevelop in the near future given the Service Commercial zoning. The partial consolidation of 10 and Junction Place is an alignment of Junction Place orthogonally through the site, until the intersection with local connection 9. At that point, Junction Place is intended to move through the S'PARK development to the east and ultimately turn north to connect to Valmont Avenue at 34th Street. TVAP notes that the alignment is flexible, and the applicant has created a more orthogonal grid to serve the urban design of the proposed new neighborhood. This would be consistent with the expected local street section for this area of Junction Place under TVAP, as described in TVAP in page 37,

- *Segment 3 is the northern section from Bluff Street to Valmont Street. This section will have more vehicle traffic than the middle section and will have a local street cross-section (see next page).*

The addition of the woonerf (shared pedestrian street) serves much of the anticipated function of Junction Place in the middle of the property on the east in that it is planned as a street that is slow moving and not intended as a “cut-through” street but one that holds a number of pedestrian amenities.

Early in the planning process for S’PARK and S’PARK_west in 2014, the applicant worked with the city’s senior urban designer and senior transportation staff to review the proposed changes to the TVAP connections plan. Because it was acknowledged at this time that the number of connections thorough the three properties on Bluff (3085, 3155 and 3195) significantly constrained the developability. Staff, in working with the applicant noted that the changes planned to the connections could be made in a manner that is equivalent to the TVAP connections plan in that there ultimately could be three connections that intersect with Valmont Road in this location. Staff finds that the proposed changes will meet the intent of connection no. 7 and that it will serve the properties to the west and ultimately connect to the north; connection no. 10 will also connect to the north as properties redevelop and Junction Place will connect to Valmont Road through this project.

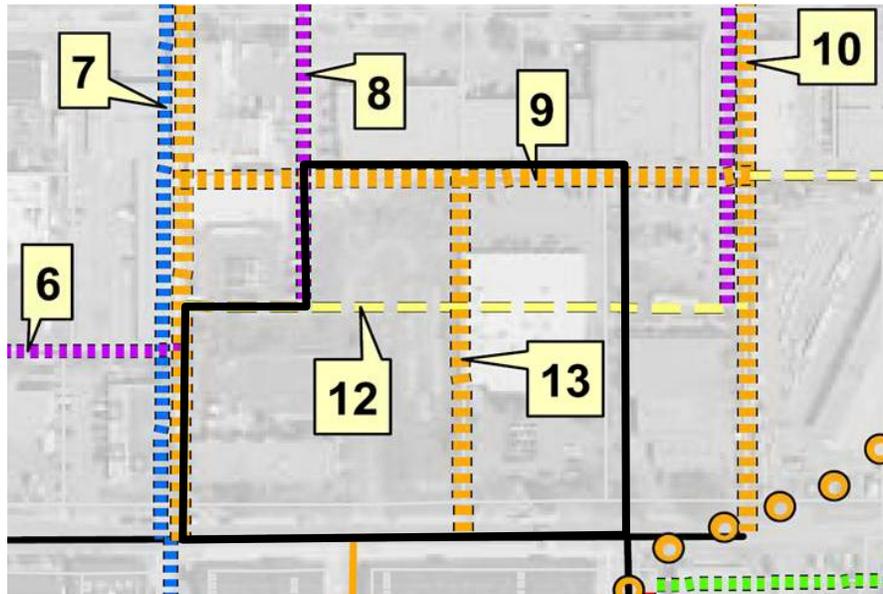


Figure 14a: Existing TVAP Connections Plan within S’PARK_west



Figure 14b: Proposed TVAP Connections Plan within S’PARK_west

As originally planned, one of the primary functions for Junction Place will be as a spine road and a bike, pedestrian and transit connector between the bus facility at Depot Square and the future rail stop, planned to be located within the subject property.

While the potential for commuter rail into Boulder is uncertain, with most reports indicating a timeline for potential construction 30 years from now, the proposed project does illustrate the civic rail plaza space that adjoins the planned woonerf. Together these spaces can serve as public gathering space that was anticipated for the plaza.

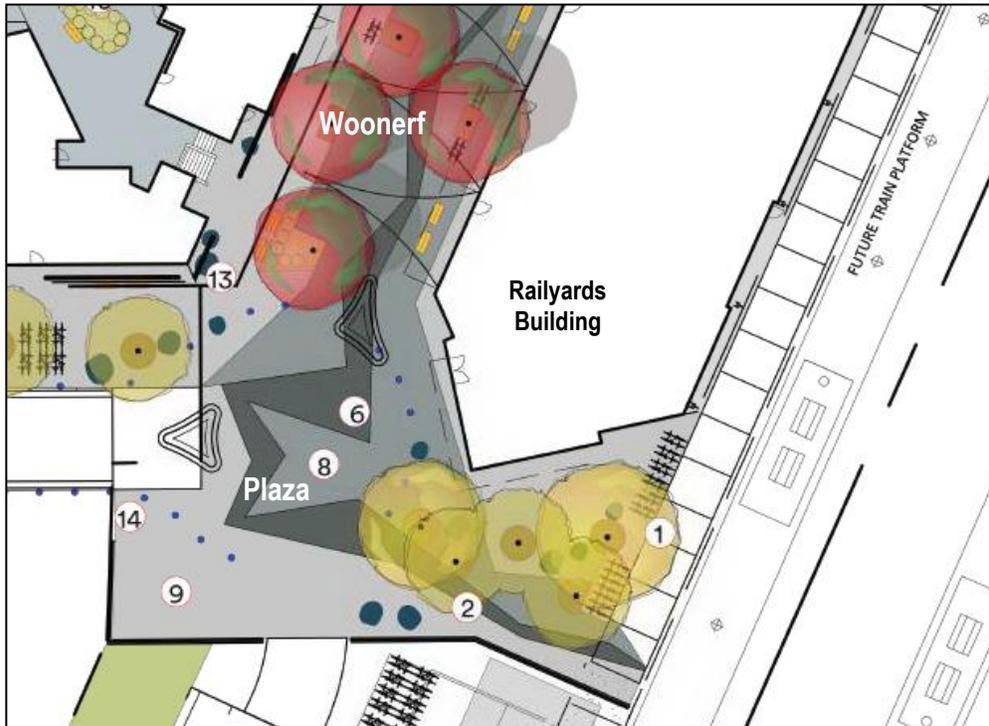


Figure 15a: Proposed Plaza and Woonerf



Figure 15b: South End of Railyards Building near Proposed Plaza and Woonerf

TVAP Connections Amendment Process. As noted on page 42 of TVAP, amendments to the Connections Plan generally will be reviewed either administratively or by the Planning Board. The process provides some flexibility to relocate proposed facilities to reflect site-specific considerations while ensuring that the connections necessary to realize a fully integrated multimodal network are created. Significant changes to key proposed connections require an amendment to the plan by the Planning Board. In most cases, elimination of a proposed connection requires approval by both the Planning Board and City Council. Minor variations from the plan can be approved by the city manager. Amendment requests can be processed in conjunction with a Site Review. In this case, what is proposed technically constitutes an elimination and therefore, City Council approval will be required (and a condition of approval has been added to that effect).

Any amendment to the Connections Plan will be permitted upon a finding that one of the criteria has been met:

1. Such amendment is due to a physical hardship or practical hardship that would prevent construction of the connection;

Relocation of connection no. 13 allows the applicant to construct the 45 proposed permanently affordable dwelling units. If the connection bisected this area, the number of affordable units would have been reduced.

2. The connection is made in a manner that is equivalent to the connection shown on the Connections Plan; or

The relocation to the west and the consolidation of connection no. 10 with Junction Place from Bluff Street to Meredith Street serves the connection and connection no. 10 is not precluded from extending north as properties to the north redevelop. In addition, the Junction Place alignment is considered flexible and the intent of an amenitized street for Junction Place is fulfilled with the woonerf.

3. Such amendment is consistent with the objectives of the Connections Plan described above. In those instances where the standards above cannot be met, the amendment will be considered legislative in nature and require approval by the Planning Board and City Council.

The amendments to connections, 9, 10, 12, and 13 are consistent with the objectives of the connections plan. However, because there is a partial elimination of connections 9 and 10, City Council must approve requested amendments as noted on page 43 of TVAP shown below.

Approval Requirements for Amendments to the Connections Plan				
	Relocation greater than 50' or onto an adjacent property	Relocation less than 50'	Addition	Elimination
Streets				
Collector Street	Planning Board	Administrative	Planning Board	Planning Board and City Council
Local Street	Planning Board	Administrative	Administrative	Planning Board and City Council
Alley	Administrative	Administrative	Administrative	Administrative except along 29 1/2 alignment ²
Paths				
Multi-use Path	Administrative	Administrative	Administrative	Planning Board and City Council
Pedestrian Walkway	Administrative	Administrative	Administrative	Planning Board and City Council

² Elimination of the alley proposed along the 29 1/2 alignment must be approved by the Planning Board and City Council.

Key Issue 4: Do the Use Review applications for the Brewpub and restaurants meet the Use Review Criteria of section 9-2-15(d), B.R.C. 1981?

The project was found to be consistent with the Use Review Criteria of section 9-2-15, B.R.C. 1981 in that, the proposed brewpub and restaurants will be operated no later than 11:00 p.m. to minimize impacts to nearby planned and existing residential. The provision of restaurants and a brew pub enhance the mix of uses proposed and was also found to provide a convenience to nearby residential and offices. A consistency analysis of the proposed project with the Use Review criteria is provided at the end of [Attachment A](#).

Public Notification

Required public notice was given in the form of written notification mailed to all property owners within 600 feet of the subject site and a sign posted on the property for at least 10 days. A second public notice was sent to all property owners within 600 feet along with neighborhood group contacts for both Orchard Grove Mobile Home Park and San Juan del Centro Apartments notifying neighbors of both the Planning Board hearing and a Good Neighbor Meeting. On Aug. 24, 2015, a Good Neighbor Meeting was held on site at 3390 Valmont Rd. The intent of the meeting was to present the project plans and the management plan for the proposed restaurants. There were five attendees, four of whom worked in nearby office buildings and one of whom was a property owner on 30th Street. All of the attendees indicated support for the proposed project. All notice requirements of section 9-4-3, B.R.C. 1981 have been met. There were several comment letters received all of which indicated support and interest in the proposed project. Refer to [Attachment C](#) for the public comments that were received. The applicant also indicated to staff that the applicant team met at various times prior to Site and Use Review application with neighbor representatives for surrounding HOAs.

Findings and Recommendation

Planning staff finds that the proposed applications for Site and Use Review meet the Site Review criteria found section 9-2-14(h), B.R.C., 1981, the Use Review Criteria of section 9-2-15, B.R.C. 1981. Therefore, staff recommends that the Planning Board approve Site Review no. LUR2015-00010 and Use Review no. LUR2015-00011, incorporating this staff memorandum and the attached Site and Use Review Criteria Checklists as findings of fact, subject to the following recommended conditions of approval.

Recommended Conditions of Approval

SITE REVIEW LUR2015-00010

1. The Applicant shall ensure that the **development shall be in compliance with all plans prepared by the Applicant** on August 21, 2015 on file in the City of Boulder Planning Department, except to the extent that the development may be modified by the conditions of this approval.
2. Prior to a building permit application, the Applicant shall submit a Technical Document Review application for the following items, subject to the approval of the City Manager:
 - a. **Final architectural plans**, including material samples and colors, to insure compliance with the intent of this approval and compatibility with the surrounding area. The architectural intent, elevations, plans and details shown on the approved plans dated August 21, 2015 is acceptable. The final architectural plans shall show the transformer boxes currently shown near the southwest corner of the proposed Markt building and on the northwest corner of the planned Railyards Building; relocated to a less visible location or architecturally screened or integrated into a building or other site feature. The City Manager will review plans to assure that the architectural intent is performed. The project plans shall also illustrate an outdoor seating area for the micro restaurants of less than 300 square feet per micro restaurant or be subject to Use Review for outdoor seating of 300 feet or greater within 500 feet of a residential use module.
 - b. A **final site plan** which includes detailed floor plans and section drawings.
 - c. A **final utility plan** meeting the City of Boulder Design and Construction Standards.
 - d. A **final storm water report and plan** meeting the City of Boulder Design and Construction Standards.
 - e. **Final transportation plans** meeting the City of Boulder Design and Construction Standards, Standard Specifications for Traffic Signal Materials and Installation and CDOT Access Code Standards, for all transportation improvements. These plans must include, but are not limited to: street plan and profile drawings, multi-use path plan and profile drawings; street and multi-use path cross-sectional drawings, traffic signal plans; signage and striping plans in conformance with Manual on Uniform Traffic Control Devices (MUTCD) standards, transportation and transit stop detail drawings, geotechnical soils and pavement analysis.
 - f. A **detailed landscape plan**, including size, quantity, and type of plants existing and proposed; type and quality of non-living landscaping materials; any site grading proposed; and any irrigation system proposed, to insure compliance with this approval and the City's landscaping requirements. Landscape plans shall provide significant amounts of plant material sized in excess of the landscaping requirements of Sections [9-9-12](#), "Landscaping and Screening Standards," and [9-9-13](#), "Streetscape Design Standards," B.R.C. 1981. Removal of trees must receive prior approval of the Planning Department. Removal of any tree in City right of way must also receive prior approval of the City Forester.

- g. A **detailed outdoor lighting plan** showing location, size, and intensity of illumination units, indicating compliance with section 9-9-16, B.R.C.1981.
 - h. A **detailed shadow analysis** to insure compliance with the City's solar access requirements of section 9-9-17, B.R.C.
3. Prior to a building permit application, the Applicant shall submit an application for a revised **Preliminary Plat** and a Technical Document Review application for a **Final Plat**, subject to the review and approval of the City Manager and execute a subdivision agreement meeting the requirements of chapter 9-12, "Subdivision," B.R.C. 1981 and which provides, without limitation and at no cost to the City, for the following:
- a. The dedication, to the City of all easements and right-of-way necessary to serve the development, including, but not limited to, the easements shown on the approved plans dated August 21, 2015 on file in the City of Boulder Planning Department, and the dedication of a 14-foot wide public access easement (for pedestrians and bicyclists) from the terminus of the Bluff Street right-of-way to the public access easement being dedicated for the north/south multi-use path adjacent to the BNSF Railway right-of-way.
 - b. The vacation of all easements where vacations are necessary for construction of the development.
 - c. The construction of all public improvements necessary to serve the development, including, but not limited to, the following:
 - i. A traffic signal at the intersection of Valmont Road at 34th Street.
 - ii. A left-turn lane and "quiet zone" raised median on westbound Valmont Road east of 34th Street.
 - iii. A transit stop on Valmont Road.
 - iv. A six-foot side landscape strip and ten-foot wide detached multi-use path/sidewalk on Valmont Road.
 - v. A 12-foot wide multi-use path along the eastern property line and adjacent to the BNSF Railway Company right-of-way from the southern property line to Valmont Road. The applicant is responsible for connecting the multi-use path to the approved multi-use path to be constructed by "The Commons" Project (located at 2440 and 2490 Junction Place) and for providing a temporary multi-use path around the existing building on Lot 4 of Block 5.
 - vi. A raised concrete table on 34th Street within the street's roadway curve at a location approved by staff which will allow for a future crosswalk pursuant to the City's Pedestrian Crossing Treatment Installation Guidelines when warrants are meet.
 - vii. Street lighting along Junction Place, Meredith Street and 34th Street.
 - viii. Extending the left-turn lane on southbound 30th Street at Bluff to 75-feet.
 - ix. The north side of Bluff Street from 31st Street to the road's terminus at the proposed Woonerf.
 - x. 32nd Street from Bluff Street to the north property line.
 - xi. Junction Place from Bluff Street to the north property line.
 - xii. Meredith Street from 32nd Street to 34th Street.
 - xiii. All alleys with a dedicated public access easement.
 - xiv. All sidewalks with a dedicated public access easement.

- xv. Stormwater quality improvements and stormwater detention improvements, including but not limited to permeable parking lot paving.

USE REVIEW BREW PUB: LUR2015-00011

1. The Applicant shall ensure that the **development shall be in compliance with all plans prepared by the Applicant** on August 21, 2015 and the Applicant's written statement dated August 21, 2015 on file in the City of Boulder Planning Department, except to the extent that the development may be modified by the conditions of this approval. Further, the Applicant shall ensure that the approved use is operated in compliance with the following restrictions:
 - a. The Applicant shall operate the business in accordance with the management plan dated August 21, 2015 which is attached to this Notice of Disposition.
 - b. The approved use shall be closed from 11:00 p.m. to 7:00 a.m. seven days per week.
 - c. No electronically amplified music or other entertainment shall be provided on the patio after 10:00 p.m.
 - d. Size of the approved use shall be limited to 9,700 square feet. The total number of indoor seats for the approved use shall not exceed 182. Patio area will not exceed 50 outdoor seats.
 - e. All trash located within the outdoor dining area, on the restaurant property and adjacent streets, sidewalks and properties shall be picked up and properly disposed of immediately after closing.
2. The Applicant **shall not expand or modify the approved use**, except pursuant to subsection 9-2-15(h), B.R.C. 1981.

USE REVIEW CONDITIONS FOR THE 3,500 SQUARE FOOT RESTAURANT LOCATED AT THE SOUTH END OF THE RAILYARDS SUITE: LUR2015-00011

1. The Applicant shall ensure that the **development shall be in compliance with all plans prepared by the Applicant** on August 21, 2015 and the Applicant's written statement dated August 21, 2015 on file in the City of Boulder Planning Department, except to the extent that the development may be modified by the conditions of this approval. Further, the Applicant shall ensure that the approved use is operated in compliance with the following restrictions:
 - a. The Applicant shall operate the business in accordance with the management plan dated August 21, 2015 which is attached to this Notice of Disposition.
 - b. The approved use shall be closed from 11:00 p.m. to 7:00 a.m. seven days per week.
 - c. No electronically amplified music or other entertainment shall be provided on the patio after 10:00 p.m.
 - d. Size of the approved use shall be limited to 3,500 square feet. The total number of indoor seats for the approved use shall not exceed 120 seats. Patio area will not exceed 30 outdoor seats. All

trash located within the outdoor dining area, on the restaurant property and adjacent streets, sidewalks and properties shall be picked up and properly disposed of immediately after closing.

2. The Applicant **shall not expand or modify the approved use**, except pursuant to subsection 9-2-15(h), B.R.C. 1981.

USE REVIEW CONDITIONS FOR THE 2,500 SQUARE FOOT RESTAURANT LOCATED AT THE NORTH END OF THE RAILYARDS SUITE: LUR2015-00011

1. The Applicant shall ensure that the **development shall be in compliance with all plans prepared by the Applicant** on August 21, 2015 and the Applicant's written statement dated August 21, 2015 on file in the City of Boulder Planning Department, except to the extent that the development may be modified by the conditions of this approval. Further, the Applicant shall ensure that the approved use is operated in compliance with the following restrictions:
 - a. The Applicant shall operate the business in accordance with the management plan dated August 21, 2015 which is attached to this Notice of Disposition.
 - b. The approved use shall be closed from 11:00 p.m. to 7:00 a.m. seven days per week.
 - c. No electronically amplified music or other entertainment shall be provided on the patio after 10:00 p.m.
 - d. Size of the approved use shall be limited to 2,500 square feet. The total number of indoor seats for the approved use shall not exceed 82 seats. Patio area will not exceed 30 outdoor seats.
 - e. All trash located within the outdoor dining area, on the restaurant property and adjacent streets, sidewalks and properties shall be picked up and properly disposed of immediately after closing.
2. The Applicant **shall not expand or modify the approved use**, except pursuant to subsection 9-2-15(h), B.R.C. 1981.

Approved By:



David Driskell, Executive Director
Department of Community Planning and Sustainability

ATTACHMENTS:

- A: Analysis with Site and Use Review Criteria and Design Guidelines**
- B: Consistency Analysis with Transit Village Area Plan Design Guidelines**
- C: Correspondence Received**
- D: Development Review Comments**
- E: Boulder Design Advisory Board Minutes**
- F: Plan Responses to Concept Plan Review Comments**
- G: Applicant Written Statement and Project Plans**

CRITERIA FOR REVIEW

No site review application shall be approved unless the approving agency finds that:

(1) Boulder Valley Comprehensive Plan:

√ (A) The proposed site plan is consistent with the land use map and the service area map and, on balance, the policies of the Boulder Valley Comprehensive Plan.

The site is located within the service area of the city and is being developed consistent with each of the two BVCP land use map designations which are defined as: High Density Residential and Mixed Use Business. On page 66 of the Comprehensive Plan, High Density Residential is defined as follows:

“High density (more than 14 units per acre). It is assumed that variations of the density on a small area basis may occur within an particular classification, but an average density will be maintained for that classification.”

On page 67 of the Comprehensive Plan, the Mixed Use-Business land use is defined as follows:

“Mixed Use-Business development may be deemed appropriate and will be encouraged in some business areas. These areas may be designated Mixed Use-Business where business or residential character will predominate. Housing and public uses supporting housing will be encouraged and may be required. Specific zoning and other regulations will be adopted which defined the desired intensity, mix, location and design characteristics of these uses.”

The policies of the BVCP also encourage a compact form of development and promote higher density development along existing and future multi-modal corridors, in compatible surroundings. Policies within the BVCP also aim to mitigate the increasingly significant in-commuting trend due to the current jobs-to-housing imbalance by requiring development projects to provide affordable housing. The development pattern established by the relationship of the transit facility to the proposed (and future) residential, in concert with the mix of uses including retail and nearby office industrial directly fulfills a number of the BVCP policies.

The Transit Village Area Plan is identified in the Comprehensive Plan as one of four adopted area plans in the city. On pages 67 to 69 of the BVCP, the plan describes the purpose of Area Plans as a means to provide direction for specific geographic areas, and bridge the gap between the broad policies of the Comprehensive Plan and site specific project review. The Comprehensive Plan notes that Area Plans may address appropriate character, scale and mix of uses and if regulatory changes are needed to ensure or encourage appropriate development. In the BVCP, the Transit Village Area Plan (TVAP) has the following stated purpose:

“To describe the city’s vision for the future of the 160-acre Transit Village area and guide the long term development of the area. The area is defined as within walking distance to the future FasTracks transit services – commuter rail, bus rapid transit, and regional bus services.”

The analysis of consistency with the TVAP Area Plan is provided in Key Issue 2. In essence, the proposed project is consistent with the land use designations of the comprehensive plan and TVAP and fulfills the intent of the Transit Village Area Plan.

√ (B) The proposed development shall not exceed the maximum density associated with the Boulder Valley Comprehensive Plan residential land use designation. Additionally, if the density of existing residential development within a three-hundred-foot area surrounding the site is at or exceeds the density permitted in the Boulder Valley Comprehensive Plan, then the maximum density permitted on the site shall not exceed the lesser of:

Regarding the first half of this criterion, the proposed development under the Comprehensive Plan Mixed Use Business (MUB) land use designation (equating to the MU-4 zoned area) doesn't have a density requirement under the comprehensive plan and therefore this doesn't apply to that area.

The proposed development under the Comprehensive Plan High Density Residential land use designation (equating to the RH-6) zoning meets the maximum density associated with the Boulder Valley Comprehensive Plan residential land use designation: "more than 14 dwelling units per acre" as the HDR land use was implemented within this area of TVAP to be RH-6 zoning. In this location S'PARK_west and the Timber Commons Townhomes are located with a total of 77 units proposed on 4.09 acres equating to a density of 18.9 du/acre; consistent with the density planned for the high density residential land use designation.

The existing residential development within 300 feet of the site includes portions of Orchard Grove Mobile Home Park, the Valmont Condos, and Steelyards. All of those properties have densities that meet the density permitted in the BVCP and because there is no BVCP density requirement for Mixed Use Business, the second of the two criteria applies:

n/a (i) The density permitted in the Boulder Valley Comprehensive Plan, or,

√ (ii) The maximum number of units that could be placed on the site without waiving or varying any of the requirements of [chapter 9-8](#), "Intensity Standards," B.R.C. 1981.

Both the land use designations within the site are considered high density residential uses within the BVCP. On page 66 of the BVCP it states that high density residential is "more than 14 dwelling units per acre." It also notes that, "It is assumed that variations of the densities on a small area basis may occur within any particular classification, but an average density will be maintained for that classification." The Transit Village Area Plan further defines the densities anticipated within the land uses on the site: for Mixed Use 2 land use – up to a 2.0 FAR is anticipated and for High Density Residential 1 (RH-6 zoning), 15 to 24 dwelling units per acre are anticipated. In this case, the area within the Mixed Use 2 land use (the MU-4 zoning) equates to an average of a 1.49 FAR across the five areas within the Mixed Use 2 land use: Markt, Ciclo, Railyards, Meredith House and Timber Loft Apartments and is well within the 2.0 FAR anticipated. The area within the High Density Residential 1 land use (RH-6 zoning) is equivalent to 18.8 dwelling units per acre which is well within the anticipated density under the BVCP.

√ (C) The proposed development's success in meeting the broad range of BVCP policies considers the economic feasibility of implementation techniques require to meet other site review criteria.

The proposed project incorporates high quality and durable building materials with contemporary architecture along with a range of types and sizes of residential units including 77 permanently affordable

residential units; a mix of non-residential units and public spaces that will help to establish a memorable place with a goal of maintaining value and aesthetics over time.

(2) Site Design: Projects should preserve and enhance the community's unique sense of place through creative design that respects historic character, relationship to the natural environment, multi-modal transportation connectivity and its physical setting. Projects should utilize site design techniques which are consistent with the purpose of site review in subsection (a) of this section and enhance the quality of the project. In determining whether this subsection is met, the approving agency will consider the following factors:

√ **(A) Open Space:** Open space, including, without limitation, parks, recreation areas, and playgrounds:

√ **(i) Useable open space is arranged to be accessible and functional and incorporates quality landscaping, a mixture of sun and shade and places to gather;**

There are a variety of open space areas planned including park spaces, the woonerf (shared pedestrian street), large shared open space areas with a pool area and fitness facilities along with multi-use path connections, and private deck and yard space.

√ **(ii) Private open space is provided for each detached residential unit;**

With urban apartments and townhomes planned, the applicant is proposing deck spaces for the apartments and porches or small yards for the townhome units.

√ **(iii) The project provides for the preservation of or mitigation of adverse impacts to natural features, including, without limitation, healthy long-lived trees, significant plant communities, ground and surface water, wetlands, riparian areas, drainage areas and species on the federal Endangered Species List, "Species of Special Concern in Boulder County" designated by Boulder County, or prairie dogs (*Cynomys ludovicianus*), which is a species of local concern, and their habitat;**

There are no known special status plant or animal species on the project site.

√ **(iv) The open space provides a relief to the density, both within the project and from surrounding development;**

The park space within S'PARK_west, on the west end of the area along with Meredith Park, the large central courtyard space at Timber Lofts, the community deck space at Ciclo, the woonerf, detached walkways and multi-use path all create a relief to the planned density and offer permeability and walkability throughout the planned neighborhood. Opportunities to connect to and through the site from surrounding areas, particularly the adjacent Steelyards neighborhood are created with the site and landscape planning proposed.

√ **(v) Open space designed for active recreational purposes is of a size that it will be functionally useable and located in a safe and convenient proximity to the uses to which it is meant to serve;**

The planned urban park spaces including the S'PARK_west park space is approximately 60x150 feet and provides ample space for active play. The Meredith Park is approximately 60 x 90 and has climbing boulders and reclaimed wood benches for both active and passive use.



√ (vi) **The open space provides a buffer to protect sensitive environmental features and natural areas;**

There are no known sensitive environmental features within the site, the majority of which has been developed and/or paved as industrial sites for decades.

and

√ (vii) **If possible, open space is linked to an area- or city-wide system.**

The proposed redevelopment is establishing connectivity where none exists today with narrow streets that have detached walkways and shared streets. This will link to the Steelyards neighborhood along with the extension of Junction Place that establishes a new street interconnecting Boulder Junction from south to north.

√ (B) ***Open Space in Mixed Use Developments (Developments that contain a mix of residential and non-residential uses)***

√ (i) **The open space provides for a balance of private and shared areas for the residential uses and common open space that is available for use by both the residential and non-residential uses that will meet the needs of the anticipated residents, occupants, tenants, and visitors of the property;**

The park spaces, detached walkways, woonerf shared street and the new extension of a multi-use path adjoining the railroad tracks provide opportunities for shared use. In addition, there is a newly proposed plaza space at the Market building that will be open to the public and extends into the shared woonerf space. The terminus of that space is the planned rail plaza.

and

√ (ii) The open space provides active areas and passive areas that will meet the needs of the anticipated residents, occupants, tenants, and visitors of the property and are compatible with the surrounding area or an adopted plan for the area.

The open space areas planned are varied and lend opportunities for both active and passive use. The intent of the Rail Plaza Character District and the Steelyards District is high density and urban, the open space areas are designed accordingly and would not establish a suburban appearance or character.

√ (C) Landscaping

√ (i) The project provides for aesthetic enhancement and a variety of plant and hard surface materials, and the selection of materials provides for a variety of colors and contrasts and the preservation or use of local native vegetation where appropriate;

The applicant is providing a variety of hardscape and softscape materials. The “palette of materials” that the applicant provides in the project landscape plans demonstrate the range and creative use of materials planned.

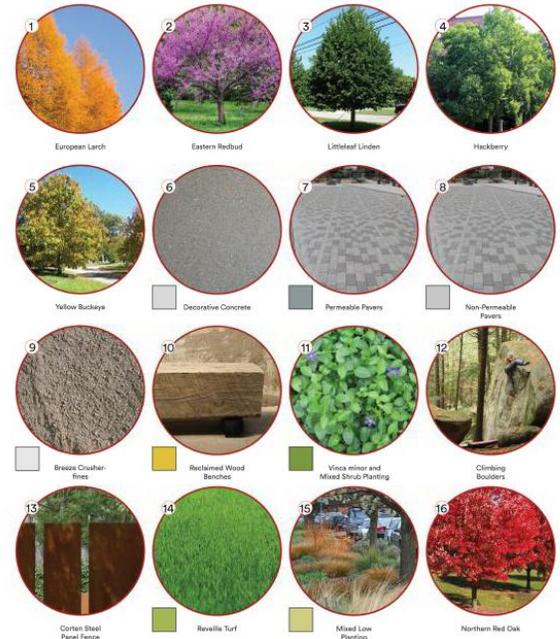
n/a (ii) Landscape design attempts to avoid, minimize, or mitigate impacts to important native species, plant communities of special concern, threatened and endangered species and habitat by integrating the existing natural environment into the project;

There are no know special states plant or animal species within the project site.

√ (iii) The project provides significant amounts of plant material sized in excess of the landscaping requirements of [sections 9-9-12](#), "Landscaping and Screening Standards" and [9-9-13](#), "Streetscape Design Standards," B.R.C. 1981;

The landscape plans illustrate street tree, park and plaza plantings in formal rows as well as in clustered, varied plantings. The landscape plans illustrate a design and planting intent that will be followed through the Technical Document review process to ensure plant materials are sized in excess of the requirements of the land use code.

and



√ (iv) **The setbacks, yards, and useable open space along public rights-of-way are landscaped to provide attractive streetscapes, to enhance architectural features, and to contribute to the development of an attractive site plan.**

The proposed public rights of way are predominately planned with detached walkways with street tree lawns. The proposed landscape plan demonstrates variety and creative mixtures of plant materials that will provide an enhancement to the buildings and streetscape.

√ (D) **Circulation: Circulation, including, without limitation, the transportation system that serves the property, whether public or private and whether constructed by the developer or not:**

√ (i) **High speeds are discouraged or a physical separation between streets and the project is provided;**

The roadways are planned to be relatively narrow and slow moving, this combined with the use of formal detached walks with tree lawns or tree grates with street tree plantings will help to discourage high speeds of vehicles through the proposed project.

√ (ii) **Potential conflicts with vehicles are minimized;**

The rights of way are predominately planned with detached walkways and interior walkway passages or small mews areas that will help to keep pedestrians removed from vehicle conflicts.



√ (iii) **Safe and convenient connections are provided that support multi-modal mobility through and between properties, accessible to the public within the project and between the project and the existing and proposed transportation systems, including, without limitation, streets, bikeways, pedestrianways and trails;**

The detached walkways provided throughout S'PARK with street tree plantings will provide a safe pedestrian way, the streets that are planned to be interconnected in a modified grid pattern create connections for bikes as well as motor vehicles to circulate throughout the neighborhood as well as creating connections to areas outside of the neighborhood. The newly planned multi-use path adjoining the railroad tracks will provide an enhanced opportunity for commuter bicyclists while the surface streets and the woonerf shared street will provide multi-modal mobility throughout S'PARK.

√ (iv) **Alternatives to the automobile are promoted by incorporating site design techniques, land use patterns, and supporting infrastructure that supports and encourages walking, biking, and other alternatives to the single-occupant vehicle;**

The mixed use, relatively high density land use pattern encourages people to walk and bike through the area, in particular given the location of the recently completed RTD bus facility located less than

one quarter mile from S'PARK. All parking in the proposed project is shared and unbundled (residents and tenants would pay for parking separately from housing or office space) with the exception of S'PARK_west. The unbundled parking proposed will allow residents to choose whether or not to pay for a parking space. SPARK_west under the RH-6 zone is also part of the TVAP TDM district, though, that has an additional mill levy (5 mills per year) to participate in trip reduction strategies managed by the TDM Board such as bike share, car share, and Eco-Passes. The MU-4 zones — where the Boulder Junction Access District (BJAD) has purview — are really where the shared and unbundled concepts are emphasized in TVAP due to the common garage concept. Therefore, while the SPARK_west side is not shared and unbundled, it is taxing itself with the mill levy to create the trip reduction strategies.

√ (v) **Where practical and beneficial, a significant shift away from single-occupant vehicle use to alternate modes is promoted through the use of travel demand management techniques;**

Location of the project site within the Boulder Junction Access District (BJAD) establishes TDM techniques that include provision of eco-passes for residents and employees; unbundled parking for residential uses; and excess bike parking throughout the neighborhood. The TDM strategies are augmented by the less-than one-quarter mile distance to the regional RTD bus facility along with access to local bus service along both Bluff Street and Valmont Road.

√ (vi) **On-site facilities for external linkage are provided with other modes of transportation, where applicable;**

The applicant is proposing a link to the regional multi-use path adjoining the railroad tracks that connects south and to the Goose Creek regional path.

√ (vii) **The amount of land devoted to the street system is minimized;**

The streets planned within S'PARK are planned to be relatively narrow in keeping with the TVAP recommended street cross-sections. While one surface parking area is planned as an interim use near the future planned rail platform and rail plaza, the bulk of the planned parking is within the structure below the Timber Lofts and in parallel parking spaces on the streets.

and

√ (viii) **The project is designed for the types of traffic expected, including, without limitation, automobiles, bicycles, and pedestrians, and provides safety, separation from living areas, and control of noise and exhaust.**

With detached walkways proposed predominately throughout the neighborhood, pedestrian design of the streets is well considered in the plans. The bicyclists will share the slow moving streets with autos, and in the case of the woonerf shared street, the autos will be considered “guests” in the space.

√ (E) **Parking**

√ (i) The project incorporates into the design of parking areas measures to provide safety, convenience, and separation of pedestrian movements from vehicular movements;

There are parallel parking spaces proposed along the streets that are planned to be separated from the pedestrian by detached walks with trees in tree lawns or tree grates. The only surface parking lot is located on the southern-most "leg" of the neighborhood and is intended as an interim use that is separated from the nearby walkways.

√ (ii) The design of parking areas makes efficient use of the land and uses the minimum amount of land necessary to meet the parking needs of the project;

The majority of the parking is planned in a structure located below the Timber Lofts building.

√ (iii) Parking areas and lighting are designed to reduce the visual impact on the project, adjacent properties, and adjacent streets;

The majority of the parking is planned in a structure located below the Timber Lofts building.

and

√ (iv) Parking areas utilize landscaping materials to provide shade in excess of the requirements in Subsection 9-9-6 (d), "Parking Area Design Standards," and Section 9-9-14, "Parking Lot Landscaping Standards," B.R.C. 1981.

The parallel parking planned on the streets are aligned with street trees within tree lawns or tree grates. This will augment the pedestrian experience but will also help with reducing the heat island effect along streets.

√ (F) ***Building Design, Livability, and Relationship to the Existing or Proposed Surrounding Area***

√ (i) The building height, mass, scale, orientation, and configuration are compatible with the existing character of the area or the character established by an adopted plan for the area;

For the planned buildings located within the MU-4 zoned area (Markt, Ciclo, Railyards, Meredith House, and Timber Lofts) through TVAP the city envisioned three- to four-story buildings and within the Rail Plaza

Character District, up to five stories (in 55-feet) are envisioned. For the S'PARK_west area of the planned project, through TVAP the city envisioned urban townhomes and garden apartments of two and three stories as is proposed.



Urban townhomes and garden apartments with individual garages, surface parking lots, or underground parking. Mainly two to three stories.

√ (ii) The height of buildings is in general proportion to the height of existing buildings and the proposed or projected heights of approved buildings or approved plans for the immediate area;

The buildings proposed within the MU-4 zoned areas (MU2 land use) are all within the height's the city anticipated through TVAP: the Ciclo building is planned to be three stories and 45 feet in height. The Markt building is planned up to four stories and 49 feet; Railyards is planned to be four stories at 54'-5"; Timber Lofts is planned to be four stories and up to 52'-9"; Meredith House is planned at four stories and up to 48'-9" in height. Similarly, the city anticipated heights of up to three stories for the RH-6 zoned areas (HDR-1 land use) where the proposed residential buildings in the S'PARK_west area are planned as one, two and three stories buildings.

√ (iii) The orientation of buildings minimizes shadows on and blocking of views from adjacent properties;

During the Concept Plan review for S'PARK, the neighboring property owners across Valmont Road, and slightly east. from S'PARK indicated concern about the potential loss of viewshed. The applicant has since proposed the Markt building to maintain a viewshed toward the Flatirons.

√ (iv) If the character of the area is identifiable, the project is made compatible by the appropriate use of color, materials, landscaping, signs, and lighting;

The character of the area is varied, however, the existing site has been industrial for decades. Like the adjacent Steelyards neighborhood, the plans for S'PARK are intended to be reminiscent of the site's past through use of finish materials such as metal, brick, Concrete Masonry Units, and wood.

√ (v) Projects are designed to a human scale and promote a safe and vibrant pedestrian experience through the location of building frontages along public streets, plazas, sidewalks and paths, and through the use of building elements, design details and landscape materials that include, without limitation, the location of entrances and windows, and the creation of transparency and activity at the pedestrian level;

The mixed use buildings planned in the MU-4 zoned areas all have a ground level that offers either non-residential activity or pedestrian interest in materials or ground floor use.

√ (vi) To the extent practical, the project provides public amenities and planned public facilities;

Each building in S'PARK, particularly in the Mixed Use (MU-4) areas provide public space at the ground level: Ciclo has a ground floor devoted to the Community Cycles – a non-profit organization for bicycle enthusiast that provide recycling, repair and refurbishing of bikes. Markt has a public plaza space for gathering that connects to the woonerf shared street along the Railyards building. The Timber Lofts have a non-residential ground floor that is intended for corner retail in areas as a public amenity. Adjacent to the Meredith House is a planned neighborhood park. The rail plaza is proposed adjacent to the future rail platform, that will blend into the woonerf shared street and provide a community gathering space. S'PARK_west has a small neighborhood park planned.

√ (vii) **For residential projects, the project assists the community in producing a variety of housing types, such as multifamily, townhouses and detached single family units, as well as mixed lot sizes, number of bedrooms and sizes of units;**

S'PARK is proposed with studio apartments, efficiency living units, one-, two-, and three-bedroom apartments and townhomes. .

√ (viii) **For residential projects, noise is minimized between units, between buildings, and from either on-site or off-site external sources through spacing, landscaping, and building materials;**

One of the more dominate noise generating uses in the area is the railroad. S'PARK is planned with non-residential uses aligning the railroad tracks to help buffer the noise from the train. The individual residential units are planned with double paned windows to assist in noise mitigation.

√ (ix) **A lighting plan is provided which augments security, energy conservation, safety, and aesthetics;**

The applicant is illustrating sculptural light poles along the woonerf shared street that will support string lights across the private shared street. Each building has lighting intended to be utilized both for safety as well as night time architectural effect. A lighting plan is required to meet the city's lighting standards during Technical Document Review.

√ (x) **The project incorporates the natural environment into the design and avoids, minimizes, or mitigates impacts to natural systems;**

While there are no significant long lived trees on the project site, the proposed landscape plan illustrates an urban tree canopy where new streets will be aligned by street trees.

√ (xi) **Buildings minimize or mitigate energy use; support on-site renewable energy generation and/or energy management systems; construction wastes are minimized; the project mitigates urban heat island effects; and the project reasonably mitigates or minimizes water use and impacts on water quality.**

The applicant has engaged a sustainability consultant that intends to establish energy efficiency based upon the city's stringent building code standards of the 2012 International Energy Conservation Code plus 30 percent efficiency as well as a achieve a LEED-ND (Leadership in Energy and Environmental Design- Neighborhood Design) platinum rating. In addition, the city requires 60 percent construction waste diversion which the applicant will be required to meet.

The applicant indicated that a sustainability consultant and a "green" mechanical engineer team was engaged prior to the design of the project to ensure that all project consultants were guided by an adopted goal statement, had access to professional advice, and were aware of the range of current sustainable development opportunities.

According to the applicant, the project is a registered applicant for the U.S. Green Building Council's (USGBC) LEED for Neighborhood Development (ND) under LEED's new v4 program and a Platinum rating. As the applicant noted, the LEED Neighborhood Development program addresses a broader level of holistic sustainable development practices than only building energy envelope (though at least one building must achieve LEED Platinum for Buildings under the ND program). The applicant's written statement also indicates that they will pursue a LEED ND designation.

The applicant has indicated a desire to provide the two affordable housing sites, Ciclo and a portion of S'PARK_west, as "net zero" as a benefit to those residents, however, this is dependent on the applicant's ability to attract and obtain favorable tax credit market pricing for on-site renewable energy options, such as photovoltaic panels, from 3rd party financiers. Both of those buildings have been designed to provide exceptional access to southern sunlight exposure and to minimize roof penetrations (such as Studor vents for plumbing) to allow for the installation of PV renewables.

The applicant has also indicated that they plan to recycle as much of the current asphalt parking lot as possible on-site for re-use and at least three (3) buildings contemplate the re-use of wood lumber racks currently on-site as a source of re-usable non-structural material for soffits, interior paneling, etc. Finally, as a recommendation from TVAP, the applicant has engineered to entirely avoid stormwater detention basins or vaults to deal with surface water runoff and has provided usable planted greenspaces and permeable paving systems to allow for natural percolation and water quality treatment for all runoff.

√ (xii) **Exteriors or buildings present a sense of permanence through the use of authentic materials such as stone, brick, wood, metal or similar products and building material detailing;**

There are a variety of building finish materials planned throughout S'PARK including wood, metal, and brick. With three different architectural firms providing design of buildings at S'PARK there will be a diversity of design. The unifying elements are that of contemporary buildings utilizing authentic and durable materials that are reminiscent of the industrial past of the site.

√ (xiii) **Cut and fill are minimized on the site, the design of buildings conforms to the natural contours of the land, and the site design minimizes erosion, slope instability, landslide, mudflow or subsidence, and minimizes the potential threat to property caused by geological hazards;**

The grading plan includes excavation for the planned below grade parking structure, however because the site is essentially flat, there will be no slope related instability resulting from the excavation planned.

n/a (xiv) **In the urbanizing areas along the Boulder Valley Comprehensive Plan boundaries between Area II and Area III, the building and site design provide for a well-defined urban edge; and**

n/a (xv) **In the urbanizing areas located on the major streets shown on the map in Appendix A of this title near the Boulder Valley Comprehensive Plan boundaries between**

Area II and Area III, the buildings and site design establish a sense of entry and arrival to the City by creating a defined urban edge and a transition between rural and urban areas.

√ (G) **Solar Siting and Construction:** For the purpose of ensuring the maximum potential for utilization of solar energy in the City, all applicants for residential site reviews shall place streets, lots, open spaces, and buildings so as to maximize the potential for the use of solar energy in accordance with the following solar siting criteria:

√ (i) **Placement of Open Space and Streets:** Open space areas are located wherever practical to protect buildings from shading by other buildings within the development or from buildings on adjacent properties. Topography and other natural features and constraints may justify deviations from this criterion.

The streets are planned in an orthogonal grid for the most part. This will provide an opportunity for solar access throughout S'PARK.

√ (ii) **Lot Layout and Building Siting:** Lots are oriented and buildings are sited in a way which maximizes the solar potential of each principal building. Lots are designed to facilitate siting a structure which is unshaded by other nearby structures. Wherever practical, buildings are sited close to the north lot line to increase yard space to the south for better owner control of shading.

Given the intent to create a relatively high density setting with three, four and five story buildings within the MU-4 zoned area, solar access for all buildings throughout the entire year will not occur. However, the Meredith House and the Timber Lofts buildings both have large wall areas that face south. Similarly, within the S'PARK_west area there are buildings that have large wall surfaces that face south.

√ (iii) **Building Form:** The shapes of buildings are designed to maximize utilization of solar energy. Buildings shall meet the solar access protection and solar siting requirements of [section 9-9-17](#), "Solar Access," B.R.C. 1981.

The Meredith House and the Timber Lofts buildings both have large wall areas that face south. Similarly, within the S'PARK_west area there are buildings that have large wall surfaces that face south. The Ciclo, Markt, and Railyard buildings all have large wall surfaces facing east and west that can capture early morning and late afternoon solar access.

√ (iv) **Landscaping:** The shading effects of proposed landscaping on adjacent buildings are minimized.

Street trees are planned to be deciduous, as are trees adjacent to buildings. This has the effect of providing shading during the hot summer months, and solar access during the winter when the trees are denuded of leaves.

n/a (H) **Additional Criteria for Poles Above the Permitted Height:** No site review application for a pole above the permitted height will be approved unless the approving agency finds all of the following:

n/a (i) The light pole is required for nighttime recreation activities, which are compatible with the surrounding neighborhood, or the light or traffic signal pole is required for safety, or the electrical utility pole is required to serve the needs of the City; and

n/a (ii) The pole is at the minimum height appropriate to accomplish the purposes for which the pole was erected and is designed and constructed so as to minimize light and electromagnetic pollution.

n/a (l) *Land Use Intensity Modifications:*

n/a (i) *Potential Land Use Intensity Modifications:*

(a) The density of a project may be increased in the BR-1 district through a reduction of the lot area requirement or in the Downtown (DT), BR-2, or MU-3 districts through a reduction in the open space requirements.

(b) The open space requirements in all Downtown (DT) districts may be reduced by up to one hundred percent.

(c) The open space per lot requirements for the total amount of open space required on the lot in the BR-2 district may be reduced by up to fifty percent.

(d) Land use intensity may be increased up to 25 percent in the BR-1 district through a reduction of the lot area requirement.

n/a (ii) *Additional Criteria for Land Use Intensity Modifications:* A land use intensity increase will be permitted up to the maximum amount set forth below if the approving agency finds that the criteria in paragraph (h)(1) through subparagraph (h)(2)(H) of this section and following criteria have been met:

(a) *Open Space Needs Met:* The needs of the project's occupants and visitors for high quality and functional useable open space can be met adequately;

(b) *Character of Project and Area:* The open space reduction does not adversely affect the character of the development or the character of the surrounding area; and

(c) *Open Space and Lot Area Reductions:* The specific percentage reduction in open space or lot area requested by the applicant is justified by any one or combination of the following site design features not to exceed the maximum reduction set forth above:

(i) Close proximity to a public mall or park for which the development is specially assessed or to which the project contributes funding of capital improvements beyond that required by the parks and recreation component of the development excise tax set forth in chapter 3-8, "Development Excise Tax," B.R.C. 1981: maximum one hundred percent reduction in all Downtown (DT) districts and ten percent in the BR-1 district;

(ii) Architectural treatment that results in reducing the apparent bulk and mass of the structure or structures and site planning which increases the openness of the site: maximum five percent reduction;

(iii) A common park, recreation, or playground area functionally useable and accessible by the development's occupants for active recreational purposes and sized for the number of inhabitants of the development, maximum five percent reduction; or developed facilities within the project designed to meet the active recreational needs of the occupants: maximum five percent reduction;

(iv) Permanent dedication of the development to use by a unique residential population whose needs for conventional open space are reduced: maximum five percent reduction;

(v) The reduction in open space is part of a development with a mix of residential and non-residential uses within an BR-2 zoning district that, due to the ratio of residential to non-residential uses and because of the size, type, and mix of dwelling units, the need for open space is reduced: maximum reduction fifteen percent; and

(vi) The reduction in open space is part of a development with a mix of residential and non-residential uses within an BR-2 zoning district that provides high quality urban design elements that will meet the needs of anticipated residents, occupants, tenants, and visitors of the property or will accommodate public gatherings, important activities, or events in the life of the community and its people, that may include, without limitation, recreational or cultural amenities, intimate spaces that foster social interaction, street furniture, landscaping, and hard surface treatments for the open space: maximum reduction 25 percent.

n/a (J) *Additional Criteria for Floor Area Ratio Increase for Buildings in the BR-1 District:*

n/a (i) *Process:* For buildings in the BR-1 district, the floor area ratio ("FAR") permitted under table 8-2, section 9-8-2, "Floor Area Ratio Requirements," B.R.C. 1981, may be increased by the city manager under the criteria set forth in this subparagraph.

n/a (ii) *Maximum FAR Increase:* The maximum FAR increase allowed for buildings thirty-five feet and over in height in the BR-1 district shall be from 2:1 to 4:1.

n/a (iii) *Criteria for the BR-1 District:* The FAR may be increased in the BR-1 district to the extent allowed in subparagraph (h)(2)(J)(ii) of this section if the approving agency finds that the following criteria are met:

(a) Site and building design provide open space exceeding the required useable open space by at least ten percent: an increase in FAR not to exceed 0.25:1.

(b) Site and building design provide private outdoor space for each office unit equal to at least ten percent of the lot area for buildings 25 feet and under and at least 20 percent of the lot area for buildings above 25 feet: an increase in FAR not to exceed 0.25:1.

(c) Site and building design provide a street front facade and an alley facade at a pedestrian scale, including, without limitation, features such as awnings and windows, well-defined building entrances, and other building details: an increase in FAR not to exceed 0.25:1.

(d) For a building containing residential and non-residential uses in which neither use comprises less than 25 percent of the total square footage: an increase in FAR not to exceed 1:1.

(e) The unused portion of the allowed FAR of historic buildings designated as landmarks under chapter 9-11, "Historic Preservation," B.R.C. 1981, may be transferred to other sites in the same zoning district. However, the increase in FAR of a proposed building to which FAR is transferred under this paragraph may not exceed an increase of 0.5:1.

(f) For a building which provides one full level of parking below grade, an increase in FAR not to exceed 0.5:1 may be granted.

n/a (K) *Additional Criteria for Parking Reductions: The off-street parking requirements of section 9-9-6, "Parking Standards," B.R.C. 1981, may be modified as follows:*

n/a (i) *Process:* The city manager may grant a parking reduction not to exceed fifty percent of the required parking. The planning board or city council may grant a reduction exceeding fifty percent.

n/a (ii) *Criteria:* Upon submission of documentation by the applicant of how the project meets the following criteria, the approving agency may approve proposed modifications to the parking requirements of section 9-9-6, "Parking Standards," B.R.C. 1981 (see tables 9-1, 9-2, 9-3 and 9-4), if it finds that:

(a) For residential uses, the probable number of motor vehicles to be owned by occupants of and visitors to dwellings in the project will be adequately accommodated;

(b) The parking needs of any non-residential uses will be adequately accommodated through on-street parking or off-street parking;

(c) A mix of residential with either office or retail uses is proposed, and the parking needs of all uses will be accommodated through shared parking;

(d) If joint use of common parking areas is proposed, varying time periods of use will accommodate proposed parking needs; and

(e) If the number of off-street parking spaces is reduced because of the nature of the occupancy, the applicant provides assurances that the nature of the occupancy will not change.

n/a (L) *Additional Criteria for Off-Site Parking:* The parking required under section 9-9-6, "Parking Standards," B.R.C. 1981, may be located on a separate lot if the following conditions are met:

n/a (i) The lots are held in common ownership;

n/a (ii) The separate lot is in the same zoning district and located within three hundred feet of the lot that it serves; and

n/a (iii) The property used for off-site parking under this Subsection continues under common ownership or control.

USE REVIEW CRITERIA: Brew Pub Restaurant in Markt Building

Criteria for Review: No use review application will be approved unless the approving agency finds all of the following:

√ **(1) Consistency with Zoning and Non-Conformity:** The use is consistent with the purpose of the zoning district as set forth in **Section 9-5-2(c), "Zoning Districts Purposes," B.R.C. 1981, except in the case of a non-conforming use;**

The MU-4 zoning is "generally intended for residential uses with neighborhood-serving retail and office uses; where complementary uses may be allowed." A brewpub is a permitted use within the MU-4 zoning district, but the outdoor seating associated with the use must be evaluated under Use Review.

√ **(2) Rationale:** The use either:

√ **(A) Provides direct service or convenience to or reduces adverse impacts to the surrounding uses or neighborhood;**

The proposed brewpub is located on the ground level of the Markt building that is planned to have upper story offices. The brewpub restaurant provides a convenience to the office and residential uses within S'PARK. The intent to mix the uses is to allow walkability within the neighborhood.

_____ **(B) Provides a compatible transition between higher intensity and lower intensity uses;**

_____ **(C) Is necessary to foster a specific city policy, as expressed in the Boulder Valley Comprehensive Plan, including, without limitation, historic preservation, moderate income housing, residential and non-residential mixed uses in appropriate locations, and group living arrangements for special populations; or**

_____ **(D) Is an existing legal non-conforming use or a change thereto that is permitted under subsection (e) of this section;**

√ **3) Compatibility:** The location, size, design, and operating characteristics of the proposed development or change to an existing development are such that the use will be reasonably compatible with and have minimal negative impact on the use of nearby properties or for residential uses in industrial zoning districts, the proposed development reasonably mitigates the potential negative impacts from nearby properties;

The vibrancy of the area with a mix of uses will be enhanced by an "anchor" tenant of a brew pub that can provide a use to activate the street. The size of the brew pub, where the restaurant size is approximately 3,200 square feet with 182 seats is compatible with the office use during the day and the residential neighborhood use during the evening hours. The hours of operation are

restricted to 11:00 p.m. in the evening which is consistent with other mixed use areas within Boulder. Outdoor music is restricted in the management plan and conditions of approval for the brewpub, which could have the greatest impact on surrounding residential, particularly in the evening hours.

√ (4) **Infrastructure:** As compared to development permitted under [Section 9-6-1](#), "Schedule of Permitted Uses of Land," B.R.C. 1981, in the zone, or as compared to the existing level of impact of a non-conforming use, the proposed development will not significantly adversely affect the infrastructure of the surrounding area, including, without limitation, water, wastewater, and storm drainage utilities and streets;

The proposed Brew Pub will not significantly adversely affect the infrastructure of the surrounding area.

√ (5) **Character of Area:** The use will not change the predominant character of the surrounding area or the character established by adopted design guidelines or plans for the area;

The character of the area will change as it is currently a former lumber yard which is planned to be converted to a mixed use neighborhood. The brew pub restaurant will add a vibrancy to the area that can be catalytic in establishing a strong sense of place and activity.

and

n/a (6) **Conversion of Dwelling Units to Non-Residential Uses:** There shall be a presumption against approving the conversion of dwelling units in the residential zoning districts set forth in [Subsection 9-5-2\(c\)\(1\)\(a\)](#), B.R.C. 1981, to non-residential uses that are allowed pursuant to a use review, or through the change of one non-conforming use to another non-conforming use. The presumption against such a conversion may be overcome by a finding that the use to be approved serves another compelling social, human services, governmental, or recreational need in the community including, without limitation, a use for a day care center, park, religious assembly, social service use, benevolent organization use, art or craft studio space, museum, or an educational use.

USE REVIEW CRITERIA: 2,500 Square Foot Restaurant In Railyards

Criteria for Review: No use review application will be approved unless the approving agency finds all of the following:

(1) Consistency with Zoning and Non-Conformity: The use is consistent with the purpose of the zoning district as set forth in [Section 9-5-2\(c\)](#), "Zoning Districts Purposes," B.R.C. 1981, except in the case of a non-conforming use;

The MU-4 zoning is "generally intended for residential uses with neighborhood-serving retail and office uses; where complementary uses may be allowed." A small restaurant is a permitted use within the MU-4 zoning district, but the outdoor seating associated with the use must be evaluated under Use Review.

(2) Rationale: The use either:

(A) Provides direct service or convenience to or reduces adverse impacts to the surrounding uses or neighborhood;

The proposed restaurant is located on the ground level of the Railyards building that is planned to have upper story offices along with other ground floor retail uses. The restaurant can provide a convenience to the office and residential uses within S'PARK as well as the larger surrounding neighborhood. The intent to mix the uses is to allow walkability within the neighborhood, and the outdoor seating can add a vibrancy to the setting.

(B) Provides a compatible transition between higher intensity and lower intensity uses;

(C) Is necessary to foster a specific city policy, as expressed in the Boulder Valley Comprehensive Plan, including, without limitation, historic preservation, moderate income housing, residential and non-residential mixed uses in appropriate locations, and group living arrangements for special populations; or

(D) Is an existing legal non-conforming use or a change thereto that is permitted under subsection (e) of this section;

3) Compatibility: The location, size, design, and operating characteristics of the proposed development or change to an existing development are such that the use will be reasonably compatible with and have minimal negative impact on the use of nearby properties or for residential uses in industrial zoning districts, the proposed development reasonably mitigates the potential negative impacts from nearby properties;

The planned, relatively small restaurant is compatible with a mixed use neighborhood. Office tenants would likely use the restaurant during the daytime hours as would nearby office users in

the Wilderness Place, Center Green or other office parks. The evening and weekend hours would likely be used by surrounding residential neighbors. The hours of operation are restricted to 11:00 p.m. in the evening which is consistent with other mixed use areas within Boulder. Outdoor music is restricted in the management plan and conditions of approval for the restaurant, which could have the greatest impact on surrounding residential, particularly in the evening hours.

√ **(4) Infrastructure:** As compared to development permitted under [Section 9-6-1](#), "Schedule of Permitted Uses of Land," B.R.C. 1981, in the zone, or as compared to the existing level of impact of a non-conforming use, the proposed development will not significantly adversely affect the infrastructure of the surrounding area, including, without limitation, water, wastewater, and storm drainage utilities and streets;

The proposed relatively small restaurant will not significantly adversely affect the infrastructure of the surrounding area.

√ **(5) Character of Area:** The use will not change the predominant character of the surrounding area or the character established by adopted design guidelines or plans for the area;

The character of the area will change as it is currently a former lumber yard which is planned to be converted to a mixed use neighborhood. The small restaurant with outdoor dining, along with the other mix of uses proposed, will add a vibrancy to the area that can be catalytic in establishing a strong sense of place and activity.

and

n/a **(6) Conversion of Dwelling Units to Non-Residential Uses:** There shall be a presumption against approving the conversion of dwelling units in the residential zoning districts set forth in [Subsection 9-5-2\(c\)\(1\)\(a\)](#), B.R.C. 1981, to non-residential uses that are allowed pursuant to a use review, or through the change of one non-conforming use to another non-conforming use. The presumption against such a conversion may be overcome by a finding that the use to be approved serves another compelling social, human services, governmental, or recreational need in the community including, without limitation, a use for a day care center, park, religious assembly, social service use, benevolent organization use, art or craft studio space, museum, or an educational use.

USE REVIEW CRITERIA: Brew Pub Restaurant in Markt Building

Criteria for Review: No use review application will be approved unless the approving agency finds all of the following:

√ **(1) Consistency with Zoning and Non-Conformity:** The use is consistent with the purpose of the zoning district as set forth in [Section 9-5-2\(c\)](#), "Zoning Districts Purposes," B.R.C. 1981, except in the case of a non-conforming use;

The MU-4 zoning is "generally intended for residential uses with neighborhood-serving retail and office uses; where complementary uses may be allowed." A brewpub is a permitted use within the MU-4 zoning district, but the outdoor seating associated with the use must be evaluated under Use Review.

√ **(2) Rationale:** The use either:

√ **(A) Provides direct service or convenience to or reduces adverse impacts to the surrounding uses or neighborhood;**

The proposed brewpub is located on the ground level of the Markt building that is planned to have upper story offices. The brewpub restaurant provides a convenience to the office and residential uses within S'PARK. The intent to mix the uses is to allow walkability within the neighborhood.

_____ **(B) Provides a compatible transition between higher intensity and lower intensity uses;**

_____ **(C) Is necessary to foster a specific city policy, as expressed in the Boulder Valley Comprehensive Plan, including, without limitation, historic preservation, moderate income housing, residential and non-residential mixed uses in appropriate locations, and group living arrangements for special populations; or**

_____ **(D) Is an existing legal non-conforming use or a change thereto that is permitted under subsection (e) of this section;**

√ **3) Compatibility:** The location, size, design, and operating characteristics of the proposed development or change to an existing development are such that the use will be reasonably compatible with and have minimal negative impact on the use of nearby properties or for residential uses in industrial zoning districts, the proposed development reasonably mitigates the potential negative impacts from nearby properties;

The vibrancy of the area with a mix of uses will be enhanced by an "anchor" tenant of a brew pub that can provide a use to activate the street. The size of the brew pub, where the restaurant size is approximately 3,200 square feet with 182 seats is compatible with the office use during the day and the residential neighborhood use during the evening hours. The hours of operation are

restricted to 11:00 p.m. in the evening which is consistent with other mixed use areas within Boulder. Outdoor music is restricted in the management plan and conditions of approval for the brewpub, which could have the greatest impact on surrounding residential, particularly in the evening hours.

√ (4) **Infrastructure:** As compared to development permitted under [Section 9-6-1](#), "Schedule of Permitted Uses of Land," B.R.C. 1981, in the zone, or as compared to the existing level of impact of a non-conforming use, the proposed development will not significantly adversely affect the infrastructure of the surrounding area, including, without limitation, water, wastewater, and storm drainage utilities and streets;

The proposed Brew Pub will not significantly adversely affect the infrastructure of the surrounding area.

√ (5) **Character of Area:** The use will not change the predominant character of the surrounding area or the character established by adopted design guidelines or plans for the area;

The character of the area will change as it is currently a former lumber yard which is planned to be converted to a mixed use neighborhood. The brew pub restaurant will add a vibrancy to the area that can be catalytic in establishing a strong sense of place and activity.

and

n/a (6) **Conversion of Dwelling Units to Non-Residential Uses:** There shall be a presumption against approving the conversion of dwelling units in the residential zoning districts set forth in [Subsection 9-5-2\(c\)\(1\)\(a\)](#), B.R.C. 1981, to non-residential uses that are allowed pursuant to a use review, or through the change of one non-conforming use to another non-conforming use. The presumption against such a conversion may be overcome by a finding that the use to be approved serves another compelling social, human services, governmental, or recreational need in the community including, without limitation, a use for a day care center, park, religious assembly, social service use, benevolent organization use, art or craft studio space, museum, or an educational use.

USE REVIEW CRITERIA: 3,500 Square Foot Restaurant In Railyards

Criteria for Review: No use review application will be approved unless the approving agency finds all of the following:

(1) Consistency with Zoning and Non-Conformity: The use is consistent with the purpose of the zoning district as set forth in [Section 9-5-2\(c\)](#), "Zoning Districts Purposes," B.R.C. 1981, except in the case of a non-conforming use;

The MU-4 zoning is "generally intended for residential uses with neighborhood-serving retail and office uses; where complementary uses may be allowed." A small restaurant is a permitted use within the MU-4 zoning district, but the outdoor seating associated with the use must be evaluated under Use Review.

(2) Rationale: The use either:

(A) Provides direct service or convenience to or reduces adverse impacts to the surrounding uses or neighborhood;

The proposed restaurant is located on the ground level of the Railyards building that is planned to have upper story offices along with other ground floor retail uses. The restaurant can provide a convenience to the office and residential uses within S'PARK as well as the larger surrounding neighborhood. This restaurant with the outdoor seating, coupled with the other retail and restaurants planned, could create a vibrant mix of uses that will promote walkability and sociability.

(B) Provides a compatible transition between higher intensity and lower intensity uses;

(C) Is necessary to foster a specific city policy, as expressed in the Boulder Valley Comprehensive Plan, including, without limitation, historic preservation, moderate income housing, residential and non-residential mixed uses in appropriate locations, and group living arrangements for special populations; or

(D) Is an existing legal non-conforming use or a change thereto that is permitted under subsection (e) of this section;

3) Compatibility: The location, size, design, and operating characteristics of the proposed development or change to an existing development are such that the use will be reasonably compatible with and have minimal negative impact on the use of nearby properties or for residential uses in industrial zoning districts, the proposed development reasonably mitigates the potential negative impacts from nearby properties;

The planned, relatively small restaurant is compatible with a mixed use neighborhood. Office tenants would likely use the restaurant during the daytime hours as would nearby office users in

the Wilderness Place, Center Green or other office parks. The evening and weekend hours would likely be used by surrounding residential neighbors. The hours of operation are restricted to 11:00 p.m. in the evening which is consistent with other mixed use areas within Boulder. Outdoor music is restricted in the management plan and conditions of approval for the restaurant, which could have the greatest impact on surrounding residential, particularly in the evening hours.

√ **(4) Infrastructure:** As compared to development permitted under [Section 9-6-1](#), "Schedule of Permitted Uses of Land," B.R.C. 1981, in the zone, or as compared to the existing level of impact of a non-conforming use, the proposed development will not significantly adversely affect the infrastructure of the surrounding area, including, without limitation, water, wastewater, and storm drainage utilities and streets;

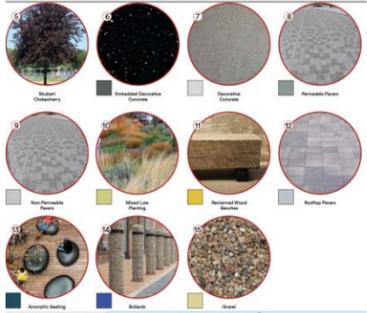
The proposed relatively small restaurant will not significantly adversely affect the infrastructure of the surrounding area.

√ **(5) Character of Area:** The use will not change the predominant character of the surrounding area or the character established by adopted design guidelines or plans for the area;

The character of the area will change as it is currently a former lumber yard which is planned to be converted to a mixed use neighborhood. The small restaurant with outdoor dining, along with the other mix of uses proposed, will add a vibrancy to the area that can be catalytic in establishing a strong sense of place and activity.

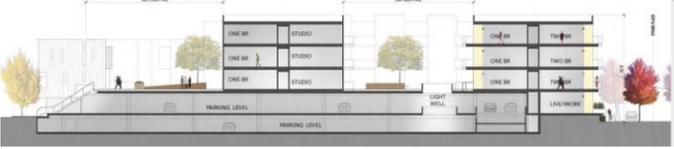
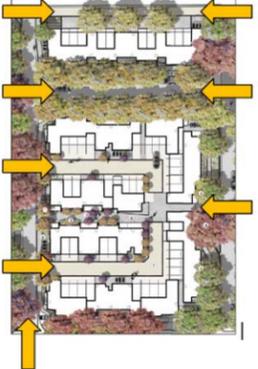
and

n/a **(6) Conversion of Dwelling Units to Non-Residential Uses:** There shall be a presumption against approving the conversion of dwelling units in the residential zoning districts set forth in [Subsection 9-5-2\(c\)\(1\)\(a\)](#), B.R.C. 1981, to non-residential uses that are allowed pursuant to a use review, or through the change of one non-conforming use to another non-conforming use. The presumption against such a conversion may be overcome by a finding that the use to be approved serves another compelling social, human services, governmental, or recreational need in the community including, without limitation, a use for a day care center, park, religious assembly, social service use, benevolent organization use, art or craft studio space, museum, or an educational use.

<p>General Guidelines The following guidelines apply to all character districts.</p>	<p>MARKT</p>	<p>CICLO</p>	<p>RAILYARDS</p>
<p>Building Placement and Design</p>			
<p>Orient the main facade to the street and provide an entrance on the street side of the building.</p>	<p>The building has entrances along both 34th and Meredith Streets.</p>	<p>The building has entrances along both 34th and Meredith Streets.</p>	<p>The building has entrances along the “woonerf” shared street.</p>
<p>Design buildings with pedestrian-scale materials and architectural articulation, particularly on the first floor. Avoid large blank walls. Along streets and sidewalks provide pedestrian interest, including transparent windows and well-defined building entrances.</p>	<p>Approximately 83 percent of the ground floor is transparent with storefront windows that face the pedestrian plaza space along Junction Place, activated by either the planned brewpub or a micro-restaurant.</p>  <p><i>Ground Floor Transparency and Activity along Junction Place</i></p>	<p>The building is planned to have pedestrian scale materials and transparency on the ground floor, as well as well-defined building entrances.</p>	<p>The “woonerf” side of the building appears to meet this guidelines, the east side of the building, planned to align with the railroad tracks is designed to be more inward focused to buffer noise. The applicant has designed the building with bold graphics on the track side to provide interest along the multi-use path.</p>
<p>Consider opportunities to frame or preserve views of the Flatirons to the southwest.</p>	<p>The applicant is proposing that Junction Place on the north and the woonerf focus on the Flatirons with the Markt and Timber buildings framing the view.</p> 	<p>From Valmont, a viewshed it captured with the Ciclo and Markt buildings framing the view.</p> 	<p>The applicant is proposing that Junction Place on the north and the woonerf focus on the Flatirons with the buildings framing the view.</p> 
<p>Useable Open Space</p>			
<p>Incorporate well-designed, functional open spaces with tree, quality landscaping and art, access to sunlight and places to sit comfortably. Where public parks or open spaces are not within close proximity, provide shared open spaces for a variety of activities. Where close to parks, open spaces provided by development may be smaller.</p>	<p>Proposed as a part of the Markt building is a plaza space that is intended to extend from the woonerf. The open space is wrapped by the building and provides for seating areas and public art. An outdoor fireplace is proposed as a part of the plaza for year-around gathering space.</p>	<p>Ciclo is planned as an urban apartment building where the at-grade shared open space of S’PARK such as the woonerf, the plaza space at Markt, the park at the corner of Meredith and Junction Place are available to all residents and community members. Ciclo also has a rooftop deck space facing west.</p>	<p>The Railyards building opens to the planned woonerf shared street. The space is intended to be a place where cars are a “guest” and is highly amenitized with elements such as reclaimed wood benches, mixed low planting, amorphous seating, permeable pavers and decorative concrete as well as a canopy of street trees.</p> 

Building Placement and Design	MEREDITH HOUSE	TIMBER LOFTS/COMMONS	S'PARK_west TOWNHOMES (3085 Bluff)	S'PARK_west (3155 Bluff) Permanently Affordable Units
Orient the main facade to the street and provide an entrance on the street side of the building.	The building has the main entrances on Meredith Street	Both the apartments and the townhomes orient toward the street, with the townhomes providing stoops directly on-to the street.	All of the street facing walls provide an entrance to the building.	All of the street facing walls provide an entrance to the building.
Design buildings with pedestrian-scale materials and architectural articulation, particularly on the first floor. Avoid large blank walls. Along streets and sidewalks provide pedestrian interest, including transparent windows and well-defined building entrances.	The building appears to have pedestrian scale materials and transparency on the ground floor, as well as well-defined building entrances. There are areas that contain broad blank walls. Including the CMU wall that is adjacent to the front entry, and the walls that "hide" the storage units.	The building is proposed to have entrances into individual units from the ground floor including some with stoops. Because it is primarily a residential building, transparency is not a consideration, but use of pedestrian scaled materials such standard sized brick and wood help to create texture and interest.	Yes, the buildings utilize pedestrian scaled materials including standard sized brick and wood.	Yes, the buildings utilize pedestrian scaled materials including standard sized brick and wood.
Consider opportunities to frame or preserve views of the Flatirons to the southwest.	Not applicable. It's a relatively small buildable site and the building is located on the north side of Meredith/Junction Place and backs to the rear of the properties along Valmont. There is no opportunity in this location to frame views from a public corridor.	The applicant demonstrated that the focal point of the Flatirons is framed on the woonerf shared street in front of the Timber Lofts.	There is little in the way of opportunities to capture views given the existing development on the Steelyards. However, some of the upper story balconies may be able to capture Flatiron views.	There is little in the way of opportunities to capture views given the existing development on the Steelyards. However, some of the upper story balconies may be able to capture Flatiron views.
Useable Open Space				
Incorporate well-designed, functional open spaces with tree, quality landscaping and art, access to sunlight and places to sit comfortably. Where public parks or open spaces are not within close proximity, provide shared open spaces for a variety of activities. Where close to parks, open spaces provided by development may be smaller.	<p>The applicant is proposing a park adjacent to Meredith House Condominiums that is available for the residents as well as all community members. Amenities include climbing rocks and reclaimed wood benches and a variety of hardscape and softscape plant materials.</p> 	<p>An interior courtyard space is proposed by the applicant within the Timber Lofts and Timber Commons. Designed to be urban and in keeping with the contemporary architecture, the landscape plans illustrate a variety of hardscape and softscape elements including raised planting beds for vegetables; reclaimed wood benches and a pool area.</p> 	<p>The planned urban park spaces including the S'PARK_west park space is approximately 35 x 135 feet and provides ample space for active play and passive gathering space. The park is at the terminus of access roadways and given the grid of streets is easily accessible through the S'PARK_west area.</p> 	See previous response.

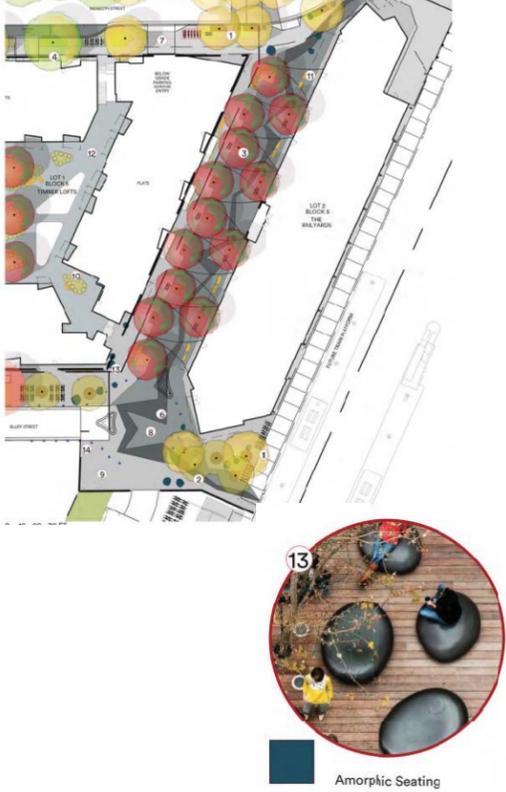
<p><u>General Guidelines</u> The following guidelines apply to all character districts.</p>	<p>MARKT</p>	<p>CICLO</p>	<p>RAILYARDS</p>
<p>Permeability</p>			
<p>While the improved street network will provide more frequent pedestrian connections, also provide multiple opportunities to walk from the street into projects, thus presenting a street face that is permeable. Also provide opportunities to walk within the interior between abutting properties.</p>	<p>The Markt building is proposed with ground floor storefront transparency and active uses that include a brew pub and micro restaurants.</p> 	<p>The Ciclo Building has a ground floor transparent storefront planned to house Community Cycles, a non-profit organization for bicycling enthusiasts who repair, rent and recycle bicycles.</p> 	<p>The Railyards building has a transparent storefront on the ground floor that will provide multiple opportunities to walk from the street into the building as shown below through overhead roll-up doors as well as retail doorway entries. The ground floor is anticipated to have a restaurant on either end of the building with commercial tenants in between. Demising walls and separate entrances illustrate five retail tenants and two restaurants. These design considerations will help to activate the woonerf shared street.</p> 
<p>Parking</p>			
<p>Design the ground level of a parking structure to be interesting and appealing for pedestrians, for example, by wrapping the ground level with active uses, such as retail. Include pedestrian-scale façade articulation, architectural detailing and quality materials.</p>	<p>Not applicable: no parking structure in this building of S'PARK.</p>	<p>The structured parking for the Ciclo building is located at the back of the site on the first floor and is wrapped on the three streetfacing sides by active space. The portions of the building that wrap the parking have pedestrian scale façade articulation, architectural detailing and high quality material including storefront window systems and corten steel.</p>	<p>Not applicable: no parking structure in this building of S'PARK.</p>
<p>Where the ground level is open or exposed to interior drives, paths, or parking lots, screen it with a low wall and/or evergreen landscaping.</p>	<p>Not applicable: no parking structure in this building of S'PARK.</p>	<p>The ground level of the parking structure is not open.</p>	<p>Not applicable: no parking structure in this building of S'PARK.</p>
<p>If tuck-under parking or podium parking (half-level underground) is provided, locate it at the rear of the property or wrap with active uses if feasible.</p>	<p>Not applicable: no parking structure in this building of S'PARK.</p>	<p>The ground level parking structure on the first floor is located at the rear of the building with active uses wrapped around the parking on the three street facing sides.</p>	<p>Not applicable: no parking structure in this building of S'PARK.</p>
<p>Where feasible, locate structure entries/exits on secondary, not primary streets. Avoid locating entries/exits on main pedestrian routes. Entries/exits should be carefully designed to ensure safe, comfortable, and uninterrupted pedestrian flow on adjacent sidewalks.</p>	<p>Not applicable: no parking structure in this building of S'PARK.</p>	<p>The garage pedestrian entries are located through lobbies and/or the groundfloor "retail" space of the Community Cycles.</p>	<p>Not applicable: no parking structure in this building of S'PARK.</p>

<p><u>General Guidelines</u> The following guidelines apply to all character districts.</p> <p>Permeability</p>	<p>MEREDITH HOUSE</p>	<p>TIMBER LOFTS/COMMONS</p>	<p>S'PARK_west TOWNHOMES (3085 Bluff)</p>	<p>S'PARK_west (3155 Bluff) Permanently Affordable Units</p>
<p>While the improved street network will provide more frequent pedestrian connections, also provide multiple opportunities to walk from the street into projects, thus presenting a street face that is permeable. Also provide opportunities to walk within the interior between abutting properties.</p>	<p>The building has two entrances visible from the public right-of-way: the main lobby entrance to the condominiums is located on the east end of the street face, and there is a roll up door facing the planned Meredith Park. The building is relatively narrow along the streetscape and permeability is therefore provided along the streetscape.</p>	<p>There is an interior courtyard for the residential building. The courtyard is planned above the parking structure which is located with two levels below grade and partially below grade. There are multiple opportunities to walk from the street into the elevated courtyard area; however, the applicant has indicated that while the intent is to maintain permeability through the site, the elevated courtyard is also intended to be resident open space. Therefore, the intent is to leave access open but with a vertical separation of stairs to define the space as being a resident's space primarily.</p> 	<p>The townhomes are designed to align public streets and walkways, from which there is access into the site.</p> 	<p>As with the market rate townhomes, the area designed to be permanently affordable has units that align the public streets with opportunities to access the site from various points.</p> 
<p>Parking</p>				
<p>Design the ground level of a parking structure to be interesting and appealing for pedestrians, for example, by wrapping the ground level with active uses, such as retail. Include pedestrian-scale façade articulation, architectural detailing and quality materials.</p>	<p>The Meredith House is proposed to have tuck under parking at the back of the structure, wrapped by ground floor lobby and meeting spaces.</p>	<p>The entire parking structure located under Timber Lofts is planned to be wrapped by residential units or corner retail.</p>	<p>The townhomes have parking garages and parking space on the rear of the units facing away from the public streets.</p>	<p>Parking garages are located at the rear of the buildings away from the public streets.</p>
<p>Where the ground level is open or exposed to interior drives, paths, or parking lots, screen it with a low wall and/or evergreen landscaping.</p>	<p>In keeping with the contemporary character of the planned building, the parking side adjacent to the Meredith Park is wrapped by a CMU wall with a Corten Steel Panel Fence and plantings in the foreground.</p> 	<p>The parking is not exposed except for the access drive into the parking structure where screening isn't practical.</p>	<p>The garages are not highly visible from outside of the access drives. However, the applicant is illustrating small ornamental tree plantings at the rear alley of the garages.</p>	<p>The garages are in alleys at the rear of the buildings and not highly visible. However, the applicant is illustrating plantings of small ornamental trees and shrubs in the alley access way to amenitize the spaces.</p>
<p>If tuck-under parking or podium parking (half-level underground) is provided, locate it at the rear of the property or wrap with active uses if feasible.</p>	<p>The tuck under parking is located at the rear of the property with active uses on the street including a meeting space and the building's lobby.</p>	<p>The structured parking has one level of "podium" parking above grade, the entire podium level is wrapped by active uses or residential units.</p>	<p>Not applicable, the parking garages are at the rear of the building and are not tuck under or podium.</p>	<p>Not applicable, the parking garages are at the rear of the building and are not tuck under or podium.</p>
<p>Where feasible, locate structure entries/exits on secondary, not primary streets. Avoid locating entries/exits on main pedestrian routes. Entries/exits should be carefully designed to ensure safe, comfortable, and uninterrupted pedestrian flow on adjacent sidewalks.</p>	<p>The access to the parking is from an access drive with adequate sight triangles demonstrated on the project plans.</p>	<p>The access to the parking structure is located near the intersection of Junction Place and the woonerf shared street. While it does face Junction Place, the access is necessary to get cars off the street and into the garage from Valmont Road as quickly as possible. The entrance to the garage is designed to meet the minimum width necessary to function properly.</p>	<p>The garages are accessed from an internal alley and would not disrupt pedestrian activity along Bluff Street or 32nd Street.</p>	<p>The garages are accessed from internal alleys and would not disrupt pedestrian activity along Bluff Street, Junction Place, Meredith Street or 32nd Street.</p>

<u>General Guidelines</u> The following guidelines apply to all character districts.	MARKT	CICLO	RAILYARDS
Bus Stops			
Include the following for bus stops adjacent to development projects: a shelter, benches, route and schedule signage. Additional enhancements are encouraged, such as pedestrian lighting, art, landscaping, and waste receptacles. Bike racks should be provided at regional route stops.	Currently located just outside of Markt building on Valmont Road is an existing bus stop. As a requirement of Technical Document Review, the applicant will be required to build an enhanced transit stop to include a transit shelter, bench, trash receptacle and short term bike parking.	not applicable, no bus stop in this location.	not applicable, no bus stop in this location.
Junction Place			
In addition to the street trees, sidewalks and bike facilities specified by the Junction Place streetscape section, provide seating, planters, art, special pavement and lighting along Junction Place. (See the Implementation Plan for information on funding of the city share.)	Important Note: The project plans label the street that will be renamed as Junction Place as Meredith Street (where Junction Place turns eastbound) and 34 th Street where Junction Place turns northbound and intersects with Valmont Road. However, the intent is that Junction Place carry through the entire Boulder Junction. At the time of Technical Document review, the applicant will be required to do an address plat where the naming convention of Junction Place is established throughout the site. The Markt fronts onto the portion labeled as 34 th Street and provides a public plaza aligning the street continues north from the woonerf shared street to Valmont Road. In this area, there are tree plantings in street tree grates and special paving that extends from the woonerf that blends decorative concrete and embedded decorative concrete to form a geometric pattern. Along this plaza space are reclaimed wood benches decorative light valences and an outdoor fire pit.	Important Note: The project plans label the street that will be renamed as Junction Place as Meredith Street (where Junction Place turns eastbound) and 34 th Street where Junction Place turns northbound and intersects with Valmont Road. However, the intent is that Junction Place carry through the entire Boulder Junction. At the time of Technical Document review, the applicant will be required to do an address plat where the naming convention of Junction Place is established throughout the site. Ciclo fronts onto the portion of the street labeled as 34 th Street and utilizes reclaimed wood benches along with special paving and tree plantings in street tree grates.	Important Note: The project plans label the street that will be renamed as Junction Place as Meredith Street (where Junction Place turns eastbound) and 34 th Street where Junction Place turns northbound and intersects with Valmont Road. However, the intent is that Junction Place carry through the entire Boulder Junction. At the time of Technical Document review, the applicant will be required to do an address plat where the naming convention of Junction Place is established throughout the site. The woonerf shared street adjacent to the Railyards, is located in an area that is roughly where an original alignment for Junction Place was shown. However, the alignment of Junction Place in TVAP is considered to be "flexible." The woonerf shared street serves the same function of a "special street" within Boulder Junction as the intent of Junction Place in that it provides a host of amenities that include seating, planters, art, special paving and lighting.
Where feasible, place active uses, such as retail or commercial services on the first floor of buildings along Junction Place.	There is a planned Brewpub and micro restaurants planned along the plaza that fronts 34 th Street that will become Junction Place.	The Community Cycles is planned to occupy the ground floor of the building.	The ground floor is anticipated to have a restaurant on either end of the building with commercial tenants in between. Demising walls and separate entrances illustrate five retail tenants and two restaurants.
Mixed-Use Buildings			
The potential for conflicts between residential and non-residential uses within mixed-use buildings should be minimized through careful design and building system planning. Consider the compatibility of specific uses. Issues could include noise, vibration, privacy, and entrance locations.	These considerations will be required at Technical Document Review and building permit, where the building code has standards for vertical mixed use construction to mitigate or minimize noise.	These considerations will be required at Technical Document Review and building permit, where the building code has standards for vertical mixed use construction to mitigate or minimize noise.	These considerations will be required at Technical Document Review and building permit, where the building code has standards for vertical mixed use construction to mitigate or minimize noise.

	MEREDITH HOUSE	TIMBER LOFTS/COMMONS	S'PARK_west TOWNHOMES (3085 Bluff)	S'PARK_west (3155 Bluff) Permanently Affordable Units
Bus Stops				
Include the following for bus stops adjacent to development projects: a shelter, benches, route and schedule signage. Additional enhancements are encouraged, such as pedestrian lighting, art, landscaping, and waste receptacles. Bike racks should be provided at regional route stops.	Not applicable, no bus stop in this location, planned adjacent to Markt along Valmont Road.	Not applicable, no bus stop in this location, planned adjacent to Markt along Valmont Road.	Not applicable, no bus stop in this location, the nearest bus stops will be located on Valmont Road next to Markt building and at the RTD bus facility, both of which are less than one-quarter mile walking distance.	Not applicable, no bus stop in this location, the nearest bus stops will be located on Valmont Road next to Markt building and at the RTD bus facility, both of which are less than one-quarter mile walking distance.
Junction Place				
In addition to the street trees, sidewalks and bike facilities specified by the Junction Place streetscape section, provide seating, planters, art, special pavement and lighting along Junction Place. (See the Implementation Plan for information on funding of the city share.)	The Meredith House fronts onto the street currently labeled as Meredith Street but which will become Junction Place. While the woonerf shared street provides the social function envisioned for Junction Place, the street in this location and along other residential portions of S'PARK has a tree lawn for strong street tree plantings and includes bike parking and planters in front of the building with a number of amenities including benches and special paving within the adjacent Meredith Park space.	Along Timber Lofts and Commons, the street labeled as Meredith Street will become Junction Place. In this area, the uses are much more residential and the street is intended to be a slower moving, quieter residential street lined with street trees. The social function of Junction Place as envisioned in TVAP will be accomplished more with the woonerf shared street, where a mix of amenities such as special seating and paving, and special lighting is planned.	Not applicable, doesn't front Junction Place.	Along S'PARK_west for the Permanently Affordable Units, the street labeled as Meredith Street will become Junction Place. In this area, the uses are much more residential and the street is intended to be a slower moving, quieter residential street lined with street trees. The social function of Junction Place as envisioned in TVAP will be accomplished more with the woonerf shared street, where a mix of amenities such as special seating and paving, and special lighting is planned.
Where feasible, place active uses, such as retail or commercial services on the first floor of buildings along Junction Place.	The Meredith building is planned as an entirely residential condominium building with just 15 dwelling units. Therefore, the ground level provides amenities that serve the residential such as a lobby and a meeting space that can still provide the active streetface that is anticipated.	Similarly to the Meredith building, the Timber Lofts and Commons are planned as predominately a residential building. However, the building's streetface is enhanced along Junction Place with a co-working office space and fitness clubhouse on the southwestern corner of the building; and as the building rounds the corner on Junction Place, the townhomes and apartments that front the street all have direct access to the street with stoops.	Not applicable, doesn't front Junction Place.	Not feasible as the intent is to have a quiet residential street in this location with the social function of Junction Place as envisioned in TVAP accomplished more with the woonerf shared street, where a mix of amenities such as special seating and paving, and special lighting is planned.
Provide way-finding features such as special pavements, signs, or art, to facilitate pedestrian movement between Junction Place, Rail Plaza, the rail platform and under/overpass, the bus station, Goose Creek Greenway, Pearl, Valmont, 30 th Street and Wilderness Place. (See the Implementation Plan for funding information.)	Not feasible as the intent is to have a quiet residential street in this location with the social function of Junction Place as envisioned in TVAP accomplished more with the woonerf shared street, where a mix of amenities such as special seating and paving, and special lighting is planned.	Not feasible as the intent is to have a quiet residential street in this location with the social function of Junction Place as envisioned in TVAP accomplished more with the woonerf shared street, where a mix of amenities such as special seating and paving, and special lighting is planned.	Not applicable, doesn't front Junction Place.	Not feasible as the intent is to have a quiet residential street in this location with the social function of Junction Place as envisioned in TVAP accomplished more with the woonerf shared street, where a mix of amenities such as special seating and paving, and special lighting is planned.
Mixed-Use Buildings				
The potential for conflicts between residential and non-residential uses within mixed-use buildings should be minimized through careful design and building system planning. Consider the compatibility of specific uses. Issues could include noise, vibration, privacy, and entrance locations.	These considerations will be required at Technical Document Review.	These considerations will be required at Technical Document Review.	These considerations will be required at Technical Document Review.	These considerations will be required at Technical Document Review.

Rail Plaza District Guidelines:	MARKT	CICLO	RAILYARDS
Locate buildings along the street with parking behind.	Not applicable, no on-site parking	Yes, meets this guideline	Not applicable, no on-site parking
Place active uses on the ground level of buildings adjacent to Rail Plaza, for example, stores, restaurants, cafes, or commercial services, where feasible. They should have entrances directly onto the plaza.	Not applicable, not located near plaza	Not applicable, not located near plaza	The rail plaza to function in the interim before a passenger rail line comes to this location is "spread" into the woonerf shared street in this location. The Railyards is proposed with uses that would activate this woonerf as well as a restaurant facing the future rail plaza.
Orient buildings to Junction Place (see Junction Place guidelines), as well as to the tracks. If feasible, place active uses on the first floor. Consider making the track-side frontage a car-free zone with pedestrian amenities.	The building is oriented to Junction Place, and there is a multi-use path planned to align between the building and the railroad tracks. Roll-up doors will allow users to see into the brewpub and create interest along the multi-use path.	Building is oriented to Junction Place/34 th Street but away from the tracks, not applicable.	<p>Important Note: The project plans label the street that will be renamed as Junction Place as Meredith Street (where Junction Place turns eastbound) and 34th Street where Junction Place turns northbound and intersects with Valmont Road. However, the intent is that Junction Place carry through the entire Boulder Junction. At the time of Technical Document review, the applicant will be required to do an address plat where the naming convention of Junction Place is established throughout the site.</p> <p>The woonerf shared street adjacent to the Railyards, is located in an area that is roughly where an original alignment for Junction Place was shown. However, the alignment of Junction Place in TVAP is considered to be "flexible." The woonerf shared street serves the same function of a "special street" within Boulder Junction as the intent of Junction Place in that it provides a host of amenities that include seating, planters, art, special paving and lighting.</p>
Civic Plaza Guidelines:	MARKT	CICLO	RAILYARDS
Design the plaza to be approximately a third of an acre. Err on the side of smaller rather than larger.	Not applicable	Not applicable	The area set aside for the plaza meets the size recommendation of 0.3 acres.
Frame the plaza with buildings, with one side open (or partially open) to Bluff Street and/or Junction Place. The intent is to create a partially enclosed space that is both inviting and intimate.	Not applicable	Not applicable	The Railyards building is intended to open to the future rail plaza. The current surface parking lot located in the "finger" of the Sutherlands property where the existing small train depot is located will ultimately redevelop as a building that can frame the south side of the rail plaza.

Civic Plaza Guidelines (cont.)	MARKT	CICLO	RAILYARDS
<p>Provide flexible space to accommodate a variety of public uses, such as a mercado, farmers' market, and festivals. Also provide flexibility for different uses during different times of the day, week and year. Anticipated uses and associated maintenance should be an integral part of the plaza design, particularly layout, furnishings, materials and plant selection.</p>	Not applicable	Not applicable	<p>The rail plaza to function in the interim before a passenger rail line comes to this location is "spread" into the woonerf shared street in this location. The Railyards is proposed with uses that would activate this woonerf as well as a restaurant facing the future rail plaza.</p> <p>The woonerf shared street serves the same function of a "special street" within Boulder Junction as the intent of Junction Place in that it provides a host of amenities that include seating, planters, art, special paving and lighting. These elements have been blended into the rail plaza spaces with special paving, amorphous seating and street trees.</p> 
<p>Design the plaza so its use could be combined with temporary closure of the east end of Bluff Street for special events.</p>	Not applicable	Not applicable	<p>The rail plaza in combination with the woonerf shared street could be closed for special events that would necessitate "temporary special event" permits.</p>
<p>Include a variety of smaller "places" (activities or destinations within the plaza. These could be as simple as a "vendor cart."</p>	Not applicable	Not applicable	<p>These types of uses will become programmed as greater activity is established over time.</p>
<p>Provide essential and "comfort" amenities such as bike racks, a drinking fountain, recycling and trash receptacles, pedestrian scale lighting, shade and soft surfaces, in carefully chosen locations.</p>	Not applicable	Not applicable	<p>Some of these elements will become programmed as greater activity is established over time.</p>
<p>Provide an adequate amount of seating and carefully consider its location orientation, type and materials.</p>	Not applicable	Not applicable	<p>Some of these elements will become programmed as greater activity is established over time.</p>

Civic Plaza Guidelines (cont.)	MARKT	CICLO	RAILYARDS
Look for opportunities to incorporate art into built elements such as paving, railings, signage, seating or overhead structures.	Not applicable	Not applicable	Some of these elements will become programmed as greater activity is established over time.
Incorporate environmentally friendly features such as pervious surfaces, biofilter landscaping beds, high efficiency lighting and solar powered amenities (e.g., bubble fountains). Explore possible demonstration or education aspects for these features.	Not applicable	Not applicable	Some of these elements will become programmed as greater activity is established over time.
Use high-quality, authentic materials	Not applicable	Not applicable	Some of these elements will become programmed as greater activity is established over time.
Utilize trees and plants to soften the space	Not applicable	Not applicable	Applicant has provided tree plantings on the landscape plan for this area.
Carefully design the new pedestrian underpass (or overpass) at the tracks so that it does not negatively impact the aesthetics or function of the plaza.	Not applicable	Not applicable	The below grade underpass will be developed by the city and RTD at the time the passenger rail warrants it.
Provide way-finding features, such as signage, special pavement and art, to direct people to the plaza from 30 th Street, Bluff Street, Valmont Road, Junction Place, and Pearl Parkway.	Not applicable	Not applicable	Some of these elements will become programmed as greater activity is established over time and passenger rail service is established.
Actively manage the plaza to ensure on-going security, cleanliness and liveliness. Gear events to attract both existing users and new users. Program uses to change as the seasons change.	Not applicable	Not applicable	Some of these elements will become programmed as greater activity is established over time and passenger rail service is established.

STEELYARDS CHARACTER DISTRICT GUIDELINES	MEREDITH HOUSE	TIMBER LOFTS/COMMONS	S'PARK_west TOWNHOMES (3085 Bluff)	S'PARK_west (3155 Bluff) Permanently Affordable Units
Locate buildings along the street with parking behind	Not applicable, as Meredith House is not located within the Steelyards District.	Timber Commons townhomes are a part of the building that is located within the Steelyards Character District (equivalent to the RH-6 zoned area). The Timber Commons has below grade parking lot area that meets the intent of this guideline.	S'PARK_west (at 3085 Bluff) has parking and garages behind the buildings on the lot. There is on-street parallel parking within the right-of-way consistent with the TVAP streetscape guidelines.	S'PARK_west (at 3155 Bluff) has parking and garages behind the buildings on the lot. There is on-street parallel parking within the right-of-way consistent with the TVAP streetscape guidelines.
Look for opportunities for car-free or car reduced zones	Not applicable, as Meredith House is not located within the Steelyards District.	Timber Commons townhomes are a part of the building that is located within the Steelyards Character District (equivalent to the RH-6 zoned area). The Timber Commons and the Timber Loft apartments both have a larger interior courtyard space that is a car-free zone.	The TVAP connections plan requires specific street connections in this area of TVAP and therefore, the opportunity to create car-free zones is limited. However, there is a park space that does not have a roadway aligning the park in this area.	The TVAP connections plan requires specific street connections in this area of TVAP and therefore, the opportunity to create car-free zones is limited. However, there are small pedestrian only access points that establish permeability into the site that create car-free zones in this location.

From: Amy Tremper [mailto:1401401401@gmail.com]
Sent: Friday, August 14, 2015 3:37 PM
To: McLaughlin, Elaine
Subject: S'PARK

Dear Boulder Planning Board:

I have to be honest. I never thought Boulder would have a hip, vibrant, artsy and industrial development with market rate and affordable housing mixed together near public transportation. The Holiday Neighborhood is wonderful but I yearn for the energy that a place like the Source or Taxi in Denver could bring to Boulder. I have been worried that Boulder would not support a creative project that's walk-able, workable and also a Platinum Certified Leeds project. We seem to be held back by the critics who are afraid of change.

S'PARK could be Boulder's iconic development that moves us forward instead of holding us back! Please vote yes to move this project forward.

Sincerely,

Amy Helen Tremper

PS As someone who has attended every community meeting about Form Based Code, this project sure looks like it would fit "hand-in-glove" with your Pilot program.

From: Ben Tremper [mailto:bentremper@gmail.com]
Sent: Saturday, August 15, 2015 3:34 PM
To: planningboard@bouldercolorado.gov; McLaughlin, Elaine
Subject: Spark Boulder

Dear Boulder Planning Board,

I was recently forwarded concepts for the Spark development in Boulder. This is one of the few new developments in Boulder I'm excited by. As a designer, I really crave contemporary and artistically driven projects like The Source and Taxi in Denver. Our city is ripe for similar infill and with our burgeoning startup and creative agency community, projects like this are sure to be embraced by many other like minded individuals.

Thanks for your time.
Best,
Ben

Ben Tremper Design
Interaction Design / Visual design

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M: (720) 346-3882

-----Original Message-----

From: Jill Grano [mailto:jill@jillgrano.com]
Sent: Tuesday, August 18, 2015 1:59 PM
To: McLaughlin, Elaine
Subject: S*Park

Dead Elaine,

My name is Jill Grano and I am writing to lend enthusiastic support of the proposed new development, S*Park.

First, let me tell you a bit about myself. I am a 34 year old mother of two children, living and working full time in the City Boulder. I currently serve on the Board of Zoning Adjustments, and was one of the recent housing working group members in the "Creating Diverse Housing in Every Neighborhood" group. I also served on the Board of Directors of New Era Colorado for four years and am presently on their Advisory Board. I recently filed for my own non-profit called Beyond Shelter to help provide sustainable housing solutions to some of the most needy families in Boulder. So far in 2015, I have raised over \$50,000 for two incredible families. All this to say, I am engaged in our community, and - knock on wood - I will be an active community member for a long time to come.

That said, I am SO excited about the new S*Park development proposal!! This kind of diverse neighborhood is EXACTLY what Boulder needs. Here's why I love it:

1. It is in line with the City's growth plan, as well as the City's need to increase affordable housing options.
2. It provides a community where people have the option to rent, own through the city of Boulder's Affordable Program, or own on the open market... Truly diverse options!
3. It values green space and trees, a component desperately missing in other new developments.
4. It redevelops an area that needs redevelopment... The 30th corridor is important for Boulder and this fills in an important missing puzzle piece.
5. It values commercial and retail partners, creating a community where amenities are close by, thus reducing car traffic.
6. The architecture is diverse, helping solve what's been a recent problem in my opinion, which is uninspiring design. This may seem trivial, but it is very important for the long term health of our community... This has been proven by other cities.

Beyond simply hoping that this development passes, my hope is that it receives enthusiastic support! In fact, I think it should be held as a model to other developers of how to "do it right" (so-to-speak) in the City of Boulder. We could use many more S*Parks in the future! Done right, which I am confident it will be, this will serve as a hub for some of our most valued community members... Teachers, nurses, etc. who are presently struggling to stay in Boulder.

Thank you for your time, consideration, and service to our community.

Warmly,

Jill Grano
303.945.0601

From: Jeff Donaldson [mailto:jeff@atomics20.com]
Sent: Sunday, August 23, 2015 2:16 PM
To: McLaughlin, Elaine
Subject: Support for Spark Development Project

Elaine, I'm writing to express my support of the Spark development project being done by Scott Holton. I understand that this project is going to be under review on 9/2 (which I'll be attending) and I just wanted to connect with you before hand.

I'm an entrepreneur in Boulder that has owned a business here for the last six years. My marketing agency swarms freelance creatives together to accomplish bigger things than anyone can do on individually. I moved to Boulder because

of its density of creative people, business opportunities and culture. But what's missing is a venue for more significant creativity. I believe this development project accomplishes just that. It's a place where designers, makers, innovators and especially Millennials will want to work and live. Rather than losing them to surrounding towns we can attract and retain them in Boulder.

This project has purpose and is very aspirational. I want to be part of something like that in Boulder.

Thanks very much.

Jeff

From: Graham Casden [mailto:graham@oceanfirst.blue]
Sent: Sunday, August 23, 2015 3:19 PM
To: McLaughlin, Elaine
Subject: Re: S'PARK Development

Good afternoon, Elaine.

I'm writing to express my support for the S'PARK Development on Bluff St. I own the building at 3015, next to PC's Pet Pantry, and have been in the Steelyards for about eight years. I firmly believe a modern village with a vibrant and fun atmosphere, affordable housing, active green spaces and a focus on sustainability meshes well with both Boulder County and Ocean First's community vision.

Please let me know if you have any questions or would like to discuss further.

Warm regards,

Graham



Graham Casden
Chief Visionary Officer
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CITY OF BOULDER
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CITY OF BOULDER
LAND USE REVIEW RESULTS AND COMMENTS

DATE OF COMMENTS: March 4, 2015
CASE MANAGER: Elaine McLaughlin
PROJECT NAME: S'PARK
LOCATION: 3390 VALMONT RD, 3085, 3155, 3195 BLUFF STREET
COORDINATES: N04W03
REVIEW TYPE: Site and Use Review
REVIEW NUMBER: LUR2015-00010 and LUR2015-00011
APPLICANT: SCOTT HOLTON
DESCRIPTION: Proposed redevelopment of a 10.9 acre site within the northern portion of Boulder Junction to create a new mixed use commercial and mixed-income residential neighborhood comprised of six different projects: Maarket: a 52,454 square foot, three story non-residential building with a brew pub and micro-restaurants; Ciclo: a four story residential/office building; Railyards: a 67,039 square foot, four story non-residential office and retail building with two restaurants; Timber: a 115,000 square, foot four-story apartment building with ground floor retail; Meredith House: a four story apartment building of 20,690 square feet; and S'PARK_west: a three-story 97,000 square foot apartment and townhome building. Proposed for residential are condominiums, townhomes and apartments some of which are permanently affordable. Proposed non-residential uses include restaurarants, micro-restaurants, retail, office, brewpub. Multi-use path and various new transportation connections per the Transit Village Area Plan, new plaza/woonerf, new pocket park.

REQUESTED VARIATIONS FROM THE LAND USE REGULATIONS:

- Section 9-7, "*Form and Bulk Standards*," B.R.C. 1981;
- permitted height from 38 feet to 55 feet
 - maximum number of stories from three to four
 - setbacks (various modifications throughout the site)
- Section 9-9-17, "*Solar Access*," B.R.C. 1981;
- Solar Exception
- Section 9-6-1, "*Use Standards*," B.R.C. 1981;
- Restaurants >1,500 sf - Use Review required

I. REVIEW FINDINGS

The proposed project represents an exciting opportunity for a new mixed use neighborhood in the northern part of Boulder Junction, and will help to complete Phase I of the Transit Village Area Plan (TVAP). The review findings conclude that additional information will be necessary for staff to determine consistency of this large project with the Site Review criteria and TVAP guidelines. As the applicant is aware, a Form Based Code (FBC) pilot project is planned for Boulder Junction. While it is intended to be initiated in the next several weeks, it will likely progress in tandem with the review process for this application. Initial comments are based on current regulations, and especially the adopted TVAP intent and guidelines for which a preliminary consistency analysis is provided in Attachment A. Staff will facilitate discussions between the applicant and the FBC code consultants which could mutually benefit both the application and in turn, help to inform the FBC process. Additional information will be forthcoming about the FBC process.

In addition, staff is recommending that the Boulder Design Advisory Board (BDAB) review the proposed buildings prior to resubmittal of the revisions. Staff then recommends a two-step Site Review discussion with the Planning Board. Because the buildings in Concept Plan were generalized massing diagrams and because the project is one of the largest sites that the city has reviewed in recent years, the review process would benefit from a two-part Site Review hearing. Staff is happy to meet at your convenience to discuss the comments found herein.

II. CITY REQUIREMENTS

This section addresses issues that must be resolved prior to a project decision or items that will be required conditions of a project approval. Requirements are organized by topic area so that each department's comments of a similar topic are grouped together. Each reviewer's comment will be followed by the reviewer's department or agency and telephone number. Reviewers are asked to submit comments by section and topic area so that the comments can be more efficiently organized into one document. Topics are listed here alphabetically for reference.

Access/Circulation David Thompson, 303-441-4417

1. At time of re-submittal, please include a written statement on the TVAP connections to be modified as part of the S*PARK project and how vehicle and pedestrian / bike mobility through the site will be achieved.
2. The public improvements to be constructed by S*PARK must include the installation of a traffic signal on Valmont Street and Junction Place as mitigation for the removal of the existing pedestrian crossing in order to accommodate a left-turn lane into the site for access. The scope for the construction of the traffic signal must include the installation of a railroad preemptive device and related railroad signal improvements. Please revise the site plan to show the removal of the existing pedestrian signal and the installation of a traffic signal at the intersection of Junction Place and Valmont Road to also include a left-turn lane on Valmont Road.
3. S*PARK will be responsible for funding its' fair-share of the future traffic signal at the 30th and Bluff Street intersection to mitigate the traffic impacts at the intersection generated by the S*PARK project. Please contact staff to discuss how the project's fair-share will be determined.
4. The public improvement to be constructed by S*PARK must include the installation of street lighting pursuant to Section 2.12 of the Design and Construction Standards (DCS). Please revise the site plans to show street lighting at the following locations:
 - a. Two street lights along the Junction Place curve
 - b. One street light at the Bluff Street cul-de-sac
 - c. One street light at the intersection of Junction Place and Meredith Street
 - d. One street light at the intersection of Junction Place and Bluff Street
 - e. One street light at each of the proposed raised pedestrian crosswalks

Please contact David Thompson to set up a meeting to discuss review comments on the right-of-way lighting plan shown on the Photometric Lighting Plans.

5. Pursuant to the Transit Village Area Plan (TVAP) connections plan, please revise the site plan to show the dedication of right-of-way and construction of the following multi-modal public improvements:
 - a. A segment of the east / west multi-use path on the south side of Bluff Street
 - b. The east / west sidewalk connection (TVAP Connection #21) on the south end of the property (this connection can be dedicated and constructed when the proposed surface lot redevelops)
6. In support of the TVAP connections plan, please revise the site plan to (1) show a east / west public pedestrian / bike connection from Junction Place to the north / south alley (TVAP Connection #7) through the center of Blocks 1 and 3 of Lot #1; (2) show a public access easement dedication over the north / south residential alley north of the Bluff Street / 31st Street intersection (TVAP Connection #7); and (3) show a public access easement dedication for the east / west alley located north of Junction Place and east of 32nd Street and adjacent to the north boundary line (TVAP Connection #9). The proposed north / south alleys with public access easements across them should be designed as shared alleys.
7. Please revise the site's roadway horizontal geometrics to show the following:
 - a. 20' wide curb cuts (measured from flow line to flow-line) for all public roadway intersections (excluding the Junction Place and Valmont Road intersection)
 - b. Locate on-street parking a minimum of 20' from any existing or proposed crosswalk or curb ramp
 - c. Use two 20' radius reverse curves for the on-street parking curb extensions
 - d. Eliminate the proposed permeable pavement encroaching within the walking widths of the sidewalks

- e. Detach the sidewalk at the intersections
 - f. The curb radii being proposed for the roadway intersections.
 - g. The Junction Place horizontal curve radius consistent with the DCS standards
 - h. Contact staff regarding the crosswalks / curb ramps being proposed on Bluff Street between 31st and 32nd Streets
 - i. Replacing permeable pavers being shown for the north / south and east / west alleys with concrete
8. Per comments made at Concept Plan Review please revise the grading sheets and the preliminary street sections sheet to provide the following typical cross-section for 32nd Street (north of Bluff), 33rd Street north of Junction Place and Meredith Street (between 32nd Street and Junction Place):
- a. 32' wide pavement (measured from flow-line to flow-line)
 - b. 8.5' wide landscape strip (measured from the flow-line)
 - c. 6' wide sidewalk
 - d. 1' behind the back of walk
 - e. A right-of-way dedication to accommodate the cross-sectional elements above
9. Per comments made at Concept Plan Review, please revise the Bluff Street typical section to show an 8.5' wide landscape strip (measured from the flow-line to the edge of sidewalk) and a 6' wide sidewalk within the existing and proposed City right-of-way.
10. Please revise the site plans to show a public access easement being dedicated for the proposed Bluff Street cul-de-sac with a design compatible with the rail plaza area and provides an accessible route for emergency vehicles.
11. Pursuant to Concept Plan Review comments, please revise the site plans to show the construction of $\frac{3}{4}$ of the width for the east / west (base) alley adjacent to the northern property line (connection #9) and the north / south (base) alley on the west side of 3085 Bluff (connection #7).
12. Please revise the site plans to show the following typical section within right-of-way dedicated to the City for Junction Place south of Valmont Road and adjacent to the commercial / retail land uses:
- a. 10' wide travel lanes
 - b. 8.5' wide parking strip
 - c. 8.5' wide landscape strip (measured from the flow-line)
 - d. 10' wide sidewalk
13. Pursuant to TVAP, the DCS, and the concept design for the Valmont Road Railroad Road Quiet Zone, please revise the site plans to show a typical section for Valmont Road and label the public improvements to be constructed by the project to provide the following cross-section:
- a. The 16' wide median / left-turning lane
 - b. Two (2) 11' wide eastbound through lanes
 - c. 7' wide bike lane which includes the curb-and-gutter
 - d. 8' wide landscape strip
 - e. 10' wide sidewalk (west of Junction Place)
 - f. 12' wide multi-use path (east of Junction Place)
 - g. 2' wide public access easement between the multi-use path and proposed building / structures
 - h. Right-of-way dedication on Valmont Road to accommodate the cross-section above
14. Consistent with the TVAP transit goals and in support of the project's TDM goals, please revise the site plans to show the relocation and enhancement of the existing transit stop on Valmont Road to include:
- a. A relocated transit stop to the far side of the Valmont Rd and Junction Place intersection
 - b. A transit shelter, bench, trash receptacle and short-term bike parking
 - c. Concrete bus pad on Valmont Road

Please refer the RTD Standard Drawings SD-C120 and SD-C123 for details on the bus stop layout and bus pad for Valmont Road.

15. Pursuant to TVAP, please revise the site plans to (1) show the right-of-way to be dedicated for the rail plaza between the Bluff Street cul-de-sac and the railroad right-of-way; and (2) show the right-of-way reservation for the proposed underpass connecting the Rail Plaza and Wilderness areas. Please contact staff to discuss the preliminary alignment of the underpass. Confirm that the future train boarding platforms as shown conform to the most recent RTD project designs.
16. Please revise the site plans to show an un-obstructed line of sight for the raised crosswalk across Junction Place on the south side of the CICLO building using the AASHTO guidelines for stopping sight distance on horizontal curves.
17. Pursuant to Table 2-12 from Section 2.08 of the DCS, please revise the site plans to show a 12' wide multi-use path within a 16' wide public access easement along the railroad tracks from Valmont Road to the south property line. The alignment of the multi-use path can temporarily go around the existing brick building within a temporary public access easement but also include a permanent public access easement adjacent to the east side of the existing brick building to the southern property line for accommodation of future path realignment.
18. Please revise the horizontal alignment for Junction Place at Valmont Road by shifting the roadway to the east by approximately seven feet in order to accommodate the opposing 34th Street / Junction Place vehicle movements at the signalized intersection.
19. Please revise the site plans to include the preliminary design for the center lane raised medians required for the Valmont Road railroad quiet zone (west of the tracks) in order to demonstrate the proposed left-turn lane has been designed to accommodate the raised median design requirements for a railroad quiet zone. Please contact staff to obtain the preliminary design of the railroad quiet zone.
20. improvements without the demonstrate that final street cross-section accommodations for center median / left-turn lane are factored into addition right-of-way dedication in order to accommodate the future Valmont Road railroad quiet zone. A minimum raised median length of 100' is required west of the railroad. Please revise the site plans to show the proposed quiet zone accommodations and contract staff to obtain the preliminary design.
21. Please revise the site plans to show a bike / pedestrian connection along with a public access easement across the driveway serving the Trackage building and the Lot 3 of Block 5 building in order to provide a multi-modal connection between Junction Place and the north / south multi-use path along the east property line.
22. Please revise the site plans to disperse the short term bike parking so that the location of the short-term bike parking is consistent with section 9-9-6(g)(3) of the Boulder Revised Code.
23. Please revise the site plan to include a summary sheet on the number of short term and long term vehicle and bicycle parking stalls being provided and the number of accessible stalls being provided along with the number required to be provided.
24. Please revise the site plans to include a minimum turning radii sheet which demonstrates the proposed curb cut widths and curb radii can accommodate the turning movements of the appropriate design vehicles. The radius of curb radii should only be as large as necessary in order to accommodate the turning path of the appropriate design vehicle. Staff will review and provide additional comments on the width of the curb cuts and curb radii once the turning radii have been evaluated.
25. At time of site plan re-submittal, please include a cross-section for the proposed Woonerf in order to better understand the cross-section design elements being proposed for the shared street. Please note, the parallel parking on the Woonerf will be managed by the TVAP Parking District.
26. Pursuant to review comments at Concept Plan Review, please confirm the Boulder Land Consultants Survey Control Diagram dated 6-30-11 for Boulder Junction was used for the horizontal and vertical control and horizontal coordinate basis for the site or revise the ALTA surveys to use the Boulder Land Consultants Survey Control Diagram. Applicant can contact Alex May at (303) 579-9317 to obtain the data.
27. Please contact staff to discuss the TDM elements to be installed within the site in support of the TVAP TDM goals.
28. Please have the traffic engineer contact David Thompson for comments regarding the traffic study.

29. Please revise the site plan to show the north / south primary roadway from the existing 33rd Street to Valmont Road as Junction Place.

Addressing Elaine McLaughlin, 303-441-4130

Revise the street labels on the project plans such that the north/south primary roadway from 33rd St. to Valmont is: Junction Place.

Building Design Elaine McLaughlin, 303-441-4130

1. A simple-form built model must be provided that illustrates the entire neighborhood proposed with a SketchUp model for the individual projects to best understand the new neighborhood.
2. Refer to the preliminary analysis of consistency with TVAP in Attachment A.
3. As a part of the Site Review process the Boulder Design Advisory Board (BDAB) must review the proposed project. Staff will contact the applicant regarding the timing on that review.
4. Refer to plan document deficiency comments for each specific building/project below under "Plan Documents."

Drainage Scott Kuhna, 303-441-4071

1. The plans show grading on neighboring property owned by the BNSF Railway Company beyond the limits of an "Ex. 25' Easement Reservation Railroad Track and Drainage". This grading will also increase the volume of ponding for the recessed area between the development and the railroad tracks. Written approval from the BNSF Railway Company for grading and additional ponding on their property is required.
2. Detention and/or water quality ponds intended to detain and treat stormwater runoff for multiple properties shall be located in "Outlots", with maintenance responsibilities detailed in the subdivision agreement.
3. Specific maintenance requirements, methods, etc. for the proposed porous pavers in the public right-of-way must be included in the *Preliminary Drainage Report – SPARK Redevelopment (Drainage Report)* for city staff to evaluate the long term ramifications to the city of the paver systems. Maintenance responsibilities for the paver systems (underdrains, etc.) will remain with the adjacent property owners or HOA.
4. Per Section 7.13(C)(2) of the City of Boulder *Design and Construction Standards (DCS)*, all proposed projects and developments (over 1 acre) shall provide Water Quality Capture Volume (WQCV) and a Water Quality Outlet in accordance with the *UDFCD Drainage Criteria Manual – Volume 3 (Manual)*. While some of this information is detailed for some of the sub-basins, an overall analysis for the site as a whole needs to be included in the *Drainage Report*.
5. Page 15 of the *Drainage Report* states that Basin A30 "will pass through a water quality BMP before entering the proposed storm sewer system". Additional information about this BMP is required at this time.
6. Page 20 of the *Drainage Report* states that Basin B45 "will be directed to a water quality BMP before discharging into the proposed storm sewer system". Additional information about this BMP is required at this time.
7. It is not clear if the "alleys" (with porous pavers) mentioned in the *Drainage Report* will be public or private. Revise the plans and report accordingly.
8. Storm sewer laterals should be approximately perpendicular to the storm main. Clarification is needed for the proposed layouts at the intersection of 32nd Street and Meredith Street and south of the intersection at 34th Street and Valmont.

Fees

Please note that 2015 development review fees include a \$131 hourly rate for reviewer services following the initial city response (these written comments). Please see the P&DS Questions and Answers brochure for more information about the hourly billing system.

Fire Protection David Lowrey, 303.441.4356

1. Parts of 32nd St. and parts of Meredith St. do not meet the minimum width for emergency access vehicles per the City of Boulder Design and Construction Standard. Referencing page C.41 and C.42 the width of these streets will be 16'. Boulder Fire has met with the architect and civil engineer and will use the side walk (no parking side) as part of emergency access width in these limited areas. The plans need to indicate that the sidewalk will support the maximum weight of our heaviest apparatus as well as the "outrigger" weight. However, staff is recommending that there be no attached sidewalks in this location.

2. Fire Hydrants must meet the City of Boulder Design and Construction Standard. There might need to be additional hydrants added based on distance from certain structures. Possible mid-way on 32nd street and some of the private streets to the west of 32nd.

Groundwater Scott Kuhna, 303-441-4071

Groundwater is a concern in many areas of the City of Boulder. Please be advised that if it is encountered at this site, an underdrain/dewatering system may be required to reduce groundwater infiltration, and information pertaining to the quality of the groundwater encountered on the site will be required to determine if treatment is necessary prior to discharge from the site. City and/or State permits are required for the discharge of any groundwater to the public storm sewer system. It should be noted that the Installation of underground utilities may also provide a conveyance for any contaminated groundwater associated with the properties.

Landscaping Elizabeth Lokocz, 303-441-3138

Many of the proposed landscape elements are consistent with current city goals and TVAP guidelines. The overall level of detail provided needs to be developed for staff to be able to provide feedback and determine if all criteria are met. Please review all comments and note that a number of reviewer comments directly affect the landscape plans, such as the inconsistent street cross sections currently shown. Staff anticipates that additional comments will be provided as the project continues to develop. It is particularly important to note all landscape modifications requested as part of the site review approval, such as a reduction in minimum street tree standards per section 9-9-13 B.R.C. or a reduction in minimum planting strip widths per the approved TVAP cross-sections.

1. Label all buildings, streets and scales across all sheets.
2. Evaluate all pedestrian connections and the proposed pavement adjacent to the curb. The connections from the street to the sidewalk seem overly large. Staff supports alternative forms of pavement, but the many areas of pervious pavers may be extremely high maintenance in the event that future utility work is required. Evaluate if the proposed design will read strongly from a street perspective, rather than in plan view which can exaggerate such shapes. The paved strip next to the curb may be need for high turn-over parking areas, but it needs clarification. Is it concrete or a lower impact paver? Include it in the street cross-sections (civil sheets). Given the already reduced TVAP cross-sections (the typical minimum planting width is eight feet), staff does not support their use on low-turnover residential streets.
3. The landscape sheet open space calculation plan presents a number of different categories of needed information. Ultimately, the project will need separate open space analysis calculations and landscape requirements calculations. The proposed tree plan might be the better location for the landscape requirements tables. Staff anticipates that as the project develops it will need overall summary tables and a breakdown by block. According to staff's analysis, the street tree calculations show a significant gap between the number of required and proposed street trees; approximately a third of the required trees are not provided. Reformat the table by street. Include the total length of each street used as the basis for the table and columns for the spacing, required trees and provided trees. What has been proposed would require a modification to the city's landscape regulations through the Site Review process. This is an extremely difficult modification to support given the goals found in the Boulder Valley Comprehensive Plan (policies 3.10 & 3.11) as well as the threat of Emerald Ash Borer beetle (which will likely consume a significant amount of the city's urban canopy) . It appears that several factors are contributing to the reduction in street trees including the attached sidewalk alignment at intersections, attached sidewalks in general, and utility conflicts.
4. To the extent feasible, eliminate utility conflicts. The overall plan is generally coordinated and the dry utility locations are much appreciated, but some locations are problematic, especially given the overall shortage of street trees. Staff can provide a redlined plan for easy coordination if needed or schedule a meeting to review alternatives.
5. Soil quality grows increasing poor moving east in Boulder and tree diversity should be considered for such a significant project area. The following recommendations increase diversity with species appropriate to the conditions.
 - a. Limit the number of proposed ginkgo to around ten overall; they do not perform well universally in Colorado and may not be successful at this site. The grove of ginkgo in the round-about could be a number of different trees. False cypress would be a very interesting selection for this location and is likely to do well. Common hackberry would be another good option for the planting around the perimeter of the round-about. Turkish Filbert would also be an option in the planting areas on 34th St. adjacent to the larger mixed use buildings. All three have fall color in the yellow range.
 - b. The overall number of maples and species selected could be problematic. Some sugar maple in larger planting areas is supportable, but substitute the Crimson Sunset and Red Sunset with other options. City forestry has planted some Acer negundo 'Sensation' and would support a small number (10) within the overall project. *A. grandidentatum* or bigtooth maple is another option in a limited number.
 - c. Other species to mix into the overall street tree plan include planetree (great bark), Japanese pagoda tree, yellow

buckeye, Espresso Kentucky coffee tree, swamp white oak, shingle oak and (in larger areas only) English oak. Consider that diversity must be across genres and not just varying species within a genus.

- d. Other trees to consider in some of the private planting areas include American linden (Redmond?), Japanese tree lilac (small and clump form), Russian hawthorne, European larch,
6. The round-about area needs to be carefully detailed and developed as early as possible. Will this area treat storm water? How will the grate (illustrated in perspective) be constructed and accessed for maintenance? Please consider the constructability as early as possible to avoid future changes.
7. There are a number of narrow planting areas between walks and buildings. Please consider how these areas will be treated and maintained as early as possible. They are particularly problematic between five foot sidewalks and buildings within the residential areas.
8. The plans indicate a silva cell detail on sheet 27, but staff was not able to locate any additional information on their use. If silva cells are proposed to increase soil volume or as part of the stormwater management system, please provide additional detail on their numbers and locations. They should not be included as part of the Site Review submittal if they are optional.
9. Sheet 27 also includes a tree grate detail. It's not clear where the grate is proposed, but without significant additional detail, a 48 inch square grate would not be supported. Please see the Design and Construction Standards for the city's approved detail.
10. The plans indicate a silva cell detail on sheet 27, but staff was not able to locate any additional information on their use. If silva cells are proposed to increase soil volume or as part of the stormwater management system, please provide additional detail on their numbers and locations. They should not be included as part of the Site Review submittal if they are optional.
11. Sheet 27 also includes a tree grate detail. It's not clear where the grate is proposed, but without significant additional detail, a 48 inch square grate would not be supported. Please see the Design and Construction Standards for the city's approved detail.

Legal Documents Julia Chase, City Attorney's Office, Ph. (303) 441-3020

The Applicant will be required to sign a Development Agreement, if approved. When staff requests, the Applicant shall provide the following;

- 1) an updated title commitment current within 30 days; and
- 2) proof of authorization to sign on behalf of the owner (such as an operating agreement or statement of authority).

Lot Layout Elaine McLaughlin, 303-441-4130

1. Indicate if there is intent to subdivide the property. For staff to determine setbacks, and in turn setback modifications proposed for specific buildings provide greater clarity on the intent of the lot layout.
2. The identification of the proposed lots is also necessary to determine Floor Area Ratio proposed. Note that in the "Project Fact Sheet" the FAR for the Ciclo building is identified as being a 2.2 FAR, which exceeds the maximum possible within the MU-4 zoning of 2.0 FAR. Please clarify and correct or revise as necessary. Refer to the land use code section 9-16, found here, for definitions of "floor area" and "uninhabitable space" to understand what it included and excluded from Floor Area calculations.
3. For Timber Lofts/Timber Townhomes there are two different zoning districts and it appears that the building was designed to intentionally place the townhomes within the RH-6 zoning which is appropriate. However, because of the separate zoning, each portion of the building and lot must stand on its own in terms of consistency with the underlying zoning. In other words, the townhomes must count the open space only on the townhome parcel and can't count the open space on the MU-4 portion. This is best communicated within the Project Fact Sheet. However, staff also recommends that there be open space diagrams/exhibits for each project within each zone to best communicate how open space requirements are being met. Refer to the Land Use Code section 9-9-11 found here to understand what can be counted toward "useable open space."
4. Note that if the intent is to subdivide, please submit an application specifically for a Preliminary Plat upon application resubmittal in response to these comments.

Neighborhood Comments Elaine McLaughlin, 303-441-4130

1. At the time of the preparation of these comments, one comment letter was received and appears to support the application. It is provided in Attachment B.

2. Based on the size of the proposal and the proposed restaurant uses, neighborhood meeting(s) must be organized by the applicant with staff present, and using the city's notification process to receive input from neighbors on the proposed project. The Use Review applications for all three restaurants necessitates a Good Neighbor Meeting, consistent with the land use code section 9-2-4, B.R.C. 1981 found [here](#). The applicant is required to host the meeting and must coordinate with staff on an appropriate time and location. City staff will send the public notification out to the neighborhood once a time and location is established. The applicant must provide notice to staff no later than two weeks prior to the meeting to ensure adequate notification time.

Plan Documents: GENERAL Elaine McLaughlin, 303-441-4130

1. The Fact Sheet included as a plan element is not legible. Please format this information onto an 8½ x 11 sheet format with a more legible font size.
2. For each project, provide a Site Plan and on it place a Data Summary Table (as well as part of an overall Site Plan Data Summary) that includes the following information:
 - a. Lot Area
 - b. Building Area: amount of floor area for each use and total Floor Area
 - c. Floor Area Ratio
 - d. Number of residential units and bedroom count within each unit
 - e. Parking Required/Provided
 - f. Bike Parking Required/Provided
 - g. Useable Open Space Required/Provided
 - h. Setbacks Required/Proposed (modifications requested)
3. Place Street Names on the Site Plan and first floor plans of all projects.
4. Indicate any plans to phase any aspects of the project(s).
5. Provide street cross sections for each street, indicate consistency or inconsistency with the TVAP cross-sections.
6. A detailed plan showing the useable open space for each site, along with a written statement of how it serves the public interest, is a requirement for projects that request a height modification, under the Land Use Code section 9-2-14(e), B.R.C. 1981.
7. Note that prior to any Planning Board hearings a materials sample board for each building will be required.
8. As project plans progress, provide a more detailed energy efficiency plan that articulates how the applicant will specifically meet the city's energy efficiency standards of the IECC 2012 +30%.
9. Provide window details, use of vinyl windows and window frames without a return or reveal are discouraged.

Plan Documents: MAARKET Elaine McLaughlin, 303-441-4130

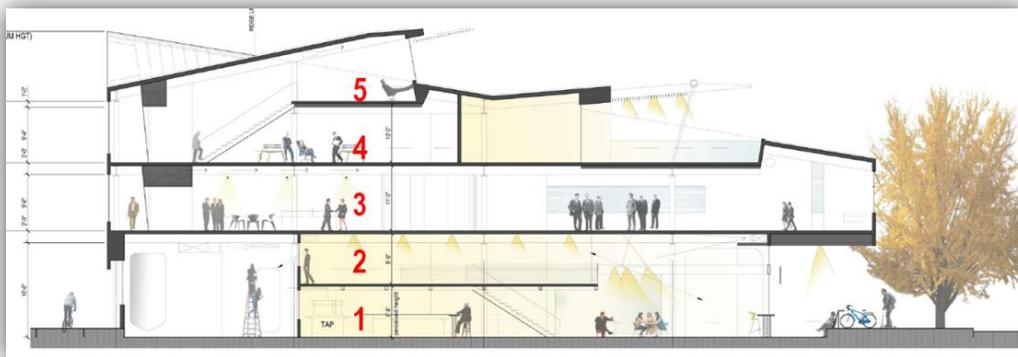
1. Staff recommends the applicant provide a SketchUp (or similar) 3D model to help convey the relatively complex design intent. The fairly atypical and "organic" form of the building along with the random window patterning and varied material application needs to be more clearly presented.
2. Provide precedent images of the use of horizontal cedar siding mixed with vertical cedar siding to help convey the design intent.
3. There is no delineation of a property line on the project plans. Therefore, it is unclear what lot area is being used to determine the 1.8 FAR and the 15 percent open space. The fact sheet indicates a "lot size" of 30,159. Indicate if there is a plan for replatting the site and if, so file an application for Preliminary Plat review.

- On sheet 47, Level 1 and Level 1 Mezzanine: a number of floor plan elements and rooms are not labeled please label all of the



elements shown on the plan. Those include, but are not limited to those shown below as indicated with red arrows.

- The mezzanine counts as a story under the land use code, therefore revise the information on page 45 as well as on the Fact Sheet to indicate that it is a five story building.



- On sheet 47, Level 1, the outdoor stage/plinth needs additional information not only on the plan set with details, but also with regard to programming of the space (see Use Review comments below). The “plinth” implies that the stage is elevated, however, there are no details, please provide greater information in the resubmittal of the height, materials, covering (if any), location of speakers, etc.
- Note that the floor plans must indicate a net square footage rather than gross square footage, as it is the net from which the parking requirements are established. For elements that are not factored into a net floor area total, refer to the land use code

definitions of “Floor Area” and “Uninhabitable Space” found [here](#).

8. On page 51, materials, provide information on the durability and long term maintenance of the: clear glass guardrail and the cedar siding given intense seasonal variation in Boulder’s climate. Note that there are similar concerns about the translucent fiberglass sign/light valence however, that element may not be permitted in the sign code, see comment under “signage.”
9. Relocate the transformer/screen away from front of the building. The location would not be conducive to pedestrian activity.
10. For the elevations, provide labels as shown in the figures below.
11. The image on sheet 51 of the 4” cedar siding in silver gray is pixilated; to best communicate the material, please provide a stronger image. In addition, a materials sample board will be required prior to a hearing before the Planning Board.
12. Indicate the location of short and long term bike parking/storage.
13. Refer to the Level 2, 3 and 4 plan below that indicates areas where additional labels or information is required.
14. The east elevation is essentially a 250 foot long blank wall adjacent to the multi-use path. The Rail Plaza District Guidelines state, “orient buildings to Junction Place as well as to the tracks. If feasible, place active uses on the first floor. Consider making the track-side frontage a car-free zone with pedestrian amenities. Because this path is a public way, additional windows and/or doorways such as roll-up doors would provide enhanced access and activity along the east elevation.

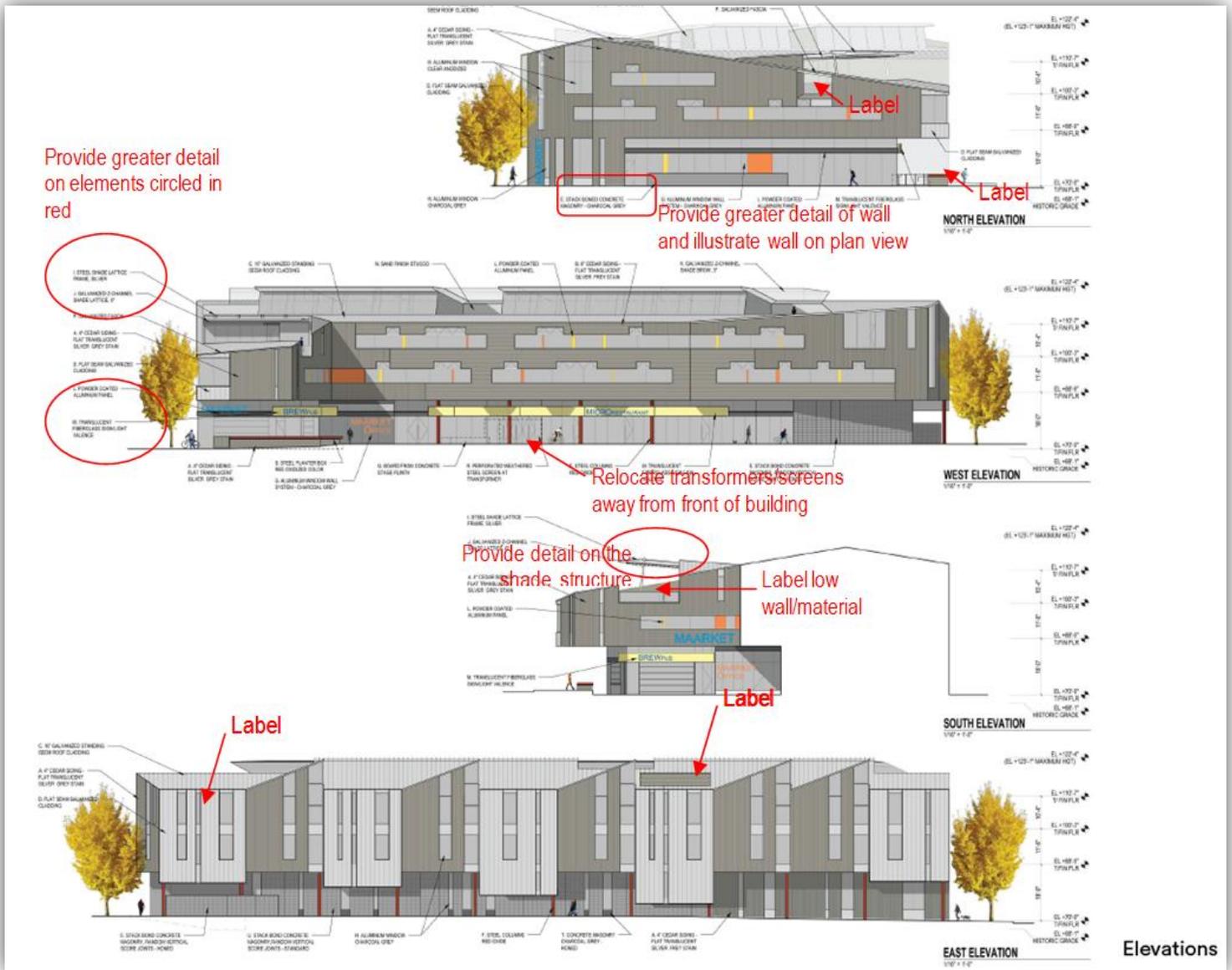


15. The perspective shown below illustrates the second floor of the building projecting over the first floor and illustrates that there may be a view of the Flatirons available from the location of the image. The TVAP General Design Guidelines state, “consider opportunities to frame or preserve views of the Flatirons to the southwest.” Therefore, a more definitive analysis of this potential should be performed using Google Earth. The projection of the second floor may impose on this viewshed and may not actually contribute to either framing the street or creating pedestrian interest when the second floor cantilevers over the first floor. Additional exhibits will assist staff in determining if the building’s configuration meets the design guidelines and site review criteria for:
 - Blocking views
 - Pedestrian scale articulation and building elements

The Ciclo Building across the street is planned with a more “chamfered” corner. It may be more appropriate to pull back the corner for this building instead given the potential for capturing the viewshed corridor toward the Flatirons. Provide greater information on how best to capture this existing viewshed and preserve it from a public view corridor.



16. The elevations are lacking in detail and information, as identified below.



Plan Documents: Ciclo Elaine McLaughlin, 303-441-4130

1. The plans illustrate a property line for the subject site however there is no indication that the overall S'PARK site will be replatted. If that is the intent, an application for a Preliminary Plat review will be necessary to track alongside the Site Review application. If the Ciclo site is intended to be replatted, note that the Ciclo building appears to exceed the maximum 2.0 FAR under the MU-4 zoning. Note that the minimum open space for residential lots is 15 percent and that the minimum private open space for residential uses is 60 square feet. There is no mechanism for modifying the Floor Area Ratio for the MU-4 zoning. Therefore, a redesign of the building will be necessary to meet the standards for FAR and open space.
2. On the Ground Level plan, label the use planned on the east and north sides of the building. Also label the dashed box elements.
3. Provide cross-sections through the building to help convey the mezzanine in relation to the ground floor portico on the east side of the building.
4. The Site Review criteria within the land use code requires, "Exteriors of buildings present a sense of permanence through the use

of authentic materials such as stone, brick, wood, metal or similar products and building material detailing.” While under specific circumstances an authentic stucco application can be considered as an accent material, the use of stucco as a primary building material doesn’t typically convey a sense of permanence and is discouraged.

5. Provide window details, use of vinyl windows and window frames without a return or reveal are discouraged.
6. For the corner Gallery/Flex Space note that the two story height appears truncated for a corner element. Staff recommends pulling the space up to the full height of the building.
7. Provide a Site Plan Data Summary Table for Ciclo on the Site Plan (as well as part of an overall Site Plan Data Summary) that includes the following information:
 - a. Lot Area
 - b. Building Area: amount of floor area for each use and total Floor Area
 - c. Floor Area Ratio
 - d. Number of residential units and bedroom count within each unit
 - e. Parking Required/Provided
 - f. Bike Parking Required/Provided
 - g. Useable Open Space Required/Provided
 - h. Setbacks Required/Proposed (modifications requested)
7. Note that signage can’t be approved through a Site Review application, rather through separate sign permit. However, staff understands the desire to integrate the signage into the architecture. Therefore, please “ghost” in any of the signage on the elevations and label them as “*Sign, not a part of Site Review application.*”

Plan Documents: Railyards Elaine McLaughlin, 303-441-4130

1. The plans illustrate a property line for the subject site however there is no indication that the overall S’PARK site will be replatted. Please clarify on this site plan as well as for the overall site plan and submit an application for a preliminary plat if that is the intent.
2. Indicate if the 0.73 FAR is calculated using a Gross or Net Site Area. As a public access easement, the woonerf would be deducted from the overall site area to calculate FAR. Therefore, based upon the net area (deducting for the public access easement) the FAR is determined as follows: $92,175\text{sf} / 67,039\text{sf} = 1.34$ FAR, rather than the 0.73 FAR listed on the Fact sheet.
3. Illustrate any outdoor seating planned for the proposed restaurant.
4. There is not a Use Review application or management plan within the application materials for the Proposed Restaurant. Provide an application for Use Review for the restaurant if the intent is to entitle it simultaneous to the Site Review. Include a management plan and indicate number of seats, size, and if outdoor seating is proposed.
5. Label the material proposed on the fourth floor outdoor deck railing.
6. Note that signage can’t be approved through a Site Review application, rather through separate sign permit. However, staff understands the desire to integrate the signage into the architecture. Therefore, please “ghost” in any of the signage on the elevations and label them as “*Sign, not a part of Site Review application.*”
7. In Concept Plan discussions there was a desire to have access, or at least visual permeability, into the building from the multi-use path along the tracks. At the time, the discussions included the possibility of “roll-up” doors or other accesses into the building to activate that side the public multi-use path. The concern is that there would be a nearly 300 foot long blank wall along the multi-use path adjacent to the tracks creating a lack of visibility and “eyes on the street” as well as a lack of activity along a public way.
8. Indicate if the woonerf is intended to fulfill the required 15-20 percent open space for the building or lot. Note that the woonerf will

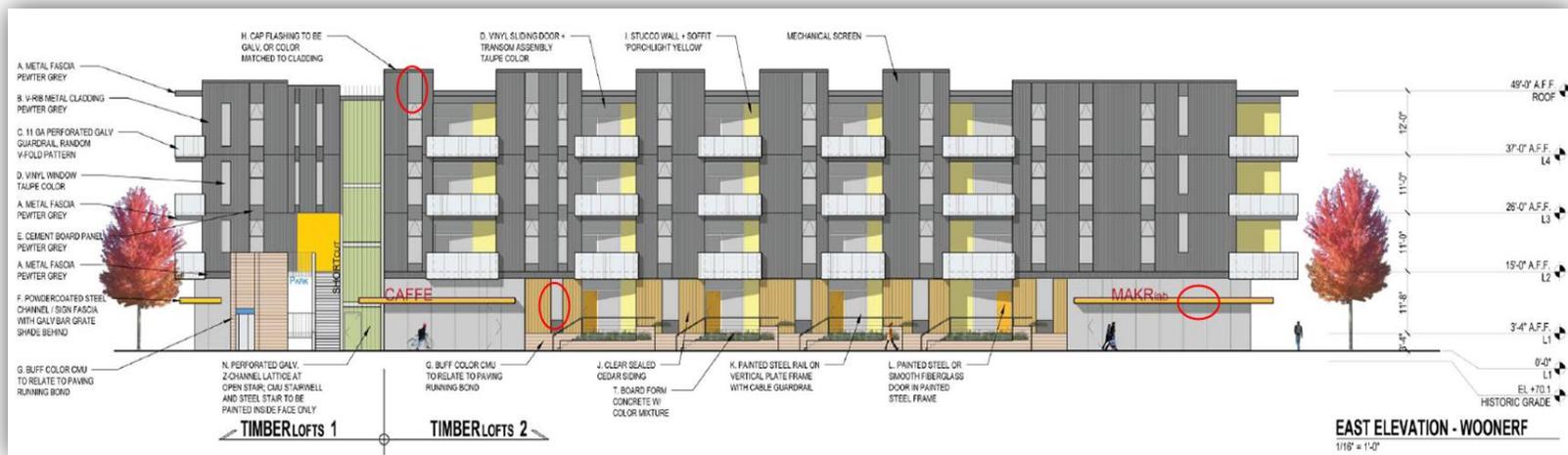


not count 100 percent toward open space and the travel lane for vehicles must be deducted from the total.

Plan Documents: Timber Lofts/Townhomes Elaine McLaughlin, 303-441-4130

1. The plans illustrate a property line for the subject site however there is no indication that the overall S'PARK site will be replatted. Please clarify on this site plan as well as for the overall site plan and submit an application for a preliminary plat if that is the intent.
2. The number of units proposed is not indicated on the project plans or within the Summary Fact Sheet. Please provide additional information on the number of units and the composition of bedrooms within each unit.
3. A portion of the Timber Lofts development is within an area zoned as RH-6 and includes an area labeled as "office" which is not a permitted use in the RH-6 zoning district. Clarify if this office use is associated with the Lofts as a leasing office (and label as such) or what the intended use will be. If it's associated with the lofts as a leasing office, it is permitted as an accessory use.
4. Calculate the density of the Timber Commons that is within the RH-6 zoning separately from the Timber Lofts which is in the MU-4 zoning. It appears that the building was intentionally designed to have the two different portions of the building (the lofts versus the townhomes "Commons") within each respective zoning district. As noted under "Lot Layout" each side of the project must stand on its own in terms of consistency with the underlying zoning. Therefore, the open space and density must be calculated for the RH-6 zoned portion separately from the MU-4 zoned area.
5. Indicate a Site Plan Data Summary Table for Timber Lofts that includes information detailed above under "Plan Documents: Ciclo." Note that certain areas count toward floor area and certain areas do not. Please reference the Land Use Code definitions found [here](#) for "Floor Area" and "Uninhabitable Space."
6. The perspective sketches are not detailed enough to clearly communicate the intent. Please refine the sketches for better clarity and communication and add street names and or key to understand where the image is focused upon. Similarly, some elements warrant labels on the perspectives as shown below.
7. For mechanical screens note that the land use code discourages tall building elements to screen the mechanical, *"Screening does not increase the apparent height of the walls of the building. The use of parapet walls to screen mechanical equipment is discouraged. The height of parapet walls should be the minimum necessary to screen mechanical equipment."*
8. B-1 Floor Plan: add labels to the plan, include type of parking space for each space "C" for Compact, "S" for Standard, "HC" for Handicap. Label areas within the plan that are shaded and unlabeled.
9. Clarify what the difference is between Timber Lofts 1 and Timber Lofts 2. Unless there's a use, design or zoning distinction, the label is not necessary on the Site Review plan submittal.
10. B-2 Floor Plan: add labels to the plan including parking. All spaces that appear to be enclosed with walls must be labeled. Note that there are three spaces in a yellow-orange color on the west side that have doorways but no labels.
11. On the Level 1 Plan, indicate the USGS spot elevation for the low point of the building for the purpose of measuring height based on the city's standard found [here](#).
12. Elevations: Label the USGS base height elevation (from the lowest point shown on the site plan) and label the high point of the roof with the USGS height along with a calculation of the height in feet from the low point to the high point on the building. Note that "historic grade" as labeled on the elevations is not relevant as a point from which to measure the height of the building. Use the City's standard for measuring height found [here](#).
13. East Elevation: label the type of storefront window system proposed for the retail spaces or provide a manufacturers' cut sheet.
14. Provide a detail of the types of windows proposed for the residential units.

15. Label any material not currently labeled and as circled below.



16. Inherent within all of the TVAP guidelines and the site review criteria is the need to ensure appropriate pedestrian level of articulation and interest. For project approval, findings must be made that the project meets the Site Review Criteria, among which are section 9-2-14(h)(2)(F) which states,

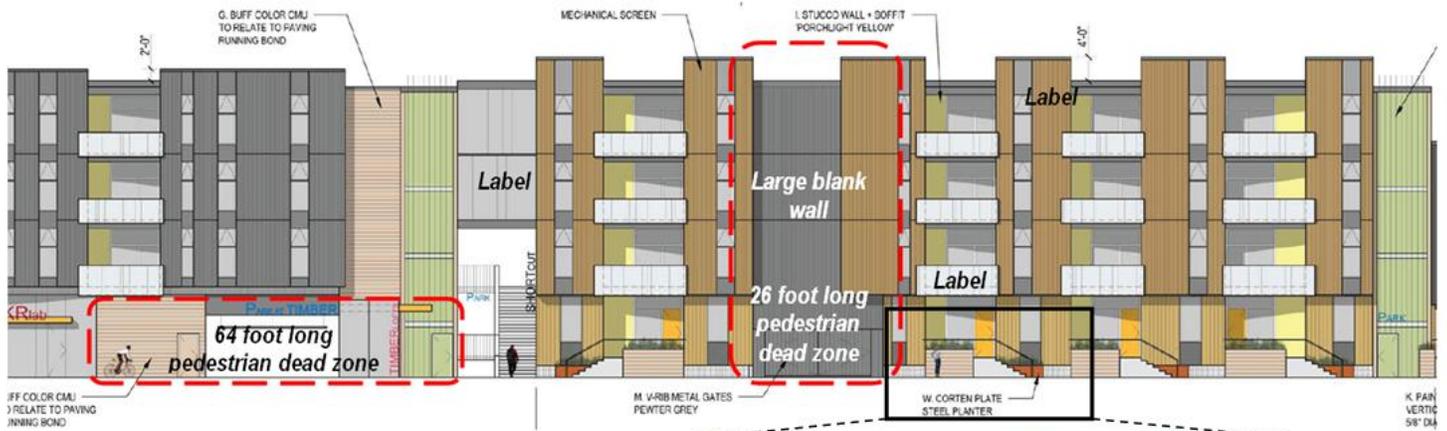
“Projects are designed to a human scale and promote a safe and vibrant pedestrian experience through the location of building frontages along public streets, plazas, sidewalks and paths, and through the use of building elements, design details and landscape materials that include, without limitation, the location of entrances and windows, and the creation of transparency and activity at the pedestrian level.”

Similarly, within the General Design Guidelines it states,

“Design buildings with pedestrian-scale materials and architectural articulation, particularly on the first floor. Avoid large blank walls. Along streets and sidewalks provide pedestrian interest, including transparent windows and well-defined building entrances.”

To that end, it is noted that while there are entries to units facing the street, stoop access is turned 90 degrees, such that the element facing the walkway is a tall wall. And, while the café spaces could engage the pedestrian, there are also a number of areas along all of the elevations that create “pedestrian dead zones” where there is little in the way of either transparency or interest for the pedestrian.

Identified below in the elevation are just some of the areas that would not meet the TVAP guidelines or Site Review criteria for human and pedestrian scale design. Please ensure that the buildings have pedestrian level interest, if not through transparencies into the building then through building details.

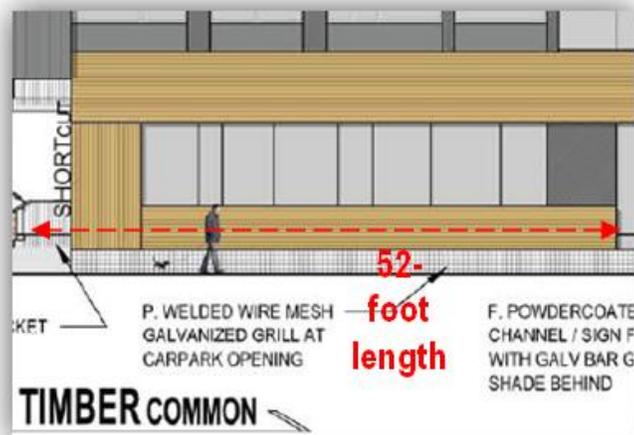
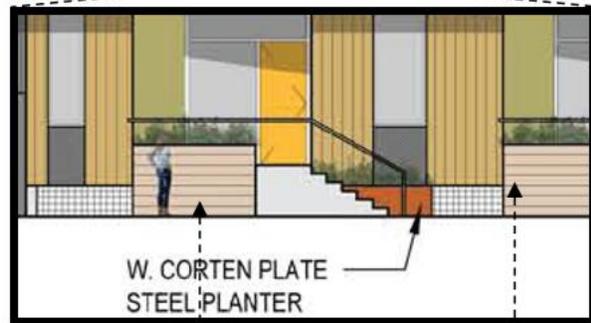


17. The five-foot height of the entry patios with planters, while creating “defensible” space, creates a pedestrian barrier. The use of buff-colored CMU would likely not create a strong pedestrian experience.

The other materials in combination could enhance the overall pedestrian experience if the wall height were lowered and/or the stoops were turned 90 degrees to face the street to create more of an inviting appearance from the view of the pedestrian.

18. Label the windows on the Timber Townhomes.

19. In a similar manner, the Timber Common has window heights that wouldn't serve the pedestrian experience, shown below.



20. The base of the Timber Common being the opening to the garage creates additional height in this location of the building, and also doesn't serve the pedestrian experience.

21. Provide better images of the standing seam cladding in photos as precedent images of the material as an exterior finish material.

22. The photo image of the 11 gauge perforated galvanized metal guardrail is pixilated and not legible, please provide a better image.

23. The elevator access along with the staircase to the second floor and the access to the parking structure all face a critical corner where the terminus of both Bluff Street and the woonerf are located and the expectation of an important public space is anticipated. These elements could be better located away from this critical corner.

24. There is no Use Review application or management plan within the application materials for the Proposed Cafe. Provide an application for Use Review for the cafe if the intent is to entitle it simultaneous to the Site Review. Include a management plan and indicate number of seats, size, and if outdoor seating is proposed.

17. The signage shown for the café must meet the sign code. Note that the size of the sign may include the band upon which the lettering is mounted. However, signage should be simply “ghosted” in on the elevations as a separate sign permit is required.

Plan Documents: Meredith House Elaine McLaughlin, 303-441-4130

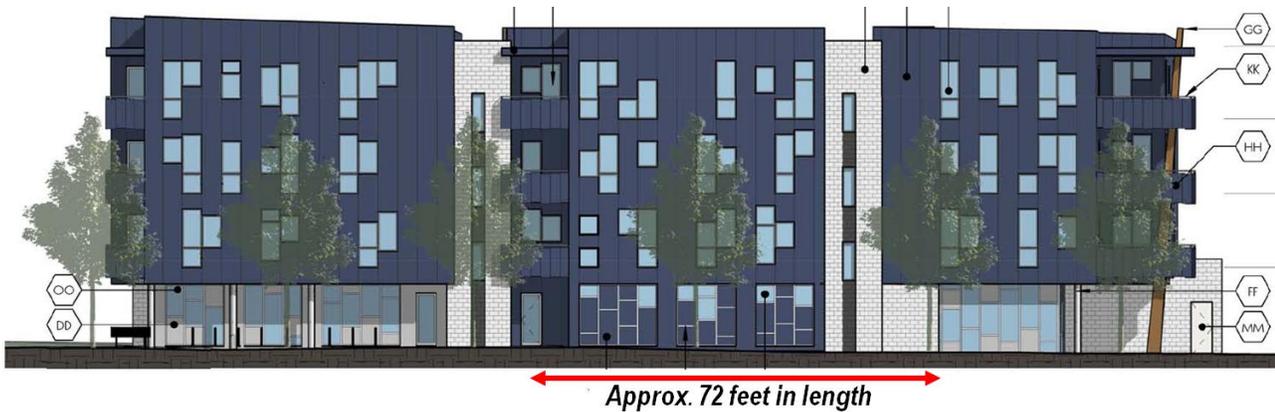
1. A site plan must be provided that illustrates how the project meets the required 20 percent useable open space per the land use code section 9-9-11(c) found [here](#). It is unclear if the Meredith Park is intended to serve the Meredith House as open space and would thus become part of that lot, or if it is intended to be dedicated.
2. On the building elevations, provide the base height elevation (USGS datum) from which the overall height of the building is measured per the city's standards for measuring height found [here](#).
3. Please provide a scale on the Elevations, staff assumes 1/16 scale for purposes of the comments herein.
4. For project approval, findings must be made that the project meets the Site Review Criteria, among which are section 9-2-14(h)(2)(F) which states,

"Projects are designed to a human scale and promote a safe and vibrant pedestrian experience through the location of building frontages along public streets, plazas, sidewalks and paths, and through the use of building elements, design details and landscape materials that include, without limitation, the location of entrances and windows, and the creation of transparency and activity at the pedestrian level."

Similarly, within the General Design Guidelines it states,

"Design buildings with pedestrian-scale materials and architectural articulation, particularly on the first floor. Avoid large blank walls. Along streets and sidewalks provide pedestrian interest, including transparent windows and well-defined building entrances."

As shown below, there is a significant portion of the building along Meredith Street (intended as an active and key link through this portion of Boulder Junction) that wouldn't meet the guidelines or the Site Review criteria. Because the storage use is placed at the ground level and at the center of the building the need to shroud this use with dark spandrel glass and insulated glazing units creates a blank wall along the streetscape.



Similarly, the CMU wall on the east end that is intended to "screen" the parking creates little in the way of pedestrian interest. The applicant must refine the ground floor to create greater pedestrian interest and less of a fortified appearance. The CMU wall is blank except for the metal door that accesses the fire sprinkler system. This, combined with the tall CMU wall beyond the entry and blank windows of the storage areas, doesn't contribute to a pedestrian streetscape. Revise for better consistency with the guidelines and Site Review criteria.



5. Consider other ground floor uses than the storage at the front of the building consistent with the guidelines, refer to Attachment A.

Plan Documents: S'PARK_west Elaine McLaughlin, 303-441-4130

1. The streetscape of the permanently affordable units doesn't meet city standards or the TVAP cross-section for local streets and the proposed right of way would be unusual to apply only to an affordable housing street. Among the Findings of the Inclusionary Housing ordinance is that affordable units shall be *"indistinguishable from the surrounding market housing in quality, design and general appearance."* The streetscape would be different for this portion of the overall S'PARK development and therefore must be revised to meet city standards. In review of the minutes from the Concept Plan discussion at Planning Board, while there appeared to be support for narrower roadways, there was no discussion or indication of support for an attached walkway on one side of the street with no street trees. Refer to comments within Access and Circulation as well as Landscape for specific details.
2. Provide a site plan and on that plan place a data summary table with information enumerated above under "Plan Documents: General." Note on the Fact Sheet summary with the heading, "Residential Density (Allowable = 1800 sf/unit)" where it states, "Remainder of original SPARK_west Site Blocks 4&5" with a total of 8 units please change that notation to read: "RH-6 zoned units within the Timber Condominiums)"
3. Because the minimum open space per dwelling unit requirement within RH-6 is 600 square feet, provide an open space diagram that illustrates what areas are being counted as Useable Open Space, reference the definition of Useable Open Space under the Land Use Code [here](#).
4. The floor plans lack any labels. Please ensure that there is enough information provided to convey the various typical elements of each plan. For example, there are areas that are shaded an "ochre" color that are not labeled as well as what appears to be roofs of lower stories on upper floor plans. There are also what appears to be decks that are not labeled as such.
5. Label the direction and location of the perspective sketches.
6. The photos of materials on page 102 don't include all of the types of materials proposed please ensure all materials are represented by an image or photo. Note that an actual materials sample board will be required prior to a Planning Board hearing.
7. Provide color elevations for the townhomes.
8. Ghost-in tree locations on the site plans.
9. Identify what the type of planking material will be used as balustrades.
10. In a review of the Planning Board minutes from the Concept Plan hearing held on Sept. 4, 2014 it is noted,

Some members would prefer to see more gabled roofs for a homier and less industrial aesthetic. Other members thought that a mixture of styles and rooflines would be appropriate. Flat roofs could accommodate terraces. Most members would like to see a simplified and more restrained aesthetic but with a charming character. Look at the Holiday neighborhood for ideas.

Similarly, within TVAP it states,

A place that is not overly planned, with a "charming chaos" that exhibits a variety of building sizes, styles, and densities where not everything looks the same.

Considering the comments from Planning Board and the TVAP intent that not everything look the same, staff recommends the applicant consider the use of variation in building or roof forms on some of the buildings. As currently shown there are some simple shed roof forms in several locations but overall there are flat roofs and boxy shapes for the 69 units in 10 different buildings creating excessive uniformity (refer to elevations on the following page). Note that the length of the streetscape along Bluff Street is equivalent to a city block. The concern is that the length of the facades would not address the Concept Plan comments or the intent of TVAP to avoid development where "everything looks the same." The applicant is encouraged to consider other building forms, articulation, varied roof forms or other means to punctuate the long, repetitive elevations.



Bluff Elevation

←----- 410 foot length -----→



Meredith/Junction Place

←----- 320 foot length -----→



←----- @ 355 foot length -----→

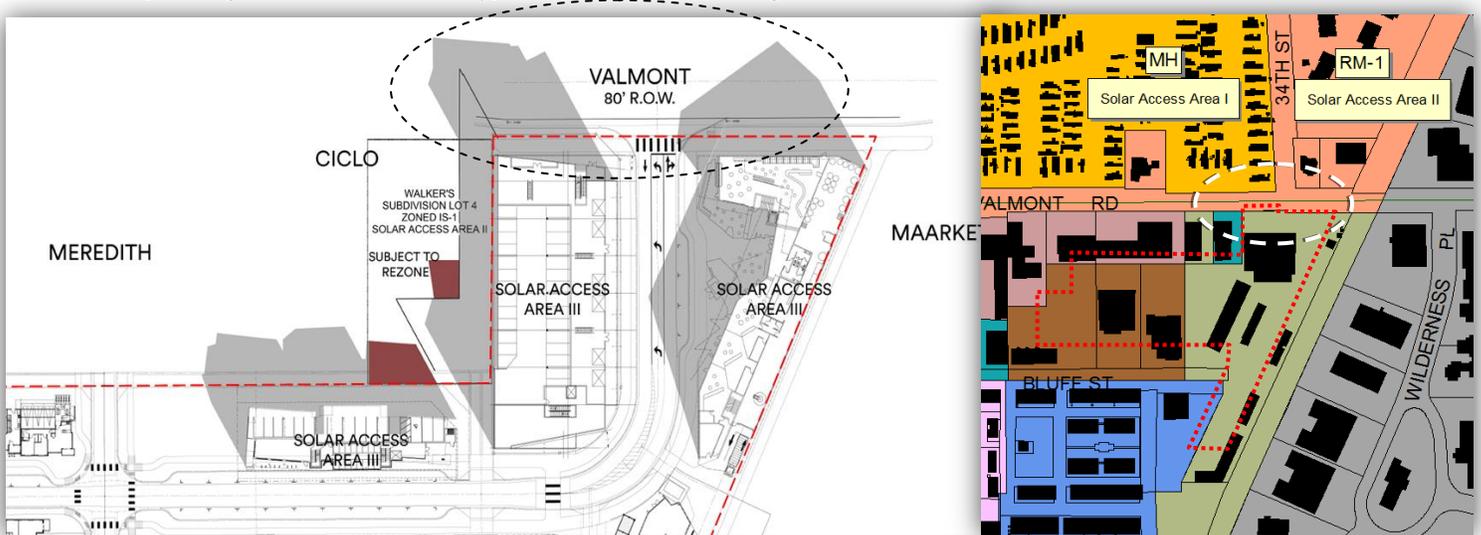
Example of a city block and variation that occurs in 300+ feet

Review Process Elaine McLaughlin, 303-441-4130

1. Please submit the application materials for a review by the Boulder Design Advisory Board (BDAB) prior to resubmittal of plans.
2. Staff recommends that at the time the project is found to meet Site Review criteria, and staff can provide a recommendation to the Planning Board for approval, that a two-step hearing before the Planning Board be completed on this application. Because the size of the redevelopment area and given the limited architectural information at the Concept Plan hearing, the initial part of the hearing would be done to address key issues such as mass and scale and connections. The second part of the hearing could be done to address other key issues such as design details and overall architecture. Staff is happy to discuss this with you further at your convenience.

Solar Access Elaine McLaughlin, 303-441-4130

1. The Solar Access Plan for 10 a.m. illustrates encroachments of shadows onto adjacent properties that are protected under the Solar Access standards of 9-9-17, B.R.C. 1981 found [here](#). Note that the encroachment shown in maroon below, onto the Industrial Services (IS) zoned property is within not within Solar Access Area II, rather it is in Solar Access Area III defined as follows, “where because of planned densities, topography or lot configuration or orientation, uniform solar access protection of south yards, and walls, or for rooftops may unduly restrict permissible development.” Therefore, the exemption will not be necessary for encroachment of shadows from Ciclo onto the property to the west and the diagram should be revised.
2. However, there does appear to be solar encroachments from both Ciclo and Market, across Valmont to the properties that are located within Solar Access Area I (the Mobile Home – MH zoning district) and Solar Access Area II (the Residential Medium – RM-1 zoning district). However, the Solar Shade Analysis sheets do not have any scale for reference and the analyses do not have complete information. Refer to the attached worksheets and resubmit with a corrected solar access analysis to confirm if there is or is not an actual encroachment onto these protected properties. Note that the analyses must indicate specific shadow lengths produced by specific points on each proposed building. Note that two separate Solar Access analyses should be completed, given the two different hypothetical Solar Fence heights of the Solar Access Area I and Solar Access Area II.



Signage Elaine McLaughlin, 303-441-4130

1. Signage must be approved through a separate sign permit, typically done prior to building permit. However, given that preliminary signage is shown on the project plans, note in particular with regard to the Market Signage/Light Ribbon and the powder coated steel sign fascia on Timber Lofts commercial, it must be consistent with the land use code section 9-9-21 B.R.C. 1981 found [here](#). In particular subsection (b)(3)(E) describes limitations on internally illuminated signs. Ensure consistency with this element and the sign code.
2. In the resubmittal provide greater detail on the proposed Signage/Light Ribbon: details, size, design intent. Also note within the sign code that there are limitations on digital advertising and internally lit lettering.

Site Design Elaine McLaughlin, 303-441-4130

Some of the street rights-of-way illustrate five foot attached walkways. This is inconsistent with the TVAP cross sections and must be revised to be consistent.

Utilities Scott Kuhna, 303-441-4071

1. Per Section 5.08(d) of the City of Boulder *Design and Construction Standards (DCS)*, all terminal mains shall have a fire hydrant at the terminus (dead end). Also, service taps along terminal mains shall not be located closer than 3 feet to the terminus nor located between the fire hydrant connection and the terminus. The following conditions need to be revised:
 - Dead end main north on 32nd Street – Services to Building 5
 - Dead end main north on 33rd Street – Services to Building 6 and to Meredith Park
 - Dead end main east on Bluff Street – Services to Ex. Brick Building
2. It appears that a wastewater main and manhole extend to the east of 34th Street between The Market and The Railyards buildings. This area is shown to have porous pavers. All of the proposed public utility mains (not services or laterals) must remain outside of any porous paver areas.
3. Per Section 6.06(l) of the *DCS*, where there exists a possibility that ground water may be diverted by the construction of new water or wastewater collection mains, ground water barriers shall be constructed within the collection main trench to prevent ground water migration or diversion along the water or wastewater main. Revise the plans and *Utility Report* as needed.
4. Trees need to be located at least 10 feet away from existing or future utilities. The following utility lines (or trees) were identified as not meeting separation requirements.
 - Proposed tree southeast of Building 2 – Proposed fire hydrant
 - Proposed tree west of Building 3 – Proposed electrical and gas lines and equipment
 - Proposed tree southwest of Building 10 – Existing fire hydrant
 - Proposed tree southeast of Building 10 – Proposed fire hydrant
 - Proposed trees (4) southwest of Bluff Street cul-de-sac – Existing and proposed wet and dry utilities
 - Proposed trees (3) southeast of Bluff Street cul-de-sac – Existing wastewater main
5. Per Section 5.08(C) of the *DCS*, three valves shall be installed at all cross-type connections. Changes to the water main cross at the intersection of 33rd Street and Meredith Street are required.
6. There are several locations where multiple fire hydrants are shown in close proximity to each other including:
 - Two (2) hydrants directly south of The CICLO building
 - Two (2) hydrants across the street from each other at 32nd Street and Bluff
 - Two (2) hydrants across the street from each other at 33rd Street and BluffClarification is necessary.
7. There appear to be conflicts between water line valves for the services to The Railyards and the storm sewer main in the street. Valves shall be connected directly to the swivel tee at all tee type connections. Revise accordingly.
8. Sheet C2.4 shows the existing wastewater main in Valmont Road to be a 12-inch main, where 21-inch applies. Revise accordingly.
9. The types and sizes of all existing water, wastewater, and storm sewer lines need to be included on the utility drawings.
10. Per Section 6.06(A) of the *DCS*, wastewater collection mains shall be designed to carry the peak flow with a flow depth of one-half (50%) of the full pipe. Table 5.3 in the *Utility Report* shows that the Bluff Street outfall has a “max. day flow rate/one-half full depth flow” of 52.5%. Clarification is necessary.
11. Per [Section 9-12-12\(a\)\(3\)](#), *B.R.C. 1981*, existing overhead utilities (telephone, electric, and cable television lines and other similar utility services) are to be placed underground, unless the subdivider demonstrates that the cost substantially outweighs the visual benefit from doing so.

Use Review: Brew Pub Elaine McLaughlin 303-441-4130

1. Note that a Brewery is not permitted within the MU-4 zoning district, but a Brew Pub/Restaurant is permitted and there is a distinction based on the definitions in the land use code section 9-16. A Brew Pub is defined as follows: “*Brewpub means an establishment that is primarily a restaurant where malt liquor is manufactured on the premises as an accessory use. A brewpub*”

may include some off-site distribution of its malt liquor consistent with state law.” Therefore, provide a detailed description of the operating characteristics of the Brew Pub/Restaurant in a Management Plan and remove the label, “Brewery” from the floor plan on Sheet 47.

2. A management plan for operation of the Brew Pub is required per the Land use Code section 9-6-1, B.R.C. 1981 as the written statement for the Site Review isn't consistent with the standards that apply to a management plan. Therefore, prior to the Good Neighbor Meeting (see Neighborhood Comments section above), the applicant should prepare a management plan consistent with the land use code section 9-2-4(c):

“Elements of a Management Plan: The management plan shall contain the following components that describe the business operation and address the mitigation of potential adverse impacts the facility may have on the surrounding neighborhood, to the extent necessary, including, without limitation:

- (A) A (brief) description of the food service offered;
- (B) Hours of operation;
- (C) Client and visitor arrival and departure times;
- (D) Coordinated times for deliveries and trash collection;
- (E) A description of the type of entertainment provided;
- (F) Size, location, and number of electronic amplifiers;
- (G) Techniques and strategies to mitigate noise impacts;
- (H) A description of how the applicant will prevent littering and maintain an orderly appearance of the premises and any adjacent right of way;
- (I) A security plan describing security features, including, without limitation, personnel and equipment;
- (J) The facility's drug and alcohol policy;
- (K) Strategies to avoid loitering;
- (L) Employee education;
- (M) The facility's responsibilities as good neighbors;
- (N) Neighborhood outreach and methods for future communication; and
- (O) Dispute resolution strategies for any conflicts with the surrounding neighborhood.”

3. On sheet 47, Level 1 and Level 1 Mezzanine: a number of floor plan elements are not labeled, to provide greater clarity please label all of the elements shown on the plan. Note that a separate floor plan illustrating specific tenant finish of the brew pub must be submitted for the Use Review including clear delineation of the kitchen space and the number of seats planned.
4. The applicant must host a Good Neighbor Meeting as noted above under “Neighborhood Comments.”
5. On the project plans, there is a stage/plinth shown. While greater detail on the design of the stage must be provided (see Plan Document comments above) the operating characteristics of the stage must also be delineated. Indicate if the stage is associated with the Brew Pub? If not, indicate if it is intended as a separate use? While the Land Use Code does not permit “*Outdoor Entertainment*” in the MU-4 zoning district, “*Temporary Outdoor Entertainment*” is conditionally permitted. The standards for Temporary Outdoor Entertainment are found [here](#). Note that the uses are “*limited to two consecutive weeks in any three-month period, unless otherwise approved by the city manager.*” Additional information is needed on the operating characteristics to determine the most appropriate course for permitting the use and a management plan for the use along with presentation to and input from neighbors must be solicited through the Good Neighbor Meeting. If performances are intended to be scheduled on a regular basis, there may be a means to secure a Use Review that permits amplified music under specific approved conditions of a management plan. However, greater information about this space must be provided for staff to guide the applicant in how to proceed with review and approval of the stage

Use Review: Micro Restaurants

1. The micro restaurants appear to have a shared outdoor dining area. The land use code requires that restaurants with an outdoor seating area of 300 square feet or more within 500 feet of a residential zoning district apply for a Use Review. Therefore, either upon resubmittal of revisions in response to these comments or prior to Technical Document Review application, a Use Review Application must be submitted for the micro restaurants given the shared outdoor seating. As a part of the application, a Management Plan, as described above must be included.
2. Detailed plans that indicate number of indoor and outdoor seats must also be provided with the application for Use Review.

Use Review: Other Restaurants - Railyard

1. The application references other restaurant spaces within the Railyard building. However, there is no application for a Use Review for this other restaurant. The restaurant appears to be larger than 1,500 square feet and would necessitate a Use Review that includes a management plan (described above) and discussion of the operating characteristics with the neighbors in a Good Neighbor Meeting. The project plans do not appear to illustrate any outdoor seating, please clarify if that is the intent to complete as a part of the site and use review applications.

III. INFORMATIONAL COMMENTS

This section addresses issues that are for the applicant's reference but are not required to be resolved prior to a project decision or as a condition of approval. Informational Comments are organized by topic area so that each department's comments of a similar topic are grouped together. Each reviewer's comment will be followed by the reviewer's department or agency and telephone number. Reviewers are asked to submit comments by section and topic area so that the comments can be more efficiently organized into one document. Topics are listed here alphabetically for reference in the template.

Access/Circulation

Please ensure building door swings do not encroach into the City right-of-way or dedicated public easements.

Drainage Scott Kuhna, 303-441-4071

1. A Final Storm Water Report and Plan will be required as part of the Technical Document Review process. All plans and reports shall be prepared in accordance with the City of Boulder *Design and Construction Standards*.
2. All inlet grates in proposed streets, alleys, parking lot travel lanes, bike paths, or sidewalks shall utilize a safety grate approved for bicycle traffic.
3. A construction stormwater discharge permit is required from the State of Colorado for projects disturbing greater than 1-acre. The applicant is advised to contact the Colorado Department of Public Health and Environment.

Inclusionary Housing Beth Roberts 303 -441-1828

1. Each new residential unit developed on the property is subject to the Land Use Code section 9-13 B.R.C., 1981, "Inclusionary Housing." The general Inclusionary Housing (IH) requirement is that all residential developments must dedicate 20 percent of the total dwelling units as permanently affordable housing. For rental projects this requirement may be met through the provision of on-site affordable rental units or comparable existing or newly built off-site permanently affordable rental units or through the dedication of land appropriate for affordable housing or by payment of a cash-in-lieu contribution. For for-sale housing this requirement may be met through the provision of at least half of the required affordable units on-site. The other half of the requirement may be met by providing comparable existing or newly built permanently affordable units off-site, the dedication of land appropriate for affordable housing or by payment of a cash-in-lieu contribution.
2. Per the Land Use Code section 9-13 B.R.C., 1981, and associated regulations, permanently affordable dwelling units must be:
 - Proportionate in type (such as detached, attached or stacked units) and number of bedrooms to the market rate units;
 - Proportionate in tenure (for-sale and rental);
 - Have an average floor area no less than 80 percent of the market-rate units; and
 - Meet the "Livability Standards for Permanently Affordable Housing."
3. Included in the submittal are plans for permanently affordable units at 3155 Bluff, proposed to be funded by the city. This development is not included in the following analysis.
4. Rental Units (Including proposed affordable) - Applicant is proposing 163 rental units resulting in an IH requirement of 32.6 affordable rental units. Applicant is proposing to provide 32 permanently affordable units on-site, in the building noted as Ciclo, on page 52 of the plan set dated February 2, 2015. The proposed mix of affordable units includes 6 fewer studio units, 4 additional 1 bedroom units, and an equivalent number of 2 and 3 bedroom units, and 4,012 additional square feet than required. Staff finds this mix is equivalent to the IH requirement and acceptable.
5. For-Sale Units - Applicant is proposing 39 for-sale units resulting in an IH requirement of 7.8 permanently affordable for-sale units.
 - a. Please indicate how this requirement will be met.

6. Livability Standards - The floor plans for the third floor units were not included in the plan set, therefore the submittal does not include sufficient information to fully assess if the proposed affordable units are equivalent, exceed or do not meet the Livability Standards.
 - a. The unit data spread sheet information submitted contains inconsistencies showing studio units with two bathrooms. Please confirm or submit a corrected Affordable Housing Unit Data Spread Sheet consistent with your site plan in an unlocked excel spreadsheet format to robertsb@bouldercolorado.gov.
 - b. No plans were included for the third floor of the Ciclo. Please submit scalable floor plans delineating rooms and closets, specific unit identification (number or letter/bldg), linear feet of proposed cabinetry, bathroom and kitchen fixtures and appliances.
7. Applicant proposes that the affordable units be made available to low income artists. Further discussions are needed to determine if this is a desirable outcome for the Inclusionary Housing program and if so, what that would entail to meet any legal and fair housing requirements.
8. Indicate if and which specific amenities the residents of Ciclo will have access to in the market rate rental projects.
9. Affordable rental units must be owned all or in part by a Housing Authority or similar agency or may be owned and operated by a private entity if the owner voluntarily proposes to serve the housing needs of low income residents of Boulder by exceeding the inclusionary requirement in return for city compensation.
10. Any required documents including the Determination of Inclusionary Housing Compliance form, Covenants to secure the permanent affordability of the units, and an Agreement must be signed and recorded prior to application for any residential building permit. On or off-site permanently affordable units must be marketed and constructed concurrently with the market-rate units.
11. Additional information about the Inclusionary Housing program including the "Affordable Housing Unit Data Spread Sheet", "Livability Standards for Permanently Affordable Housing" may be found on-line at www.boulderaffordablehomes.com

Miscellaneous Scott Kuhna, 303-441-4071

1. The applicant is notified that any groundwater discharge to the storm sewer system will require both a state permit and a city agreement. The steps for obtaining the proper approvals are as follows:
 - Step 1 -- Identify applicable Colorado Discharge Permit System requirements for the site.
 - Step 2 -- Determine any history of site contamination (underground storage tanks, groundwater contamination, industrial activities, landfills, etc.) If there is contamination on the site or in the groundwater, water quality monitoring is required.
 - Step 3 -- Submit a written request to the city to use the municipal separate storm sewer system (MS4). This submittal should include a copy of the Colorado Department of Public Health and Environment (CDPHE) permit application. The written request should include the location, description of the discharge, and brief discussion of all discharge options (e.g., discharge to MS4, groundwater infiltration, off-site disposal, etc.) The request should be addressed to: City of Boulder, Stormwater Quality, 4049 75th St, Boulder, CO 80301 Fax: 303-413-7364
 - Step 4 -- The city's Stormwater Quality Office will respond with a DRAFT agreement, which will need to be submitted with the CDPHE permit application. CDPHE will not finalize the discharge permit without permission from the city to use the MS4.
 - Step 5 -- Submit a copy of the final discharge permit issued by CDPHE back to the City's Stormwater Quality Office so that the MS4 agreement can be finalized.

For further information regarding stormwater quality within the City of Boulder contact the City's Stormwater Quality Office at 303-413-7350. All applicable permits must be in place prior to building permit application.
2. No portion of any structure, including footings and eaves, may encroach into any public right-of-way or easement.

Parking (Molly Winter Director, Downtown and University Hill Management Division/Parking Services, (303) 413-7317)

As manager of the Boulder Junction Access Districts – Parking and Travel Demand Management, I am responding to the site review submittal for the S'Park project which is located in Boulder Junction regarding potential parking management strategies for the project. Staff has had initial discussions with Element Properties representative, Scott Holton. The project is totally within the TDM District and thus will be included in the district benefits of Eco Passes for residents and employees, and support for car and bike share

memberships. The eastern portion of the project is within the Parking District. From the earliest discussions about the Boulder Junction Access District, the goal was to have a shared private/district parking facilities in both the northern (Sutherlands) and southern (Depot Square) sections of the district to be managed by the district in order meet the SUMP parking principles (shared, unbundled, managed and paid) and to provide for district parking demand. The following are parking management strategies to be pursued by staff:

1. Surface Parking Lot on the South Finger Future Phase Area: Since this parcel is planned for future use, the temporary use is proposed as a surface parking lot. The district would propose an arrangement with the developer that the district manage the parking in the lot for an interim period.
2. Residential area on-street parking management: In order to manage the on-street parking in the residential only areas (32nd Street, and portions of Meredith and Junction Streets), staff would propose a parking management program, possibly the existing Neighborhood Parking Permit (NPP) program, to insure neighborhood access and livability. NPP's are designed to give priority to residents but also allow for short term parkers and commuters.
3. Public/private Partnership for Parking as part of the Timber Lofts: Staff would recommend further discussions with the developers about options for Boulder Junction Access District's participation in the underground parking proposed as part of Timber Lofts. As mentioned above, the long term plan for the Parking District was to partner with the developer of the Sutherlands site on a shared parking approach. The other initial location for District parking was in the Depot Square area which resulted in public/private partnership between RTD, the hotel, the housing, the Depot and the Boulder Junction Access District which resulted in unbundled, shared parking that satisfies the needs of the users while maximizing efficiency of an expensive resource – structured parking. This would be the model for discussions with Element Properties for the Timber Lofts location.

In addition, staff will be working with the developer on locations for bike parking, B-cycle and car share locations within the project. I look forward to further discussions regarding this project to explore mutually beneficial partnerships and strategies.

Residential Growth Management System, Sloane Walbert, 303-441-4231

Please be advised that you must apply for and obtain growth management allocations before you may submit for a building permit for any residential unit. An agreement for meeting city affordable housing requirements must be in place before an allocation may be issued.

Comprehensive Planning, Jeff Hirt, 303-441-4497

- Future Land Use: The BVCP future land use category is Mixed Use Business and the TVAP future land use category is Mixed Use 2. The overall land use concept is consistent with the descriptions for both categories, and please note the MU-2 category calls for an FAR of 1.5-2. The main exception appears to be the FLEX building that involves some industrial. Both BVCP and TVAP policies support protection and provision of service industrial uses, so long range planning staff supports inclusion of this land use in the development, provided the spaces and uses are designed to be compatible with the surrounding area as it builds out. Specifically, this land use provides more opportunities for people to live in work in close proximity.
- Adaptability of Ground Floor Spaces; Retail, Restaurant, Office Uses: Long range planning staff also suggests a coordinated analysis of retail and office proposed square footages between this development, and pending, surrounding developments to ensure that the appropriate amounts of both are developing according to the TVAP economic analysis (Economic and Planning Systems study). In general, the concept of building flexible spaces, particularly on the ground floor of mixed use buildings is supported so as market conditions change, the spaces can be adapted over time (e.g., designing spaces that may not be utilized for retail now, but could be converted later with appropriate sizes, floor to ceiling height, accessibility, etc.). In particular, the ground floor uses along Junction Place should incorporate this design, and TVAP emphasizes the importance of active ground floor uses along Junction Plan (TVAP page 20).
- Connections: The TVAP connections plan requires a north-south multiuse path on the west side of the railroad tracks connecting Valmont to the future rail platform, and continuing south along the same alignment connecting to future development to the south and ultimately the Goose Creek path. This should align with the same required connection to the south, as that development proceeds.
- Public Art: The TVAP plan calls for public art at multiple areas on the subject site. First, the rail plaza (TVAP page 55, Public Art plan), second, along Junction Place (Public Art Plan page 83 – Junction Place should have a strong urban character), and third, along the railroad corridor (Public Art Plan page 85).

Utilities Scott Kuhna, 303-441-4071

1. The applicant is advised that any proposed street trees along the property frontage may conflict with existing utilities, including without limitation: gas, electric, and telecommunications, within and adjacent to the development site. It is the applicant's responsibility to resolve such conflicts with appropriate methods conforming to the Boulder Revised Code 1981, the City of Boulder Design and Construction Standards, and any private/franchise utility specifications.
2. Final utility construction drawings will be required as part of the Technical Document Review process (which must be completed prior to building permit application).
3. Further detail of the ground water barriers used to prevent ground water migration or diversion along the water, wastewater, and storm sewer mains will be required at time of Technical Document Review.
4. Maintenance of sand/oil interceptors and all private wastewater and storm sewer lines and structures shall remain the responsibility of the owner.
5. The landscape irrigation systems requires a separate water services and meters. A separate water Plant Investment Fee must be paid at time of building permit. Service, meter and tap sizes will be required at time of building permit submittal.
6. The applicant is advised that at the time of building permit application the following requirements will apply:
 - a. The applicant will be required to provide accurate proposed plumbing fixture count forms to determine if the proposed meters and services are adequate for the proposed use.
 - b. Water and wastewater Plant Investment Fees and service line sizing will be evaluated.
 - c. If the buildings will be sprinklered, the approved fire line plans must accompany the fire sprinkler service line connection permit application.
7. All water meters are to be placed in city right-of-way or a public utility easement, but meters are not to be placed in driveways, sidewalks or behind fences.
8. Trees proposed to be planted shall be located at least 10 feet away from existing or future utility mains and services.

IV. NEXT STEPS

1. A review of comments with the applicant and staff teams is scheduled for Thursday March 5 from 3 to 4:30 p.m. Following the group discussion, separate meetings with respective disciplines may be necessary.
2. The applicant must submit an application for review of the buildings with the Boulder Design Advisory Board (BDAB) for all of the buildings prior to resubmittal of the revisions.
3. Staff then recommends a two-step Site Review discussion with the Planning Board. Because the buildings in Concept Plan were generalized massing diagrams and because the project is one of the largest sites that the city has reviewed in recent years, the review process would benefit from a two-part hearing.

V. CITY CODE CRITERIA CHECKLIST

A review with the Site Review criteria will be completed upon resubmittal of revisions that require additional information to make conclusions with regard to the criteria. Staff has prepared a preliminary analysis with the TVAP design guidelines. Refer to Attachment A.

VI. Conditions on Case

To be provided at final comments.

PRELIMINARY CONSISTENCY ANALYSIS OF PROPOSED PROJECT WITH CRITERIA FOR REVIEW- SITE REVIEW

No site review application shall be approved unless the approving agency finds that:

(1) Boulder Valley Comprehensive Plan:

√ (A) The proposed site plan is consistent with the purposes and policies of the Boulder Valley Comprehensive Plan.

Inherent within the BVCP are policies based upon the three interrelated components of community sustainability: economic, social, environmental sustainability. The BVCP also encourage a compact form of development and promotes higher density development along multi-modal corridors. Policies within the BVCP also aim to mitigate the increasingly significant in-commuting trend due to the current jobs-to-housing imbalance by requiring development projects to provide a variety of housing types and levels of affordability. The proposed redevelopment with the mix of uses and public spaces fulfills a number of the BVCP policies including:

- | | | | |
|------|----------------------------|------|--|
| 2.01 | Unique Community Identity | 2.37 | Enhanced Design for Private Sector Projects |
| 2.03 | Compact Land Use Pattern | 5.02 | Regional Job Center |
| 2.16 | Range of Land Uses | 5.05 | Support for Local Business. |
| 2.32 | Physical Design for People | 7.06 | Encourage a range and variety of housing types |

TVAP. The site is located within Boulder Junction which was established through the Transit Village Area Plan (TVAP). The TVAP is intended to be a more focused area plan that provides guidance to implement the goals and policies within the BVCP. Adopted in the spring of 2007, TVAP envisions the redevelopment of a defined 160 acre area. As noted in TVAP the Transit Village is envisioned to be,

“A vibrant, sustainable center in Boulder. Designed in partnership with the community, the Transit Village Area Plan will be a catalyst for a 25-year revitalization and redevelopment with a new transit center, new neighborhoods, improved business and industrial districts, transportation improvements, and public spaces.”

TVAP CHARACTER DISTRICTS. There are eight “character districts” within TVAP, as shown in Figure 1, primarily based on future land use and to promote a particular urban design character for each area.

The S’PARK site is primarily encompassed within the “Rail Plaza District” and the “Steelyards District.”

As noted on page 23 of TVAP, the Rail Plaza District will ultimately, “host the Boulder stop on the new commuter rail service to Denver and Longmont.” The intent of the district is further defined, “The district will evolve into a high-density, commercial and residential mixed use area, with three- to five-story buildings.”

The Steelyards District south of Bluff Street from the project site was mostly developed in recent years as a mixture of housing, shops and small-scale service businesses. The TVAP defines the area encompassed within the project site on page 28 as follows: “The industrial uses on the north side of Bluff Street will transition to high-density residential, such as urban townhouses.” There are a number of guidelines associated with the character districts as well as the General Design Guidelines.

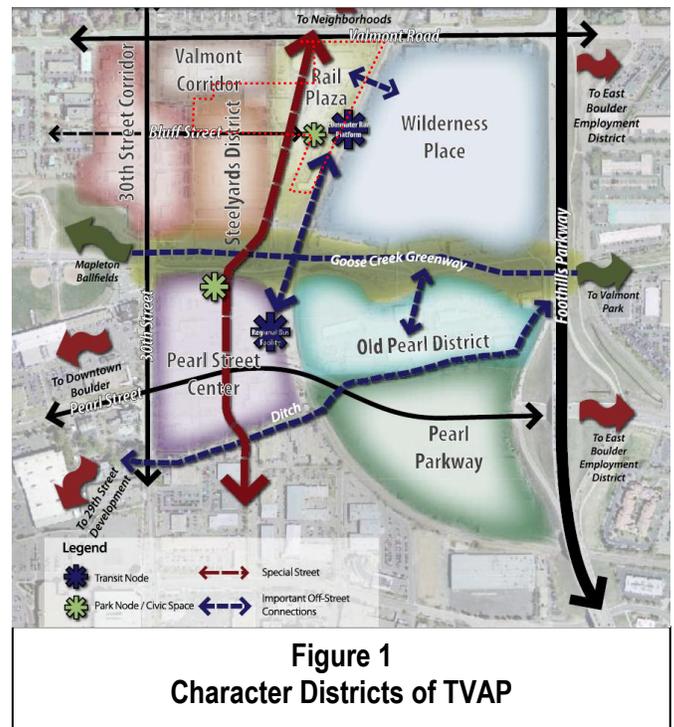


Figure 1
Character Districts of TVAP

TVAP MU2 LAND USE. The land use for the area encompassed on the east side of the project site is defined in TVAP as “MU2” or “Mixed Use -2” Land Use prototypes are provided in TVAP to show the building forms and uses typically associated with each land use category. The MU2 land use prototype is shown in Figure 2.

As currently proposed, the building forms, uses, density and massing of the various buildings within the proposed project appear to be consistent with these land use prototypes. The buildings proposed within the MU2 area: Maarket; Cyclo; Railyards; Meredith House, and Timber Lofts are proposed to be three to five stories, consistent with the MU2 land use and as shown in Figures 3a, b, c, d and e.



Three- to four-story mixed-use buildings. Predominant use may be business or residential. Mostly structured or first-floor parking; may have some surface parking.

Figure 2: Excerpt from TVAP: Intent of MU2 land use



Maarket



Railyards



Cyclo



Timber Lofts

Figures 3a thru e:

Illustrating the massing of the proposed five buildings within the MU2 Land Use area of the proposed project.



Meredith House

The area encompassed on the west side of the project site is within TVAP Land Use “HDR-1” or “High Density Residential – 1”. The HDR-1 land use prototype is shown in Figure 4.

The SPARK_west project located within the HDR-1 land use area of TVAP consists of two and three story condominiums as shown below that appear to meet the intent of the massing and scale of the and use as shown below in Figure 5 a thru c.

High-Density Residential -1-	15-24 Dwelling Units per Acre
	
Urban townhomes and garden apartments with individual garages, surface parking lots, or underground parking. Mainly two to three stories.	
Figure 4: Excerpt from TVAP: Intent of HDR1 land use	



The goals adopted as part of TVAP are included below, with a description of how the proposed site meets these goals. The TVAP also outlines how that desired future will be achieved.

1. **Create a well-used and well-loved pedestrian-oriented place of enduring value that serves all of Boulder:**
The proposed project establishes a area built around pedestrian oriented streets including a ‘woonerf’ where the cars are permitted as “guests” in a shared street. The streets coupled with the planned mix of uses including a variety of residential uses; office; retail; restaurants and a brew pub will help to establish a lively alchemy that is necessary to generate the pedestrian activity.
2. **Support diversity through land use and travel options that expand opportunities for employees and residents of differing incomes, ethnicities, ages and abilities:**
The proposed project will provide permanently affordable residential units as well as market rate apartments and condominiums. This mix will help to meet diverse residential housing needs of the city in a context with the nearby RTD bus station that support residents, adjacent neighbors and employers; as well as, provides accessible public spaces to that lay the foundation for use by all community members.
3. **Enhance economic vitality: Increase economic activity for businesses, increase revenues for the city of Boulder, reduce transportation costs and expand travel options for residents and employees.**
The proposed project provides a new urban neighborhood with close proximity to office and industrial, Depot Square, Steelyards, Twenty Ninth Street shopping and entertainment area, along with Whole Foods and Barnes and Noble. This context will help to further support economic activity and reduce transportation costs for the area.
4. **Connect to the natural and built environment: Create a place that reflects Boulder’s commitment to environmental sustainability and “green” development is integrated with the natural features in the area and connects to the larger city fabric.**
While there is little in the way of the “natural environment” on this developed and paved site, there are some existing long lived trees that should be integrated into the plans when possible.
5. **Maximize the community benefit of the transit investment: Locate homes and employment to maximize access to local and regional bus service, future commuter rail and bus rapid transit, and to allow for a pedestrian-oriented lifestyle.**
The project plans meet this goal.
6. **Create a plan that will adapt to and be resilient for Boulder’s long term future. Building in flexibility and allowing for serendipity and changes in use over time and providing for increased density in targeted locations.**
There are opportunity sites within and adjacent to the proposed project that can help to meet this guideline.

General Guidelines The following guidelines apply to all character districts.	MAARKET	CYCLO	RAILYARDS	MEREDITH HOUSE	TIMBER LOFTS/COMMONS	S'PARK_west
Building Placement and Design						
Orient the main facade to the street and provide an entrance on the street side of the building.	Yes, the building has entrances along both 34 th and Meredith Streets.	Yes, the building has entrances along both 34 th and Meredith Streets.	Yes, the building has entrances along the “woonerf” shared street.	Yes, the building has the main entrances on Meredith Street	Partially. However, there are separations from the entrances and windows to the pedestrian that should be reconsidered. See comments under Plan Documents.	Yes, for the most part this guideline is met.
Design buildings with pedestrian-scale materials and architectural articulation, particularly on the first floor. Avoid large blank walls. Along streets and sidewalks provide pedestrian interest, including transparent windows and well-defined building entrances.	Additional information needs to be provided to understand if this guideline is met.	Yes. The building appears to have pedestrian scale materials and transparency on the ground floor, as well as well-defined building entrances.	Partially. The “woonerf” side of the building appears to meet this guidelines. However, along the multi-use path that is planned to align the tracks, there is an approximately 300 foot blank wall. Staff recommends greater transparency and access from this side of the building.	Partially. The building appears to have pedestrian scale materials and transparency on the ground floor, as well as well-defined building entrances. There are areas that contain broad blank walls. Including the CMU wall that is adjacent to the front entry, and the walls that “hide” the storage units.	Partially. There are areas that have large blank walls and/or entry porches and windows that rise above a typical eye level. These areas of the building should be revised.	Yes, the buildings primarily meet this guideline.
Consider opportunities to frame or preserve views of the Flatirons to the southwest.	Additional information needs to be provided to understand if this guideline is met. It appears that there may be opportunities to better configure the building at the intersection of 34 th and Valmont to provide a more open view toward the Flatirons which exists today.	Additional information needs to be provided to understand if this guideline is met. It appears that there may be opportunities to better configure the building at the intersection of 34 th and Valmont to provide a more open view toward the Flatirons.	Not applicable. The building is relatively long and located adjacent to the railroad tracks. There is no intersection in this location that could be studied for potential view corridors through building design.	Not applicable. It’s a relatively small buildable site and the building is located on the north side of Meredith/Junction Place and backs to the rear of the properties along Valmont. There is no opportunity in this location to frame views from a public corridor.	The configuration of the southeast corner of the building should be evaluated in concert with the Ciclo and Market buildings for potential ways to frame views. Perhaps through greater viewshed analysis the broad views of the Flatirons could be captured through chamfering the corners of the buildings.	There is little in the way of opportunities to capture views given the existing development on the Steelyards. However, this should be studied in greater detail to see if the possibility exists for building configurations adjacent to internal open space to frame views.
Useable Open Space						
Incorporate well-designed, functional open spaces with tree, quality landscaping and art, access to sunlight and places to sit comfortably. Where public parks or open spaces are not within close proximity, provide shared open spaces for a variety of activities. Where close to parks, open spaces provided by development may be smaller.	Additional information must be provided to understand if this guideline is met. There are labels missing within the open space plaza area and outdoor seating which need to be clarified.	No, as currently proposed, the site does not appear to meet the required 20 percent open space, and the FAR exceeds the maximum of 2.0.	It is unclear if the Woonerf is intended as useable open space. Currently, the land use code is silent on a Woonerf and therefore any area of the travel lane would not fit the definition of “useable open space.”	No, as currently proposed, there doesn’t appear to be adequate useable open space on the site or to meet the required 20 percent of useable open space. The park that is shown to the west of the site appears to be intended as a community park, rather than meeting open space. As stated in the guideline, “where close to parks, open spaces provided by development may be smaller.”	Additional information must be provided to understand if this guideline is met. There is no landscape plan provided for the site.	The project appears to meet this guideline. One exception to this is the narrow rights of way on Meredith and 32 nd streets. To count toward a percentage of useable open space the rights of way must meet the guidelines and Design and Construction Standards.

Permeability	MAARKET	CYCLO	RAILYARDS	MEREDITH HOUSE	TIMBER LOFTS/COMMONS	S'PARK_west
While the improved street network will provide more frequent pedestrian connections, also provide multiple opportunities to walk from the street into projects, thus presenting a street face that is permeable. Also provide opportunities to walk within the interior between abutting properties.	<i>Partially. The streetside building elevation appears permeable with multiple opportunities to walk from the street into projects. However, given that the multi-use path would be a public right of way staff also recommends providing greater permeability along the multi use path adjacent to the railroad track. The building is approximately 210 linear feet of ground floor building (equivalent to one city block) with no access into the building only egress doors.</i>	<i>Yes, meets this guideline.</i>	<i>Partially. Like the Maarket project, the streetside building elevation appears permeable with multiple opportunities to walk from the street into projects. However, given that the multi-use path would be a public right of way staff also recommends providing greater permeability along the multi use path adjacent to the railroad track. The building is approximately 300 linear feet on the ground floor (equivalent to one city block) with no access into the building only egress doors.</i>	<i>Yes, meets this guideline</i>	<i>Partially meets this guideline. However, as noted in the comments under "Plan Documents" while there are entries to units facing the street, staircase access is turned 90 degrees such that the element facing the walkway is a fairly tall wall staircase wall. And while the café spaces could engage the pedestrian, there are also a number of areas along all of the elevations that create "pedestrian dead zones" where there's little in the way of either transparency or interest for the pedestrian. Below are just of few of the areas that do not meet the TVAP guidelines or the Site Review Criteria for human and pedestrian scale design. Please ensure that the buildings have pedestrian interest, if not through transparencies then through building details.</i>	<i>Yes, the project meets this guideline.</i>
Parking Structures	MAARKET	CYCLO	RAILYARDS	MEREDITH HOUSE	TIMBER LOFTS/COMMONS	S'PARK_west
Design the ground level of a parking structure to be interesting and appealing for pedestrians, for example, by wrapping the ground level with active uses, such as retail. Include pedestrian-scale façade articulation, architectural detailing and quality materials.	<i>Not applicable.</i>	<i>Partially. However, as noted in the comments under "Plan Documents" the uses on the north and east aren't labeled. It's therefore not clear if it meets this guideline.</i>	<i>It is not clear from the project plans where the 30 standard and 29 compact parking spaces are located.</i>	<i>Doesn't yet meet this guideline, refer to comments under "Plan Documents"</i>	<i>Partially. However, there are areas of the building that neither have active uses nor address the street, with doors and windows extending above a pedestrian sight line. Refer to comments under "Plan Documents."</i>	<i>Yes. Meets this guideline.</i>
Where the ground level is open or exposed to interior drives, paths, or parking lots, screen it with a low wall and/or evergreen landscaping.	<i>Not applicable.</i>	<i>Not applicable.</i>	<i>Not applicable</i>	<i>Not applicable</i>	<i>Not applicable</i>	<i>Yes. Meets this guideline.</i>
If tuck-under parking or podium parking (half-level underground) is provided, locate it at the rear of the property or wrap with active uses if feasible.	<i>Not applicable.</i>	<i>Partially. However, as noted in the comments under "Plan Documents" the uses on the north and east aren't labeled. It's therefore not clear if it meets this guideline.</i>	<i>Not applicable.</i>	<i>Doesn't yet meet this guideline, refer to comments under "Plan Documents"</i>	<i>Partially. However, there are areas of the building that neither have active uses nor address the street, with doors and windows extending above a pedestrian sight line. Refer to comments under "Plan Documents."</i>	<i>Not applicable.</i>
Where feasible, locate structure entries/exits on secondary, not primary streets. Avoid locating entries/exits on main pedestrian routes. Entries/exits should be carefully designed to ensure safe, comfortable, and uninterrupted pedestrian flow on adjacent sidewalks.	<i>Not applicable.</i>	<i>Additional information is required prior to understanding if this guideline is met. Refer to comments under "Plan Documents."</i>	<i>Not applicable.</i>	<i>The parking is located behind the structure, accessed off of an entry drive. However, the access to the fire sprinkler system with a metal door on the blank CMU wall is located along the main pedestrian route of Meredith/Junction Place.</i>	<i>No, doesn't meet this guideline, the entry to the parking structure is located near the key intersection of Junction Place and the woonerf. The entry should be located in a less conspicuous area away from a key pedestrian location.</i>	<i>Yes, meets this guideline.</i>

Bus Stops	MAARKET	CYCLO	RAILYARDS	MEREDITH HOUSE	TIMBER LOFTS/COMMONS	S'PARK_west
Include the following for bus stops adjacent to development projects: a shelter, benches, route and schedule signage. Additional enhancements are encouraged, such as pedestrian lighting, art, landscaping, and waste receptacles. Bike racks should be provided at regional route stops.	<i>Currently no information on these elements, as project plans progress greater detail will be required.</i>	<i>Currently no information on these elements, as project plans progress greater detail will be required.</i>	<i>Currently no information on these elements, as project plans progress greater detail will be required.</i>	<i>Currently no information on these elements, as project plans progress greater detail will be required.</i>	<i>Currently no information on these elements, as project plans progress greater detail will be required.</i>	<i>Currently no information on these elements, as project plans progress greater detail will be required.</i>
Junction Place	MAARKET	CYCLO	RAILYARDS	MEREDITH HOUSE	TIMBER LOFTS/COMMONS	S'PARK_west
In addition to the street trees, sidewalks and bike facilities specified by the Junction Place streetscape section, provide seating, planters, art, special pavement and lighting along Junction Place. (See the Implementation Plan for information on funding of the city share.)	<i>Additional information is required to understand if this guideline has been met.</i>	<i>Additional information is required to understand if this guideline has been met.</i>	<i>Additional information is required to understand if this guideline has been met.</i>	<i>Additional information is required to understand if this guideline has been met.</i>	<i>Additional information is required to understand if this guideline has been met.</i>	<i>Additional information is required to understand if this guideline has been met.</i>
Where feasible, place active uses, such as retail or commercial services on the first floor of buildings along Junction Place.	<i>Yes. The ground floor appears to be planned with active uses.</i>	<i>Yes. The ground floor appears to be planned with active uses.</i>	<i>Yes. The ground floor appears to be planned with active uses.</i>	<i>Partially. The westernmost portion of the ground floor has a community room. However, the center of the building has storage units located adjacent to Junction Place/Meredith.</i>	<i>Partially. There is a café located at the corner of Meredith/Junction Place and the woonerf, and there is a leasing office located on the southwest corner of the building. However, there is little in the way of activity generated from the leasing office as the windows are set above eye level. The applicant should consider additional active uses along Junction Place.</i>	<i>Not applicable</i>
Provide way-finding features such as special pavements, signs, or art, to facilitate pedestrian movement between Junction Place, Rail Plaza, the rail platform and under/overpass, the bus station, Goose Creek Greenway, Pearl, Valmont, 30th Street and Wilderness Place. (See the Implementation Plan for funding information.)	<i>Currently no information on these elements, as project plans progress greater detail will be required.</i>	<i>Currently no information on these elements, as project plans progress greater detail will be required.</i>	<i>Currently no information on these elements, as project plans progress greater detail will be required.</i>	<i>Currently no information on these elements, as project plans progress greater detail will be required.</i>	<i>Currently no information on these elements, as project plans progress greater detail will be required.</i>	<i>Currently no information on these elements, as project plans progress greater detail will be required.</i>
Mixed-Use Buildings	MAARKET	CYCLO	RAILYARDS	MEREDITH HOUSE	TIMBER LOFTS/COMMONS	S'PARK_west
The potential for conflicts between residential and non-residential uses within mixed-use buildings should be minimized through careful design and building system planning. Consider the compatibility of specific uses. Issues could include noise, vibration, privacy, and entrance locations.	<i>Currently no information on these elements, as project plans progress greater detail will be required.</i>	<i>Currently no information on these elements, as project plans progress greater detail will be required.</i>	<i>Currently no information on these elements, as project plans progress greater detail will be required.</i>	<i>Not applicable, not mixed use.</i>	<i>Currently no information on these elements, as project plans progress greater detail will be required.</i>	<i>Not applicable, not mixed use.</i>

Rail Plaza District Guidelines:	MAARKET	CYCLO	RAILYARDS	MEREDITH HOUSE	TIMBER LOFTS/COMMONS	S'PARK_west
Locate buildings along the street with parking behind.	<i>Not applicable, no on-site parking</i>	<i>Yes, meets this guideline</i>	<i>Not applicable, no on-site parking</i>	<i>Yes, meets this guideline</i>	<i>Yes, meets this guideline</i>	<i>Yes, meets this guideline</i>
Place active uses on the ground level of buildings adjacent to Rail Plaza, for example, stores, restaurants, cafes, or commercial services, where feasible. They should have entrances directly onto the plaza.	<i>Not applicable, not located near plaza</i>	<i>Not applicable, not located near plaza</i>	<i>Yes, there is a restaurant planned facing the plaza</i>	<i>Not applicable, not located near plaza</i>	<i>Yes, there are retail spaces located adjacent to the woonerf and future rail plaza.</i>	<i>Not applicable, not located near plaza</i>
Orient buildings to Junction Place (see Junction Place guidelines), as well as to the tracks. If feasible, place active uses on the first floor. Consider making the track-side frontage a car-free zone with pedestrian amenities.	<i>The building is oriented to Junction Place. However, there are no pedestrian amenities along the track-side frontage. Refer to comments under "Plan Documents."</i>	<i>Building is oriented to Junction Place</i>	<i>The building is oriented to Junction Place. However, there are no pedestrian amenities along the track-side frontage. Refer to comments under "Plan Documents."</i>	<i>Building is oriented to Junction Place</i>	<i>Building is oriented to Junction Place</i>	<i>Yes, where applicable buildings are oriented to Junction Place</i>
Civic Plaza Guidelines:	MAARKET	CYCLO	RAILYARDS	MEREDITH HOUSE	TIMBER LOFTS/COMMONS	S'PARK_west
Design the plaza to be approximately a third of an acre. Err on the side of smaller rather than larger.	<i>Not applicable</i>	<i>Not applicable</i>	<i>The area set aside for the plaza is too undersized. The south portion of the building should be moved back to accommodate a greater amount of area. As currently shown, deducting for the round-about, there's approximately 3,000 square feet of area where there should be approximately 15,000 square feet.</i>	<i>Not applicable</i>	<i>The area set aside for the plaza is too undersized. The south portion of the building should be moved back to accommodate a greater amount of area. As currently shown, deducting for the round-about, there's approximately 3,000 square feet of area where there should be approximately 15,000 square feet.</i>	<i>Not applicable</i>
Frame the plaza with buildings, with one side open (or partially open) to Bluff Street and/or Junction Place. The intent is to create a partially enclosed space that is both inviting and intimate.	<i>Not applicable</i>	<i>Not applicable</i>	<i>The building appears to encroach into the area where the plaza space is intended and should be pulled back further toward the north.</i>	<i>Not applicable</i>	<i>The buildings should work in concert with the Railyards to create a more viable plaza space, while still framing the space with building frontage.</i>	<i>Not applicable</i>
Provide flexible space to accommodate a variety of public uses, such as a mercado, farmers' market, and festivals. Also provide flexibility for different uses during different times of the day, week and year. Anticipated uses and associated maintenance should be an integral part of the plaza design, particularly layout, furnishings, materials and plant selection.	<i>Not applicable</i>	<i>Not applicable</i>	<i>Currently no information on these elements, as project plans progress greater detail will be required. City is responsible for developing the plaza, at this stage we need to ensure that enough room is provided.</i>	<i>Not applicable.</i>	<i>Currently no information on these elements, as project plans progress greater detail will be required. City is responsible for developing the plaza, at this stage we need to ensure that enough room is provided.</i>	
Design the plaza so its use could be combined with temporary closure of the east end of Bluff Street for special events.	<i>Not applicable</i>	<i>Not applicable</i>	<i>The use of the turnaround could facilitate closure of the plaza.</i>	<i>Not applicable</i>	<i>The use of the turnaround could facilitate closure of the plaza.</i>	<i>Not applicable</i>
Include a variety of smaller "places" (activities or destinations within the plaza. These could be as simple as a "vendor cart.")	<i>Not applicable</i>	<i>Not applicable</i>	<i>Currently no information on these elements, as project plans progress greater detail will be required. However, the placement of trees appears to be intended to set up this space. City is responsible for developing the</i>	<i>Not applicable</i>	<i>Currently no information on these elements, as project plans progress greater detail will be required. However, the placement of trees appears to be intended to set up this space. City is responsible for developing the</i>	<i>Not applicable</i>

			plaza, at this stage we need to ensure that enough room is provided.		plaza, at this stage we need to ensure that enough room is provided.	
Provide essential and “comfort” amenities such as bike racks, a drinking fountain, recycling and trash receptacles, pedestrian scale lighting, shade and soft surfaces, in carefully chosen locations.	<i>Not applicable</i>	<i>Not applicable</i>	Currently no information on these elements, as project plans progress greater detail will be required. However, there does not appear to be any soft surfaces to respond to future transit users. City is responsible for developing the plaza, at this stage we need to ensure that enough room is provided.	<i>Not applicable</i>	Currently no information on these elements, as project plans progress greater detail will be required. However, there does not appear to be any soft surfaces to respond to future transit users. City is responsible for developing the plaza, at this stage we need to ensure that enough room is provided.	<i>Not applicable</i>
Provide an adequate amount of seating and carefully consider its location orientation, type and materials.	<i>Not applicable</i>	<i>Not applicable</i>	Currently no information on these elements, as project plans progress greater detail will be required. However, there does not appear to be any soft surfaces to respond to future transit users. City is responsible for developing the plaza, at this stage we need to ensure that enough room is provided.	<i>Not applicable</i>	Currently no information on these elements, as project plans progress greater detail will be required. However, there does not appear to be any soft surfaces to respond to future transit users. City is responsible for developing the plaza, at this stage we need to ensure that enough room is provided.	<i>Not applicable</i>
Look for opportunities to incorporate art into built elements such as paving, railings, signage, seating or overhead structures.	<i>Not applicable</i>	<i>Not applicable</i>	City is responsible for developing the plaza, at this stage we need to ensure that enough room is provided.	<i>Not applicable</i>	City is responsible for developing the plaza, at this stage we need to ensure that enough room is provided.	<i>Not applicable</i>
Incorporate environmentally friendly features such as pervious surfaces, biofilter landscaping beds, high efficiency lighting and solar powered amenities (e.g., bubble fountains). Explore possible demonstration or education aspects for these features.	<i>Not applicable</i>	<i>Not applicable</i>	City is responsible for developing the plaza, at this stage we need to ensure that enough room is provided.	<i>Not applicable</i>	City is responsible for developing the plaza, at this stage we need to ensure that enough room is provided.	<i>Not applicable</i>
Use high-quality, authentic materials	<i>Not applicable</i>	<i>Not applicable</i>	City is responsible for developing the plaza, at this stage we need to ensure that enough room is provided.	<i>Not applicable</i>	City is responsible for developing the plaza, at this stage we need to ensure that enough room is provided.	<i>Not applicable</i>
Utilize trees and plans to soften the space	<i>Not applicable</i>	<i>Not applicable</i>	Applicant has provided trees	<i>Not applicable</i>	Applicant has provided trees	<i>Not applicable</i>
Carefully design the new pedestrian underpass (or overpass) at the tracks so that it does not negatively impact the aesthetics or function of the plaza.	<i>Not applicable</i>	<i>Not applicable</i>	City is responsible for developing the plaza, at this stage we need to ensure that enough room is provided.	<i>Not applicable</i>	City is responsible for developing the plaza, at this stage we need to ensure that enough room is provided.	<i>Not applicable</i>
Provide way-finding features, such as signage, special pavement and art, to direct people to the plaza from 30th Street, Bluff Street, Valmont Road, Junction Place, and Pearl Parkway.	<i>Not applicable</i>	<i>Not applicable</i>	City is responsible for developing the plaza, at this stage we need to ensure that enough room is provided.	<i>Not applicable</i>	City is responsible for developing the plaza, at this stage we need to ensure that enough room is provided.	<i>Not applicable</i>
Actively manage the plaza to ensure on-going security, cleanliness and liveliness. Gear events to attract both existing users and new users. Program uses to change as the seasons change.	<i>Not applicable</i>	<i>Not applicable</i>	City is responsible for developing the plaza, at this stage we need to ensure that enough room is provided.	<i>Not applicable</i>	City is responsible for developing the plaza, at this stage we need to ensure that enough room is provided.	<i>Not applicable</i>

From: jennifer sorkin [mailto:]
Sent: Thursday, February 12, 2015 2:13 PM
To: McLaughlin, Elaine
Subject: S'PARK

Hi Elaine,

My name is Jennifer Sorkin. I have been a homeowner and resident in the Steel Yards since 2007. Last night, in our monthly HOA meeting, we received a wonderful presentation by Scott Holton about his proposed development, "S'PARK". I just wanted to send you a quick note to express my excitement and support of this project. I love living in the Steel Yards -- the close proximity to restaurants, shopping, the bike path, etc. and also the diversity of the architecture styles here all make it a very unique community in Boulder. The S'PARK development would offer a *tremendous* enhancement to our neighborhood. I have expressed my full support to Scott, but wanted someone on the planning board to hear this as well.

Thank you for your time,

Jennifer Sorkin
3200 Carbon Pl, #S-208
Boulder, CO 80301

STEEL YARDS CONDOMINIUM ASSOCIATION
BOARD OF DIRECTORS

February 18, 2015

City of Boulder
Community Planning and Sustainability
1739 Broadway, Third Floor
Boulder, Colorado 80306-0791

STEELYARDS

RE: 2440 and 2490 Junction Place – “The Commons”

On behalf of the Board of Directors of the Steel Yards Condominium Association, I am submitting these comments in response to the above referenced project. Because we cannot present a formal position of the Steel Yards Condominium Association without a vote of the property owners, this letter communicates the Board’s position. The applicant, Coburn Architects, appeared at our Board meeting on February 11, 2015 and made a useful and informative presentation. We also have had considerable contact with our members, several of whom approached the Board to express concern about this proposed project.

Our parking committee met on February 17 to discuss parking issues associated with the proposed Commons development. Members and area residents attended this meeting, at which a draft of this letter was discussed. Given the accelerated timeframe for the City’s review process, we have done all we can for now to consolidate and represent the neighborhood’s concerns. We would like to reserve the opportunity to comment further as the development review process moves along, but in general, we have no major issues with continued build-out of Steelyards or with the general design of the proposed office buildings.

However, we are greatly concerned about the amount of parking proposed by the developer. Interpretations of the site plan vary, but it appears the developer is proposing to build about 103,000 square feet of commercial space, most of which would be leased as office space, but is proposing to build only 75 parking spaces rather than the 300 to 400 in parking demand that would be generated by this development.

We were told by the developer at the February 11 Board meeting that City staff at the pre-application meeting took the position that there is too much parking shown in the plans and that perhaps there should be no new parking supplied with the project. While our members have been puzzled by this, we are familiar with the City’s plans for the Transit Village/Boulder Junction site and “transit village” area, and we understand the concept that parking demand might be reduced by the site’s proximity to the transit center.

However, it is unclear to us what transit services will be provided to Boulder Junction. As you know, transit service levels in the area today are quite low. The Bound service along 30th Street is frequent, but local. The 208 route comes within about 1,300 feet of the site along Valmont and the 206 passes through on Pearl. Both of these local routes offer half-hour service over a short service day. No regional routes currently serve either Steel Yards or Boulder Junction. Perhaps in the future some of the US 36 BRT buses would come to Boulder Junction, but the details of that or other future regional bus service levels and timing of new services are not available to us.

We understand the concept that if a high level of commuter and local transit service were directly available near the project site, the transit mode share could be high and parking demand thereby reduced, but we are unaware of any commitment from either the City or RTD that this will happen.

We also have been told that the Commons is now part of a general improvement district and would pay taxes in lieu of parking, similar to the way developers in downtown have no on-site parking requirements but must pay taxes to CAGID. The technical parking under-supply in the Commons application would be somewhere in the range of 225 to 325 spaces, depending on actual parking demand. We estimate each new structured parking space in our area costs at least \$25,000 to build, so the parking under-supply represents a cost savings/windfall of between \$5.6 million and \$8.1 million for the developer.

We assume there has been an accounting of the parking demand for the development site, the tax level that will be imposed, and the estimated amount of annual taxes the Commons owners would pay. Also, we assume there is a timeline showing how Boulder Junction parking demand will increase over time and when the new parking garages would come on line. If the new parking supply to be built by the Boulder Junction general improvement district lags the development of new buildings generating parking demand, Steelyards will bear the brunt of the parking overflow.

I believe you know that The Steelyards development is already significantly under-parked. Apparently, in project permitting years ago, the developer was allowed to take credit for new on-street supply, among other considerations. However, most of our streets are public, with the City responsible for management. We have been told the City is considering imposing paid parking on this on-street supply as a means of discouraging overflow of transit center parking into the neighborhood. While there may be an argument for doing that, this could further affect the viability of our neighborhood businesses, which have been struggling. And our members are questioning the equity of allowing Steelyards developers to under-supply parking and then responding to the parking shortage by imposing on-street paid parking on subsequent owners.

Finally, we were told on February 11 by the developer for the proposed S'PARK project, that they, too, would be relying on the Boulder Junction district parking supply. The S'PARK project appears to have been planned with proposed parking supply less than technical parking demand from land uses in the development. Our members are concerned about the City's intent for the district parking program. We understand the concepts of transit-oriented development, mode share and shared parking. But, concepts will not be enough to prevent significant financial and quality of life impacts to the property owners, residents and businesses in our neighborhood.

We have been meeting with staff of the Downtown & University Hill Management Division and Parking Services over the past year to explore potential for a Residential Parking Permit district, a Neighborhood EcoPass, and other actions to manage the problem. Our parking committee has been meeting regularly and we have spent Association money to retain a parking consultant (Walker) to help us address the issue.

We appreciate the support and assistance City staff has provided over the past year. However, it will be difficult to sell these programs to our members if there are active doubts about how well-planned the parking district is, what the timing of new parking supply will be, and what transit services will actually be provided, not to mention the equity issues, which probably have no answer.

We respectfully request answers to the following questions:

1. What tax rate will owners of the Commons pay to the Boulder Junction GID?
2. How much resulting tax revenue will that generate annually?
3. How much district parking supply will the City/district build for Boulder Junction and when will those projects be open for use?
4. Will the S'PARK development also be part of the Boulder Junction GID and will it, too, be allowed to undersupply parking as part of the City's district parking/transit concepts?
5. What mode share assumptions have been made to support the City's district parking strategy for Boulder Junction?
6. Has there been a study similar to the periodic analysis of parking demand, supply, and management prepared for CAGID and if so, could we have access to that data?
7. What regional and local transit routes will serve Boulder Junction, when will these services be implemented, and what service levels will result (frequency, hours of service, etc.)?
8. Will the US 36 BRT service reach Boulder Junction, and if so, what will the routing and service frequency be?

Thank you, in advance, for providing answers to these questions and for the opportunity to address our concerns with this proposed development.

Sincerely,



Catherine Hunziker,
Steelyards HOA Board, President



CITY OF BOULDER
Community Planning & Sustainability

1739 Broadway, Third Floor • P.O. Box 791, Boulder, CO 80306-0791
 phone 303-441-1880 • fax 303-441-3241 • web www.bouldercolorado.gov

LAND USE REVIEW RESULTS AND COMMENTS

DATE OF COMMENTS: June 22, 2015
CASE MANAGER: Elaine McLaughlin
PROJECT NAME: S'PARK
LOCATION: 3390 VALMONT RD, 3085, 3155, 3195 BLUFF STREET
COORDINATES: N04W03
REVIEW TYPE: Site and Use Review
REVIEW NUMBER: LUR2015-00010 and LUR2015-00011
APPLICANT: SCOTT HOLTON
DESCRIPTION: Proposed redevelopment of a 10.9 acre site within the northern portion of Boulder Junction to create a new mixed use commercial and mixed-income residential neighborhood comprised of six different projects: Markt: a 52,454 square foot, three story non-residential building with a brew pub and micro-restaurants; Ciclo: a four story residential/office building; Railyards: a 67,039 square foot, four story non-residential office and retail building with two restaurants; Timber: a 115,000 square, foot four-story apartment building with ground floor retail; Meredith House: a four story apartment building of 20,690 square feet; and S'PARK_west: a three-story 97,000 square foot apartment and townhome building. Proposed for residential are condominiums, townhomes and apartments some of which are permanently affordable. Proposed non-residential uses include restaurarants, micro-restaurants, retail, office, a brewpub. Multi-use path and various new transportation connections per the Transit Village Area Plan, new plaza/woonerf, new pocket park.

REQUESTED VARIATIONS FROM THE LAND USE REGULATIONS:

Section 9-7, "Form and Bulk Standards," B.R.C. 1981;

- permitted height from 38 feet to 55 feet
- maximum number of stories from three to four
- setbacks (various modifications throughout the site)

Section 9-9-17, "Solar Access," B.R.C. 1981;

- Solar Exception

Section 9-6-1, "Use Standards," B.R.C. 1981;

- Restaurants >1,500 sf - Use Review required

I. REVIEW FINDINGS

While some of staff's comments have been addressed and the prospects of a new mixed use, mixed income neighborhood are very exciting, additional information is required before staff can find the proposal consistent with the Site Review criteria. While additional information is required, staff recognizes the design team for providing very helpful sections and other graphic material that allowed for a more detailed explanation of the proposed buildings. Since the initial review, the applicant had a review before BDAB, in addition to the adopted area plan and the Site Review criteria, that review helped to inform staff's comments herein. There are a number of comments within the resubmittal that didn't respond to either BDAB or staff's comments from the initial review. Where those comments remain, they are indicated with "INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT" with an explanation follows. In addition, while some information was provide that does provided a somewhat better understanding of the proposed project materials, design elements and building layout, new questions and comments have been prompted. The plans particularly for Markt and Timber Lofts continue to lack in sufficient detail to be able to discern if the plans could meet the Site Review criteria. The project plans must provide enough information to clearly communicate the design to the staff, the public and the decision makers and the plans still lack in such detail and information. Then, there remains key issues with regard to the 3155 Bluff permanently affordable including walkway locations and livability. The staff team is committed to meeting with you to review these comments in both an upcoming meeting as well as follow up meetings at your convenience.

II. CITY REQUIREMENTS

This section addresses issues that must be resolved prior to a project decision or items that will be required conditions of a project approval. Requirements are organized by topic area so that each department's comments of a similar topic are grouped together. Each reviewer's comment will be followed by the reviewer's department or agency and telephone number. Reviewers are asked to submit comments by section and topic area so that the comments can be more efficiently organized into one document. Topics are listed here alphabetically for reference.

Access/Circulation David Thompson, 303-441-4417

1. Staff supports the applicant's willingness to contribute financially towards the Valmont Road railroad quiet zone and will set-up a future meeting discuss and finalize the applicant's offer.
2. INCOMPLETE RESPONSE TO THE FOLLOW COMMENT: Pursuant to the Transit Village Area Plan (TVAP), please revise the site plans to show the right-of-way to be dedicated for the rail plaza area. Please contact the Case Manger to schedule a meeting to discuss the area of right-of-way to be dedicated for the Plaza.
3. Staff will provide review comments from the transportation division and on the revised traffic impact study and proposed vehicle parking by Friday, June 26th.
4. Pursuant to section 9-9-15(b)(3) of the Boulder Revised Code, 1981 (BRC), please revise the site plans to show walls, buildings walls and fences placed 18-inches from public sidewalks. The Ciclo building and building #7 south of Meredith Street are closer than 18-inches to the proposed adjacent sidewalk.
5. Staff does not support the access curb-cut being shown for the Timber Lofts on the south side of the street. Please revise the plans to remove the access curb cut.
6. Please revise the site plans to include sight triangles as described in section 9-9-7 of the BRC.

General Comments – Sheets C4.1 thru C4.6

7. Please revise the sheets to show smooth curves where the sidewalks are being realigned.
8. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT: Please revise the site plans to show the roadway landscape strip within a public access easement.
9. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT: Please revise the site plans to remove the pavers encroaching within the sidewalk and multi-use path surfaces.
10. Pursuant to City design policy, please revise the curb ramps to include wings when possible.
11. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT: Please revise the civil plan sheets to show street lighting at the following locations:
 - a. Two street lights along the Junction Place curve
 - b. One street light at the Bluff Street cul-de-sac
 - c. One street light at the intersection of Junction Place and Meredith Street
 - d. One street light at the intersection of Junction Place and Bluff Street
 - e. One street light at each of the proposed raised pedestrian crosswalks
12. Staff would still like to meet to discuss comments on the right-of-way lighting plan shown on the Photometric Lighting Plans from the previous submittal, because the photometrics will affect the right-of-way.

Bluff Street – Sheets C4.1, C4.2 and C4.5

13. As a result of eliminating the Bluff Street roundabout, please revise the site plans to show a public access easement dedication through the Woonerf in order to provide public access connectivity from the terminus of Bluff Street to another public street. This will require changing the direction of the one-way multi-modal traffic flow of the Woonerf. Additionally, please demonstrate an emergency vehicle and drive thru the Woonerf.
14. Staff does not support the Woonerf encroaching within the City owned right-of-way. That said, staff will support vacating a section of the Bluff Street right-of-way in order to accommodate the Woonerf being shown, and re-dedication of a public access easement.
15. Please revise the sheets to remove the offsets being shown for the Bluff Street sidewalk and show a straight five-foot wide sidewalk two feet from right-of-way line and adjust the curb ramps to match the alignment of the sidewalk.
16. On sheet C4.1, please revise the sheet to show an 8' wide east / west public path from the private access lane to its current terminus to the west.
17. On the preliminary street section, please revise the Bluff Street Section to:
 - a. Remove the reference to an "easement" in the landscape and utility strip (on the north side)
 - b. Show the 1' paver behind the curb as shown on the layout sheets
 - c. Show how the drainage will be accommodated between the travel lane and the on-street parking
 - d. Show the landscape width varying between 7' and 14' wide

32nd Street – Sheets C4.1 and C4.2

18. Please revise the sheets to remove the offsets being shown for the 32nd Street sidewalk (west side) and show a straight five-foot wide sidewalk detached 14' from the back of the roadway curb and adjust the curb ramps to match the alignment of the sidewalk.
19. Please revise the sheets to show a driveway ramp for the alley curb-cuts and the curb cuts serving Buildings 8 and 9 pursuant to section 2.04(l)(1) and technical drawing 2.22.A of the City's Design and Construction Standards.
20. In accordance with Section 2.10(D)(4) of the DCS, please provide supporting documentation an emergency vehicle can turn into and out of the private access lanes from 32nd Street or redesign the intersections accordingly.
21. Please revise the site plans to relocate the trash enclosure in order to accommodate the five-foot wide sidewalk and provide 18-inches of separation from the sidewalk to the face of the trash enclosure wall.
22. Please revise the sheets to show a detached sidewalk on the east side of 32nd Street between the private access lanes consistent with the typical roadway section shown in TVAP.
23. Pursuant to the City's Pedestrian Crossing Treatment Installation Guidelines, please revise the site plans to remove the marked crosswalk being shown across Junction Place.

Meredith Street (West of Junction Place)

24. Please revise the plans to show a public access easement being dedicated from the back of roadway curb to a half-a-foot behind the sidewalk.
25. Please revise sheet C4.2 to remove the offset being shown for the Meredith Street sidewalk and show a straight five-foot wide sidewalk and adjust the curb ramps to match the alignment of the sidewalk.
26. It appears Building #7 is encroaching within the public access easement along Meredith Street, please revise as necessary.

Junction Place (33rd Street) – north / south alignment

27. Please revise sheet C4.2 to remove the offsets being shown for the Junction Place sidewalks and show a straight five-foot wide

sidewalk and adjust the curb ramps to match the alignment of the sidewalk.

28. Please revise the site plans to relocate the trash enclosure in order to accommodate the five-foot wide sidewalk and provide 18-inches of separation.

Meredith Street (east of Junction Place to 34th Street)

29. Per previous comment, please revise the street's cross-section consistent with the cross-section shown for a local street in TVAP and show the dedication of a public access easement from the back of the roadway curb to a ½' foot behind the sidewalk.
30. Please revise the site plans to remove the Woonerf being shown within the right-of-way to be dedicated to the City because staff has concerns regarding the safety of the Woonerf design.

East / West Private Alley

31. Pursuant to Section 2.10(D)(3)(b) of the DCS, please revise the site plans to increase the alley's paved surface width to 12-feet within a 12' wide public access easement.
32. In accordance with Section 2.10(D)(4) of the DCS, please provide supporting documentation an emergency vehicle can turn left onto the alley from 32nd Street or redesign the intersection accordingly.
33. Please revise the curb cuts for the east / west private alley to show a driveway ramp curb cut which will eliminate the need for a curb ramp on the east side of 32nd Street at the alley.

Valmont Road / 34th Street Intersection

34. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT: Please revise the site plan to show the dedication of a wider public access easement on the east boundary of the site due to the irregular property right-of-way.
35. Please revise the site plans to show a 8-foot wide landscape strip along Valmont Rd consistent with landscape strip shown in the Transit Village Area Plan (TVAP) for Valmont Road.
36. Please revise the site plans to show the removal of the existing street light in the northeast quadrant of the intersection which will be replaced with the street light attached to the traffic signal.
37. Please revise the site plans to remove the decorative concrete being shown within the intersection's crosswalk.
38. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT: Please revise the site plans to show a transit shelter, bench, trash receptacle and short-term bike parking at the Valmont Road transit stop.
39. Staff would like to better understand the enhanced sidewalk design elements being proposed for the 34th Street sidewalk prior to concurring with the design, please contact the Case Manager to set-up a meeting.
40. Please revise the site plans to show a 14.7' public access easement dedication on the south side of 34th Street (just east of Meredith) consistent with the preliminary street section.
41. Please revise the site plans to show the proposed transformer to be installed on the west side of site and south of the Valmont Road sidewalk 18-inches from the edge of the Valmont Rd sidewalk. Please be aware the doors for the transformer must open onto private property and not onto the public right-of-way.
42. Please revise the intersection to provide the following design elements and adjust the signal poles accordingly:
 - a. Revise the demo plan to show the removal of the existing controller cabinet for the Valmont Road pedestrian

Address: 3390 VALMONT RD Page 4

- crossing and the existing street light on the north side of Valmont Road.
- b. A marked crosswalk across Valmont Rd on the west side of the intersection
 - c. Reconstruct the curb ramps on 34th Street so that the two curb ramps on each side of 34th Street are aligned with each other and with the curb ramps to be constructed on Valmont Road that provides a direct crossing path between the curb ramps rather than a skewed crossing as shown on the site plans.
 - d. The reconstruction of the curb ramps on 34th Street must be consistent with the curb ramps shown on CDOT Standard Plan No. M-608-1.
 - e. Decrease the width of the painted median on the west side of the intersection on Valmont Rd in order to provide an 11' wide inside thru lane and a 12' wide outside thru lane on eastbound Valmont Road.
 - f. The design of the signalized intersection needs to include the location of the signal's controller cabinet and pull boxes in order to ensure any future utility relocation do not impact the future traffic signal.

Multi-Use Path

- 43. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT: Please revise the site plan to show a 12' wide multi-use path within a 14' wide public access easement for the north / south multi-use path paralleling the railroad tracks.
- 44. Staff does not concur with the alignment of the north / south multi-use path because the path is located within an existing railroad easement / reservation and no documentation has been provided indicating the railroad has vacated the easement / reservation or that the railroad concurs with the overlapping easements. Does the multi-use path need to be moved in order to accommodate a fence?
- 45. Please revise the site plans to show a temporary public access easement to be dedicated on the west side of the existing brick building in order to accommodate the realignment of the multi-use path on the east side of the building should rail transit be constructed on this railway corridor prior to re-development of the site.
- 46. Please revise the plan to show a 10' wide connecting path between the Markt Building / Railyards Building and the 12' wide north / south multi-use path.

Preliminary Street Sections

- 47. Please revise the preliminary street sections to include a street section for Valmont Road.
- 48. Please revise the preliminary street sections to replace the "landscape and utility easement" with a "public access and utility easement".
- 49. Please revise the preliminary street sections to replace the "public access easement" shown for the sidewalk with a "public access and utility easement".

Bicycle and Vehicle Parking

- 50. In support of the TVAP, additional long-term and short-term bicycle parking should be provided on the site.
- 51. Please revise the site plans to show the location of any proposed B-Cycle stations.
- 52. Please revise the site plans to include the details for the long-term bicycle parking in order to ensure the area shown for the long-term bike parking is adequate and will not encroach within the vehicle parking areas.
- 53. Pursuant to technical drawing 2.54 from the City's Design and Construction Standards, please revise the site plan of the Ciclo garage to show an 8' wide access aisle for the van assessable parking stall.
- 54. Pursuant to Table 9-8 and section 9-9-6(g)(3) of the BRC, please revise the site plan to show the minimum number of 86 bike parking spaces required and show short-term bike parking in front of the commercial building adjacent to 34th Street.

55. Please revise the site plans for the MARKT building to show the location of the 66 short-term bike parking spaces. Short-term bike parking spaces should be placed consistent with section 9-9-6(g)(3) of the BRC.
56. Please revise the site plans for the MARKT building to show the details of the short-term bike parking spaces shown on the ground floor and 3rd floor of the MARKT building.
57. Pursuant to section 9-9-6(g)(3) of the BRC, please revise the site plans to show short-term bike parking along the Woonerf to serve the proposed commercial buildings.
58. Pursuant to the Americans with Disabilities Act Accessibility Guidelines (ADAAG Manual) please revise the parking for the Railyards, Meredith House and Timber Lofts to show one van accessible parking stall for each building as shown in technical drawing 2.54 of the DCS.
59. Please revise the short-term bike parking shown for the Railyards to remove the parking from the Plaza Area and disperse the short-term bike parking consistent with section 9-9-6(g)(3) of the BRC.
60. Please revise the site plans for the Railyards to show the location of the long-term bike parking.
61. Please revise the site plans for the Timber Lofts and Meredith House to show how the bike parking requirements shown in Table 9-8 of the BRC are being met for the building.
62. Pursuant to section 9-9-6(g)(3), please revise the site plans to show short-term bike parking being provided for block #1 of S*PARK West.
63. Pursuant to Table 9-8 of the BRC, please show how the long-term bike parking is being met on block #3 of S*PARK West.

Building Design Elaine McLaughlin, 303-441-4130

1. PREVIOUS COMMENT: A simple-form built model must be provided that illustrates the entire neighborhood proposed with a SketchUp model for the individual projects to best understand the new neighborhood. NOT COMPLETED
2. The applicant is scheduled for review before BDAB on July 16, 2015, note that staff is trying to schedule a meeting at an earlier date.
3. Refer to plan document deficiency comments for each specific building/project below under "Plan Documents."

Drainage Scott Kuhna, 303-441-4071

The proposed porous pavers in the public right-of-way will require a Right-of-Way Lease. The city council may approve a Long-Term Lease on public right-of-way or a public easement for encroachments that are permanent in nature subject to the provisions of Section 2-2-8, "Conveyance of City Real Property Interests," B.R.C. 1981, upon a finding that the standards for a revocable permit and a short-term lease set forth in subsections (d) and (e) of said section have been met. Maintenance responsibilities for the paver systems (underdrains, etc.) will remain with the adjacent property owners and/or HOA.

Fees

Please note that 2015 development review fees include a \$131 hourly rate for reviewer services following the initial city response (these written comments). Please see the P&DS Questions and Answers brochure for more information about the hourly billing system.

Fire Protection David Lowrey, 303.441.4356

- 32nd St and Meridith west of Junction Pl. do not meet the City's Design and Construction Standard for proper Emergency Access Lanes (Section 2.10).
- There are three areas that the turning radius for fire apparatus doesn't appear to meet the minimum standard of a SU-30. The

north end of 32nd St. from the alley onto the street, and the two access areas off 32nd St. into the alleys on the east side.

- All secondary access lanes that will be used for fire apparatus must be a minimum of 12' drivable area with a 20' clear width.
- The Woornerf area between the "The Flats" and "The Railyard" needs to show the emergency access through that area and be designated as emergency access by easement.

Inclusionary & Affordable Housing Beth Roberts 303 441-1828 & Michelle Allen 303-441-4076

1. AMENDMENT TO THE FOLLOWING COMMENTS: Cash-in-lieu. Applicants re-submittal indicates they are constructing 39 for-sale units, not 40 as indicated in the first submittal, at Spark West and Meredith house. They will meet the Inclusionary Housing (IH) requirement of 7.8 units ($39 \times .2 = 7.2$) with a cash-in-lieu contribution. The IH requirement is that a minimum of half of any required affordable for-sale units be provided on-site. If these are not provided on-site additional affordable housing benefit is required. A premium in CIL of 50% meets that requirement. Since no affordable for-sale units are proposed to be provided in the development, the cash-in-lieu estimate below is based on the 2014-15 CIL amount of \$130,880 per required affordable unit plus a 50% premium on half. Please note that CIL is an estimate as the amount in place when paid will apply. CIL is updated annually on July 1st.

39 for-sale units

CIL for 4 required on-site = \$785,280

CIL for 3.8 not required on-site = \$479,344

Total CIL estimate = \$1,282,624

2. Project Design, Quality and General Appearance

Staff is concerned that as currently proposed, the affordable units are of lesser design and quality particularly concerning the Inclusionary Housing requirement of the Land Use Code section 9-13-1(g), B.R.C. that states "Affordable units shall be indistinguishable from the surrounding market housing in quality, design, and general appearance." Funded developments are held to these standards based on city discretion to fund. Because the application is part of an overall Site Review application, the city expects such projects to meet the Site Review criteria for building design of the Land Use Code section 9-2-14(h) that include, "exteriors of buildings present a sense of permanence through the use of authentic materials such as stone, brick, wood, metal or similar products and building material detailing."

There are two permanently affordable projects in S'PARK: Ciclo and S'PARK_west rentals. Ciclo is being provided to meet the IH requirement for Timber Lofts and the S'PARK west rentals are being supported with city funding.

Affordable units must be of quality materials and construction and IH units must meet the Livability Standards found on-line at www.boulderadffordablehomes.com. The affordable units are allowed to differ from the market units in some ways including:

- On average units may be smaller.
- Interior finishes and appliances can be "functionally equivalent" but are not required to be exactly the same.
- Modifications to unit mix of number of bedrooms based on city approval.

However, it is important that from the outside the affordable units not "stand out" and be identifiable as affordable housing. For this reason, the design and exterior materials need to be comparable to what is being provided for the market units within SPARK and SPARK_west.

Staff has determined that the affordable units at Ciclo are close to meeting these standards however, the following items must be addressed prior to the city manager's acceptance of the affordable units:

- a. Adequately address the staff comments in the “Plan Documents: Ciclo” of this document.
- b. Increase window size and overall window coverage to better reflect equality with Timber Loft.
- c. Replace the “corrugated metal” with the “standing seam metal cladding” or “V-rib metal cladding” as was used for Timber Loft.
- d. Take S’PARK west rental units back to BDAD with a request for comments to specifically address “Affordable units shall be indistinguishable from the surrounding market housing in quality, design, and general appearance.”

Staff is concerned that the S’PARK_west rental affordable units will be easily distinguishable from the market units due to lower quality design, amount and type of siding materials, windows, and general appearance of the exterior. When comparing Timber Lofts with the S’PARK_west rentals there are significant differences in exterior elements that must be addressed before they will be acceptable to the city manager as permanently affordable units.

Siding materials for Timber Lofts are primarily grey standing seam metal cladding, V-rib bronze and pewter colored metal cladding and Accoya wood. Openings to the interior of the site are accented with “garden colors.” Stucco is reserved for stoops and balconies with edges protected with metal claddings. Key materials such as wood cladding extend to become screens or gates at utility zones enhancing texture at the pedestrian horizon.

Window openings are generous and include corner windows to extend the four-sided quality and recessed window units. Dark color window frames will appear more recessive and harmonious.

In contrast, the S’PARK_west rental units siding materials are significantly lower quality: primarily painted plaster, concrete masonry units (CMU), and some ribbed or corrugated metal. Over-use of CMU at the pedestrian level combined with the scale, size and distribution of the buildings gives an institutional barracks appearance. The proposed colors, primarily mustard red and dark grey, are overly heavy, dark, and incongruent and in significant contrast to the softer palette of the adjacent market units. The proposed color scheme serves to distinguish the affordable building from the market rate buildings. It causes the affordable building to appear distinct from the rest of the development. The siding materials, colors, and lack of detail all combine to read as an “affordable project.”

To mitigate the above concerns, the following items must be addressed prior to the city manager’s acceptance of the affordable units:

- a. Adequately address the staff comments in the “Plan Documents: S’PARK_west” section of this document.
- b. Increase window size and overall coverage to better reflect equality with Timber Loft.
- c. Minimize the use of plaster/stucco as it tends to chip and crack and is difficult to maintain. A sturdy material equivalent to that used for the market buildings, such as brick, should be used as a primary building material. Where stucco/plaster occurs they must be capped at the roofline.
- d. Include a substantial roof line.
- e. Modify the color palette to a softer and more harmonious blend with the adjacent market projects on either side.
- f. Take S’PARK_west rental units back to BDAD with a request for comments to specifically address “Affordable units shall be indistinguishable from the surrounding market housing in quality, design, and general appearance.”

3. Unit Interior Comments

Ciclo floor plans emailed to housing staff vary significantly from the floor plans on the plan set.

- a. The R1 submittal does not include sufficient information to fully assess if the proposed bedroom affordable units meet the Livability Standards.
- b. Submit scalable, dated floor plans that are the same as the floor plans in the re-submittal.
- c. Mark each unit on the floor plans in the plan set with a unique address or identifier.

Spark West rentals. Staff has concerns about interior elements of the units including a lack of adequate linear feet of kitchen cabinets, lack of storage, and under sized bedrooms and closets.

Applicant should meet with Division of Housing staff to find acceptable solutions to these deficiencies.

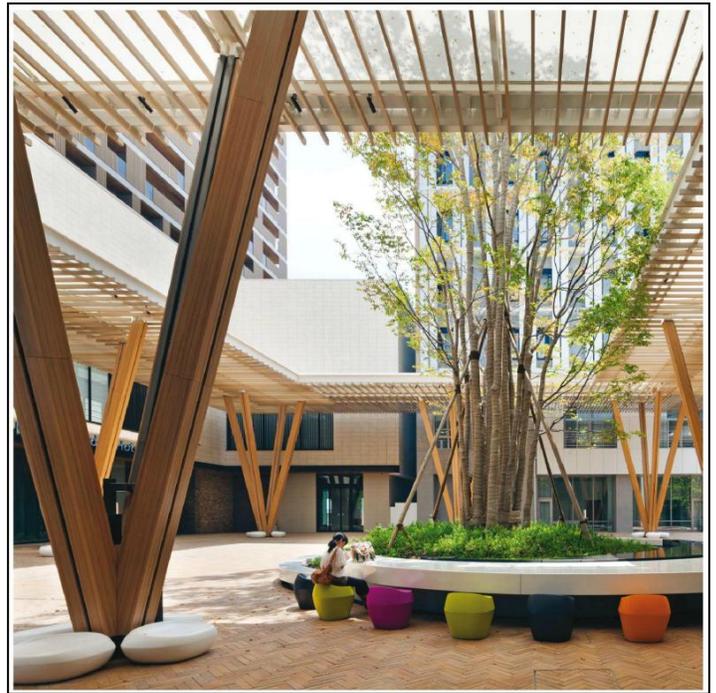
4. The affordable renters in Ciclo and S'PARK West should have the same access to all amenities in Timberlofts as renters in Timberlofts.
5. A 3rd party housing inspector will be retained by the city at the applicant's expense to ensure quality construction and materials and compliance with the city's Livability Guidelines, Affordable Covenant, Funding Agreement and any other applicable contractual agreements for the affordable developments.

Landscaping Elizabeth Lokocz, 303-441-3138

In general, the proposed plans show that improvements have been made and are headed in the right direction, however additional refinements are required as detailed herein.

1. Change the sheet title that is labeled, "Landscape: Markt Plaza Plan" to "Landscape: Ciclo and Markt Plaza Plan"
2. Many of the previous comments have been addressed. As a general comment, the application remains relatively conceptual. Staff anticipates that some elements may not be as detailed as a typical Site Review approval consistent with 100% Design Development plans; however, the intent must remain consistent as plans are developed and submitted through the Technical Document Review consistent with 100% Construction Documentation. Staff strongly recommends that final landscape plans are broken out on a building and lot basis consistent with the buildings. This will allow for a much more streamlined transition from Site Review to Technical Document Review and then building permit submittals.
3. Landscape Site Calculations (sheet 26): the table is extremely helpful to understand the overall balance of required and provided landscape material. It also brings to the light the lack of tree planting for Ciclo and Markt sites. Even with a more urban context, it seems inconsistent with Site Review criteria to provide no or far fewer than the minimum number of required trees.
4. Substitute the proposed spring snow and Shubert Chokecherry with a large maturing tree species. These trees do not meet minimum requirements or urban canopy goals and are likely to have clearance issues over the adjacent sidewalk and parking.
5. Several considerations require careful detailing for the proposed trees in the Woonerf including, but not limited to, the framing and support for the grates in coordination with the proposed paving, the vehicle rating of the grates, tree protection (i.e. guards), etc. Continue to develop this area and anticipate that full details will be required at Technical Document submittal.
6. Private open space areas need coordination and refinement:
 - a. If possible, eliminate all narrow landscape strips (less than two feet). If it is not feasible to do this, remove them from the open space calculations per [section 9-9-11\(i\)\(3\) B.R.C. 1981](#). For any space measuring less than 24 inches, include the proposed treatment in the next submittal. Call out all proposed vegetation or an alternative. This appears to include numerous areas including, but not limited to the west side of Block 5, south side of Block 2, north side of block 3, etc.

- b. Building 4 Lot 1 Block 1 (sheet 21) needs the driveway and landscape strip updated (swapped) and trees removed or shifted. Please make this change across all plans.
- c. The private garages drives are not clearly called out as any material. Most of the landscape sheets don't appear to include the light gray used in a legend, but Sheet 26 Landscape calculations appears to call these areas out as Decorative Concrete.



- 7. The planters proposed over below grade garage for Timber Lofts are inconsistently shown as planting and open space (see the open space plan no sheet number and the L2 floor plan). Provide additional detail for the planters including the depth of the soil to support trees.

Raised courtyard planters have received mixed reviews from the community in the past. Explore the feasibility of tree vaults or another mechanism that allows for “at grade” planting beds. If they are the only option, consider how to incorporate materials and design elements that integrate the planters into the larger design intent. The courtyard pictured (see the May 2015 issue of Landscape Architecture Magazine) makes good use of shade structures and natural materials. The tree and surrounding groundcover are a sculptural element adding to the quality of the overall space. Creating a sense of enclosure may also lead to a more actively used shared open space.

- 8. The sidewalk alignment shifts create numerous hard to maintain planting areas. Address any transportation comments and eliminate extreme acute angles in planting beds and the small undefined areas adjacent to ramps. Examples are shown to the right:



- 9. Clarify the locations of the proposed pole and string lights illustrated in the Timber Lofts sections. They do not clearly appear on the plan sets and while staff likes them conceptually, clearance for emergency vehicles, conflicts with below grade utilities and maintenance concerns need to be addressed. Based on the height illustrated, they are likely to have significant below grade structure.

- 10. Street trees: planting opportunities have improved overall. The following areas need to be further revised to meet all criteria:

- a. Please dimension all planting strips or refer to the civil, site or other more detailed plan sets.
- b. The intersection 34th and Meredith (current names) presents a very significant canopy gap. Given that the adjacent sites have no proposed private trees and relatively generous sidewalk widths, this is not a supportable option. The north side accommodates trees with no utility changes by simply following the edge of the dry utility easement as is currently shown further west. Add five trees spaces approximately 30 feet on center as shown below. To provide some separation, but not likely the full ten feet, swap the locations of the proposed storm sewer and water main and reduce their separation to the minimum five feet. This provides an opportunity for

continuing street trees throughout the curve. Coordinate with engineering/utility comments and submit a DCS variance request as needed. Staff will review the request keeping in mind the many goals of the project.

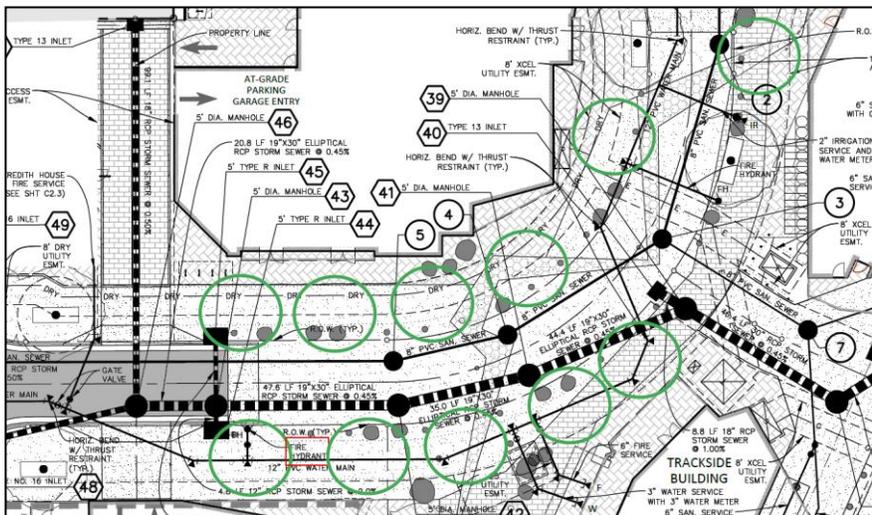
- c. The proposed trees may not necessarily be at curb line. Fire hydrant locations also appear to need slight adjustments if possible while meeting maximum distances.
- d. The attached sidewalk on 32nd street between Bluff and Meredith appears to have the same, or perhaps even slightly more, distance from back of curb to face of building as the detached walk on the south side of Meredith. This is not consistent with the TVAP street sections, underlying city standards or site review criteria. Re-align the sidewalk to be consistent with the cross section for the detached walk on Meredith and change the proposed trees accordingly. Please coordinate with all transportation related comments.
- e. The residential block of Meredith east of 33rd is illustrated with attached sidewalks and tree grates. This is inconsistent with the adjacent use and TVAP section corresponding to a local street. Tree grates are not a supportive long term growing environment and should not be used outside of high turn-over commercial areas. The image included taken in Belmar is an excellent illustration of the impact of a tree in a grate vs. an open vegetated planter. Staff also understands that a five foot sidewalk may not be adequate for the anticipated pedestrian use. However, alternative solutions should be explored that incorporate an eight foot walk and six foot six inch planting strip. The planting strip can be designed to accommodate the adjacent parallel parking. If alternative cross sections are proposed, please contact staff prior to resubmittal.



Legal Documents Julia Chase, City Attorney's Office, Ph. (303) 441-3020

The Applicant will be required to sign a Development Agreement, if approved. When staff requests, the Applicant shall provide the following;

- 1) an updated title commitment current within 30 days; and
- 2) proof of authorization to sign on behalf of the owner (such as an operating agreement or statement of authority).



Plan Documents: GENERAL Elaine McLaughlin, 303-441-4130, Sam Assefa, 303-441-4277

1. The project plans become a contractual agreement with the city that what is shown in the plans is what will be built. There are very high expectations that what is represented on the plans will carry through to Building Permit and construction. With the resubmittal, there remains a number of deficiency comments and missing details with regard to the plans for the Markt (Maarket) that must be providing for staff and the Planning Board to make findings of approval.

2. Provide balcony detailing for every building. For example, will they be framed or poured? Also, clearly indicated the types of railing systems that will be used. They should be permanent materials like metal, glass, etc. Staff is concerned that the decks are durable and long lasting, and not simply painted 2x4s or 2x6s.
3. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT:
For each project, provide a Site Plan and on it place a Data Summary Table (as well as part of an overall Site Plan Data Summary) that includes the following information:
 - a. Lot Area
 - b. Building Area: amount of floor area for each use and total Floor Area
 - c. Floor Area Ratio
 - d. Number of residential units and bedroom count within each unit
 - e. Parking Required/Provided
 - f. Bike Parking Required/Provided
 - g. Useable Open Space Required/Provided
 - h. Setbacks Required/Proposed (modifications requested)

NOTE THAT THERE IS AN EXPECTATION TO PROVIDE AN OVERALL SITE PLAN DATA SUMMARY AS NOTED ABOVE FROM THE PREVIOUS COMMENTS.

4. The names of the streets, "34th Street" and "Meredith Street" through S'PARK usurp the notion that "Junction Place" would be a key identifying roadway that moves throughout Boulder Junction from Valmont Avenue on the north, to the south side of Solana Apartments on the south. Staff is concerned that the terminus of Junction Place is at Meredith Street. Staff will discuss internally how best to proceed with this issue and inform the applicant.
5. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT:
Indicate any plans to phase any aspects of the project(s).
NOTE THAT FOR AN APPROVAL TO EXTEND BEYOND THE THREE YEARS GRANTED THROUGH A DEVELOPMENT APPROVAL, PHASING MUST BE IDENTIFIED AND WRITTEN INTO THE DEVELOPMENT AGREEMENT. PLEASE PROVIDE A SPECIFIC RESPONSE TO THE COMMENT
6. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT:
As project plans progress, provide a more detailed energy efficiency plan that articulates how the applicant will specifically meet the city's energy efficiency standards of the IECC 2012 +30%.
NOTE THAT, AT A MINIMUM, PRELIMINARY INFORMATION ABOUT HOW SPECIFICALLY THE APPLICANT WILL ADDRESS ENERGY EFFICIENCY (i.e., ON-SITE RENEWABLES...ETC.) IS REQUIRED PRIOR TO PLANNING BOARD FOR STAFF TO MAKE A FINDING OF CONSISTENCY WITH THE SITE REVIEW CRITERIA.
7. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT:
Provide window details, use of vinyl windows and window frames without a return or reveal are discouraged.
NOTE THAT THE "DESIGN EXCELLENCE" PAGES THAT THE APPLICANT POINTS TO ACTUALLY SHOW FLUSH MOUNTED WINDOWS WITH LITTLE TO NO REVEAL. REFER TO INDIVIDUAL WINDOW COMMENTS FOR EACH PROJECT.

Plan Documents: MAARKET Elaine McLaughlin, 303-441-4130, Sam Assefa, 303-441-4277

1. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT:
Staff recommends the applicant provide a SketchUp (or similar) 3D model to help convey the relatively complex design intent.
NOT PROVIDED WITH RESUBMITTAL
2. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT:
Staff requested precedent images of the combined use of horizontal and vertical wood siding in combination to understand the design intent. The applicant responded that "we have provided some precedent images pertaining to this project" however, there's one image of silver ghost wood only.

3. **INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT:**

There is no delineation of a property line on the project plans. Therefore, it is unclear what lot area is being used to determine the 1.8 FAR and the 15 percent open space. The fact sheet indicates a "lot size" of 30,159. Indicate if there is a plan for replatting the site and if, so file an application for Preliminary Plat review.

THE APPLICANT ONLY INDICATED THAT PROPERTY LINES ARE NOW SHOWN, THERE'S NO INDICATION OF ANY PRELIMINARY PLAT REQUIRED.

4. **INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT:**

On sheet 47, Level 1 and Level 1 Mezzanine: a number of floor plan elements and rooms are not labeled please label all of the elements shown on the plan. Those include, but are not limited to those shown below as indicated with red arrows.

THERE ARE STILL UNLABELED ELEMENTS INCLUDING MULTIPLE CIRCLES SHOWN ON THE SITE. SOME ARE ASSUMED TO BE TANKS FOR THE BREWPUB BUT THERE ARE NO LABELS ON THOSE CIRCLES OR ON THOSE SHOWN IN THE RESTAURANT OR BREW PUB. IF THEY ARE INTENDED TO BE TABLES, THEN SEATS (OR NUMBER OF SEATS) MUST BE SHOWN FOR PURPOSES OF THE USE REVIEW TO PERMIT THE BREW PUB.

5. Regarding the brew-pub, please ensure that the operational characteristics meet state and City of Boulder requirements and definition,

"Brewpub means an establishment that is primarily a restaurant where malt liquor is manufactured on the premises as an accessory use. A brewpub may include some off-site distribution of its malt liquor consistent with state law."

Note that a Brewpub is distinct from a Brewery which is primarily a manufacturing facility. Staff notes that now that the plans are labeled, there are elements that could be defined more as a brewery rather than a Brewpub such as the circle now labeled as a Silo, along with the a large dock area with a semi-truck sized loading space. Because a Brew Pub is permitted in MU-4 zoning, but a Brewery is not, please ensure that the operating characteristics not meet this definition:

"Brewery means a use with a manufacturer or wholesaler license issued under § 12-47-401, et seq., C.R.S., and does not include any retail type liquor license under § 12-47-309, et seq., C.R.S., on the lot or parcel, that is primarily a manufacturing facility, where malt liquors are manufactured on the premises, that may include a tap room that is less than or equal to thirty percent of the total floor area of the facility or one thousand square feet, whichever is greater."

6. **INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT:** The mezzanine counts as a story under the land use code, therefore revise the information on page 45 as well as on the Fact Sheet to indicate that it is a five story building.

ON THE SITE PLAN, THE APPLICANT DID NOT LABEL THE TOP FLOOR AS "FIFTH STORY" RATHER AS A MEZZANINE"

7. Note that the cross-sections are not keyed to the site plan or plans therefore they don't address the request for greater information on this relatively complex building.

8. The cross-section label the top floor is labeled as "roof deck" but actually appears to be a cross section through the office space, as the section illustrates the space as being enclosed. Please relabel.

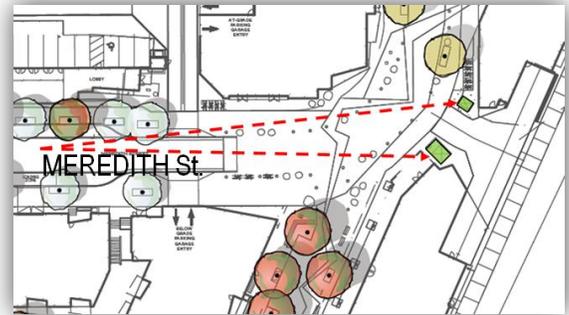
9. The sign band is not detailed in any of the plans, and it is mislabeled on the west elevation. The plans need to illustrate how it is put together and attached to the building. Note that more information was provided in the written response to comments but all of that information must be provided in the project plans, in reference to the sign band. There are no details that support the description in the written statement.

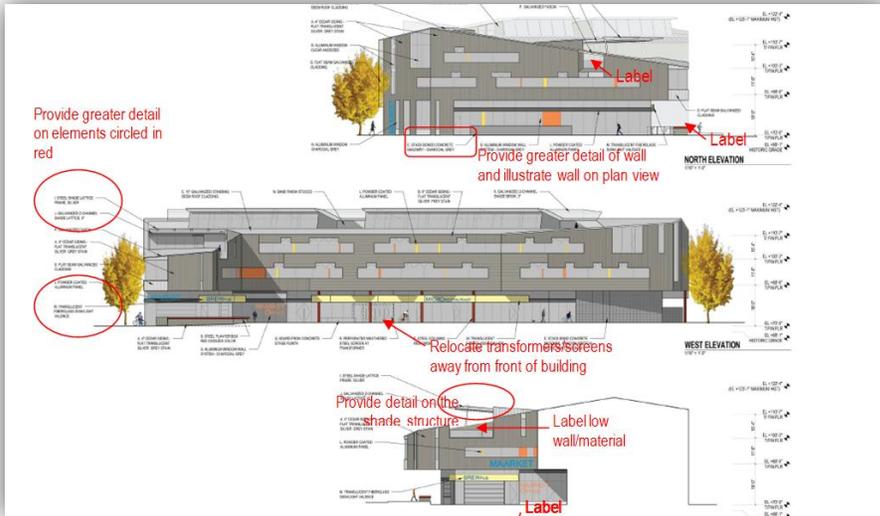
10. Similarly, there's no information or details on the "fire pylon" note what it's made of, how it would be constructed and how it would be mounted or attached.

11. Similarly, there's no information or detailing on the railing or low seat walls.



12. Similarly, there's no information about the storefront materials and detailing.
13. **INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT:** Note that the floor plans must indicate a net square footage rather than gross square footage, as it is the net from which the parking requirements are established. For elements that are not factored into a net floor area total, refer to the land use code definitions of "Floor Area" and "Uninhabitable Space" found [here](#). AS NOW SHOWN, THE TABLE ON THE SITE PLAN HAS AN OVERALL NET SQUARE FOOTAGE FOR EACH LEVEL AND GROSS SQUARE FOOTAGE FOR THE COMPONENTS OF EACH LEVEL. THIS DOES NOT RESPOND TO THE COMMENT – THE APPLICANT MUST REFER TO THE PREVIOUS CODE CITATION AND PROVIDE A TALLY OF THE AREAS WITHIN THE FLOOR AREA THAT DON'T INCLUDE "UNINHABITABLE SPACE" (NET SQUARE FEET).
14. The table on the site plan indicates "9 long term bike parking spaces" yet the project plan adjacent to the table references a bike storage room for 19 bikes. Please clarify and amend the table.
15. The table also labels the fifth floor as mezzanine please relabel. The outdoor deck does not count toward floor area so remove that from the table.
16. Staff remains concerned about the location of the transformer as relocated on the resubmittal. While it is positive that the transformer was moved from the front of the building adjacent to the retail, it now sits in a focal point on the building where Meredith Street is shown to terminate. The perspective rendering illustrates shrubs or plant material in front of the transformer, but with only low plants proposed. This is in a location that the applicant converted into and expanded area for the high amenity woonerf. It deflates the purpose of expanding the woonerf in this location if the backdrop is going to be two transformers. Find an appropriate and less visible location for this transformer (and any others throughout the plan) away from public rights of way.
17. Specifically label the materials shown in this axonometric perspective. The applicant provided design excellence narrative but not actual call-outs with leader lines to each element shown. These details will be part of the application submittal that will become part of the contractual development agreement, if and when the application is approved. Therefore, the plans must be very specific and provide enough information that the decision makers can determine consistency with the land use code and guidelines. Refer to a page from an example below that successfully conveyed the materials proposed in an enlarged detail and axo-section.





18. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT:

The elevations are lacking in detail and information, as identified below. The following were not completed in the resubmittal of the plans:

- a. Wall detail
- b. Illustrate wall on plan view
- c. Detail on the steel frame lattice

shade structure (two are shown in the axonometric section on the Design Excellence page but are not labeled)

19. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT: Indicate the

location of short and long term bike

parking/storage. Only the long term storage inside the building is shown, no bike racks are shown on the site plan. While they appear on the Landscape plan, they must also be shown on the site plan with total number of spaces identified, particularly given that the data table indicates numbers of bike parking (albeit erroneously for the long term). Reference the Site Plan for Ciclo on labeling the bike racks.

20. For the east elevation, staff notes a perspective that illustrates what appears to be two concrete benches along the multi-use path that are labeled on the elevations but there are no details. Staff questions the usefulness of these benches as they would likely be unappealing places to sit: between an east wall and a multi-use path and rail. Staff had formerly commented that there should be a means to access the building from this side of the multi-use path with a roll up door or other. The windows now shown have the appearance of a roll up door but wouldn't be functional for that purpose. Staff recommends the roll-up doors.

21. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT: The perspective shown below illustrates the second floor of the building projecting over the first floor and illustrates that there may be a view of the Flatirons available from the location of the image. The TVAP General Design Guidelines state, "consider opportunities to frame or preserve views of the Flatirons to the southwest." Therefore, a more definitive analysis of this potential should be performed using Google Earth. The projection of the second floor may impose on this viewshed and may not actually contribute to either framing the street or creating pedestrian interest when the second floor cantilevers over the first floor.

NOTE: The response the applicant provided was a new perspective illustrating a view from the Flatiron Townhomes across and above the street. While staff acknowledges the applicant for indicating the potential or preserving views from the front porches of the Flatiron Townhomes, there's no indication that any public viewshed from Valmont would be protected in this location. The applicant also didn't scale the cross-section shown of the perspective location so staff cannot cross-check the information. Because the view today, shown below (right) is broad from the rail to the existing building. As shown, only a view from the neighbors porch would be created. There's no attempt to create or frame a public view shed from this project. Therefore, as currently shown, staff cannot make findings that the proposed project would meet either this guideline or the site review criterion 9-2-14h(1)(A) that includes the following BVCP policy,

b) Relationship to the public realm. Projects should relate positively to public streets, plazas, sidewalks, paths, ditches and natural features. Buildings and landscaped areas—not parking lots—should present a well-designed face to the public realm, should not block access to sunlight, and should be sensitive to important public view corridors...

As staff noted previously, the Ciclo Building across the street is planned with a more “chamfered” corner. It may be more appropriate to pull back the corner for this building instead given the potential for capturing the viewshed corridor toward the Flatirons. Provide greater information on how best to capture this existing viewshed and preserve it from a public view corridor either from Valmont and/or from Junction Place (34th Street) given the height of the Ciclo building and the Timber Lofts. Provide a visual analysis that shows views of the Flatirons would remain moving through the site.



Valmont gateway, frame and viewshed

- 22. On the east elevation, the label for the steel door is cut-off- please clearly label.
- 23. On the Site Plan table, title the table, “Site Plan Data”
- 24. The Materials Palette illustrates a much darker wood for the “ghost wood” that’s noted as “translucent silver gray.” It may be the print but the example shown doesn’t match either the precedent images or the elevations and perspectives. Concern was raised by both staff and BDAB about the long term durability of the entire building finish materials being predominately the wood. The applicant did not respond to this comment.



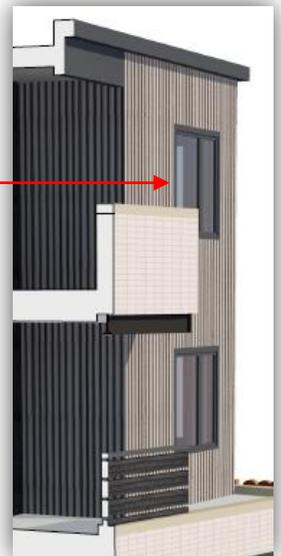
Address: 3390 VALMONT RD

25. The applicant did not address the BDAB comments, as shown in the elevation and the perspectives with regard to the following:
- a. overuse of arcades on buildings fronting the street and concern about the use of pilasters to hold up the majority of the building, consider bringing some of the building down to the ground in locations. The pedestrian experience is critical for the ground floor and the arcade would be dark and potentially uninviting, with shadows cast on the storefront windows. This would be even more pronounced with the heavy perforated, weathered steel projecting above the storefronts as sign bands.
 - c. As noted in BDAB, *“the wood looks really thin and is not convincing as a durable material for our climate for shedding moisture. With a little more detail at the cap at the top, soffit below, transition of materials and window placement, it might help gain confidence that the design details and their feasibility/execution are being considered at an early stage, particularly since the style is not traditional or predictable.”* Provide greater detail so staff and the decision makers can make a finding of consistency with the site review criteria.

Plan Documents: Ciclo Elaine McLaughlin, 303-441-4130, Sam Assefa, 303-441-4277

1. The applicant indicated that the, “FAR was confirmed and updated to be 2.0 FAR.” As noted previously, the minimum open space for residential lots is 15 percent with the minimum private open space for residential uses is 60 square feet per dwelling unit. The land use code that defines useable open space, section 9 -9-11, B.R.C. 1981 found [here](#), also notes, “any building over forty-five feet but less than fifty-five feet in height shall provide at least twenty percent of the total land area as useable open space.” Therefore, at least 20 percent of the total land area of the Ciclo site (presumed to be subdivided per the applicant) must be in useable open space. Within the data sheet on the site plan is the following notation: Open Space Percentage (no reduction requested) 63%” and that there is “15,981 square feet of open space” provided on the site. Given that please note the following:
 - a. Based upon the open space plan, the applicant is counting all areas at the ground level that don’t include the building as well as all of the elevated decks. However, to count as useable open space, the ground level area must meet section (e) of the above referenced land use code section. At a minimum, pedestrian amenities or other “useable” site elements.
 - b. To count toward useable open space, exterior paved surfaces must meet the following additional standards: *“(B)... to enhance the use of such areas, the pave areas shall include passive recreation amenities which include without limitation, benches, tables, ornamental lighting, sculpture, landscape planters or moveable planting containers, trees, tree grates, water features or active recreation features...”* As currently shown on the site plan, there are just two tables with chairs, and this differs from the landscape plan that shows “amorphous seating” and only is shown in the area where the woonerf extends into the site.
 - c. Note that roof deck is shown differently on the landscape plan than on the open space plan and that appears to have different open space area that would count toward the required 20 percent useable open space.
 - d. Per the land use code section 9-9-11(f), B.R.C. 1981, the individual balconies, decks and patios on the site, *“shall count for no more than twenty-five percent of the required useable open space”* therefore, recalculate the actual areas of open space based on this criterion. Note that the percentage of open space will likely be significantly reduced from 63 percent as was noted on the site plan and open space plan. Revise these sheets as necessary.

- e. In the table on the site plan, under “density” and “open space” please remove the phrase, “no reduction requested” as there is no open space reduction/land use intensity modification permitted in MU-4 zoning.
2. Please revise the Table on the Site Plan in the following manner:
 - a. Title the table, “Site Plan Data”
 - b. All proposed minimum setbacks must be revised consistent with the site plan: none of the “proposed setbacks” should be labeled as being “0”, as shown on the site plan, none of the building is shown to be located at the property line note that even the interior side yard setback is labeled on the site plan to be 3’-5”. Therefore, revise the table to be consistent with the actual proposed setbacks shown on the site plan.
 - c. Remove any distinction between “Residential Density” and “Non Residential Density” and provide instead a tally of the overall density of the building – there is no distinction in density per land use within the MU-4 zoning. However, keep the tally of floor area of each use within the table.
 3. On the Site Plan, label the spot elevation where height is to be measured as “Low Point for purposes of measuring height.”
 4. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT: Provide cross-sections through the building to help convey the mezzanine in relation to the ground floor. It is understood the portico has been removed.
 5. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT: Use of vinyl windows and window frames without a return or reveal are discouraged. Note that the axo-section illustrates a flush mounted window. Please revise.
 6. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT: For the corner Gallery/Flex Space note that the two story height appears truncated for a corner element. Staff recommends pulling the space up to the full height of the building. This was also expressed by BDAB.
 7. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT: Note that signage can’t be approved through a Site Review application, rather through separate sign permit. However, staff understands the desire to integrate the signage into the architecture. Therefore, please “ghost” in any of the signage on the elevations and label them as “Sign, not a part of Site Review application.”
 8. BDAB discussed ways of simplifying the corner of Valmont and Junction Place (34th Street). However, in comparison to the earlier concept, the corner appears to have become more complicated. Please revisit the BDAB comments and look at ways to simplify the building forms. Also on this corner, the window openings don’t appear to be proportional to the walls that they are on, and for the corner that combined with the flush mounting detail of the windows and the use of corrugated metal adds a less-than durable appearance.



- Staff finds the use of corrugated metal as a primary building material would challenge the ability to make findings of consistency with the Site Review criterion that states, “Exteriors of buildings present a sense of permanence through the use of authentic materials such as stone, brick wood, metal or similar products and building material detailing.” While “metal” is mentioned as a material within this criterion, corrugated metal has a less durable appearance therefore the use of this as a primary building material should be reduced.



Plan Documents: Railyards Elaine McLaughlin, 303-441-4130

- BDAB recommended reducing the use of CMU throughout the development. Staff highly recommends the applicant use a simple palette of materials of brick –rather than CMU mixed with the wood. This would create a greater distinction of this building from the other buildings in the development.
- Because the applicant indicated in the resubmittal that an application for a Use Review for the restaurant at the Railyards would be submitted at a later date, remove any outdoor seating shown to avoid implication that a restaurant would be permitted in this location without the benefit of the Use Review. A label could be added that indicates, “potential future outdoor dining: Use Review to be submitted through separate application.”
- On the axonometric section through the building shown in the Design Excellence page, specifically label the materials with leader lines that go from the descriptions to each material shown.
- INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT FOR THE SIGNAGE ON THE WEST ELEVATION:
Note that signage can’t be approved through a Site Review application, rather through separate sign permit. However, staff understands the desire to integrate the signage into the architecture. Therefore, please “ghost” in any of the signage on the elevations and label them as “Sign, not a part of Site Review application.”
- In Concept Plan discussions there was a desire to have access, or at least visual permeability, into the building from the multi-use path along the tracks. At the time, the discussions included the possibility of “roll-up” doors or other accesses into the building to activate that side the public multi-use path. The concern is that there would be a nearly 300 foot long blank wall along the multi-use path adjacent to the tracks creating a lack of visibility and “eyes on the street” as well as a lack of activity along a public way.
- The applicant did not address the BDAB comment about the overuse of arcades on buildings fronting the street and concern about the use of pilasters to hold up the majority of the building. Therefore, consider bringing some of the building down to the ground in locations. The pedestrian experience is critical for the ground floor and the arcade would be dark and potentially uninviting, with shadows cast on the storefront windows.



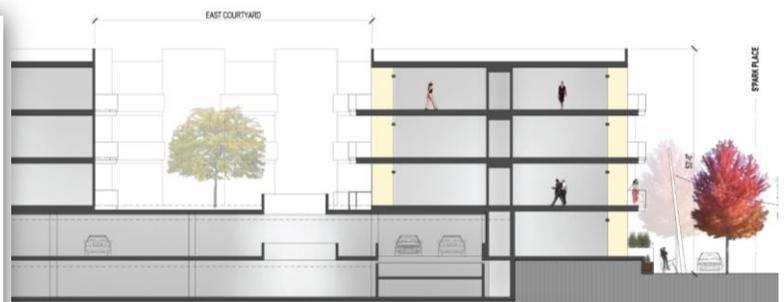
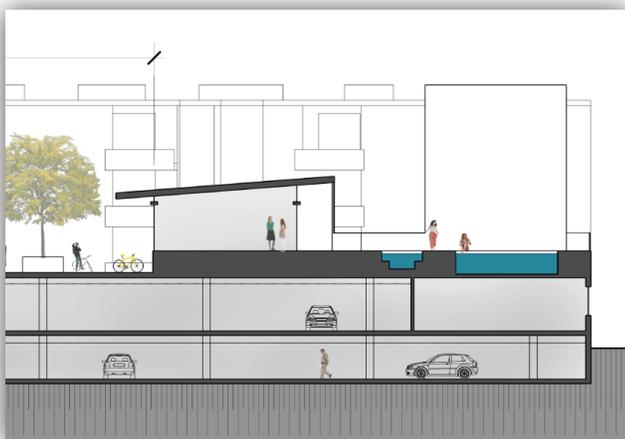
Plan Documents: Timber Lofts/Townhomes Elaine McLaughlin, 303-441-4130

- The plans illustrate a property line for the subject site however an application for a preliminary plat if that is the intent.
- For mechanical screens note that the land use code discourages tall building elements to screen the mechanical,

“Screening does not increase the apparent height of the walls of the building. The use of parapet walls to screen mechanical equipment is discouraged. The height of parapet walls should be the minimum necessary to screen mechanical equipment.”

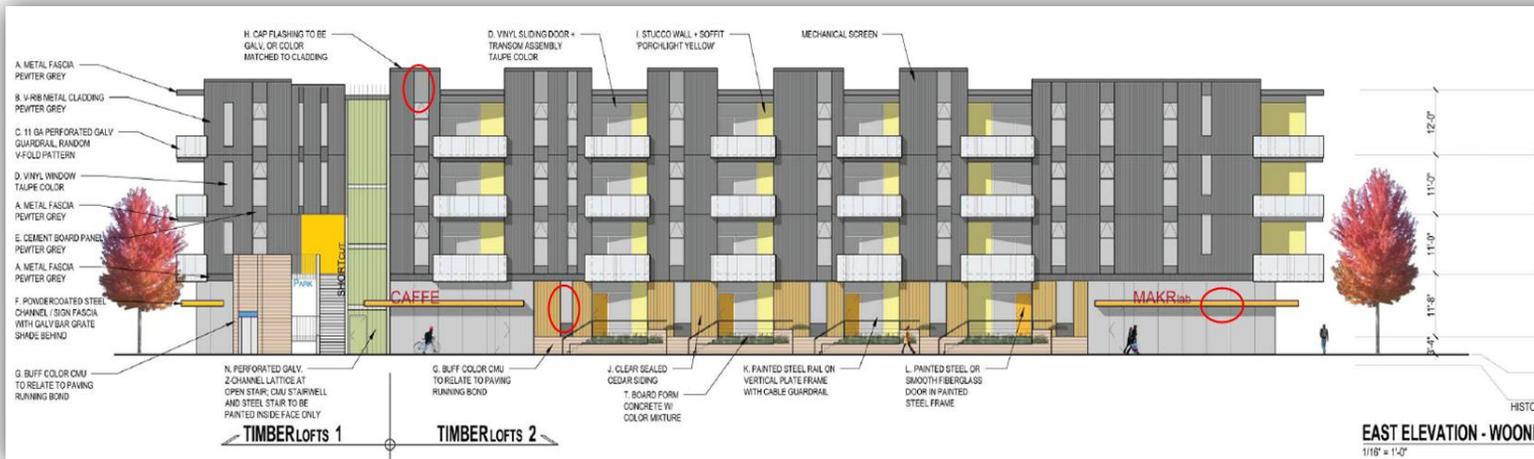
Staff does not concur with the applicant’s assessment that the parapets server to further a “sense of residential or pedestrian scale, in the rhythm of the building” as the parapets would not be perceptible at the pedestrian level but rather from a distance – which would increase the “apparent height.” Therefore, reduce the height of the parapet.

3. While the applicant clarified what the actual difference is between Timber Lofts 1 and Timber Lofts 2, the suggestion is that rather than labeling the distinction as numbers that implies a phasing, staff suggests the applicant instead refer to them as “Timber Lofts Apartments” and “Timber Lofts Townhomes” for Site Review purposes.
4. **INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT B-2 Floor Plan:** add labels to the plan including parking. All spaces that appear to be enclosed with walls must be labeled. Note that there are three spaces in a yellow-orange color on the west side that have doorways but no labels. WHILE THE APPLICANT PLACED LABELS ON THE PARKING SPACES, NOTHING ELSE IN THE PARKING AREA IS LABELED.
5. On the L-1 Floor plan correct the labels that are combined.
6. It is not clear if there’s an intent to create a hallway between the a parking and the units on the L-1 level or if the units simply step out into the parking area. One means to clarify this is to color or shade the entire parking area one color of grey.
7. On the building sections, label the different areas as “residential unit living room” or “office” to clearly indicate the different levels.

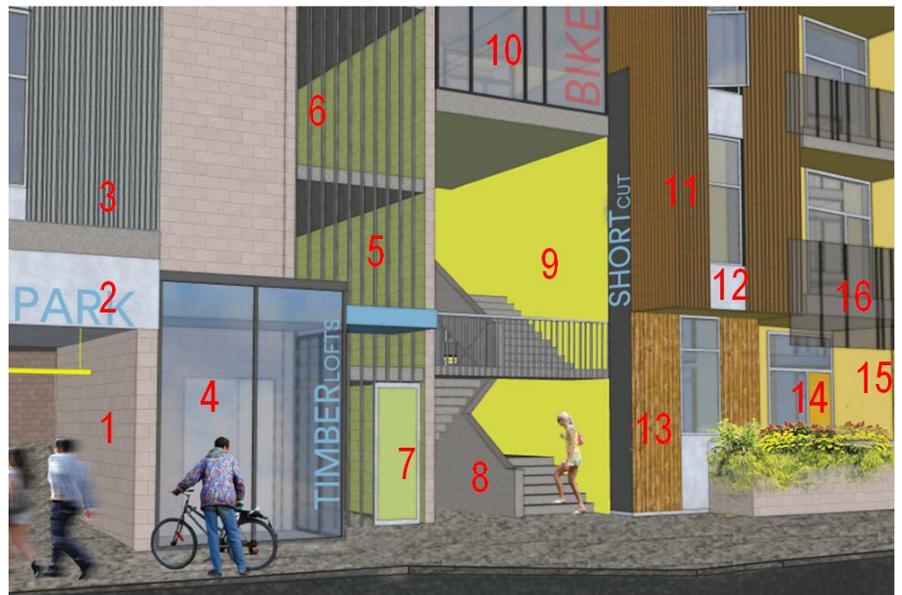


PROVIDE LABELS OF GROUND FLOOR USES TO DISTINGUISH THE VARIOUS SPACES

8. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT: On the Level 1 Plan, indicate the USGS spot elevation for the low point of the building for the purpose of measuring height based on the city's standard found [here](#). NOT SHOWN.
9. Elevations: Label the USGS base height elevation (from the lowest point shown on the site plan) and label the high point of the roof with the USGS height along with a calculation of the height in feet from the low point to the high point on the building. Note that "historic grade" as labeled on the elevations is not relevant as a point from which to measure the height of the building. Use the City's standard for measuring height found [here](#).
10. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT: provide a detail of the types of windows proposed for the residential units. THE APPLICANT REFERRED TO THE Design Excellence page for "intent of fenestration." However, site review differs from concept plan review in that actual details and specifications must be provided rather than "intent" statements.
11. While the applicant did provide an axonometric section of the windows proposed for the residential units, the illustration does indicate flush mounted windows. This is not considered an element of Design Excellence, and a reveal must be provided.
12. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT: Label any material not currently labeled and as circled below.

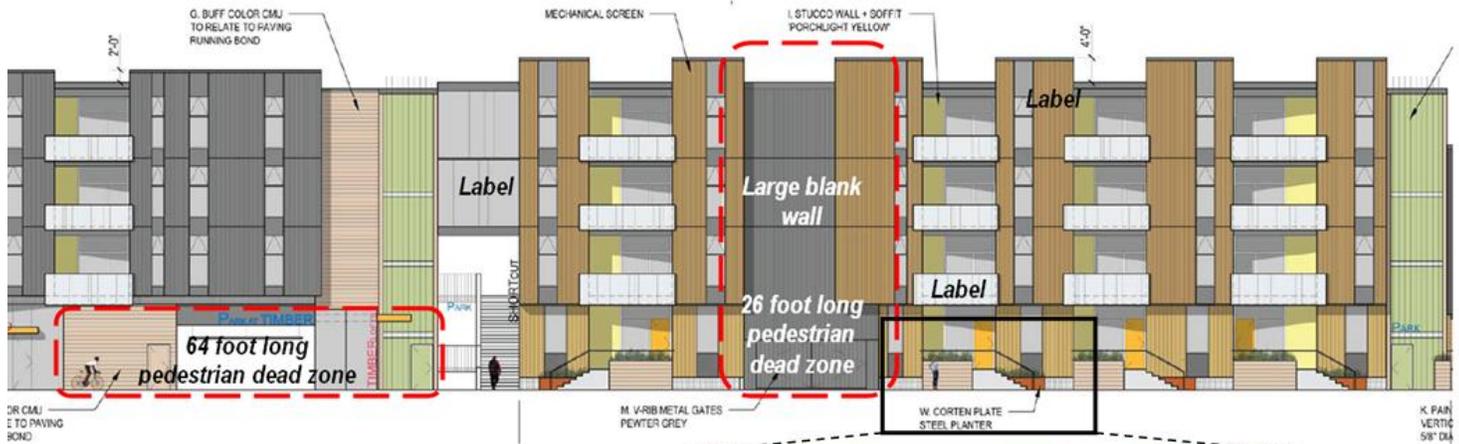


13. Label all of the materials shown on the axonometric section of the Design Excellence page (key the descriptions to the drawings).
14. The illustration of the north side of TimberLofts shown to the right, illustrates a very complicated combination of material types and colors. There is also concern about the quality of materials used and both BDAB and staff noted the overuse of CMU. Staff views this as a critical location – entering the site from Valmont Road and near the Woonerf, and for that reason will be a focal point. There are far too many materials employed, therefore, simplify the materials palette in this location.



15. The open stair that is located on the north side of the building will be in shadow all year around. In the winter, this type of open stair configuration would not only be cold, but likely slippery and predictably dangerous with ice. The applicant should indicate how to address this concern and may instead want to consider a glass curtain wall for this staircase rather than an open staircase on this north side.

16. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENTS:



The five-foot height of the entry patios with planters, while creating “defensible” space, creates a pedestrian barrier. The use of buff-colored CMU would likely not create a strong pedestrian experience.

The other materials in combination could enhance the overall pedestrian experience if the wall height were lowered and/or the stoops were turned 90 degrees to face the street to create more of an inviting appearance from the view of the pedestrian.

Label the windows on the Timber Townhomes.

In a similar manner, the Timber Common has window heights that wouldn't serve the pedestrian experience, shown below.



17. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT: The signage shown must meet the sign code. Note that the size of the sign may include the band upon which the lettering is mounted. However, signage should be simply “ghosted” in on the elevations as a separate sign permit is required.

18. The use of CMU on the stoops of the townhomes doesn't lend a sense of permanence and is not viewed as a human-scale material. BDAB had echoed this concern. Staff recommends use of brick in these locations particularly given the height of these walls along the street.

Plan Documents: Meredith House Elaine McLaughlin, 303-441-4130

1. Staff notes the responsiveness to comments from the previous submittal particularly with regard to the activation of the streetface with a use within the center of the groundfloor that will create greater pedestrian interest than the storage

units previously proposed in that location. There are some remaining comments that follow.

2. On the ground floor plan, provide the base height elevation (USGS datum) from which the overall height of the building is measured per the city's standards for measuring height found [here](#) and that correlates to the call-outs indicated on the building elevations.
3. The "Meredith House" signage can't be approved through Site Review, therefore "ghost" it in graphically on any plans that illustrate it and indicate, "signage under a separate permit."

Plan Documents: S'PARK_west (3155 Bluff) Perm. Affordable Residential Elaine McLaughlin, 303-441-4130

The concept plan comments for 3085 Bluff (then the affordable units) recommended several important considerations for project plan refinements. Staff includes these comments in the following, with a notation on whether or not the plans responded to these recommendations.

1. *The windows read neither as for a loft building nor townhome. Lofts have regular, repetitive, large windows. Townhomes have several proportions and sizes that express interior function, i.e., living room, bed room, bathroom, etc. The individual windows, as well as the overall composition, currently lack that kind of clarity.*
Staff finds this comment was not addressed.
2. *The two square blocky masses on either side appear more institutional rather than residential. The relationship of these blocky masses to the wings on either end as well as the mid section should be refined.*
Comment was addressed.
3. *While the modulation/articulation of the mass in the mid section seems to indicate some relation to some of the buildings in Steelyards, the overall composition appears to lack pedestrian interest. Elements that articulate base middle and top, including cornice line, as well as and some consistent horizontal and/or vertical window rhythm should be considered.*
There's no clear articulation of base-middle-top in the buildings except for use of CMU on the ground floor. There needs to be a terminus at the top of the roof, either a roof form or a cap.
4. *The projecting entry elements should be better integrated into the design of the rest of the building.*
No longer applicable.
5. *Ensure use of high quality materials that provide a sense of permanence and pedestrian interest.*
Both staff and BDAB questioned the extensive use of CMU throughout. This used in combination with the stucco and ribbed metal siding do not create a sense of permanence or pedestrian interest.

In general, there are a number of outstanding items that must be addressed prior to a finding that the application meets the Site Review Criteria. Among them are related to the areas intended to be counted as "useable open space" as well as the design of the units, exterior materials and colors.

1. The Open Space plan provided appears to include driveways in most locations but not all locations. There's no information on how the applicant intends to utilize the driveways to count as useable open space. Useable Open Space, reference the definition of Useable Open Space under the Land Use Code [here](#).
2. Regarding the Open Space Plan, there are two sets of lines shown surrounding the "Blocks" however neither appears to be the property lines, ROW, or easements. Therefore, it's not possible to verify what the applicant is intending to count toward open space.
3. On the Open Space Plan: please indicate the overall "required" and the overall "proposed" open space.

4. On the Site Plan, clearly label which units are proposed to be townhomes and which are proposed as flats.
5. There remain areas on the plan, where there is less than two feet of area separating the units and the walkways. These areas are not considered plantable and wouldn't count toward useable open space. Where the building has essentially a zero lot line up to a sidewalk, but there is a stoop or porch accessing the walk, it may be workable and livable. However, where the building abuts the walkway with no space in between, there's little in the way of livability. This appears to occur along Junction Place, and along Meredith Street. Staff strongly recommends creating greater separation from the walkway with the buildings by providing enough space to a meaningful landscape that would soften the building's placement near the walkway.
6. INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT: Label the direction and location of the perspective sketches, there needs to be a key to illustrate what location the sketches are intended to illustrate.
7. The windows mounted flat against the surface of the exterior material, in particular the stucco, is not considered high quality material and design and wouldn't meet the Site Review criteria. Revise with a reveal for the windows. The use of vinyl windows are also discouraged, particularly with the use of the other materials that don't create a sense of durability or permanence.
8. While staff acknowledges the referencing to the industrial use of the site in the past, the use of the grey CMU as a primary building material, particularly at the entrances to the units, wouldn't meet the Site Review criteria for human scaled material or high quality durable material. BDAB also articulated concern about the overuse of CMU throughout the entire project. Where the buildings are residential and small in scale, staff recommends revising many of the areas currently shown as CMU to the more human scaled brick as a primary building material. When zooming into the streetscape shown below, the CMU finished buildings that are at a zero lot line to the sidewalk create a pedestrian dead zone, where there's virtually no pedestrian scale materials. The result, as can be seen in the enlargement below, is an institutional appearance.



Institutional Appearance due to over-use of CMU at the ground level of the Permanently Affordable Units.

9. Staff agrees concurs with many points raised by BDAB regarding 3055 Bluff and the affordable project. BDAB recommended simplifying the building elevations for 3055 Bluff stating, *“there’s a lot of movement in these elevations with different materials and volumes.”* And, *“the massing could be simpler which would lead to the selection and transition of materials.”* BDAB encouraged the applicant to simplify the buildings and window patterns. The applicant did not address this comment. A suggestion at BDAB also included the following,

“the market rate units work well and have a nice, simple pallet of materials and colors. The affordable housing could benefit from taking the color pallet from the market rate units and tie the two color pallets together between the two. This would simplify the whole neighborhood.”

BDAB further recommended creating a “beefier roof line or a visible roof” and noted that *“the stucco is uncapped at the tops of the buildings and that it makes them look cheap and unprotected from the elements.”* They concluded that, *“this is a major Design Excellence point.”*

Similarly, as noted previously, Planning Board , at the Concept Plan review hearing held on Sept. 4, 2014 indicated similar concerns about the aesthetic of the buildings as noted in this excerpt from the minutes,

Some members would prefer to see more gabled roofs for a homier and less industrial aesthetic. Other members thought that a mixture of styles and rooflines would be appropriate. Flat roofs could accommodate terraces. Most members would like to see a simplified and more restrained aesthetic but with a charming character. Look at the Holiday neighborhood for ideas.

It’s also important to note that at the time of Concept Plan review, there were representations of some type of gabled roof element on the elevations. That concept seemed to blend well with some of the existing Steelyards residential that also have some variation through use of pitched roofs. The applicant is highly encouraged to design-in some roof forms so that the between the 69 units of SPARK_west there is some roof line variation.



10. On the above elevation, note that the windows on the ground floor on the east, don’t align with the upper story windows.

11. The elevations don't match the perspectives, and neither matches the materials pallet. The CMU in the perspectives appears grey, while elevations appear red, and the sample appears greyish red. Understandably, there may be variation due to print quality and color saturation with different graphics rendering programs. However, there needs to be consistency in the presentation to convey the proposal. Staff's recommendation is to provide precedent images of the materials and a sample board to staff prior to resubmittal or well before BDAB. Staff also recommends not using CMU along the ground level of the buildings, and where a material type is used that references the materials palette that the specific type be referenced.



12. There are random material changes mid wall, it is recommended to help simplify the character of the buildings, that the applicant transition materials at wall breaks, vertically, rather than at a mid-point on the wall face, as is shown above.
13. Provide a detail of the underside of the decks. Note that exposed floor joists will not be permitted, and a greater level of finish is expected of the underside of the decks.
14. For the axonometric section, place leader lines from the descriptions to the materials and elements called out.

Plan Documents: S'PARK_west (3085 Bluff) Townhomes

1. Elevations require a scale.
2. Staff notes the applicant's responses to both staff and BDAB for both simplification of the individual building massing along with variation in design of the different buildings. There are several remaining concerns in this regard:

- a. BDAB noted that "there is a lot of movement in these elevations with different materials and volumes" and the recommendation was to simplify them. One of the suggestions was to simplify the elevation on the ends of the buildings as they get a little busy in terms of massing and overhangs. These concerns remain with the resubmittal as is shown to the right.



- b. Also with regard to the illustration, it appears to show a railing atop the third story roof. If this is intended to be a roof top deck, please indicate that on the project plans and include the height of the railing into the overall height of the building.

- c. Another remaining concern is with regard to the flat and boxy roofs. The flat roofs overall, particularly with the two and three story buildings when planned over the entire SPARK_west site with 69 units, creates a lack of variation. Staff recommends the applicant look at pitched roofs for certain buildings to decrease the overall uniformity of roof forms for SPARK_west.
3. The narrow band windows on the top floor seem out of context and proportion to the individual walls that they are located on. The applicant should consider other window forms for the top floor.
4. For the axonometric section, place leader lines from the descriptions to the materials and elements called out.

Neighborhood Comments Elaine McLaughlin, 303-441-4130

The applicant is acknowledged for reaching out to surrounding neighbors. Please note that the Use Review for the various restaurants and the Brew Pub require a Good Neighbor meeting that complies with the protocol established in the land use code section 9-2-4, B.R.C. 1981 found [here](#) as noted previously. The applicant is required to host the meeting and must coordinate with staff on an appropriate time and location. City staff will send the public notification out to the neighborhood once a time and location is established. The applicant must provide notice to staff no later than two weeks prior to the meeting to ensure adequate notification time. This effort will be necessary prior to the Planning Board hearing, and staff recommends a time near the end of July.

Signage Elizabeth Lokocz, 303-441-3138

Please note that all proposed signs require separate review and permit approval. While it is preferable to remove all signs from the Site Review and Technical Document plan sets to avoid any potential future confusion, ghosting the images into the set with a notation that it is under a separate permit is acceptable. Please note that illustration of a sign on the plan set does not grant a modification. Modifications are limited to setbacks and separation between freestanding signs. Any modification shall be specifically called out. No increase in overall allowed signage, height of signs or number of freestanding signs, to name a few common requests, is feasible. Please refer to [section 9-9-21 B.R.C. 1981](#) for all sign related requirements.

Solar Access Elaine McLaughlin, 303-441-4130

INCOMPLETE RESPONSE TO THE FOLLOWING COMMENT:

1. The solar analysis has no scale and doesn't illustrate the specific requirements of a solar analysis. An Excel worksheet is attached to the comments for use to delineate specific solar shadow lengths. Because both Ciclo and Markt, do appear to be close to creating a solar encroachments from across Valmont to the properties that are located within Solar Access Area I (the Mobile Home – MH zoning district) and Solar Access Area II (the Residential Medium – RM-1 zoning district), a more definitive analysis is required. Therefore, please utilize the attached worksheets and resubmit with a corrected solar access analysis for staff and the decision makers to confirm if there is or is not an actual encroachment onto these protected properties. Note that the analyses must indicate specific shadow lengths produced by specific points on each proposed building. Note that two separate Solar Access analyses should be completed, given the two different hypothetical Solar Fence heights of the Solar Access Area I and Solar Access Area II.

Utilities Scott Kuhna, 303-441-4071

1. Trees need to be located at least 10 feet away from existing or future utilities. The following utility lines (or trees) were identified as not meeting separation requirements.
 - Proposed tree northeast of Building 1 – Proposed wastewater service line
 - Proposed tree south of Building 2 – Existing fire service line
 - Proposed fire hydrant south of Timber Lofts – Proposed rock chair
 - Proposed trees (8) east of Timber Lofts – Proposed storm sewer line and inlets
2. The existing wastewater service line for the “Existing Brick Building” is not shown on the utility plans. Revise accordingly.

3. Review and coordinate Landscape comment #10 regarding utility and tree locations. Submit a DCS variance request for any separation that does not meet the ten foot minimum. (Elizabeth Lokocz; 303-441-3138)

III. INFORMATIONAL COMMENTS

This section addresses issues that are for the applicant's reference but are not required to be resolved prior to a project decision or as a condition of approval. Informational Comments are organized by topic area so that each department's comments of a similar topic are grouped together. Each reviewer's comment will be followed by the reviewer's department or agency and telephone number. Reviewers are asked to submit comments by section and topic area so that the comments can be more efficiently organized into one document. Topics are listed here alphabetically for reference.

Addressing, Sloane Walbert, 303-441-4231

The City is required to notify utility companies, the County Assessor's office, emergency services and the US Post Office of proposed addressing for development projects. Please submit a Final Address Plat and list of all proposed addresses as part of the Technical Document Review process.

Drainage Scott Kuhna, 303-441-4071

1. A Final Storm Water Report and Plan will be required as part of the Technical Document Review process. All plans and reports shall be prepared in accordance with the City of Boulder *Design and Construction Standards*.
2. At time of Technical Document Review, the applicant shall submit information (geotechnical report, soil borings, etc.) regarding the groundwater conditions on the property, and all discharge points for perimeter drainage systems must be shown on the plan. The applicant is notified that any proposed groundwater discharge to the city's storm sewer system will require both a state permit and a city agreement.
3. Floor drains internal to covered parking structures, that collect drainage from rain and ice drippings from parked cars or water used to wash-down internal floors, shall be connected to the wastewater service using appropriate grease and sediment traps.
4. All inlet grates in proposed streets, alleys, parking lot travel lanes, bike paths, or sidewalks shall utilize a safety grate approved for bicycle traffic.
5. A construction stormwater discharge permit is required from the State of Colorado for projects disturbing greater than 1-acre. The applicant is advised to contact the Colorado Department of Public Health and Environment.

Groundwater Scott Kuhna, 303-441-4071

Groundwater is a concern in many areas of the City of Boulder. Please be advised that if it is encountered at this site, an underdrain/dewatering system may be required to reduce groundwater infiltration, and information pertaining to the quality of the groundwater encountered on the site will be required to determine if treatment is necessary prior to discharge from the site. City and/or State permits are required for the discharge of any groundwater to the public storm sewer system. It should be noted that the Installation of underground utilities may also provide a conveyance for any contaminated groundwater associated with the properties.

Miscellaneous Scott Kuhna, 303-441-4071

1. The applicant is notified that any groundwater discharge to the storm sewer system will require both a state permit and a city agreement. The steps for obtaining the proper approvals are as follows:
 - Step 1 -- Identify applicable Colorado Discharge Permit System requirements for the site.
 - Step 2 -- Determine any history of site contamination (underground storage tanks, groundwater contamination, industrial activities, landfills, etc.) If there is contamination on the site or in the groundwater, water quality monitoring is required.

Step 3 -- Submit a written request to the city to use the municipal separate storm sewer system (MS4). This submittal should include a copy of the Colorado Department of Public Health and Environment (CDPHE) permit application. The written request should include the location, description of the discharge, and brief discussion of all discharge options (e.g., discharge to MS4, groundwater infiltration, off-site disposal, etc.) The request should be addressed to: City of Boulder, Stormwater Quality, 4049 75th St, Boulder, CO 80301 Fax: 303-413-7364

Step 4 -- The city's Stormwater Quality Office will respond with a DRAFT agreement, which will need to be submitted with the CDPHE permit application. CDPHE will not finalize the discharge permit without permission from the city to use the MS4.

Step 5 -- Submit a copy of the final discharge permit issued by CDPHE back to the City's Stormwater Quality Office so that the MS4 agreement can be finalized.

For further information regarding stormwater quality within the City of Boulder contact the City's Stormwater Quality Office at 303-413-7350. All applicable permits must be in place prior to building permit application.

2. No portion of any structure, including footings and eaves, may encroach into any public right-of-way or easement.

Residential Growth Management System, Sloane Walbert, 303-441-4231

Growth management allocations are required to construct each dwelling unit prior to building permit submittal. Please be advised that an agreement for meeting city affordable housing requirements must be in place before a Growth Management Allocation can be issued.

Utilities Scott Kuhna, 303-441-4071

1. The applicant is advised that any proposed street trees along the property frontage may conflict with existing utilities, including without limitation: gas, electric, and telecommunications, within and adjacent to the development site. It is the applicant's responsibility to resolve such conflicts with appropriate methods conforming to the Boulder Revised Code 1981, the City of Boulder Design and Construction Standards, and any private/franchise utility specifications.
2. Final utility construction drawings will be required as part of the Technical Document Review process (which must be completed prior to building permit application).
3. Further detail of the ground water barriers used to prevent ground water migration or diversion along the water, wastewater, and storm sewer mains will be required at time of Technical Document Review.
4. Maintenance of sand/oil interceptors and all private wastewater and storm sewer lines and structures shall remain the responsibility of the owner.
5. The landscape irrigation systems requires a separate water services and meters. A separate water Plant Investment Fee must be paid at time of building permit. Service, meter and tap sizes will be required at time of building permit submittal.
6. The applicant is advised that at the time of building permit application the following requirements will apply:
 - a. The applicant will be required to provide accurate proposed plumbing fixture count forms to determine if the proposed meters and services are adequate for the proposed use.
 - b. Water and wastewater Plant Investment Fees and service line sizing will be evaluated.
 - c. If the buildings will be sprinklered, the approved fire line plans must accompany the fire sprinkler service line connection permit application.
7. All water meters are to be placed in city right-of-way or a public utility easement, but meters are not to be placed in driveways, sidewalks or behind fences.
8. Trees proposed to be planted shall be located at least 10 feet away from existing or future utility mains and services.

IV. NEXT STEPS

Resubmit five sets of revised plans, along with a letter that provides an explanation on how the plans respond to each comment. Also provide a disc with the files of the revisions and letter. These must be provided to the Project Specialists at the front counter, 3rd Floor Park Central Building at the beginning of a review track, the first or third Monday of the month before 10 a.m.

V. CITY CODE CRITERIA CHECKLIST

To be provided upon a review of revisions.

VI. Conditions on Case

To be provided upon a review of revisions.

**CITY OF BOULDER
BOULDER DESIGN ADVISORY BOARD MINUTES
April 8, 2015
West Conference Room, 1777 Broadway**

A permanent set of these minutes and a tape recording (maintained for a period of seven years) are retained in Central Records (telephone: 303-441-3043). Minutes and streaming audio are also available on the web at: <http://www.bouldercolorado.gov/>

BDAB MEMBERS PRESENT:

Jamison Brown, Chair
Jeff Dawson
David McInerney
Jim Baily
Michelle Lee

PLANNING BOARD EX-OFFICIO MEMBER PRESENT:

Bryan Bowen

STAFF PRESENT:

Sam Assefa, Senior Urban Designer
Elaine McLaughlin, Senior Planner for Planning & Development Services

1. Board Matters:

James Baily and **David McInerney** took their Oaths of Office as new BDAB members.

The board elected **Jamison Brown** as the new board Chair and **Jeff Dawson** as the Vice Chair.

2. S'PARK Project Review

BOARD COMMENTS:

Overall Plan

J. Brown noted that symmetrical streets are important for creating an attractive public space and that the streetscape treatment and ground floor program/use should be similar on both sides of a street.

- The sidewalk layout varies from block to block and often does not align at intersections. He encouraged the applicant to consider revising to create better uniformity and connection.
- He recommended expanding the woonerf to include the cul-de-sac and the service drive to better link to the plaza at Market Building.
- The use of the arcade on several buildings seems forced and unnecessary especially on the North/East oriented facades.
- All of the buildings use CMU block as a primary building material. It may not meet the guideline of using "pedestrian scale" materials.

J. Baily was less concerned about the CMU. If done with some warm colors and variation in texture he thought it could work.

J. Dawson had one major concern with the architectural approach where a lot of the materials are held up from the ground with pilasters. He asked the applicant to look for opportunities to bring materials down to the ground in some locations.

J. Baily had some concerns with the building materials and thought that the pallet could be simplified. He shared the concern with other board members regarding the durability of the materials such as the untreated wood.

D. McInerney, with reference to the General Guidelines compliance matrix, noted that the "Useable Open Space" subtopic calls for incorporating access to sunlight. He thought that access to sunlight will be particularly important at the S'PARK Place woonerf and requested that the applicant expand the shade analysis to include diagrams for 8 AM, noon, and 4 PM for the summer solstice, winter solstice, and an equinox.

- He also inquired about the life expectancy of the Maarket and the Timber buildings. Based on the applicant's response that the estimated life cycle of the buildings is 30 years, he expressed concern about the durability of the wood included in the design of the building exteriors.

M. Lee thought that the community benefit S'PARK can bring is to create safe, easy, green connections for bikes and pedestrians, with particular attention to the multi-use path on the east side. Pedestrians and cyclists will probably cut through between the Maarket and Railyards to get to 34th Street if they see activity happening, even though it is planned as a loading dock. The drawings should show the train, multi-use path with cyclists and pedestrians in their drawing sections so there is also a reference to scale and proximity.

- She noted that the 2 PM solar analysis does not show any shadows cast from Ciclo which does not seem accurate. It brings up a concern about how much of the day the plaza will be in shadow. If there is no sun getting to the plaza, it will be dead zone with no activity.
- She also suggested including a community pool similar to the Spruce Pool rather than an exclusive amenity.

Maarket Building

J. Brown - Although the building is essentially still in concept design, he liked the direction very much and supported the idea of a signature building on that corner. There is great programming for this space and it could be a common place for this and surrounding developments.

- He had concerns that the applicant stated the lifespan of the building was only 20 years.
- The applicant should consider the use of sliding or overhead doors to increase the connectivity between the large plaza and the food court type use on the first floor.

J. Dawson commented that detailing is very important on this building given the thin and somewhat temporary nature of the proposed materials.

M. Lee - As currently shown, she noticed that the rendering looks dark under the arcade and not particularly inviting. The wood looks really thin and is not convincing as a durable material for our climate for shedding moisture. With a little more detail at the cap at top, soffit below, transition of materials, and window placement, it might help gain confidence that the design details and their feasibility/execution are being considered at an early stage, particularly since the style is not traditional or predictable.

- She also noted that the main entrance faces 34th Street and that the Market does a good job of not turning its back on Valmont by providing a lot of transparency and a raised outdoor seating deck at the corner.
- Despite intending to be simple, this building is very complicated. There is a lot going on at the roof level and she believed that there should be more emphasis at the street level. This project needs to attract restaurant and retail tenants at the ground level. These tenants tend to need bigger mechanical units so the roof may need to be simpler to accommodate these future uses.
- She thought that the rendering might be more convincing, in terms of building quality, if the perspective were zoomed in closer at a pedestrian eye level. Without having to detail the entire building, perhaps the applicant can zoom in and crop to a portion of the building (the Meredith Building had a good example of the level of detail desired)

Timber Lofts

J. Brown recommended the applicant consider a live/work program for the townhouses that front the woonerf.

- Simplify the façade of the flats buildings by using less materials.
- Wrap the facade of the townhouses to hold the corner better and to more harmoniously relate to the building across the street. Avoid large blank side elevations that don't address the street.
- Consider moving the transformer so that the façade along Junction Place is less choppy.
- As designed, the elevated interior park is not inviting to pedestrians on the street and is unlikely to be used as a "cut through" or "short cut."

M. Lee recommended that more attention be given to how materials wrap corners and how materials transition from one to another. She specifically noticed the townhouse on the northwest corner where a little rhythm was lost and she hoped that the applicant simplifies the elevation.

The Meredith

J. Brown thought that the ground floor of the central “building” should be programmed space that is open to the street. Strengthen the connection between the parklet and the community room. **J. Baily** agreed.

There will be a continuation of this project review on Wednesday, April 15, from 4 – 6 PM.

Attached: April 8, 2015 notes from Kevin Knapp of Element Properties (included due to the failed recording)

APPROVED BY


Board Chair

6.10.15
DATE



April 8, 2015

Boulder Design Advisory Board (BDAB) Notes

4:00-6:00

5:15 – Bryan Bowen (Planning Board rep on BDAB) had to leave early for the PB Retreat and offered general comments before the applicant presentation was completed.

- Overall the design is great so I'm not going to pick that apart.
- Having the brewpub on the north side of the Markt building is great for activation. Make it as permeable as possible.
- Ciclo is great and working so no further comments there.
- Make sure ground floor timber residential units have access to the street. Will help activate and make the place.
- Bowen encouraged the project to open up the courtyard space at Timber Lofts to the public.

Jim Bailey - Units west of 34th St., where's the parking? Adrian answered.

5:40 Jamison Brown(chair) – Let's start with the Board's general comments.

Jeff Dawson – Asked Adrian to discuss transit parking comments from staff.
Elaine discusses the image of the pearl street section included in the staff comments.

Jeff Dawson – Asked if the applicant is willing to make ground floor space residential units.

Jamison – From a massing and scale standpoint you have been sensitive to the streetscapes. Nervous about the front of some townhomes looking to the sides of others. Loves the Woonerf and would like to see it expand. Recommended that Roundabout become part of the Woonerf. Has an overall recommendation to look at the pedestrian circulation throughout. Make sure it works.

Jeff Dawson – Recommends that there's no commercial space on the ground floor of Meredith Lofts.

Jamison – Or pull the office of Timber around to the north elevation.

5:53 Dawson - Gets a feeling that all the commercial buildings on the east side are afraid of the ground. Rarely does the building come down to embrace the ground.



Dawson suggests that all of the Railyards building be brick. He's struggling with the inter-lacing of materials.

Jamison - Has the exact same comment as Dawson. Worries about arcades and CMU. He likes the pattern language among the buildings and the use of different architects.

Jamison - Like the Community Cycles Indoor/outdoor workspace.

Dawson - There's a first floor and then a dramatic change everything above first floor.

Jamison - Big fan of the Markt Building. That architecture works in contrast to Simplicity of Meredith House that he really likes for setting the tone of the background. Timber veering away from background and is exciting which isn't necessarily bad. Timber could be a little quieter and have Markt be the marquee building.

Dawson - All of the building are very nice and exciting individually. Can there be a common theme among the buildings like University Hill, Mapleton Hill, CU.

Michelle Lee - All first floors set back which is a common theme. The good sunny areas of the plaza is after 2:00 pm. She lives in Steelyards and it's dead and quiet at night and so encourages as many restaurants as possible. Activate the uses down below.

Michelle - On the Ciclo buildings the ends need more work. She saw angular roof forms and immediately thought of Solana. SE corner there's a lot of different roof forms diving in.

6:06 Michelle - Meredith House did a great job with simplicity. Bring excitement of the buildings down to the street level. Build a people place with the deck on Railyards. Wouldn't it be great if Spruce pool were in S'PARK.

Michelle would love to see the bike path streetscape developed. It's not in any of the renderings. Bike path is a great aspect of the plan.

Jim Bailey - Building to the south is similar to the proposed building to the south. Don't turn your back to the path.



The project is FULL of creativity. SO nice to see compared to what else we've seen in this area. Concerned materials become so diverse that they become arbitrary. Look at material palate and keep the excitement but avoid the arbitrary part (?).

David McNerny – Are we willing to expand the shade analysis so they can have an idea of what's happening to sun and shade throughout the year?

Jim Bailey – Turnaround at the end of Bluff Street. Doesn't appear like there's much excitement there. Could there be a public sculpture or art piece. An activity area would be nice. Diagrammatically it's not there.

Scott – mentioned emergency vehicle access and not wanting to attract kids. Solution is the urban forest.

Adrian – we intentionally left out landscaping.

6:15 Dawson – Time check.

Sam – Board could hold a special meeting or the other option is two special meetings to allow 4 hours for comments.

Dawson - if we do a special meeting it should be done within a week.

Jamison – We could be efficient in a follow-up meeting now that we're introduced to the project.

Decision made to continue meeting.

6:38 – Jamison begins the continuance and wants to start with a discussion of Markt.

Jamison – How does the brewery space wrap to address Valmont. Matt responds.

Jamison – Is the Valmont side all glass? Matt responds.

Elaine clarifies that it's a brewpub and not a brewery.

Jeff D – Points to upper right image of the Markt page of the packet that's disconcerting. (picture taken from the west bound lane of Valmont just east of the tracks)

Jamison – Has similar concerns about eastern gateway to project. Maybe the back is a smarter place to include the lime green. It's a modern building and the modern materials are consistent and well done.



Jim Bailey – How is service handled on Markt? Matt responds.

Michelle – What’s the transition from the bike path to the brew plaza? Windows are too close to the roof. Provide more warmth.

Jamison – It would be a shame if you couldn’t cut your bike over to the plaza.

Jim Baily – Will the BNSF fencing remain?

Adrian responds that the rail issues will be left to the city and Elaine agrees.

Jeff Dawson – I think the Markt building will be spectacular, but the windows aren’t considered pedestrian scale. So important for the storefront system to work. Can’t be a regular storefront system, needs to have something else there. Matt highlights some other elements of the plaza, including the lighting and programming of the space. Jeff responds that Matt is talking about all the right things. Encourages team to think about how the entry will work, as there’s a large element hanging over the entrance.

Jamison wanted to clarify the raised platform in the brewpub plaza. Was relieved that it was only for the patio and that there’s steps/ramp down.

Jim Bailey – I like the building!

Jeff – The plaza can frame the view of the Flatirons.

Sam Assefa – Reminds the Board to comment on the materials. He says that buildings that look great in renderings hardly ever result in great buildings.

Jamison – This is the most complete materials boards that we’ve seen since I’ve been on the Board.

Sam – We received the same for 3100 Pearl.

7:00-7:25 – Sidetracked conversation on what makes a good contemporary building based on Sam’s comments to focus on the detailing.

Jeff Dawson – The Markt building will be a lightning rod. You have to nail it with the materials and the detailing. Needs fantastic detailing to be successful and need the team to take that seriously if they want the Planning Board to support it.



Jim Bailey – This is coming to us at a conceptual SD phase so he sides with Matt’s point. Can’t have all the details at this point.

Jamison - can we see the building again at the DD level to give additional design guidance at that point?

7:30 – Let’s Continue with the Timber Lofts building.

Jeff Dawson – I really like this building a lot. On the Woonerf side can we bring the materials down to the street more? North elevation is very compelling to Jeff, really likes it and wouldn’t change much there.

Jamison – Very skillfully done building and like the definition of the townhomes and other structures. He adds that it’s a big misstep to have 4 residential units on the Woonerf. Very vibrant pedestrian experience and then 4 people have to live on that space and he can’t envision it. Possibly add a live/work component for a better transition. He wants to go to the Woonerf but doesn’t want to live in this unit.

Scott responds with the Planning Board comments about people spilling out onto woonerf and agrees with Jamison about the live/work solution.

Board supports the location of the parking entrance.

Michelle – Don’t keep separate materials that mesh on a flat surface.

8:00 Discussion by all members and Leslie Ewy(civil engineer) regarding transformer placement.

Discussion by Jamison and others about the use of the CMU around the site. It could be pulled off or look bad if not done well.

Dawson cautions about the use of metal panel. If not detailed and constructed well it will look shed-like.

David McInerny - How long do you expect this building to last? How does the use of wood relate to the life expectancy?

Jeff – There’s a misconception here that the only durable material is stone or masonry.

8:20 – Move on to Meredith House



Jeff D – Would like to see a residential use/units brought down to the ground floor. Does not want to see the ground floor go dark, needs to be active. Live/work?

Michelle – Could you include operable windows facing the park? Community room looks closed off and shaded.

Jim – Remember that the roll-up door is facing west and will be very hot in the summer months.

Jamison - Loves the combination of the natural wood and blue.

Jamison – Consider attached sidewalk leading to Meredith Park. Grass not needed.

Jeff – This is an impressive amount of material you've provided for us. You've set the bar high for other projects coming after you.

8:50 – Meeting adjourned. Continuance will occur next Wednesday the 15th.

**CITY OF BOULDER
BOULDER DESIGN ADVISORY BOARD MINUTES
July 15, 2015
1777 Broadway, 1777 West Conference Room**

A permanent set of these minutes and a tape recording (maintained for a period of seven years) are retained in Central Records (telephone: 303-441-3043). Minutes and streaming audio are also available on the web at: <http://www.bouldercolorado.gov/>

BDAB MEMBERS PRESENT:

Jamison Brown, Chair
Jeff Dawson
Michelle Lee
Jim Baily
David McInerney

BDAB MEMBERS ABSENT:

PLANNING BOARD EX-OFFICIO MEMBER PRESENT:

Leonard May

STAFF PRESENT:

Sam Assefa, Senior Urban Designer
Charles Ferro, Development Review Manager
Elaine McLaughlin, Senior Planner
Chandler Van Schaack, Planner I

BOARD DISCUSSION:

1. Approval of Minutes

The board approved the June 10, 2015 BDAB minutes.

2. Boulder Commons Project Review

The applicant gave a presentation of the project.

BOARD COMMENTS:

J. Brown commented that the plaza read as a large circulation zone with not a lot of definition from a user standpoint. He also felt that the grassy park area needed more attention in terms of becoming a programmed place to enliven the area. He suggested adding moveable furniture, places to sit, something to provide more shade, interesting things to look at, food carts, etc.

M. Lee pointed out that the permanent programming should reflect the seasonal programming. She noted that the pattern of the paving was very linear and the applicant had an opportunity to add more movement and curves in the plaza and improve upon the vertical circulation in regards to the entrance to the parking. **J. Brown** agreed that the parking entrance needed more attention.

There was a discussion on the appropriateness of having a bike lane through the middle of the plaza. The board felt that the potential for the area would improve greatly if there was not a required bike lane which felt like an intrusion of the space. The board recommended

eliminating this area as a multi-modal path connection to allow more flexibility but it can still be used by bikes.

J. Baily asked if the two large transformer pads in the plaza, which were shown in the site plan, could be relocated.

The applicant acknowledged the fact that there is no ideal location for a service area and they discussed some of the design solutions they were considering.

M. Lee suggested putting in some pedestrian-scale light poles that could also be multi-purpose and decorative.

M. Lee questioned the location of the coffee shop and its ability to draw in customers.

J. Baily inquired as to how the applicant envisioned handling service with buildings of this size and also recommended that they designate a specific loading zone.

The applicant explained that they are treating it as an urban building so service trucks will come in the afternoon and evening. Most trash containers will be in the basement with the exception of the location on the southern building next to the restaurant.

M. Lee saw an opportunity to draw people in with the restaurant area on the west end of the south building, especially with the hotel being so close. She thought the massing on the building set up a strong corner but the restaurant area got tucked away and had a small amount of seating. She would like to see the landscape that is between the Goose Creek connection and the building be utilized as a social space where people could gather.

J. Baily strongly agreed with **M. Lee's** comment and felt that the seating should be pulled out as much as possible.

J. Dawson questioned the legitimacy of the masonry because of lack of enough transparency to draw people into the space. He suggested making the restaurant more present along the street.

J. Brown agreed with **J. Dawson's** comments and suggested perhaps moving the entrance of the restaurant to the front of the building so pedestrians could see into the interior and/or making the brick box on the corner an interior space rather than exterior.

J. Dawson liked the strong composition of the south building and felt that the contrast in materials was really effective and elegant along the street.

M. Lee pointed out the wood underneath the soffit on the triangular corner piece and asked if they would consider wrapping the metal underneath instead of the wood. She felt the location was a little high for wood and was such a small area.

The board expressed support for the solar panel on the south elevation of the south building.

J. Brown thought the edge of the last solar panel on the building should be inset. **J. Baily** agreed and liked how it turned into an awning at the bottom and also capped the building.

J. Brown encouraged the applicant to keep in mind the reflectivity of the metal panel in the plaza area.

J. Baily was concerned about the overall color/materials becoming very bleak in the winter. He felt there was an opportunity to animate the building a little more where the first floor retail met the second floor. He also felt the strict regularity with the patterns of the windows could use some shadow and depth.

M. Lee suggested the use of blade signs to add some life to the building in the winter months.

J. Dawson did not have a concern that the window patterning would become monotonous since the buildings were not that long. He cautioned the applicant in adding color on the fourth floor as it could disrupt the sophistication of the materials and become overly animated.

J. Brown liked the massing of the first floor of the north building but was concerned that the window materials did not quite fit in. He suggested breaking up the patterning with a textured material.

J. Dawson liked the consistency in the use of materials from top to bottom on the south building. He thought the north building felt less refined in terms of the use of materials and the openings. He suggested arranging the materials so they create a sense of continuity between the two buildings.

M. Lee liked the variety and diversity of the different buildings in Boulder Junction.

D. McInerney liked the use of the steel beams on the ground floor.

J. Baily liked the overall form of the portion of the north building facing the street and also that it was slightly different than the south building. He also liked the patterning of the top two floors and would not mind if that was pulled down to the first floor. He also considered how these buildings fit within the existing structures in Boulder Junction. It needs the retail on the first floor to be consistent with the feel of the entire area.

M. Lee strongly encouraged them to keep the retail component on the first floor especially if they pull the brick down to that level.

D. McInerney inquired as to whether the masonry specified on the south building (Lakewood brick black diamond smooth) would be darker than it appeared in the plans.

J. Dawson asked how they are using the wood on the east façade of north building and inquired if it would make sense to try to emphasize the entries a little more, especially with the wood material.

3. S’PARK Project Review

E. McLaughlin suggested that the board focus on the Ciclo and the S’PARK West buildings (permanently affordable units) in their review.

The applicant went over some concerns that the board discussed at a previous BDAB meeting and also highlighted changes that have been made since they last reviewed the project such as the shape of the roof, proportions of the windows, the use of materials on the upper two stories, materials, rhythm and height of the façade, and the way the building touched the ground.

BOARD COMMENTS:

Ciclo Building

The board generally liked the Cor-Ten Steel material used.

J. Baily shared a concern that the Cor-Ten could potentially bleed onto the sidewalk.

M. Lee thought the Community Cycles building should have a continuous singularity in the architecture with a stronger differentiation between the first floor retail units and the residential above.

J. Baily felt that this was not necessarily a negative thing. He commented that the entrance to Community Cycles was more apparent than on previous renderings. He also thought the way in which the corner was drawn in current plans helped to scale down the building and make it more welcoming.

J. Brown thought that the top two stories needed to come all the way out on the corner rather than being recessed to give the building a more complete look.

J. Dawson disagreed with **M. Lee's** comment (above) due to a concern that too many of the buildings in the S'PARK development have glass on the ground with a building floating above. He liked the overall changes and thought that the Maarket building could be something special within the development and the Community Cycles building could be a little calmer and familiar in terms of its proportions. He also liked the use of natural materials to bring in some color and recommended switching the design between the residential and public entrances on the ends of the building.

J. Brown agreed with possibly switching the design on the corners. On the 34th Street elevation, he wondered if carrying the white bond element through horizontally, instead of having transom light behind the sign-band, would help with the singularity in architecture that **M. Lee** referenced.

M. Lee suggested keeping the interesting elements on the residential level and flattening out the lower level on the same plane so it feels like it's cantilevering and more uplifting.

J. Brown struggled with the expression of the non-brick piece of the ground floor. He thought either this or the brick piece should change to express that this level is a different use.

There were some concerns expressed with the proportion of the windows at 34th and Valmont.

S'PARK West Building (3155 Bluff Street)

J. Dawson had a concern about the uniformly square proportions of the openings and thought there may be an opportunity to fit in a few more vertical portions.

J. Brown struggled with the zone between the townhouse projections and suggested having them go above the parapet for the back section as opposed to staying below it which might help diminish the long horizontal between the two ends.

The board agreed that the color palette and materials were improved from previous plans.

J. Baily agreed with **J. Dawson** to be cautious of the usage of square window openings especially in the stucco portion above the brick. This portion of the building seems to be the weakest link.

D. McInerney agreed that the stucco portion of the façade was the weakest link because the middle pair of windows at the bottom of the stucco sat right on top of the masonry.

J. Dawson pointed out that the applicant had clustered the townhomes to create doubles but that it could be interesting to arrange them in the same consistent direction to create a series of more vertical townhome forms versus bringing them together. This would give the units a private entry rather than a shared porch.

E. McLaughlin asked the board to comment on whether or not the materials used were equivalent or better quality in comparison to the market rate units that are on the site.

D. McInerney thought the materials had become much more equivalent in the current iteration.

J. Dawson agreed and thought the switch to brick over block made more sense; he liked the wood material and thought that there was a level of refinement that is not normally seen in less expensive housing.

4. Board Matters

The board went over the draft agenda for the 2015 BDAB Retreat.

The board discussed how best to gather feedback from applicants regarding the design review process.

There was discussion about the Landmarks Board's concerns with the Design Guidelines review process.

Note: The 2015 BDAB Retreat was originally scheduled for August 12, 2015 but was later rescheduled for October 14, 2015.

APPROVED BY:

Board Chair

DATE

Attachment F: Plan Responses to Concept Plan Review Comments

On March 6, 2014, the Planning Board reviewed and commented on the S'PARK Concept Plan and on Sept. 4, 2014, the board reviewed and commented on the SPARK_west application, both sets of minutes are provided [here](#). At the time, the applicant was directed to bring back a more comprehensive plan that combined sites and several key issues were discussed with comments summarized as follows. Staff finds that the applicant has addressed the comments or redesigned the project in response to the comments. Following is a summary of the Planning Board's Concept Plan review comments per the meeting minutes, with staff's findings on how the comments were addressed.

MARCH 6, 2014

- ***If there is public benefit, the board would support the proposed mass and scale and create variation.***

There is no requirement in Site Review for an applicant to demonstrate community or public benefit. That said, the applicant is proposing to build one third of the total units as permanently affordable, or a total of 77 permanently affordable residential units. The residential units would help to fulfill an important community benefit that is established as a goal within the Boulder Valley Comprehensive Plan: "support for a variety of housing types at a range of prices from market rate to affordable." While there is no requirement in Site Review that an applicant provide for a "public or community benefit" the provision of a new mixed income infill neighborhood has community benefit to support the vision of TVAP:

"The Transit Village area will evolve into a lively, mixed-use, pedestrian-oriented place where people will live, work, shop and access regional transit. It will become a new neighborhood as well as an attractive destination for the larger city, with regional transit and public spaces that will benefit the entire Boulder community."

Other related BVCP goals and policies that are fulfilled are discussed in the analysis of consistency with the Site Review criteria in [Attachment A](#) where consistency with BVCP policies is required.

Massing and Scale. Overall, the largest massing is located where the city has anticipated larger mass and scale: both within the MU-4 zoning and in particular within the "Rail Plaza" Character District.

Mixed Use -2-

1.5 - 2.0 Floor Area*



Three- to four-story mixed-use buildings. Predominant use may be business or residential. Mostly structured or first-floor parking; may have some surface parking.
TVAP p. 17

"The district will evolve into a high-density, commercial and residential mixed use area, with three- to five-story buildings."

- TVAP p. 23 the Rail Plaza District

The massing is also appropriately tall and broad along the railroad tracks where the two non-residential buildings are intended to buffer noise from the tracks to the residential units internal to the site. Within the residential and mixed use buildings of the site, the height of the Ciclo building planned at the

northwest corner of S'PARK, with a portion along Valmont Road is scaled at three stories and 45 feet. The overall massing on the site begins to transition with the Timber Commons townhomes that are planned on the west end of the Timber building at three stories. Within S'PARK_west, the site massing transitions to two and three stories within the RH-6 zoned area of the site and the "Steelyards" Character District, where the city envisioned a lower height, mass and scale.



Urban townhomes and garden apartments with individual garages, surface parking lots, or underground parking. Mainly two to three stories.

TVAP p. 16

- **Pay attention to the northern neighbors and Valmont Road.**

The site plan as it interfaces with Valmont Road has a three story building on the north of Junction Place to transition heights to the north. The applicant also has worked with neighbors throughout the process and in particular, worked with the neighbors who indicated concern at Concept Plan about the blocking of views from their property. In a viewshed analysis, the applicant demonstrated that the properties to the north and east were somewhat elevated above Valmont Road. That, in combination with holding the height of the planned Ciclo building to three stories helped to demonstrate preservation of viewshed from the front porch as well as the upper stories of the townhomes.

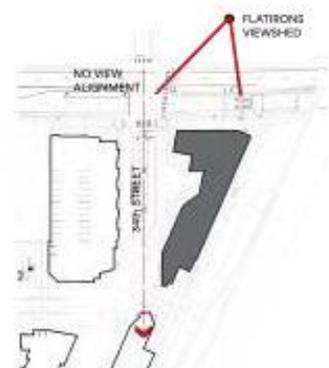
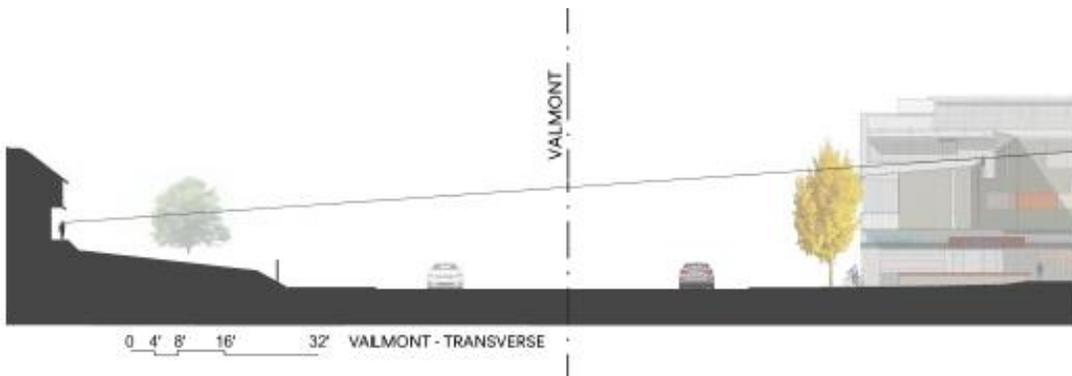
“The industrial uses on the north side of Bluff Street will transition to high-density residential, such as urban townhouses.”
 - TVAP p. 32 the Steelyards District



Valmont Viewshed

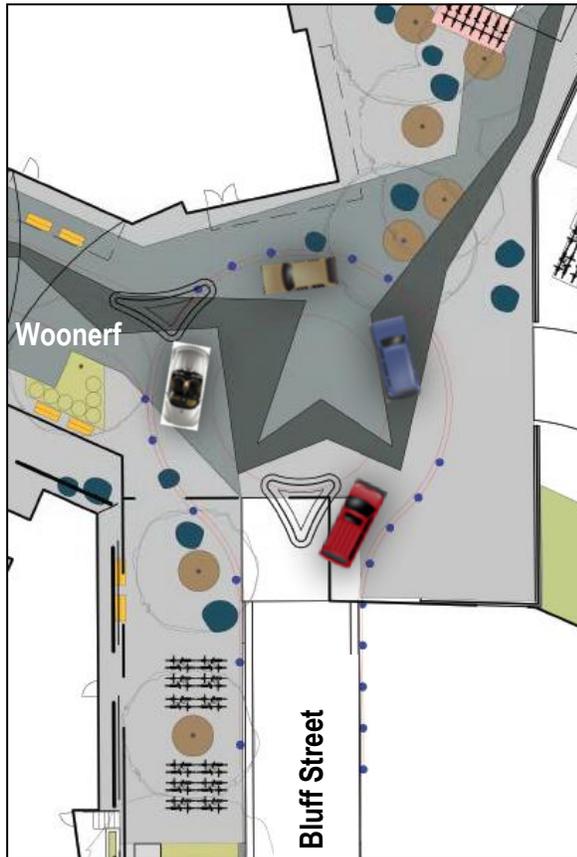
The differential slope of the Markt ridgeline relates to the neighborhood scale to the north, simultaneously preserving views to the peaks. Residents to the north have elevated lots and living levels. The view shown here has been verified with site photography and altitude data.

The 34th street alignment (due South) is actually not a beneficial viewshed. So the level 2 projection of Markt at the gateway is complemented by the level 1 undercut, which enhances pedestrian view to the plaza, while announcing the arcade walk.



- **Consider different approaches to the woonerf and keyhole drop off.**

At Concept Plan, the applicant had utilized a somewhat suburban configuration of a “kiss and ride” drop off area. In working with transportation staff, the applicant adapted the drop off area to be incorporated into the pedestrian oriented woonerf. As currently shown, there’s less of a confusing layout and more of an emphasis on pedestrian movement



- ***The multi-use path is a positive addition. There was some caution about how it is treated under the building.***

The applicant is no longer proposing the multi-use path on the east side of the site and aligning the railroad tracks to but under the building, rather the multi-use path is proposed to be separated from the Markt and Railyard buildings along the railroad tracks.

- ***Consider changing the SW office building to residential or mixed use.***

The previously proposed office building was planned to be located on the Air Gas property, which is no longer a part of the S'PARK plan.

- ***Consider underpass under train tracks.***

The underpass under the train tracks will be implemented by the city and RTD when the rail service is established as a part of the rail platform. Because that configuration has not yet been designed or funded, creation of a “temporary” underpass wouldn’t be warranted. In the interim, the distance from Bluff Street via Junction Place and Valmont Road to the offices at Wilderness Place east of the railroad tracks is in total one-quarter mile, well within the standard that most people would walk or bike.

- ***Pay attention to how bicycles navigate the site.***

The applicant has designed a woonerf or shared street on the eastern side of the site purposefully to allow the auto to feel and be like a “guest” on the street – with priority given to bikes and pedestrians. The other streets within the plan are designed to be low speed roadways. Junction Place is intentionally designed to be slow-moving and not a “through street” that autos would choose to use as a quick outlet. This is true within the proposed S'PARK and is carried through across Goose Creek bridge to the Depot Square area where Junction Place is constructed as a shared street. The streets on the west are intended to be quiet residential streets, with connectivity but not broad rights of way that encourage fast moving autos. This environment would encourage bicyclists and pedestrians. Further, the applicant is implementing TVAP connections that include the multi-use path along the rail, all of the street and alley connections and providing “stub outs’ at the terminus of the property for future connectivity outside of the property.

- ***Include parallel as opposed to diagonal parking along the private street.***

All of the streets within the S'PARK plan illustrate parallel parking. This parking is intended to be shared and unbundled, managed by the Boulder Junction Access District.

- ***Zoning changes garnished a cautious support but the project must support larger goals of the TVAP.***

The applicant is no longer considering a rezoning. Rather the project plans work with the existing zoning, particularly where the zoning transitions from MU-4 to RH-6: the Timber building also transitions from a four story apartment building into townhomes.

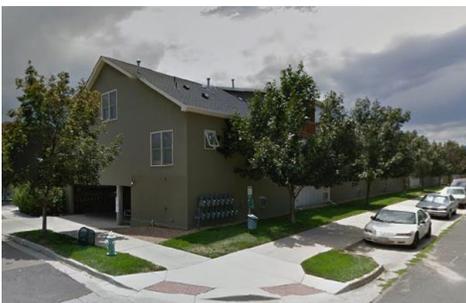
SEPTEMBER 4, 2014

This hearing did not include a summary of key issues. Staff has included the most salient and “actionable” comments herein. The minutes in their entirety are provided [here](#). The applicant’s written statement also provides responses to the Concept Plan comments found in **Attachment ____**.

- ***Draw from contextual elements, including the Steel Yards and adjacent developments to provide a sense of place and look to the Holiday neighborhood for additional concepts and design ideas.***

The applicant is utilizing some building finish materials that are like those of the adjacent buildings across Bluff Street in the Steelyards development including corrugated metal siding and stucco. There’s also use of brick as occurs nearby in Steelyards. The distinction in this case is that at the time of the Steelyards development there was no plans to redevelop the area north of Bluff Street as residential. While the Steelyards residential has essentially the rear or backs of buildings aligning Bluff Street, the proposed S’PARK_west has units facing the street. Below is reference to the Steelyards building face along Bluff Street and the proposed townhomes across the street from the building. While the buildings are compatible, there’s enough variation and a somewhat more contemporary appearance than those of the Steelyards.

The applicant also indicated that Holiday neighborhood shown bottom left was used as a precedent for massing and materials for S’PARK_west as well. Staff notes that this appears to be the case h a somewhat more contemporary form and use of materials as well.



- ***The board liked the linked and varied green spaces; consider additional green space to assure that the needs of families are met and should maintain ample open space to be attractive to families and incommuters.***

The applicant has interlinked open space areas in portions of S'PARK_west and since Concept Plan review, created a pocket park at the focal point of where Junction Place turns east.

- ***Provide some leaks or small passageways through buildings to make the communities more permeable.***

The applicant has illustrated passageways between the buildings in certain locations to establish permeability.

- ***Include one parking space per unit, but reconsider the attached parking to foster better neighbor interaction and eyes on the street.***

There are units that have attached parking garages and surface parking; others have tuck under carports. There is on-street parking also provided within the newly proposed streets.

- ***Consider including a terminus at the northern ends of streets.***

Eventually, the roads planned through the site will extend to Valmont Road, when properties redevelop along Valmont. In the interim, the applicant has proposed a park space adjacent to the “terminus” of Junction Place, as it turns east. This provides a focal point and the street is an extension of the park space in the interim.

- ***Make the alleys and stub-in streets Woonerf-like to foster hardscaped spaces for children and families, not cars.***

The applicant has designed the streets with narrower widths for traffic calming. Most street are designed with street tree plantings with raised walkways in locations for mid-block crossing. The streets are designed to be slow moving and quiet residential streets without opportunity to quickly “cut-through” the spaces in an auto. The alley’s are less likely to create thorough traffic. While they are not designed to be “woonerf-like” they have limited through travel opportunity.

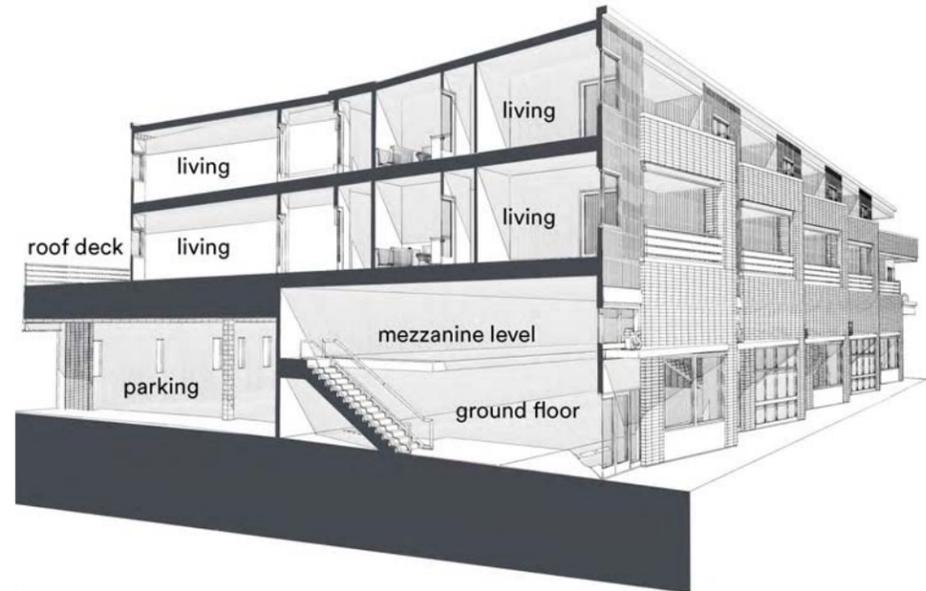
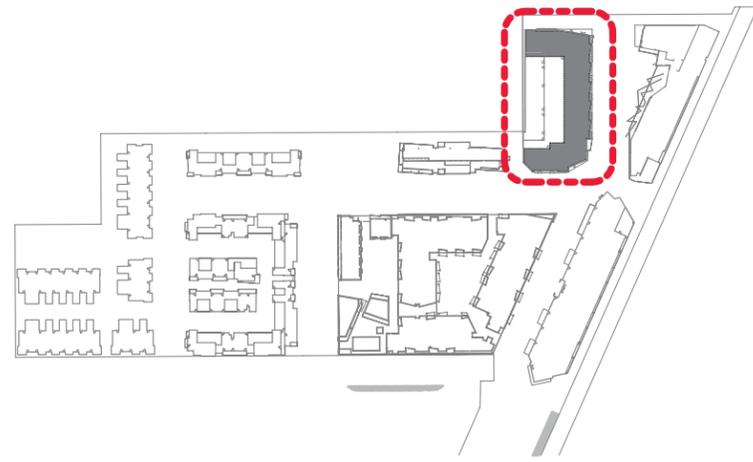
- ***Provide some form of vertical or other separation from the street to transition between public and private realms.***

There is some vertical separation of entries and porches from the street, with low stairs and porches that are framed by wood railing.

- ***The board would support a legislative approach to gaining greater density on the site and the board members agreed that 14 additional units should be permitted in the development if they are for middle income home owners and relatively modest in size.***

The applicant redesigned S'PARK_west in conjunction with S'PARK to achieve the number of units

necessary for permanently affordable funding without the need for a rezoning or special ordinance. Greater efficiency in layout of the units along with provision of townhomes as a part of the timber building; the design of the Meredith Park and other efficiencies allowed for the required density and open space standards.



Our intention was to showcase the maker/builder/crafter/bike. Working with the constraints of a site surrounded on three sides by the street, we decided that this constraint would in fact be our biggest opportunity. Ciclo provides a street frontage that will demonstrate and display the life of its occupants. The living units wrap around an elevated roof deck. Outdoor space to create and share.

The building is an exhibit that is transformed continuously, and everyone gets to watch.



Ciclo is a showcase for creativity. The intended commercial occupants will be maker/crafter/builders/bike enthusiasts/bike advocates. The design of the building is meant to showcase the occupants in a way that celebrates their ideas and the important place they occupy in the Boulder community. The ground level tenant space creates a continuous zone where proposed Community Cycles can occupy and utilize indoor and outdoor space concurrently. The building creates a space for people to ride up and work on their bikes outdoors, learn proper bike maintenance, and purchase bikes from the Community Cycles stock of two wheeled transport.

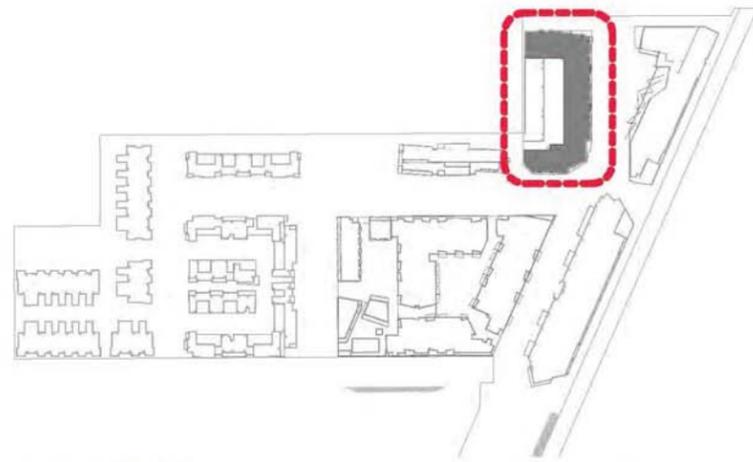
The two upper levels are 32 units of 100% Permanently Affordable Housing. The outdoor deck on the second level would provide an amenity deck that could be used for both outdoor play and deck zone. The internal circulation zone is wide enough to serve as the residents breakout area.

The overall form of the structure is intended to be a complement to the MARKET building across 34th Street, establishing a pedestrian experience that creates interest and variety along the Valmont/34th corridor.



CICLO

- Mixed Use Commercial Building
- 57,901 gsf
- 4 Story, including a Ground Floor Mezzanine; Code Height: 44'-6" (Perceived Height: 42'-0")
- Ground Floor - Proposed Community Cycles retail/workshop/education space, Leasable tenant area/Retail, Lobby area for residential, Covered parking
- Floors 2 & 3 - Apartments, Accessory Gallery, Common tenant areas
- Permanently Affordable Residential Units



CICLO Site Plan Data

SITE AREA	PROPOSED
Gross site area	31,936'

PROPOSED LAND USES OF PROPERTY GROUND LEVEL & MEZZANINE USES	QUANTITY	AREA
Attached dwelling units (lobby access to units above)		2033'
Leasable tenant areas:		
Offices, technical, professional or administrative ≤5000sf	4979	
Retail sales ≤20,000sf	9556	
Accessory Coffee shop (no more than 1,500 SF, outdoor seating less than 1/3 the floor area)	1498'	
Automobile parking as an accessory use (covered)		7987'

SECOND LEVEL USES	QUANTITY	AREA
Attached dwelling units (100% affordable housing units)		
3-bedroom units/ 2 bath	1	1350'
2-bedroom units/ 1 bath (800 SF-940 SF)	5	4828'
1-bedroom units/ 1 bath (600 SF-620 SF)	10	6765'
Accessory Use:		
Common areas for tenants		2981'

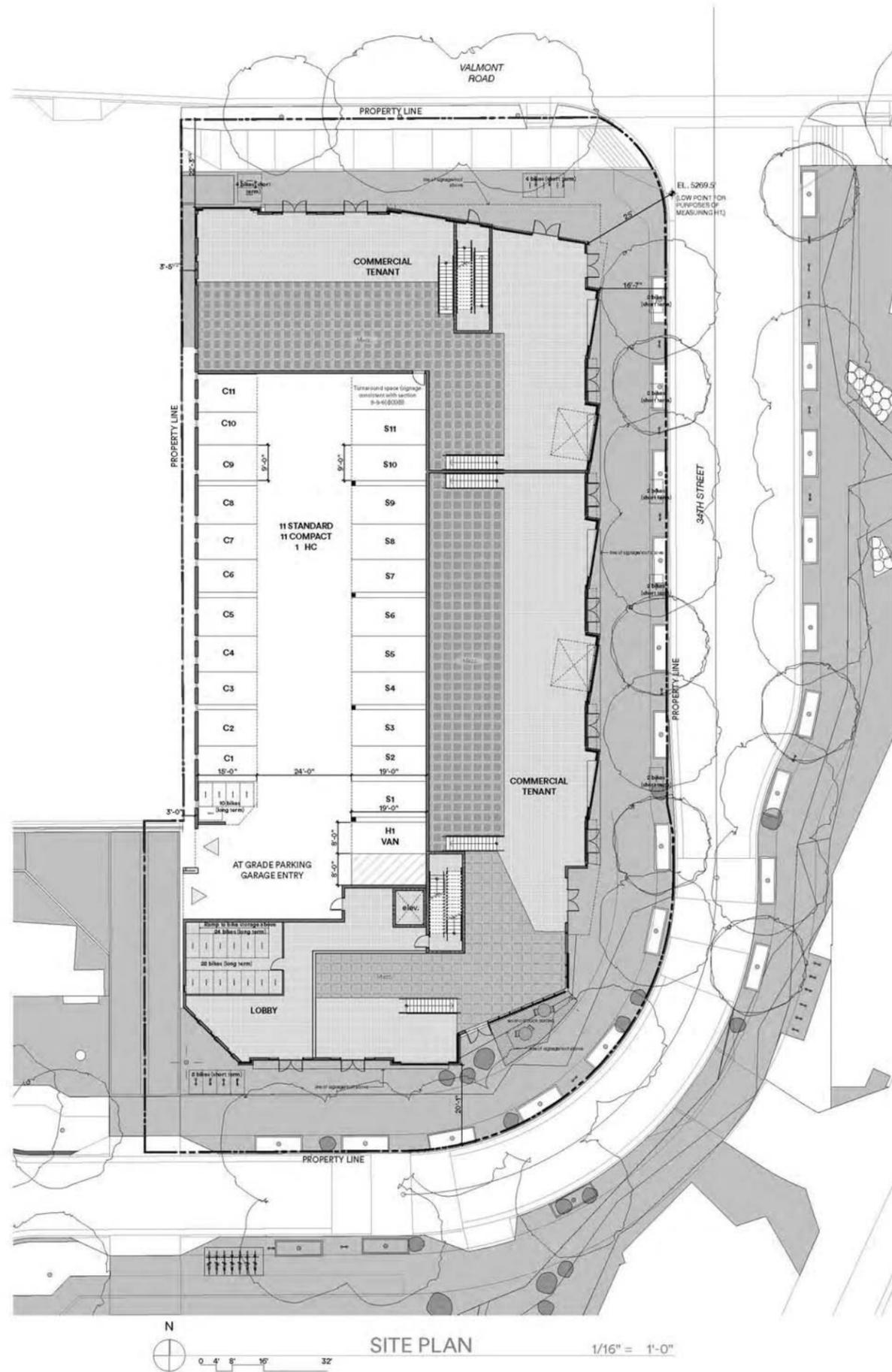
THIRD LEVEL USES	QUANTITY	AREA
Attached dwelling units (100% affordable housing units)		
3-bedroom units/ 2 bath	1	1378'
2-bedroom units/ 1 bath (850 SF-1017 SF)	5	4808'
1-bedroom units/ 1 bath (675 SF-680 SF)	10	6765'
Accessory Use:		
Common areas for tenants		2973'
TOTAL NUMBER OF UNITS	32	
TOTAL GROSS BUILDING AREA		57901'

F.A.R./ DENSITY	ALLOWED	PROPOSED
	2.0	1.81

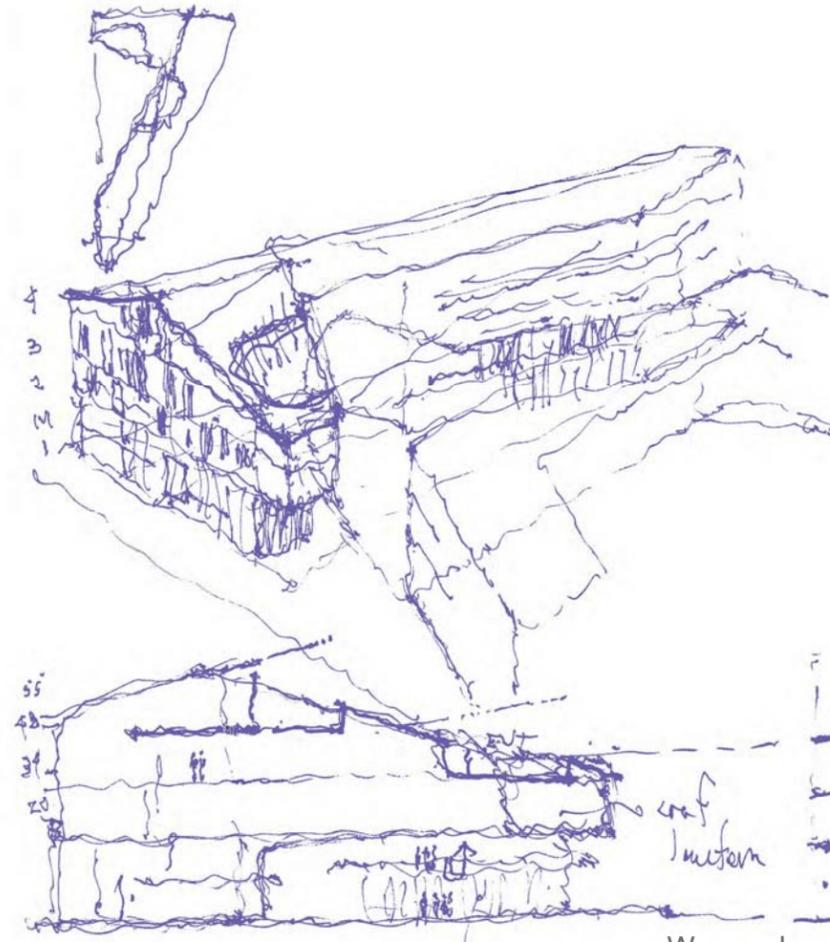
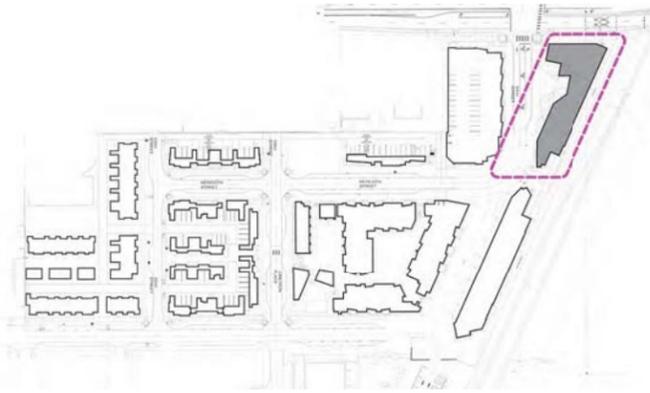
PARKING BOULDER JUNCTION ACCESS DISTRICT MEMBER	REQUIRED	PROPOSED
Standard	n/a	11
Compact	n/a	11
Accessible	n/a	1
Bicycle		
Long term		
Restaurant, Commercial & Retail - 1/750 S.F.	15	25%
Office - 1/1,500 S.F.	3	75%
Residential - 2/Dwelling Unit x 32 Units = 64	48	75%
Long term Total Required	42	
Long term Total Proposed		42
Short term		
Restaurant, Commercial & Retail - 1/750 S.F.	15	75%
Office - 1/1,500 S.F.	3	25%
Residential - 2/Dwelling Unit x 32 Units = 64	48	25%
Short term Total Required	24	
Short term Total Proposed		24

OPEN SPACE	REQUIRED	PROPOSED
Usable open space	20%	6,387'
Open space %	52%	1,6463'

BUILDING PLACEMENT & SETBACKS	STANDARD	PROPOSED
VALMONT ROAD		
Minimum frontyard setback	0'	22-3'
Minimum frontyard setback: 3rd story & above	20'	23-3'
Maximum frontyard setback for corner lots & sideyards adjacent to a street	10'	n/a
34TH STREET		
Minimum sideyard setback to a street: 1st & 2nd stories	0'	16-7"
Minimum sideyard setback to a street: 3rd story & above	12'	16-7"
MEREDITH STREET		
Minimum rearward setback to a street: 1st & 2nd stories	0'	20-1"
Minimum rearward setback to a street: 3rd story & above	20'	20-1"
WEST PROPERTY LINE		
Minimum sideyard setback to an interior lot line	0' or 5'	3'
Minimum sideyard setback to an alley	0' or 5'	3'



Site Plan



We wondered:
 What would happen if a creative Class A office building felt nothing like an office, and what if it floated over a glowing lantern-space full of people and great food and drink?

So we carved a gabled form that defers to neighborhood scale, wrapped in silvered wood, with kinetic rhythm and punches of color. It had to hover over glass. And on top, the 'lifted lid' at the event deck gives a gateway view to everyone.



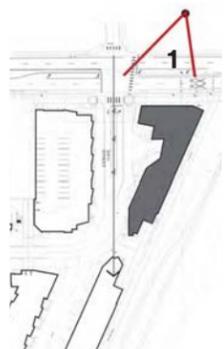
Inspired by the simple lines and texture of mountain sheds and the kinetic rhythms of railroad cars, Markt is a modest gable form carved from a triangular city block. Erosion forms its plaza and arcade overhangs. On its roof, lifted flaps create the 3rd level Aerie event deck, upper office mezzanines and terraces, and trackside dormers with northeast views. Markt is designed to work at two scales and velocities: at the speed of the train or as seen from Foothills Parkway, and the slower pedestrian pace as one enters Junction Place from Valmont.

Composed in two shades of silver grey cedar, vertical and horizontal in sympathy with building proportions, the main body of Markt is completed by a soffit of reclaimed Sutherlands lumber. Windows similarly shift from vertical to horizontal, to the advantage of creative offices and views within. A standing seam galvanized roof wraps down the east (trackside) face, interrupted by wood wedges at the sawtooth dormers. This floating volume is underpinned by steel columns and vertically score-jointed stack bond concrete masonry along the tracks. On the west, steel columns and glass with dark mullions create an arcade walk—animated by a floating light/signage ribbon. The first story soffit celebrates sustainability, repurposing the warm wood decking rescued from the Sutherlands lumber yard.

At the north, a brewpub with taproom and mezzanine creates the transparent 'lantern' on Valmont. Several micro-restaurants stretching down the arcade toward S'PARK Place complete an indoor-outdoor dining scene. The plaza at Markt will be activated by a stage plinth for spontaneous or scheduled music, and a firepit at the taproom terrace.

Mixed Use Commercial Building

- 5 Stories
- Actual Height 47'-7" to ridgeline on Valmont; 49'-1" to high point on east or west (from grade including mechanical)
- 55,340 gsf
- Floor 1-2 – Restaurant /Retail
- Floors 3-5 – Creative Class A Office



1 Valmont gateway, frame and viewshed

MARKT - Site Plan Data

SITE AREA	PROPOSED
	31,936'

BUILDING AREA & USE

GROUND LEVEL USES	QUANTITY	AREA
Leasable tenant areas:		
Taproom	1	3202'
BrewPub Production	1	4630'
Taproom Outdoor Terrace		1082'
Micro-Restaurants > 1500sf	3	3158'
Micro-Restaurants Kitchen	1	566'
Micro-Restaurant Outdoor Dining Patio		904'
Common/Circulation		1585'
Bicycle Parking	22 long-term	381'
Automobile parking as an accessory use (uncovered, not included in building area)		0'

SECOND LEVEL USES

Leasable tenant areas:		
Upper Level Taproom Dining		1756'
Common/Circulation		365'
Utility Area		533'

THIRD LEVEL USES

Leasable tenant areas:		
Offices, technical, professional or administrative ≤20,000sf		13586'
Common/Circulation		3226'
Bicycle Parking	17 long-term	262'

FOURTH LEVEL USES

Leasable tenant areas:		
Offices, technical, professional or administrative ≤20,000sf		11699'
Common/Circulation		3844'
Utility Areas		229'
Bicycle Parking	17 long-term	262'

FIFTH LEVEL USES

Leasable tenant areas:		
Offices, technical, professional or administrative ≤20,000sf		5117'
Outdoor Decks	3	1553'
Utility Area		547'

TOTAL GROSS BUILDING AREA	55340'
TOTAL NET BUILDING AREA	51616'

F.A.R./ DENSITY	ALLOWED	PROPOSED
	2.0	1.42

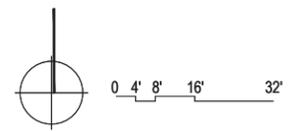
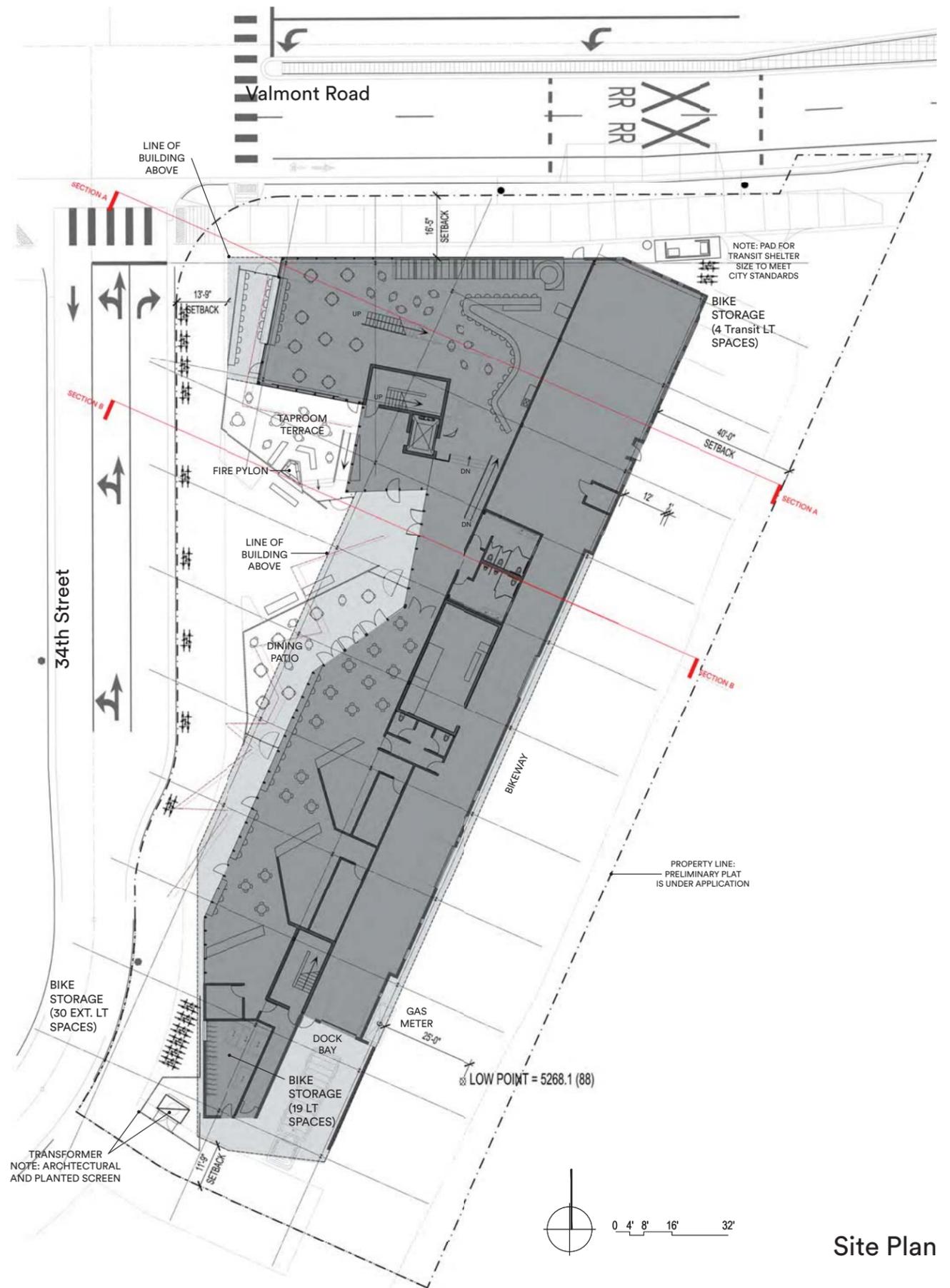
PARKING	REQUIRED	PROPOSED
BOULDER JUNCTION ACCESS DISTRICT MEMBER		
Standard	n/a	0
Compact	n/a	0
Accessible	n/a	0
Bicycle		
Long term	9	56
Restaurant, Commercial & Retail - 1/750 S.F.	0	25%
Office - 1/1,500 S.F.	0	75%
Long term Total		56
Short term	27	30
Restaurant, Commercial & Retail - 1/750 S.F.	0	75%
Office - 1/1,500 S.F.	0	25%
Short term Total		30

L1 22
L2 17
L3 17

OPEN SPACE	REQUIRED	PROPOSED
Usable open space	20%	7,794
		16028'

BUILDING PLACEMENT & SETBACKS

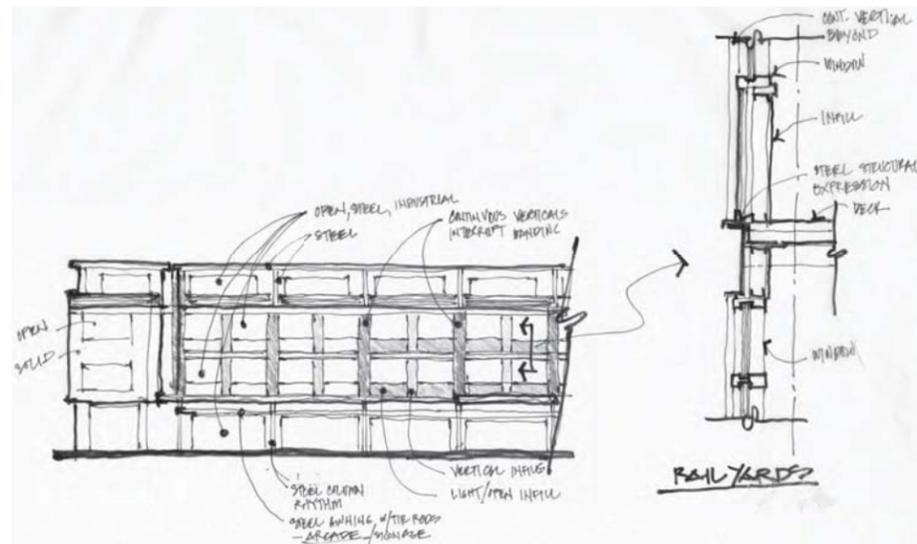
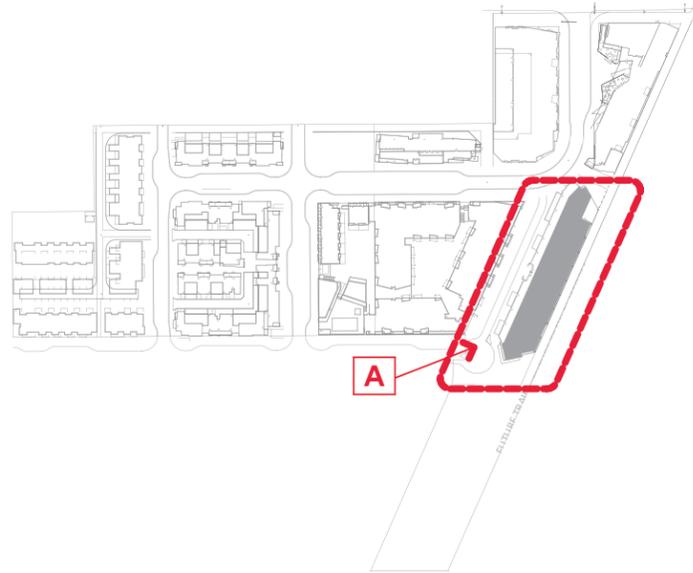
VALMONT RD	STANDARD	PROPOSED
Minimum frontyard setback	0'	16'5"
34th STREET		
Minimum sideyard setback to a street: 1st & 2nd stories	0'	13'9"
BSNF Tracks		
Minimum sideyard setback to an interior lot line	0'	40'
Southern Shared Access		
Minimum sideyard setback to an interior lot line	0'	11'9"



Site Plan

WORKSBUREAU | MARKT

S'PARK



For as long as there have been rail lines crossing this country, industry has located itself to take advantage of the unique opportunities available with proximity to the tracks.

Historically, the architecture associated with these typically industrial zones have been either very specific or it has been a simple multi-story accommodation that can allow a variety of light industries to flourish. Structures where the process itself defined the form of the building - the architecture built itself into the shape of the process

The buildings that inspired RAILYARDS essentially provided a simple space with good light and a simple internal layout of open space with a large column spacing and relatively tall space - tall enough and open enough to provide opportunity for many and differing industrial tenants. The space could then be fit-out to suit specific needs of the particular business, but the exterior form maintained a simple and clear form of multiple stories of typically concrete frame supporting concrete floors with masonry infill and glass walls.

Today, we still find these simple structures providing useful life. They may no longer house garment industry workers, or supply parts for the tool and dye industry, but often they have been retrofitted to provide the simple accommodation with good light and open space to office users, artist lofts, and residential condominium owners.

The RAILYARDS Buildings takes the same basic approach as the latter of these two types of trackside structures. The goal was to provide a structure that works at the scale of the tracks; a simple frame system, but now built from lightweight materials instead of the heavier concrete frame and masonry. The framework is steel with lightweight and highly insulated infill, glazing and translucent panel materials. The occupants will no longer be the same kinds of light industry as in decades past, but startups, office users, and small scale service companies who find that the open and small scale spaces best suit their business needs.

Also, by locating the non-residential structure along the tracks, we are able to protect the residential users who benefit from the mass and form that the trackside RAILYARDS structure can provide.

We were inspired by the rhythm and movement found in the structure of the train tracks themselves, as well as historic warehouses. Both have a permeability and simple structure.

We wanted to create a modern interpretation of these elements. The design and functional intent is to be visually open to, and encourage interaction between, the activities occurring within the building and the activities and natural beauty occurring outside in the S'PARK neighborhood and along the Flatirons and Front Range.

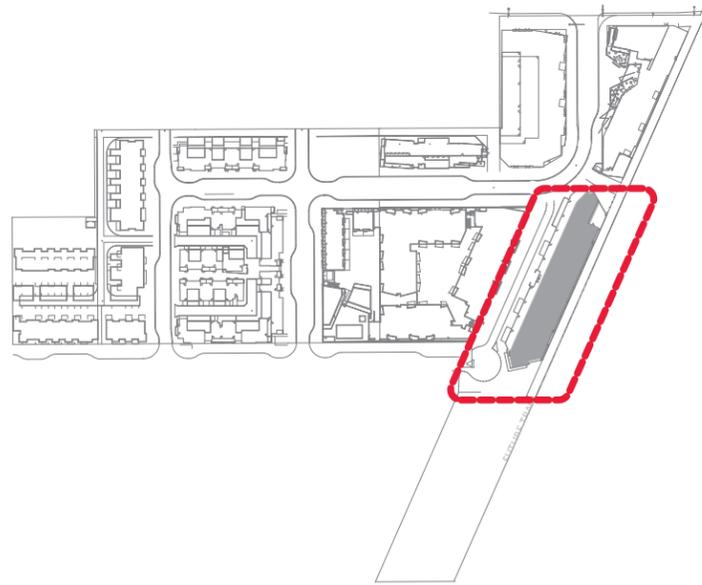


RAILYARDS

- Mixed Use Commercial Building
- 70,155 gsf
- 4 Story, Code Height: 54'-5" (Perceived Height: 50'-0")
- Ground Floor - Restaurant/ Commercial/ Retail
- Floors 2, 3 & 4 - Commercial Office

SopherSparrn | **RAILYARDS**
S'PARK

August 21, 2015



RAILYARDS & (Existing) BUILDING 3 Site Plan Data

SITE AREA		PROPOSED
Gross site area		48714'

BUILDING AREA & USE		
GROUND LEVEL USES	QUANTITY	AREA
Leasable tenant areas:		
Commercial/Retail		6710'
Restaurants >1500sf	2	7057'
Common and utility areas:		3487'
Existing Building #3		2849'
Automobile parking as an accessory use (uncovered, not included in building area)		17044'

SECOND LEVEL USES		
Leasable tenant areas:		
Offices, technical, professional or administrative ≤20,000sf		17074'
Common and utility areas:		571'

THIRD LEVEL USES		
Leasable tenant areas:		
Offices, technical, professional or administrative ≤20,000sf		17074'
Common and utility areas:		571'

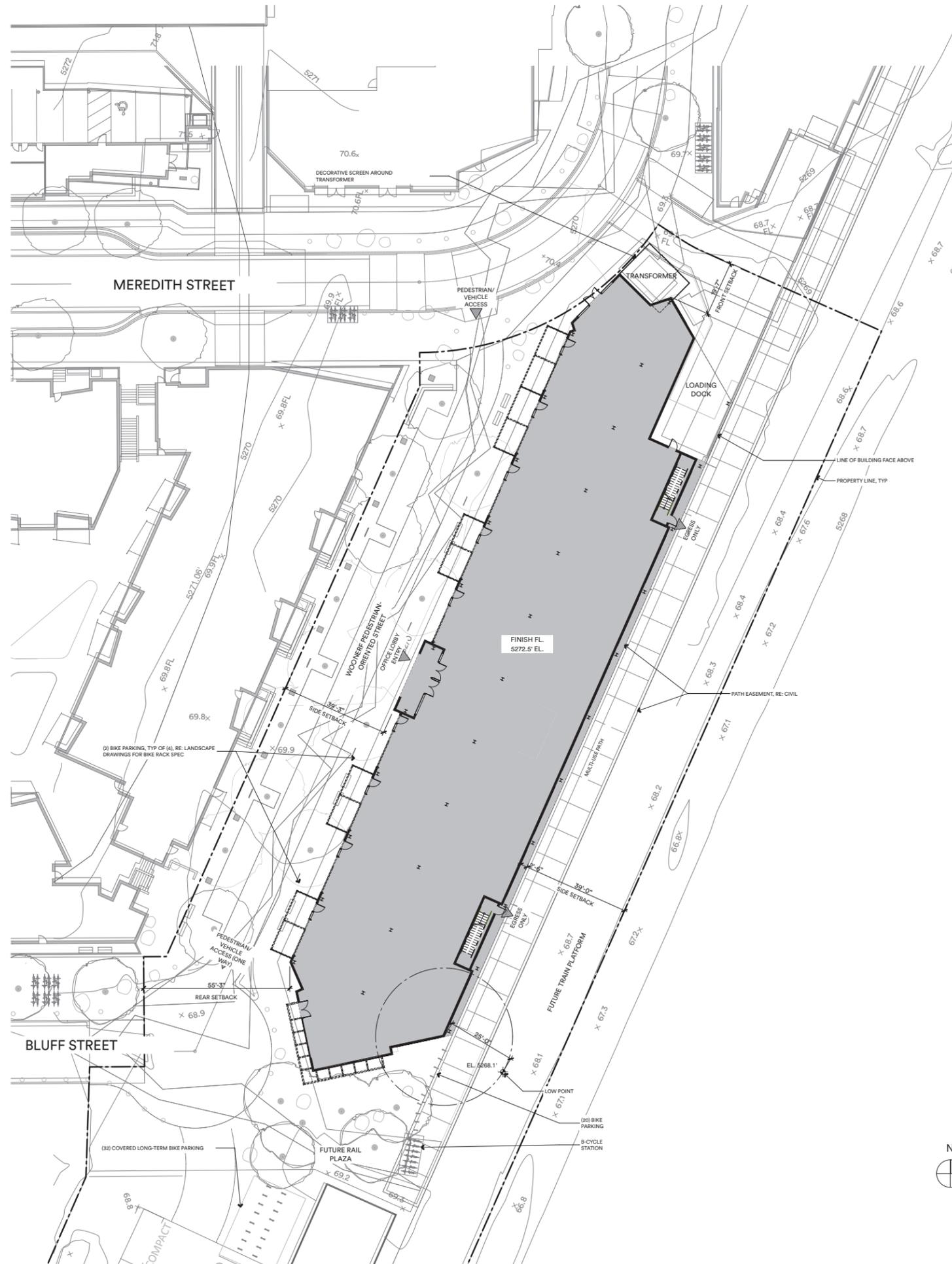
FOURTH LEVEL USES		
Leasable tenant areas:		
Offices, technical, professional or administrative ≤20,000sf		16478'
Common and utility areas:		1133'
TOTAL GROSS BUILDING AREA		70155'

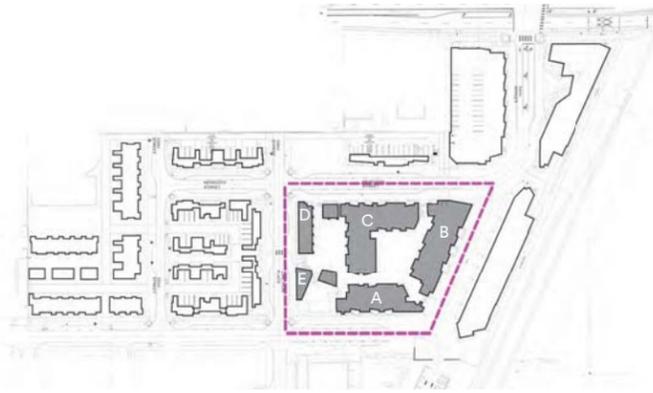
F.A.R./ DENSITY	ALLOWED	PROPOSED
	2.0	1.44

PARKING			
BOULDER JUNCTION ACCESS DISTRICT MEMBER			
	REQUIRED	PROPOSED	
Standard	n/a	24	
Compact	n/a	35	
Accessible	n/a	3	
Bicycle		REQUIRED	
Long term			
Restaurant, Commercial & Retail - 1/750 S.F.	18	25%	4
Office - 1/1,500 S.F.	34	75%	25
Long term Total Required			29
Long term Total Proposed			32
Short term			
Restaurant, Commercial & Retail - 1/750 S.F.	18	75%	14
Office - 1/1,500 S.F.	34	25%	9
Short term Total Required			23
Short term Total Proposed			28

OPEN SPACE		REQUIRED	PROPOSED
Usable open space		20%	9,743
Open space %		21%	10255'

BUILDING PLACEMENT & SETBACKS		
	STANDARD	PROPOSED
MEREDITH STREET		
Minimum frontyard setback	0'	0'
Minimum frontyard setback: 3rd story & above	20'	0'
Maximum frontyard setback for corner lots & sideyards adjacent	10'	0'
BLUFF STREET		
Minimum rear yard setback to a street: 1st & 2nd stories	0'	0'
Minimum rear yard setback to a street: 3rd story & above	12'	12'
East & West PROPERTY LINES		
Minimum sideyard setback to an interior lot line	0' or 5'	0' or 5'





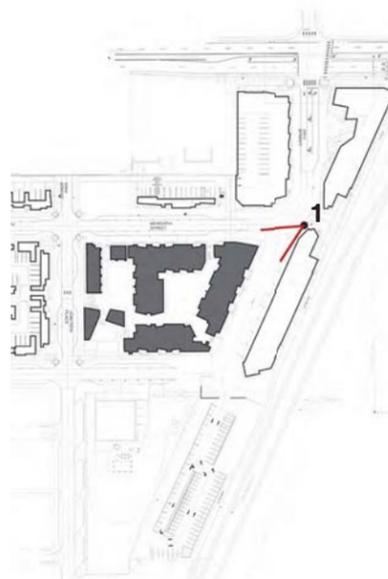
We wanted to build a place with pedestrian rhythm and front stoops between corner shops, that always feels like porch lights are on—people live here. And we can feel invited from the street to the terrace, and vice versa.

We wrapped an array of five buildings around structured parking, linked to pedestrian ways, capped by multiple gardens that ‘breathe’ with many ways in, many ways out.

TIMBERLOFTS is a framework for living within S’PARK. Its terraced scale of multi-family dwellings is split open at corners and several mid-block points, to allow residents a complete sense of connection to the courtyards and pool/fitness deck within, and to the fabric of streets, walks, offices and shops beyond. These apertures offer generous stair connections where breezes flow through. Sightlines tied into ‘lines of desire’ take residents and visitors through active or contemplative spaces, and to the S’PARK Place woonerf, Meredith Park, or MARKET Plaza connections.

As a supporting character in the urban play, TIMBERLOFTS is a backdrop to the woonerf, a veneer to structured parking, and a ‘foil’ in complement to neighboring buildings. In that sense Timber has been rendered in multiple materials and variants, as it shifts from predominant pewter grey V-rib cladding on Bluff and S’PARK Place, to light bronze (opposite the midnight blue Meredith House), charcoal on the Meredith and Junction townhomes, and finally warm wood on the southeast lobby area. This warm wood, coupled with buff concrete masonry and boardform concrete stoops, is wrapped around the first story, below the ‘Timber Line’ datum. An emphasis on warm, tactile, organic texture in the first 15 feet enhances the pedestrian horizon.

This rapidly renewable wood resource is used in the building frame. Yet the sustainable agenda is also overt, as bike rooms are glazed, visible to street, above the entrance to the garage and gardens. The rhythmic relief of the façade with thin walls and alcoved decks and stoops gives shade, a passive solar benefit to the larger glass—while introducing a play of light shadow and color, at a gentle cadence.



1 vibrant mixed-use at Meredith and the woonerf

TIMBER - 5 Buildings	Bldg A	-	TIMBER LOFTS 1	—27 units
	Bldg B	-	TIMBER LOFTS 2	—41 units
	Bldg C	-	TIMBER LOFTS 3	—53 units
	Bldg D	-	TIMBER TOWNHOMES	—8 units
	Bldg E	-	TIMBER COMMON	

Mixed Use Buildings

- 4 Story, actual height 49’-8” at northeast corner; 51’-9 1/2” technical height
- 214,043 gsf
- Ground Floor – Live/Work units, Restaurant, Walk-up Apartments, Resident Amenities & Services
- Floors 2, 3 & 4 – Apartments
- Roof – Resident Amenities, including pool, deck and enclosed areas.
- 214 Structured parking spaces (shared, unbundled, proposed as part of Boulder Junction Access District)

WORKSBUREAU | **TIMBERLOFTS**



TIMBER LOFTS - Site Plan Data

SITE AREA				
	RH-6	MU-4	PROPOSED	
	24091'	47147'	71238'	
BUILDING AREA & USE				
BASEMENT LEVEL USES	QUANTITY	RH-6	MU-4	TOTAL AREA
Leasable tenant areas:				
Storage (for residential tenants)	74	565'	4060'	4060'
Common / circulation			778'	1343'
Utility areas		394'		394'
Automobile parking as an accessory use	17453'		31966'	49419'
GROUND LEVEL USES				
Leasable tenant areas:				
Commercial retail			3230'	3230'
Attached dwelling units (market rate apartments)	1978'		7948'	9926'
studio	1			
1 bedroom	2			
2 bedroom	7			
townhome (1st of 3 levels/unit)	8			
Storage (for residential tenants)	4	122'		122'
Common areas		2640'	1663'	4303'
Bike rooms (117 bikes, long term)		1003'	354'	1357'
Utility areas (under roof)			557'	557'
Automobile parking as an accessory use	11468'		23712'	35180'
SECOND LEVEL USES				
Leasable tenant areas:				
Attached dwelling units (market rate apartments)	4280'		25147'	29427'
studio	17			
1 bedroom	18			
2 bedroom	8			
3 bedroom	2			
townhome (2nd of 3 levels)	8			
Common areas		2726'	4168'	6894'
Bike room (47 bikes)			604'	604'
Utility areas			120'	120'
THIRD LEVEL USES				
Leasable tenant areas:				
Attached dwelling units (market rate apartments)	4520'		26538'	31058'
studio	10			
1 bedroom	18			
2 bedroom	8			
3 bedroom	2			
townhome (3rd of 3 levels)	8			0'
Common areas			4810'	4810'
Bike room (22 bikes)			284'	284'
Utility areas			120'	120'
FOURTH LEVEL USES				
Leasable tenant areas:				
Attached dwelling units (market rate apartments)			26538'	26538'
studio	10			
1 bedroom	18			
2 bedroom	8			
3 bedroom	2			
Common areas			4810'	4810'
Bike room (24 bikes)			284'	284'
Utility areas			120'	120'
TOTAL GROSS BUILDING AREA	46755'	167288'	214043'	
TOTAL NET BUILDING AREA	45730'	150665'	196395'	

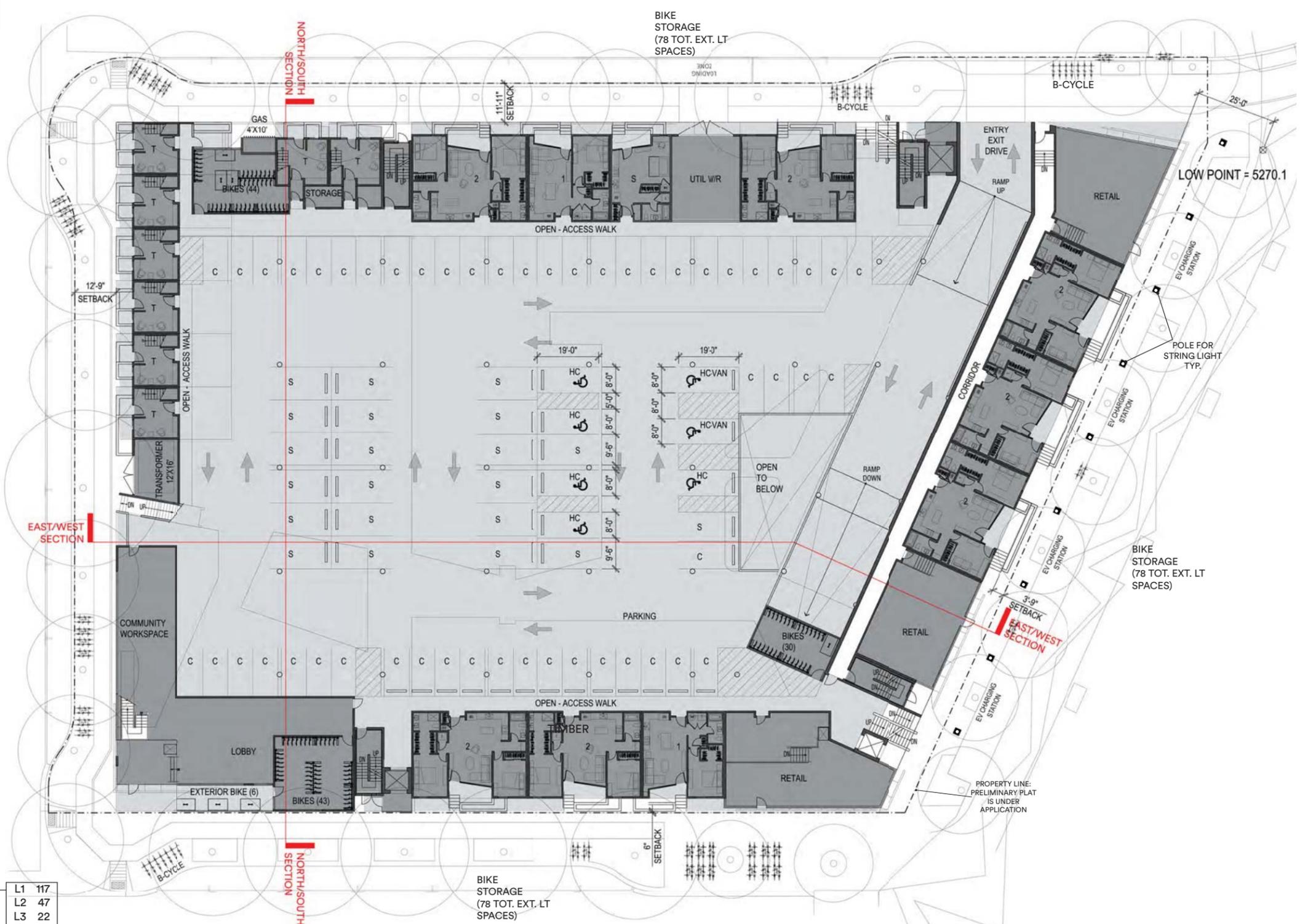
F.A.R./ DENSITY	ALLOWED / EXISTING	PROPOSED (RH-6)	PROPOSED (MU-4)
MU-4 ZONE only (RH-6 in aggregate with S'Park West)	2.0	-	3.55

PARKING				
BOULDER JUNCTION ACCESS DISTRICT MEMBER	REQUIRED			PROPOSED
Standard	8			79
Compact	n/a			130
Accessible	n/a			7
Bicycle				
Long term (provided in bicycle storage rooms)				
Residential (2 per unit)	258	75%	194	206
Commercial & Retail - 1/750 S.F.	4	25%	1	1
Office - 1/1,500 S.F. (minimum 4)	4	75%	3	3
Long term Total			198	210
Short term (n/incl. add'l 6@ L1 + 16@ L2 exterior under roof)				
Residential (2 per unit)	258	25%	64	65
Restaurant, Commercial & Retail - 1/750 S.F.	4	75%	3	6
Office - 1/1,500 S.F. (minimum 4)	4	25%	1	3
Short term Total			68	74

OPEN SPACE				
Usable open space	REQUIRED	RH-6	MU-4	PROPOSED
	20% (MU-4)	9743'	12534'	13513'
Open space % (no reduction requested)			n/a	139%

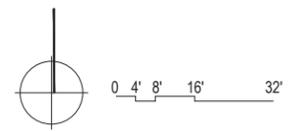
note: RH-6 calculated in aggregate with S'Park West

BUILDING PLACEMENT & SETBACKS		
	STANDARD	PROPOSED
MEREDITH STREET		
Minimum frontyard setback	0'	11'-11"
Minimum frontyard setback: 3rd story & above	20'	11'-11"
Maximum frontyard setback for interior lot	15'	n/a
BLUFF STREET		
Minimum sideyard setback to a street: 1st & 2nd stories	0'	0'-6"
Minimum sideyard setback to a street: 3rd story & above	12'	0'-6"
EAST PROPERTY LINE		
Minimum sideyard setback to an interior lot line	0' or 5'	3'-9"
JUNCTION PLACE		
Minimum sideyard setback to a street: 1st & 2nd stories	0' or 5'	12'-9"
Minimum sideyard setback to a street: 3rd story & above	12'	12'-9"



L1	117
L2	47
L3	22
L4	24

RH-6 zoning | MU-4 zoning

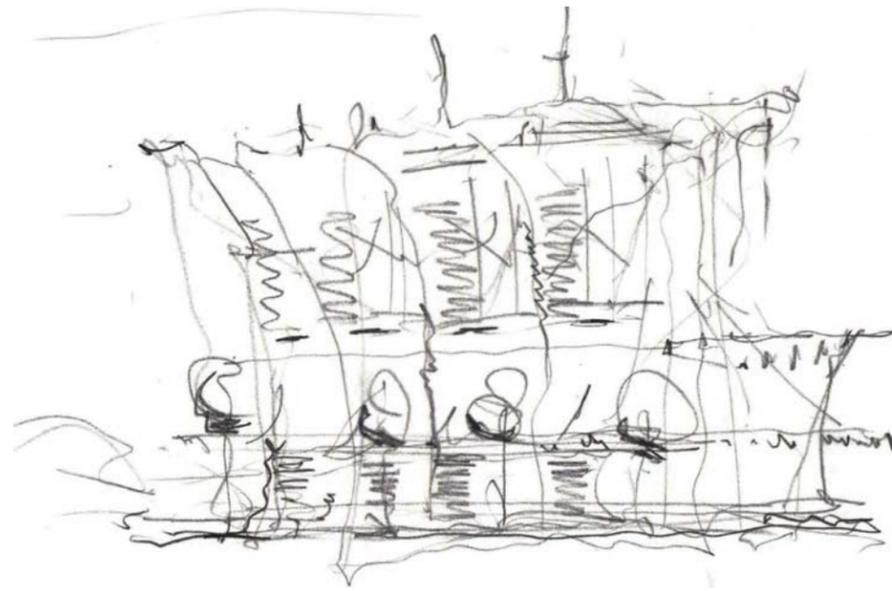
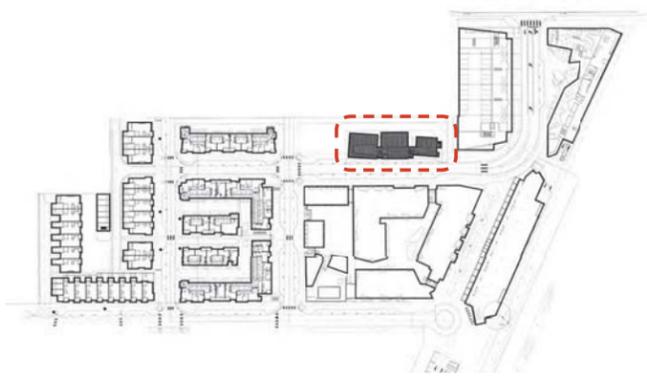


Site Plan

Figures for FAR and Open Space pertain to the portion of project in the MU-4 zone. For the portion in RH-6, refer to Project Fact Sheets, as Density and Open Space are calculated in the aggregate in this zone.

WORKSBUREAU | **TIMBERLOFTS**

S'PARK



On a long narrow site, we wanted to create a place that did not feel so long and narrow. A building that shifts slightly. One that feels like it might be moving.

So we created three pieces that twist softly, and an elevation that moves; one that feels musical and looks like a dance, with the ground floor transparent, pulled back, revealing a glimpse of Meredith Park beyond.



Situated on a long and narrow East– West oriented site, Meredith House provides a key transition element from mixed use to pure residential within S’PARK.

A dynamic and creatively designed structure that shifts in both plan and elevation, Meredith House takes advantage of its orientation to harvest ample daylighting for the 15 loft style units within.

The length of the building is separated into three main volumes with the entrance oriented east, towards the woonerf, to capture the energy and activity that will be at the heart of S’PARK. On the west side, the community room opens to the S’PARK pocket park, acting as an expansion of the ground floor. An open program to vitalize the open space.

With smaller setbacks, abundant onsite bicycle parking, and generous glazing at the ground floor, Meredith House is intentionally urban. Meredith House blends textural concrete masonry units that are produced regionally with a modern rainscreen cladding to reduce maintenance and increase material longevity. Touches of timber warm the palette and make a material nod to the history of the site.

High performance windows and superior insulation aid in the completion of an energy efficient and sustainable building envelope. The low slope roofs allow for plenty of solar PV panels to be installed.

The project houses 15 two-bedroom dwelling units that are offered at market rate. Each unit has one parking space as well as private storage at the ground level; useful for secure bicycle storage. Each floor has five units and by creating the separated building volumes, each unit is, in spirit, a corner unit. Private balconies introduce cadence to the elevations, carefully placed to take advantage of views and connect the residents to several energy centers within S’PARK.

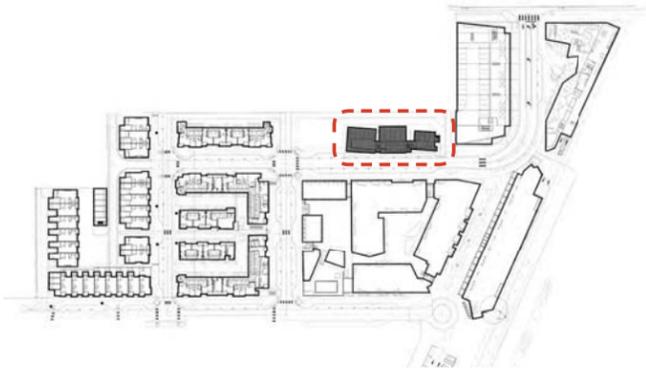
MEREDITH HOUSE

- Residential Condominiums
- 20,754 gsf
- 4 Story, 47’ - 2” (actual height from sidewalk)
- Ground Floor – Resident Parking, Amenities & Services
- Floors 2, 3 & 4 – Condos

Surround Architecture | Meredith House

August 21, 2015

S’PARK



MEREDITH HOUSE - SITE PLAN DATA

SITE AREA	PROPOSED
Site area	14552'

REQUESTED VARIATIONS FROM MU-4 ZONE REGULATIONS	
Building placement & setbacks	Front Setback for 3rd and 4th Floor
Building height/stories	

BUILDING AREA AND USE		
GROUND LEVEL USES	QUANTITY	AREA
Lobby and access to units above (Residential)		907'
Private Resident Storage (Residential)	15	464'
Tenant Community Space	1	585'
Retail/Micro Office	1	500'
Automobile parking as an accessory use (covered)		3509'
SECOND LEVEL USES		
Attached dwelling units (100% market rate housing units)		
2-bedroom units (Includes Circulation)	5	6196'
THIRD LEVEL USES		
Attached dwelling units (100% market rate housing units)		
2-bedroom units (Includes Circulation)	5	6196'
FOURTH LEVEL USES		
Attached dwelling units (100% market rate housing units)		
2-bedroom units (Includes Circulation)	5	5906'
TOTAL UNIT COUNT AND AREA (Residential)	15	20254'
TOTAL FLOOR AREA (Excludes Parking)		20754'
FAR		1.43

RESIDENTIAL DENSITY	EXISTING	PROPOSED
Units/acre	n/a	44.90
Lot area/unit	n/a	970'

PARKING	REQUIRED	PROPOSED
BOULDER JUNCTION ACCESS DISTRICT MEMBER		
Standard	10	10
Compact	5	5
Accessible	1	1
Bicycle		
Long Term (Provided in locked private storage closets)	22	30
Short Term	8	22

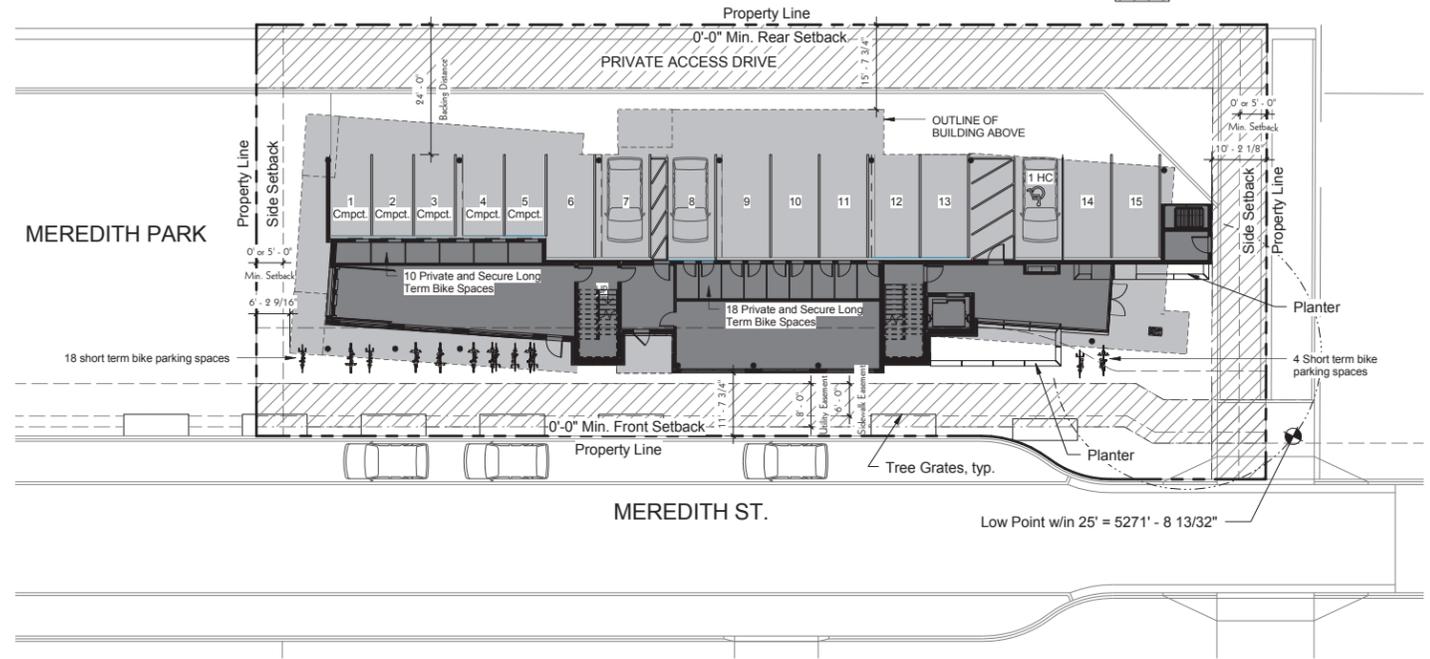
BUILDING PLACEMENT & SETBACKS	STANDARD	PROPOSED
MEREDITH STREET		
Minimum frontyard setback	0'	11'-6"
Minimum frontyard setback: 3rd story & above	20'	11'-6"
Maximum frontyard setback interior lot	15'	n/a
EAST property line		
Minimum sideyard setback from interior lot line	0' or 5'	10'
WEST property line		
Minimum sideyard setback from interior lot line	0' or 5'	6'-2"
NORTH property line		
Minimum rearward setback	0'	15'-6"

USEABLE OPEN SPACE	REQUIRED	PROPOSED
	20% of Site	
Private Open Space (Counts 25% toward Open Space)		
2nd Floor		370'
3rd Floor		370'
4th Floor		370'
TOTAL OF PRIVATE OPEN SPACE		1110'
25 % TOTAL OF PRIVATE OPEN SPACE		278'
Public Open Space		2998'
TOTAL OPEN SPACE	2910'	3276'

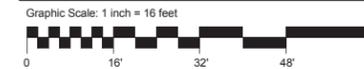
BUILDING HEIGHT		
Maximum building height (B.R.C. Calculation Method)	38'	48'-9"
Maximum number of stories	3	4

Area of Landscape at Ground Level = 303 SF
Please see the Open Space Plan for locations

- Building Footprint
- Outline of Building/Decks Above
- Easements



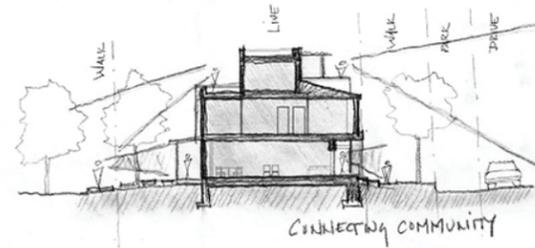
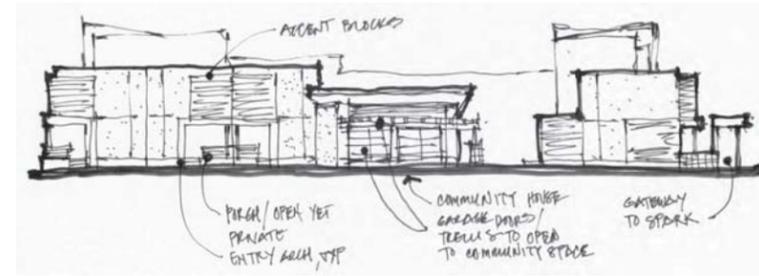
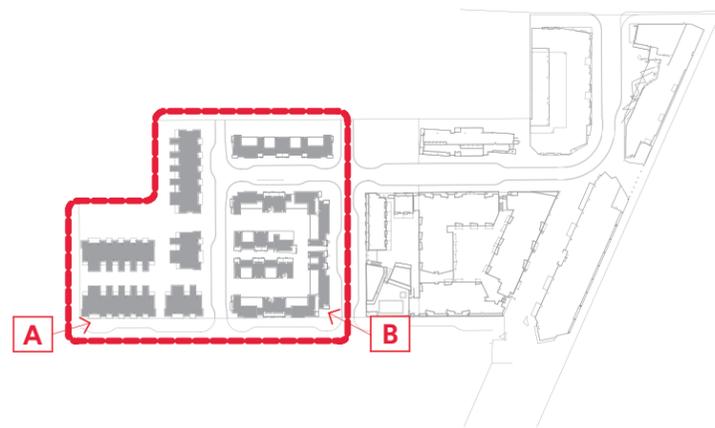
SITE PLAN



Site Plan

Surround Architecture | Meredith House





We started with a neighborhood vision, with front stoops and large porches and engaging upper decks that would connect the people with the ground, the sky, and the mountains. Knowing that there is an existing neighborhood fabric that can already be found adjacent to the site, but has not yet reached its potential, we set about to add additional architecture that adds to the neighborhood in a positive and welcoming manner. Inclusive sites with welcoming permeability throughout.



The western zone of the S'PARK project is dedicated to family housing in primarily townhouse units, with a few flats at corners of blocks, in order to help maintain the continuous block face pattern as the building turns the street corners.

All units are brought close to the street, with individual porches that are raised from 12-21", maintaining a sense of separation from the public realm, while having a very direct connection to the pedestrian zone. The inspiration for this model comes from making a pedestrian friendly and engaging walking street along Bluff and all of the townhouse zone of the project.

Internally, from west to east, there is a shared common series of open spaces that tie the site back to TIMBER and crossing the various blocks. Each have their own character and each have residences fronting directly on green space. Central to the site is a Community Commons Building that mostly serves the affordable community, while the western site has an outdoor covered picnic area fronting onto its outdoor green space.



S'PARK_{west}

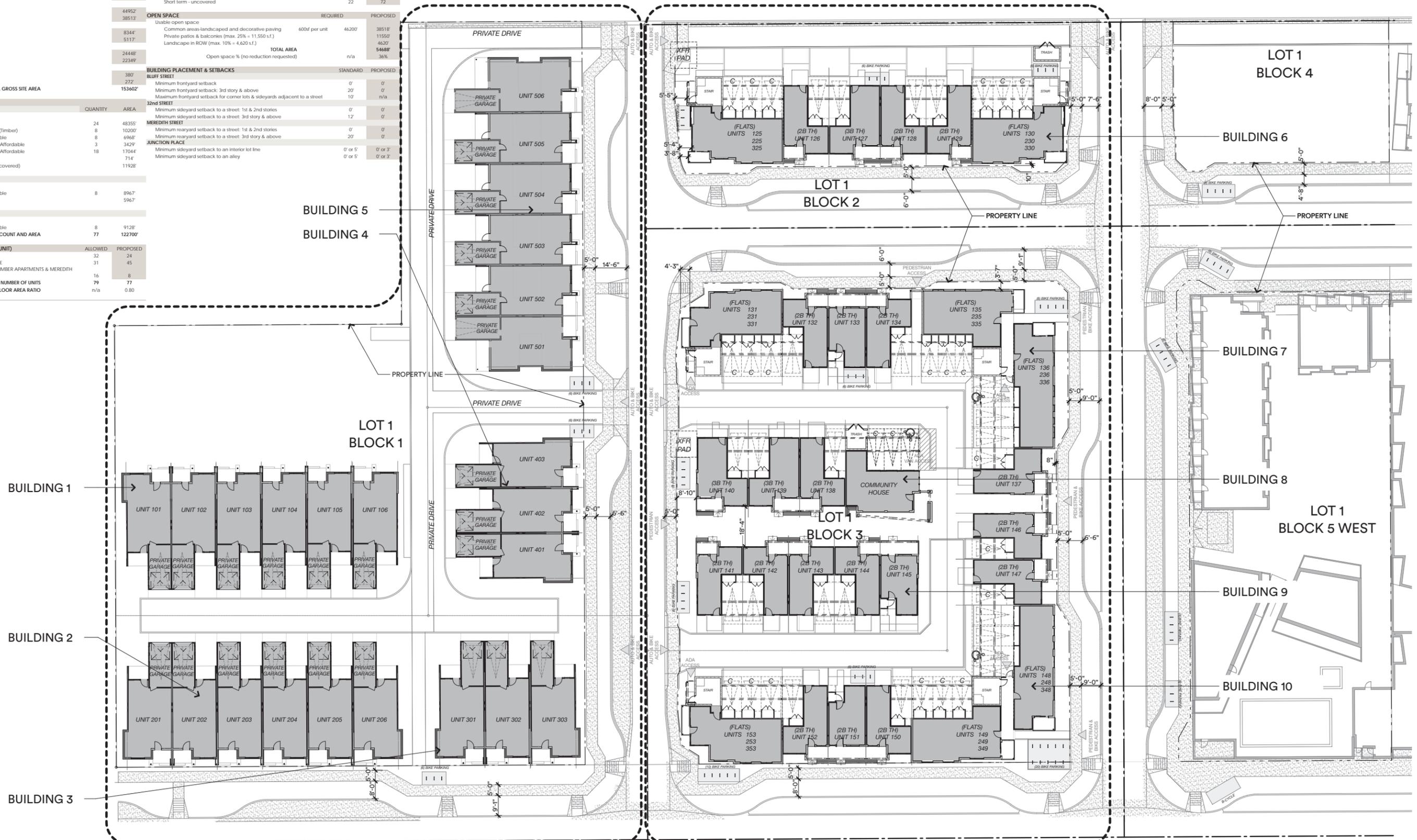
- Mixed Residential - 3085 Bluff Market Rate Townhomes and 3155 Bluff Permanently Affordable Townhomes and Flats
- 122,700 gsf
- 3 Story, 3085 Code Height: 34'-8", max. (Perceived Height: 30'-6"); 3155 Code Height: 37'-10", max. (Perceived Height: 34'-2")
- Ground Floor - Resident Parking, Flats, First Floor Townhomes, Community House
- Floors 2 & 3 - Flats and Townhomes
- 45 covered parking spaces & 24 private garages

SopherSparn | S'PARK_{west}

August 21, 2015

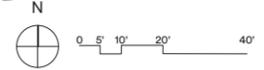
S'PARK

SITE AREA		PROPOSED	REQUIRED	PROPOSED
LOT 1, Block 1				
Gross site area		40194'	77	61
Net site area		50401'	n/a	23
LOT 1, Block 2				
Gross site area		15664'	2	3
Net site area		9984'	90	90
LOT 1, Block 3				
Gross site area		44952'	22	72
Net site area		38513'		
LOT 1, Block 4				
Gross site area		8344'	600f per unit	46200'
Net site area		5117'		38518'
LOT 1, Block 5 (RH-6 area, west of zoning line)				
Gross site area		24448'		11550'
Net site area		22349'		4620'
LOT 2, Block 4 (RH-6 area, west of zoning line)				
Gross site area				54688'
Net site area				36%
TOTAL GROSS SITE AREA				
		153602'		
BUILDING AREA & USE				
GROUND LEVEL USES				
Attached dwelling units				
3-bedroom townhouse units-Market Rate	24	48355'		
2-bedroom townhouse units-Market Rate (Timber)	8	10200'		
2-bedroom flat units-Permanently Affordable	8	6968'		
3-bedroom townhouse units-Permanently Affordable	3	3429'		
2-bedroom townhouse units-Permanently Affordable	18	17044'		
Community House		714'		
Automobile parking & stairs (accessory uses - covered)		11928'		
SECOND LEVEL USES				
Attached dwelling units				
3-bedroom flat units-Permanently Affordable	8	8967'		
Resident amenities		5967'		
THIRD LEVEL USES				
Attached dwelling units				
3-bedroom flat units-Permanently Affordable	8	9128'		
TOTAL UNIT COUNT AND AREA	77	122700'		
RESIDENTIAL DENSITY (ALLOWABLE=1800 sf/UNIT)				
BLOCK 1 - MARKET RATE SITE	32	24		
BLOCKS 2 & 3 - PERMANENTLY AFFORDABLE SITE	31	45		
BLOCKS 4 & 5 - RH-6 ZONED PORTION OF THE TIMBER APARTMENTS & MEREDITH PARK	16	8		
TOTAL NUMBER OF UNITS	79	77		
FLOOR AREA RATIO	n/a	0.80		



3085 BLUFF
(MARKET RATE TOWNHOMES)

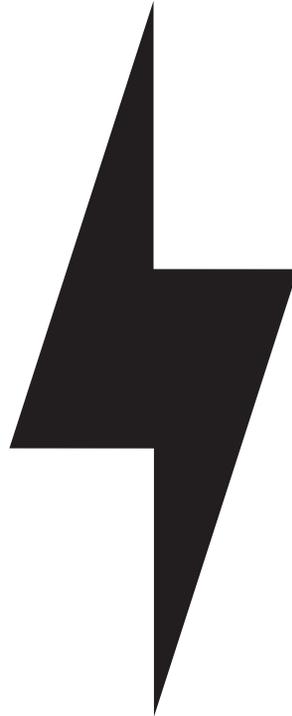
3155 BLUFF
(PERMANENTLY AFFORDABLE FLATS & TOWNHOMES)





Site Review

August 21, 2015
Boulder Junction
Boulder, CO



FROM THE TRANSIT VILLAGE AREA PLAN

Vision

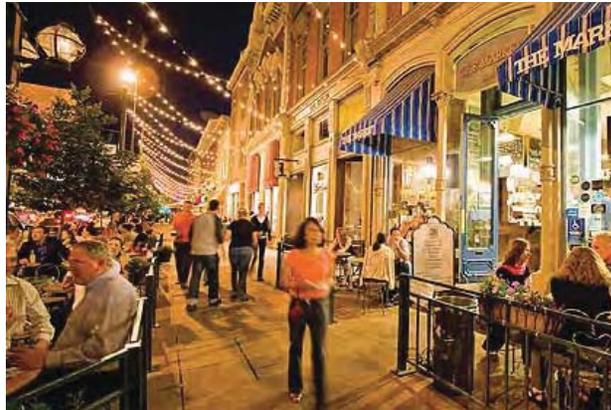
At the outset of the Transit Village area planning process, the City Council and Planning board adopted the following direction for the development of the plan.

The Transit Village area will be:

- *A lively and engaging place with a diversity of uses, including employment, retail, arts and entertainment, with housing that serves a diversity of ages, incomes and ethnicities.*
- *A place that is not overly planned, with a “charming chaos” that exhibits a variety of building sizes, styles and densities where not everything looks the same.*
- *A place with both city-wide and neighborhood-scale public spaces.*
- *A place that attracts and engages a broad spectrum of the community, not just people who live and work here or come here to access the transit in the area.*
- *A place that emphasizes and provides for alternative energy, sustainability, walking, biking and possible car-free areas, e.g. “eco-village”*



WHAT MAKES A VIBRANT URBAN PLACE?



August 21, 2015

SPARK



August 21, 2015

SPARK

S'PARK responds to our community's call to create a neighborhood center in Boulder Junction, that is designed with intention and care, and focuses on quality, not quantity. It aspires to be a world-class place utilizing cutting-edge modern urban sustainability, a remarkable venue for the exchange of interests and ideas and a socially inclusive place to live and work for Boulder's future generations.

An intentional place that fulfills the community's values and aspirations for Boulder Junction.

VISION

S'PARK: a modern village with active greenspaces and cutting-edge sustainability for people to live, work, eat and play – a true mixed-use, mixed-income and transit-oriented place for Boulder Junction. A place for the crafters, the makers and the innovators.

- Vibrant & Fun
- Modern & Sustainable
- Urban & Connected

Through a balanced and diverse mosaic of building design, uses and inhabitants, S'PARK will set an example as a world-class place that promotes innovation, social equity, and our unique local Boulder culture.

A Vibrant & Complete Place

Diversity - S'PARK offers Boulder an unprecedented diversity of housing types and functional commercial spaces – and affordable options of each.

Housing Options - S'PARK addresses diverse housing needs with an innovative mixed-income approach:

- Market Rate Townhomes, for sale (24)
- Permanently affordable townhomes, for rent (46)
- Workforce flats, live-work and townhomes, for rent (129)
- Condo Flats, for sale (16)
- Permanently affordable flats, for rent (32)

With higher-end ownership residences as well as 31% of the project's total housing units as permanently affordable flats or townhomes – and many options in between – S'PARK's program will simultaneously create the homeownership desired to anchor a newly established neighborhood and also promote upward economic mobility through access to unparalleled energy-efficiency, transit and healthy living. All residents would receive Eco-Passes to support their family's transit needs.

While much of the city's family-friendly housing is in single family residential neighborhoods, S'PARK fulfills a market need for a variety of household types and incomes in a pedestrian, transit and bike-friendly neighborhood accessible to existing amenities.

Affordable and Creative Commercial - The project's commercial spaces are designed for flexibility as Boulder Junction seasons over time, with suites intended to accommodate local creative agencies, natural food companies, non-profits and small-scale entrepreneurship.

- Eclectic ground level restaurant, micro-restaurant, retail spaces
- Office space designed for smaller companies and access to light and views
- Eco-Passes for all employees

S'PARK has already dedicated a prominent portion of its street-level commercial space under a partnership with Community Cycles for its new shop, office, and retail space to serve Boulder's insatiable appetite for bike culture – both practical and recreational. Community Cycles will also be able to play a key support role in the integration of bikes and mass transit for all S'PARK inhabitants under the TDM (Travel Demand Management) and BJAJ (Boulder Junction Access District) trip reduction strategies. Among other responsibilities, Community Cycles may be able to help administer the Eco-Pass office for BJAJ and help maintain the 714 bike parking spaces in the project.

Designed for Collaboration - Spontaneity happens when residents, workers, and visitors can meet-up and collaborate at the local coffee shop before heading to the Bus Rapid Transit station or at the brewpub after work. Old friends reunite, deals are made, future plans set – such daily occurrences will be the result of a robust walkable environment and will serve Boulder's innovation, emerging company, and non-profit sectors where making such connections is critical.

Eclectic - The S'PARK ideal will be to attract and curate a thoughtful variety of eclectic and artisan retail and restaurant tenants and businesses. As culinary crafters finish lunch service for daytime workers at one of S'PARK's unique micro-restaurants, they'll prepare for the evening crowd of S'PARK residents, neighboring families, and Boulder diners. The mix of uses and users will allow for a vibrant and culturally self-sustaining place, day and evening.

Local and Authentic - S'PARK will be a place where Boulderites can experience a simpler, more authentic and local offering of food, music, coffee, beer, and art. The S'PARK team expects to assist initially in programming monthly art walks, ciclovía-styled events, themed gatherings and cooperated cultural events in places of business and in the woonerf area. We'll know we've been successful when the first families ride their bikes down to S'PARK for ice-cream on a summer night or the first entrepreneur toasts her team at the brewpub.

Vibrant - The curating of the eclectic tenant mix along the project's woonerf will create the most vibrancy in the S'PARK atmosphere and the spaces are setup for success in that regard as they are compact and could accommodate 8-10 tenants along the woonerf alone. The S'PARK team expects an eclectic set of retail and restaurant entrepreneurs that will cater to families, diners, visitors, local gourmets, and those seeking local food and craft options. The woonerf will provide a viable and interactive place for people, for events, or simply sometimes nothing at all – the gift of space is additive to any place.

A Modern & Sustainable Place

Mindful Site Planning - At the core of S'PARK's design ethic is an equal focus on the buildings and the spaces in between:

- Buildings respond to the activities at ground level
- Site planning and building articulation honor view corridors and circulation patterns with intention
- Architecture promotes design-diversity and inspires with authentic materials that respect the industrial heritage of the site and area

A Place with Heritage but Forward Looking - What will make S'PARK modern is establishing a new context that respects the history of the site – undeniably agricultural-then-Industrial. The project intends to reuse a number of materials already on site from the previous operation of the lumberyard on the site. Materials include deconstructed and aged lumber, steel racks, recycled asphalt, and repurposed railroad track into public spaces, building features and landscape solutions.

Helletic Sustainability - As a LEED Platinum Neighborhood, S'PARK's sustainability aspects will be marked by not just the project's proposed LEED-Neighborhood Development Platinum certification, but the high-performance of its buildings and infrastructure to serve the residents.

Innovative Strategies that Leverage City Investment - The project's election to be included in the Travel Demand Management (TDM) and Boulder Junction Access District (BJAJ) is a part of its commitment to creating a transit-oriented place in partnership with City of Boulder and in alignment with the community's carbon reduction goals and TVAP. This partnership is funded significantly by a property tax mill levy placed on each of the buildings. In exchange for this participation, all S'PARK inhabitants can expect the prestigious system-wide RTD Eco-Passes, car share programs, electric vehicle charging stations and neighborhood bike share improvements (such as B-Cycle).

Transit and Energy Efficiency as Quality of Life - The S'PARK team believes that modern sustainability transcends energy issues alone. S'PARK's philosophy of sustainability goes beyond energy performance and celebrates the project's ability to provide an extraordinary transit-rich place, a central and walkable location, excellent access to bike and walking paths as well as services, shopping, healthcare, education, and employment. All of these tangential benefits translate into a more holistically sustainable place and results in healthier living and a place where people use less energy to live, work, and recreate.

An Urban & Connected Place

Transit Rich Great Spaces - S'PARK's provision of a future potential train platform at the terminus of Bluff Street is an optimistic, but prudent gesture. However, in the meantime, the woonerf and Markt plaza will serve as interactive community nodes and concentrations of activity for residents, workers and visitors before heading to Goose Creek to navigate Boulder via bicycle.

Creating an Atmosphere of Exchange - Access to the Bus Rapid Transit, as well as the unprecedented biking and walking options will make S'PARK feel connected to people and the rest of the city in a fun way. Much like Union Station in Denver (where the Boulder BRT now connects) there is an atmosphere of "exchange". That exchange applies to the people who are coming and going, but also to those peoples' diverse tastes and interests. S'PARK and the Boulder Junction area will be a unique place in Boulder that will be able to capture that concept of exchange for people, interests and ideas.

Active and Passive Outdoor Spaces - S'PARK's Meredith Park will provide a place for play for families as well as a respite and place for quiet contemplation for workers from a highly technologized world. The project team believes that not all spaces need buildings and detailed programming and that this virtue will set S'PARK apart as a place from other urban areas in Boulder. At the heart of S'PARK's spaces is an inviting sense of inclusivity and a hope that S'PARK becomes the neighborhood center to serve the greater Boulder Junction area. In addition, all of S'PARK's retail spaces are adjacent to plaza or woonerf where parents can enjoy great local food offerings, while children can explore and play in plain view. S'PARK's outdoor spaces are designed with intention so that they are utilized and enjoyed.

Using "Place" to Improve Social Equity - Improved cycling, walking and park opportunities are an anti-poverty measure, as these features return spaces for public use. S'PARK appropriates over 65,000sf of land area (or 1.92 acres) from a gross land area of 428,471sf (9.9 acres) to public use and/or right of way – not including the woonerf, Markt Plaza, Meredith Park. All buildings have primary and secondary accesses and permeability – as well as day-lit stair cores – to provide convenience and connectivity, to break up building mass and promote an inspiring walkable fabric.

Inviting Serendipitous Interaction - In a community of entrepreneurs, creators, and innovators, we are all seeking connection with one another. S'PARK leverages its woonerf, Meredith Park, Markt plaza and its active greenspaces and activity nodes and front steps on each ground level residence to leverage opportunities for interaction and advance our community ideals.

Written Statement



August 21, 2015



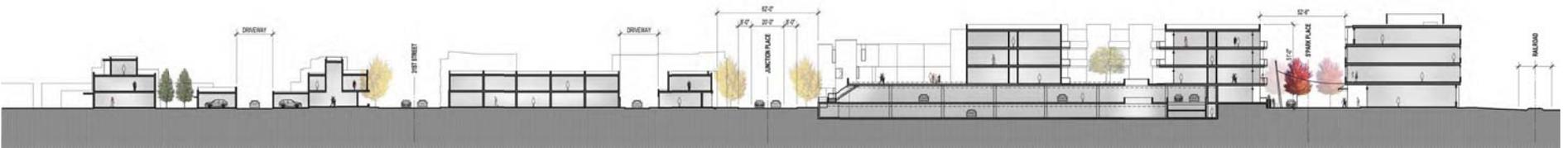
Valmont Gateway



S'PARK Place 'Woonerf'



Meredith Park Streetscape



East/West Site Section

August 21, 2015



Project Mixed Use Summary

Residential (Market Rate)

- 24 for-sale Townhomes
- 129 for-rent Apartments
- 15 for-sale Flats

Residential (Permanently Affordable)

- 45 for-rent Townhomes
- 32 for-rent Flats

Commercial (Market Rate)

- 92,717 sf Creative Office
- 32,897 sf Restaurant / Retail
- 3 Micro-Restaurants, 1 Brewpub

Project FAR Allowed / Provided:
2.00 / 1.62

Project Parking Summary

Bike Parking

Long term	460
Short term	244

Total 714

Plus: (2) B-Cycle Stations

Auto Parking

Spaces	389+2 EV
Accessible	13
On-Street	46+4 EV

Total 434

MEREDITH HOUSE / MEREDITH PARK

For Sale Condominium Lofts / Neighborhood Place

- 15 Condos, approx 1200 sf ea.
- Zone: MU-4
- Actual height: 47'-2" (4 Stories)
- Building Area 20,754 GSF
- Auto Parking Required / Provided 0/16 (Incl. 1 Accessible)
- L/T Bicycle Parking Required / Provided: 22/30
- S/T Bicycle Parking Required / Provided: 8/22
- Usable Open Space Required / Provided: 2,910sf/3,276 sf
- Large Trees: cool summer shade, warm Fall color
- Eastern Redbud bloom in Spring; European Larch in Fall
- Play turf, with cluster of climbable boulders
- Cor-ten screen as north visual backdrop to the Park

CICLO

Community-Oriented Mixed Use, Non-Profit Space, Permanently Affordable Rental Flats

- Zone: MU-4
- Actual height: 44'-6" (4 Stories: 3 + Mezzanine)
- Building Area 57,901 GSF
- Uses: Retail/Commercial, Office, (32) Residential Units (1, 2, 3-bedroom)
- 16,033sf Retail/Office (ground floor)
- Auto Parking Required / Provided 0/23 (Incl. 1 Accessible)
- L/T Bicycle Parking Required / Provided: 42/42
- S/T Bicycle Parking Required / Provided: 24/24
- Usable Open Space Required / Provided: 6,387sf/16,463sf

MARKT

Trackside Commercial Building with Brewpub and Plaza

- Zone: MU-4
- Actual height: 49'-1" (3 Stories + 2 Mezza)
- Building Area 55,340 GSF
- Uses: Brewpub, Micro-Restaurants, Office
- Auto Parking Required / Provided 0/0
- L/T Bicycle Parking Required / Provided:
- S/T Bicycle Parking Required / Provided:
- Usable Open Space Required / Provided:

NEW MULTI-USE PATH

The RAILYARDS

Trackside Commercial Building along Woone

- Zone: MU-4
- Actual height: 50'-0" (4 Stories)
- Building Area 70,155 GSF
- Uses: Retail/Commercial/Restaurant, Of
- Auto Parking Required / Provided 0/61 (l
- L/T Bicycle Parking Required / Provided:
- S/T Bicycle Parking Required / Provided:
- Usable Open Space Required / Provided:

TIMBER

Workforce Flats, Townhomes, Live-Work Units, Ground Level Retail/ Restaurant and Parking Garage -across five (5) buildings

- Zone: RH-6/MU-4
- Actual height: 49'-8" (4 Stories)
- Total Building Area (across 5 buildings) 214,043 GSF (includes 216 car parking garage)
- Uses: Residential, Retail/Restaurant, Parking - 129 Residential Units (1, 2, 3-bedroom), storage and common areas - 3,230sf Retail/Restaurant
- Auto Parking Provided/Required: 216/8 (in proposed partnership with Boulder Junction Access District for "shared and unbundled" management)
- L/T Bicycle Parking Required / Provided: 198/210
- S/T Bicycle Parking Required / Provided: 68/74
- Usable Open Space (MU-4) Required / Provided: 9,743sf / 13,513sf

S'PARK west

Family-Oriented, Mixed-Income Townhouses

- 24 'For Sale' Market Rate and 45 'For Rent' Permanently Affordable
- 2-3 Stories
- 2 and 3-bedroom townhouses
- Zone: RH-6
- Auto Parking Required / Provided 77/87 (Incl. 1 Accessible)
- L/T Bicycle Parking Required / Provided: 68/90
- S/T Bicycle Parking Required / Provided: 22/72
- Usable Open Space Required / Provided: 46,200 sf/ 54,688 sf

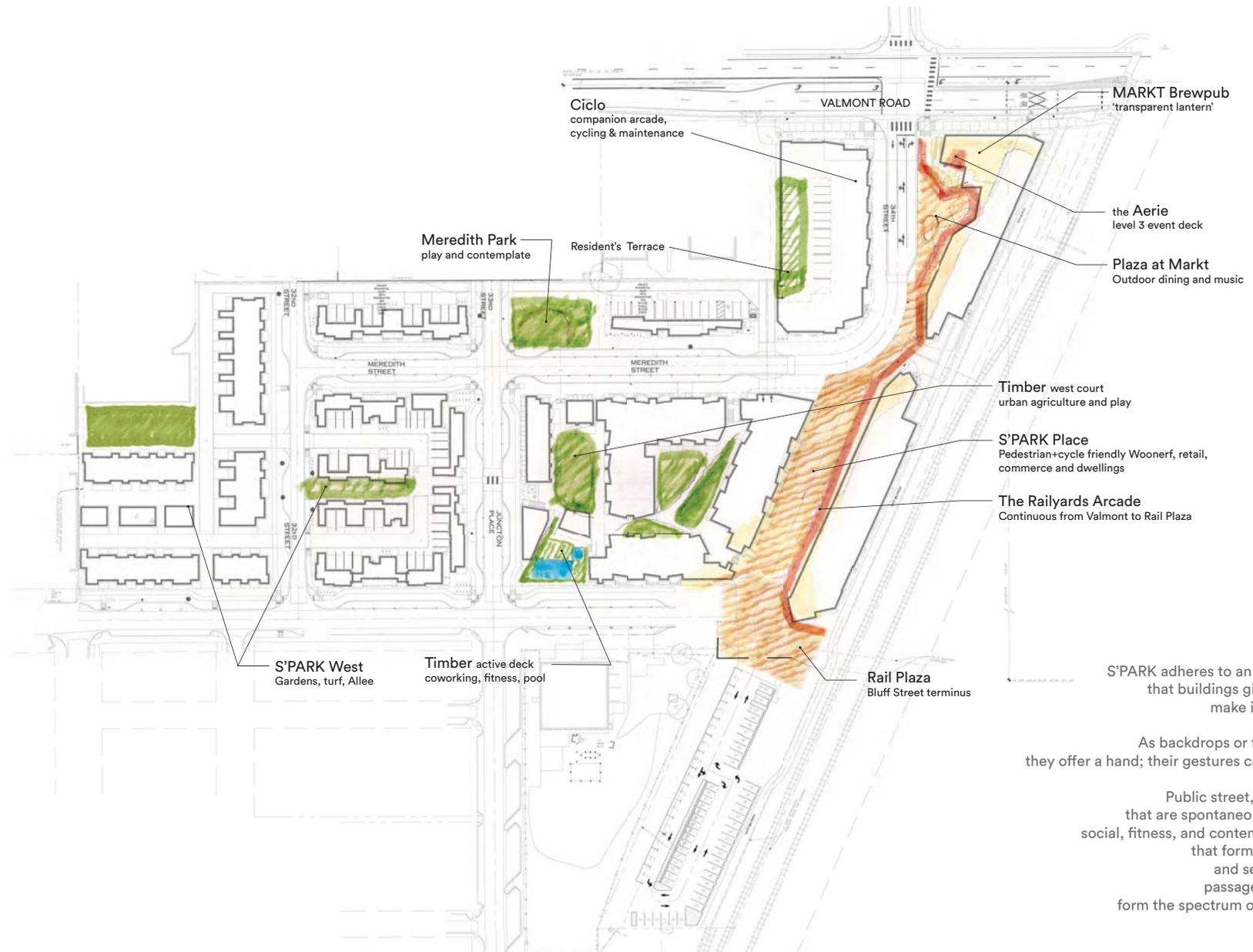
WOONERF and PLAZA

- New 'Rail' Plaza
- Integrated Woonerf pedestrian area
- 4 - EV parking spaces

SOUTH FINGER

- Future phase area
- Allows for 'organic' future growth
- Temporary parking area
- Existing 3,000sf office building (1 Story)
- 35 Temporary parking spaces





0 20' 50' 100'

Site Design: Placemaking

August 21, 2015



S'PARK adheres to an urban design principle that buildings give form to civic space, make it cohesive and bodied.

As backdrops or facets of these 'frames' they offer a hand; their gestures can invigorate a district.

Public street, plaza and park spaces that are spontaneous, flexible, and active; social, fitness, and contemplative private spaces that form counterpoint 'retreats' and semi-public, in-between passages and garden domains form the spectrum of places within S'PARK



- ▬ Grade Level View
- Elevated Vista
- ▬ 'Blue Sky' Window

The built form of S'PARK will make the most of natural assets. As structure and street section give frames to views, these opportunities have been considered throughout: from gateway entry, to the S'park Place 'woonerf' mountain alignment, to Meredith Park and Bluff Street terminus.

The cascading stair-step massing enhances viewshed for residents, while upper offices and outdoor meeting decks take in the front range.

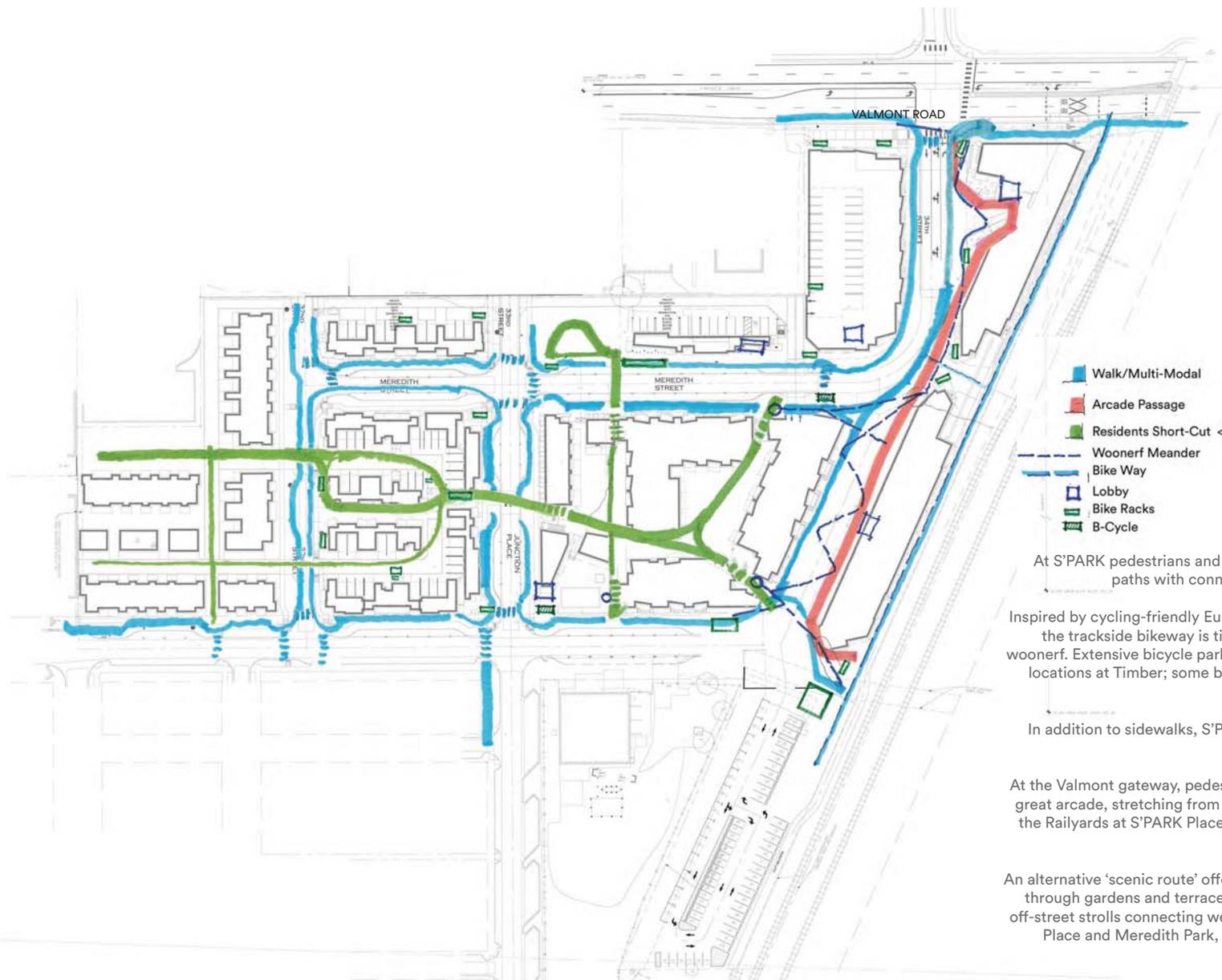
Finally the Aerie event deck, level 3 at Markt gives an elevated mountain vista to the greater public—while overlooking the active scene.

0 20' 50' 100'

Site Design: Viewsheds

August 21, 2015





- Walk/Multi-Modal
- Arcade Passage
- Residents Short-Cut <short cut>
- Woonerf Meander
- Bike Way
- Lobby
- Bike Racks
- B-Cycle

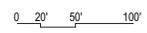
At S'PARK pedestrians and cyclists will enjoy a multiplicity of paths with connectivity provided at several levels.

Inspired by cycling-friendly European and American precedents, the trackside bikeway is tied into multi-modal paths and the woonerf. Extensive bicycle parking is provided including covered locations at Timber; some buildings will showcase visible bike storage rooms.

In addition to sidewalks, S'PARK has embedded porosity in its planning.

At the Valmont gateway, pedestrians discover the beginning of a great arcade, stretching from the brewpub 'lantern' at Markt, to the Railyards at S'PARK Place, to the Rail Plaza at the south—a complete gesture.

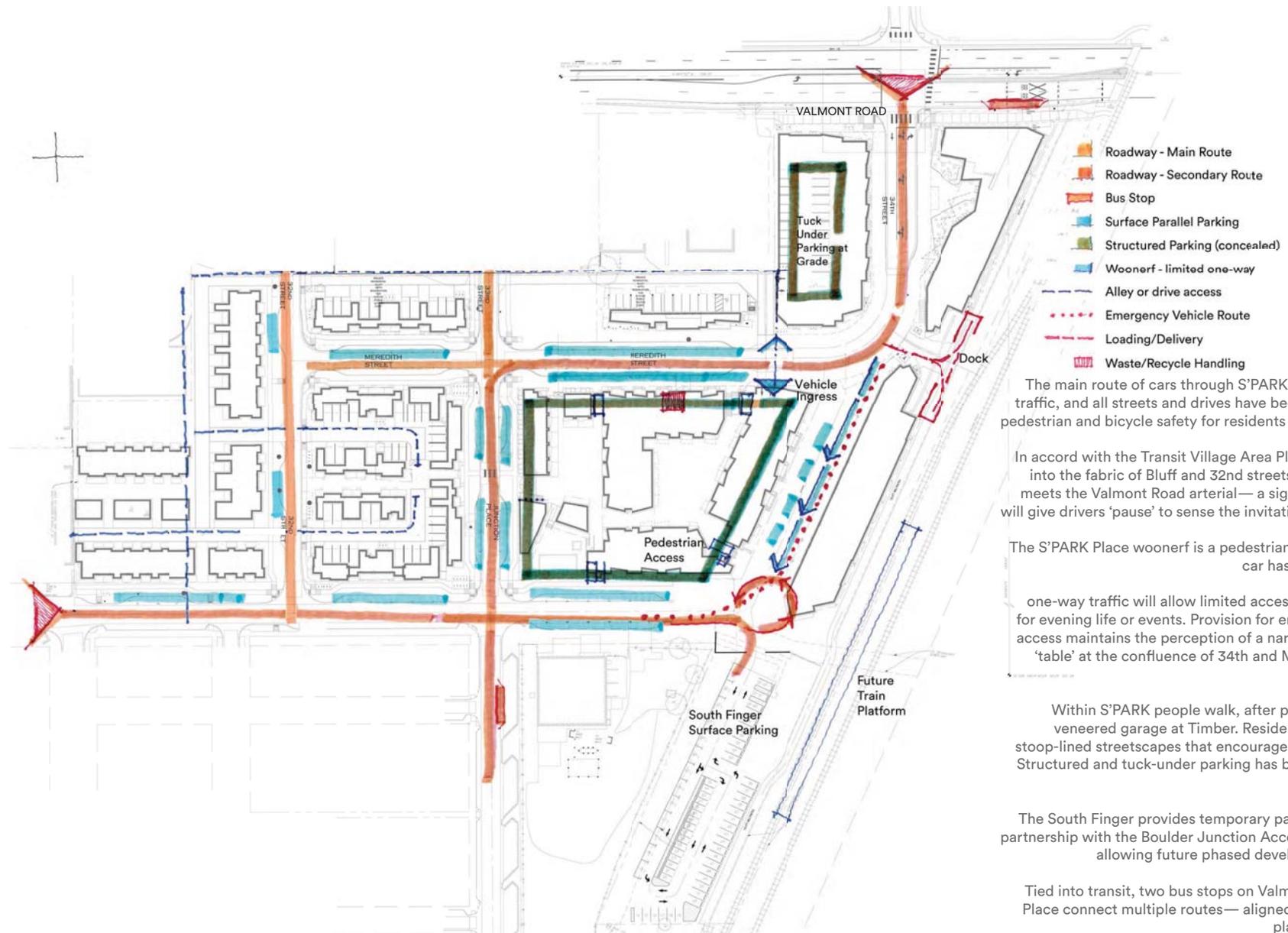
An alternative 'scenic route' offers residents and guests shortcuts through gardens and terraces of Timber. These are convenient off-street strolls connecting west to east, with linkage to S'PARK Place and Meredith Park, and amenities such as Fitness and Coworking space at Timber.



Site Design: Permeability

August 21, 2015





The main route of cars through S’PARK is geared to slow traffic, and all streets and drives have been considered for pedestrian and bicycle safety for residents and visitors alike.

In accord with the Transit Village Area Plan, roads are tied into the fabric of Bluff and 32nd streets, and 34th Street meets the Valmont Road arterial—a signaled point that will give drivers ‘pause’ to sense the invitation of the district.

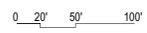
The S’PARK Place woonerf is a pedestrian street where the car has been minimized:

one-way traffic will allow limited access; at times closed for evening life or events. Provision for emergency vehicle access maintains the perception of a narrow lane, and the ‘table’ at the confluence of 34th and Meredith will slow vehicular traffic.

Within S’PARK people walk, after parking in the fully veneered garage at Timber. Residences around form stoop-lined streetscapes that encourage moderate speed. Structured and tuck-under parking has been concealed at all buildings.

The South Finger provides temporary parking solutions in partnership with the Boulder Junction Access District, while allowing future phased development to occur.

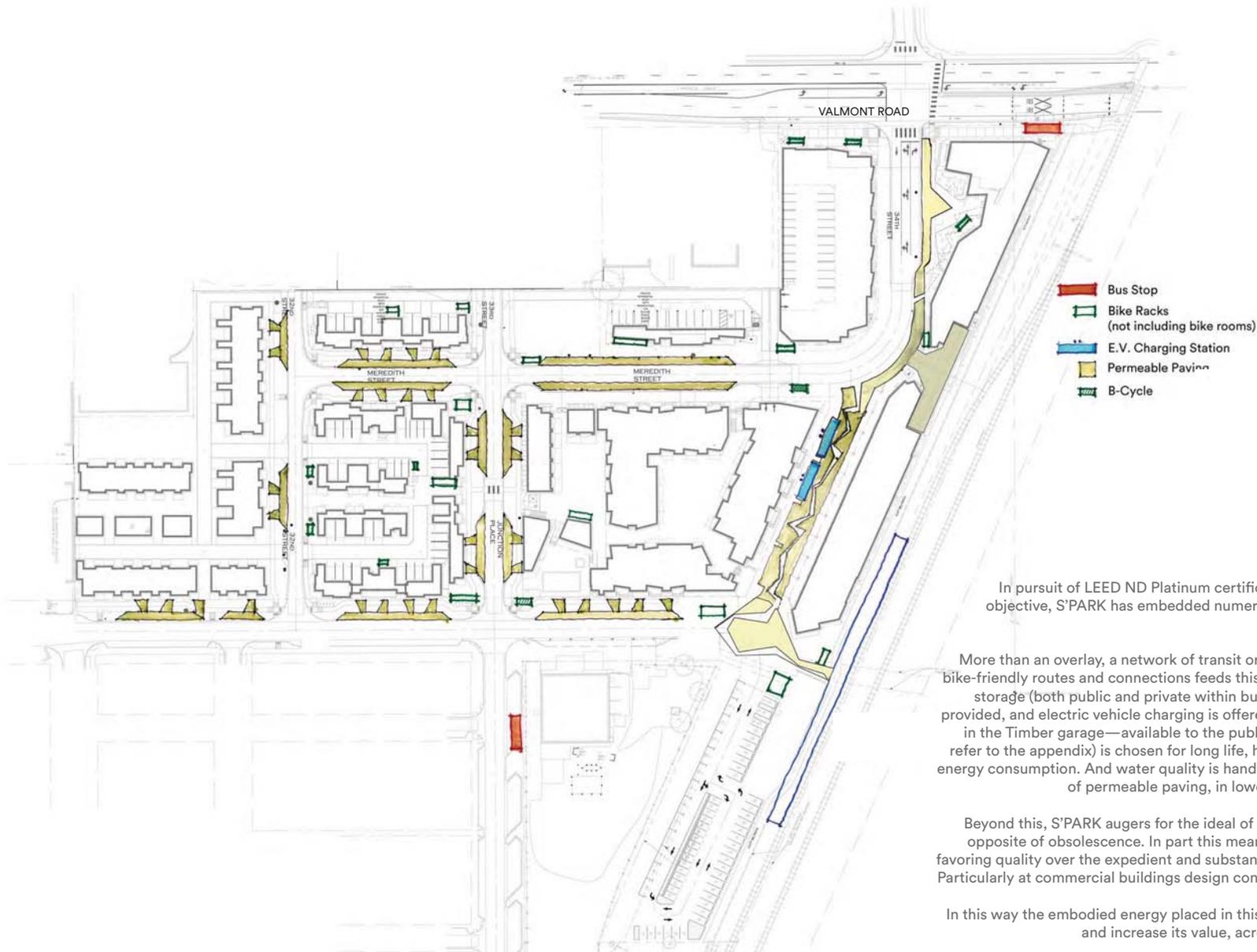
Tied into transit, two bus stops on Valmont and Junction Place connect multiple routes— aligned with sustainable planning principles.



Site Design: Vehicular Movement

August 21, 2015





In pursuit of LEED ND Platinum certification, as a sustainable objective, S'PARK has embedded numerous ecologically sound attributes in planning.

More than an overlay, a network of transit oriented, pedestrian and bike-friendly routes and connections feeds this walkable district. Bike storage (both public and private within buildings) is conveniently provided, and electric vehicle charging is offered in S'PARK Place and in the Timber garage—available to the public. Site lighting (please refer to the appendix) is chosen for long life, high efficiency, and low energy consumption. And water quality is handled in part by provision of permeable paving, in lower traffic impact zones.

Beyond this, S'PARK augurs for the ideal of planned relevance: the opposite of obsolescence. In part this means design for longevity: favoring quality over the expedient and substance over the superficial. Particularly at commercial buildings design considers future flexibility.

In this way the embodied energy placed in this community will retain and increase its value, across future generations.

0 20' 50' 100'

Site Design: Sustainability Key Features

August 21, 2015





The success and vibrancy of the urban scene, outdoor life, commerce and retail at S'PARK will be fueled by people—who in turn draw others to join the experience.

To foster this, our planning has mapped visual connections to places and nodes of activity. The plan forms of buildings, or sometimes the undercut of a ground floor (to open a sightline and offer shade) has been designed to multiply such connections. Where these 'Lines of Desire' converge the opportunity is ripe for people to gather.

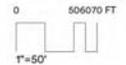
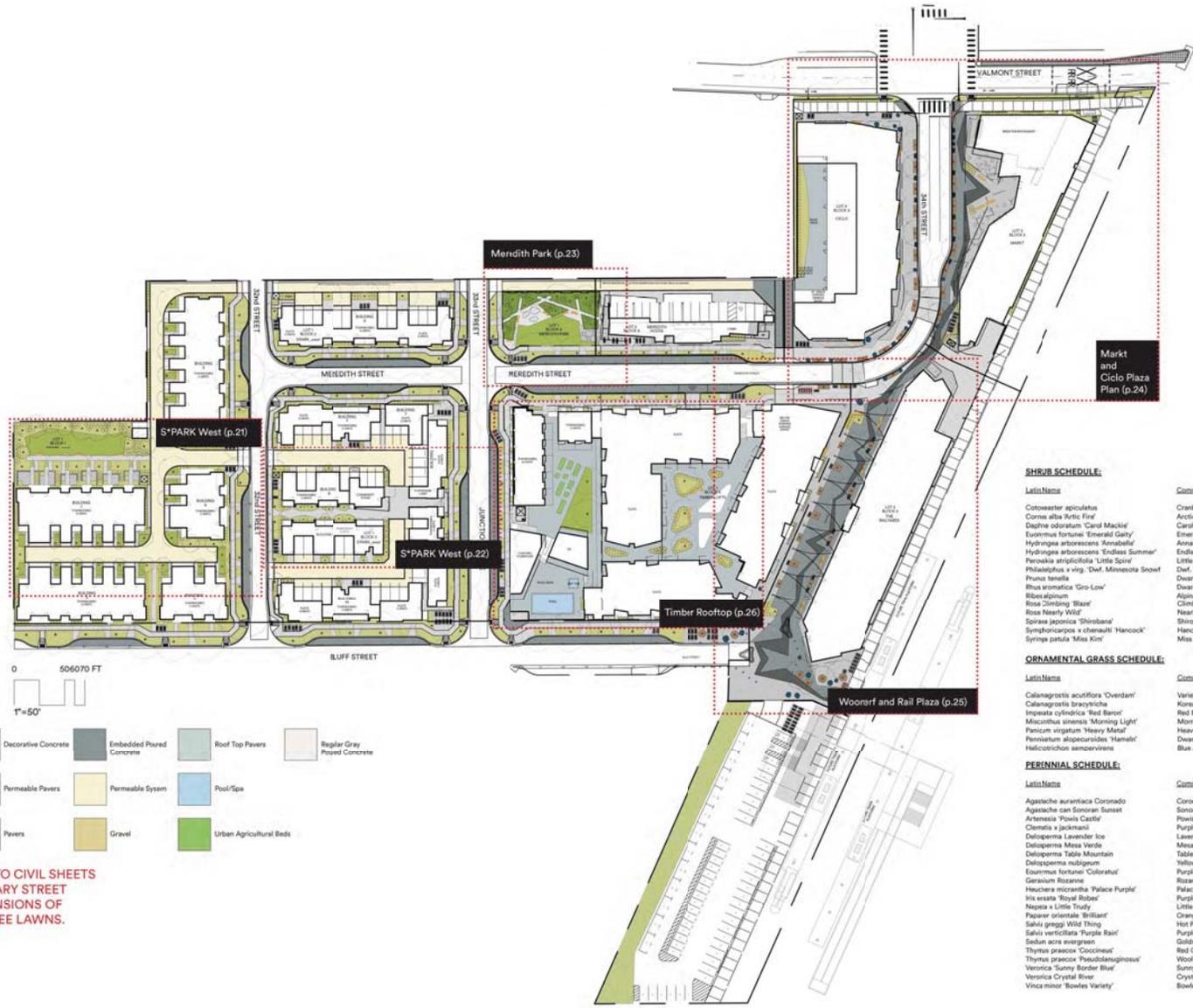
Attractions enhance the draw: restaurant terraces, live music, an outdoor fire on the plaza; shade, vegetation, play space or seating. When people park at Timber, they will be greeted by sightlines to cafés and shops. And where the pedestrian friendly environment offers the sense of security and wellbeing, people are likely to make it their own.

0 20' 50' 100'

Site Design: Activation

August 21, 2015

S'PARK



- Rivelle Turf
- Decorative Concrete
- Embedded Poured Concrete
- Roof Top Pavers
- Regular Gray Poured Concrete
- Mixed Low Planting
- Permeable Pavers
- Permeable System
- Pool/Spa
- Shade Garden
- Pavers
- Gravel
- Urban Agricultural Beds

NOTE: PLEASE REFER TO CIVIL SHEETS C4.8 - C4.9 "PRELIMINARY STREET SECTIONS" FOR DIMENSIONS OF PLANTING STRIPS / TREE LAWNS.

SHRUB SCHEDULE:

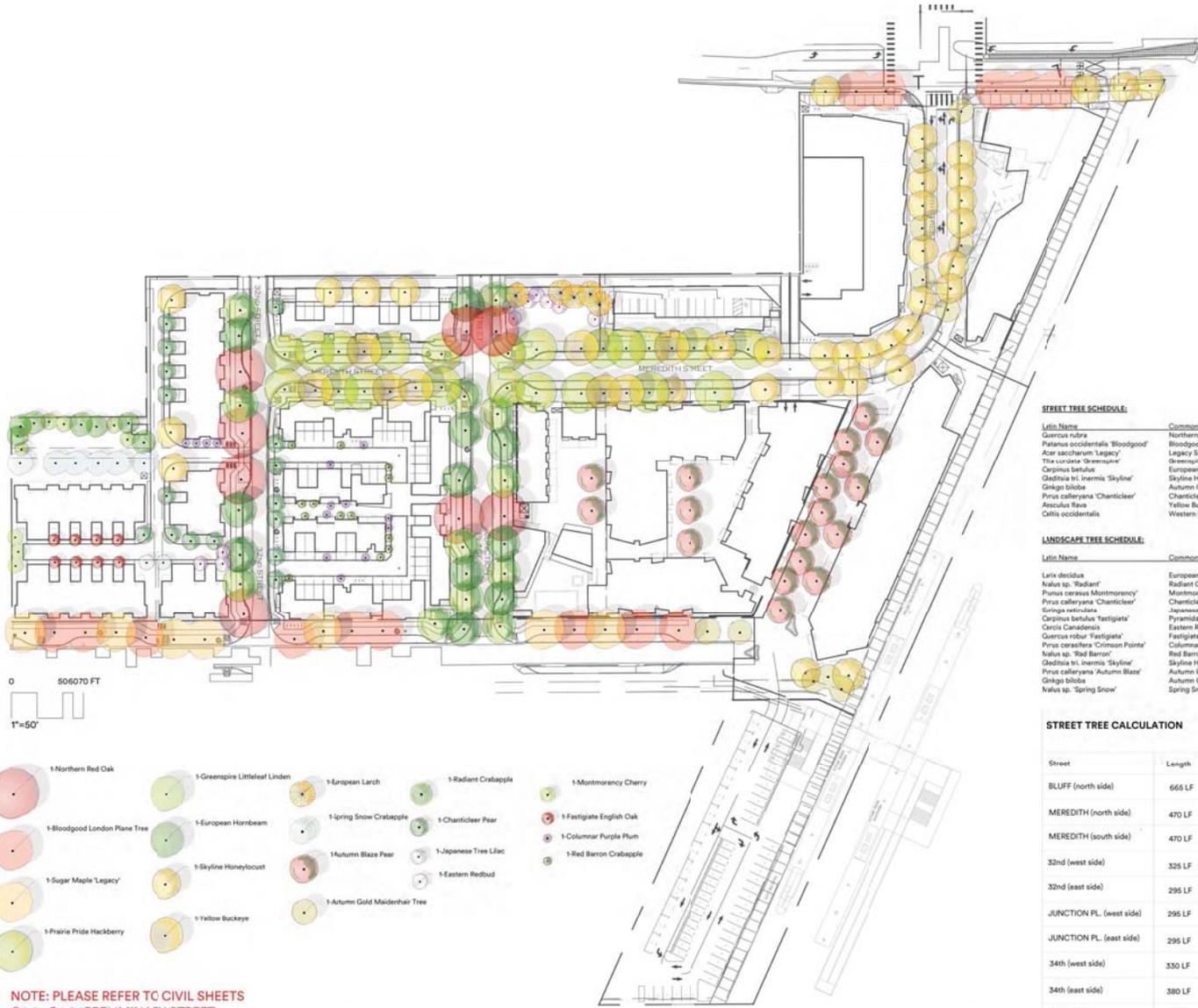
Latin Name	Common Name	Ht.	Spread	Water use	Qty.
<i>Cotoneaster spicatus</i>	Cranberry Cotoneaster	10'-24"	4'-6"	Medium	TBD
<i>Cornus alba Arctic Fire</i>	Arctic Fire Dogwood	4'-6"	4'-6"	Medium	TBD
<i>Daphne odorum 'Carol Mackii'</i>	Carol Mackii Daphne	3'-5"	3'-5"	Medium	TBD
<i>Equisetum hyemale 'Emerald Gully'</i>	Emerald Gully Equisetum	18'-24"	3'-4"	Medium	TBD
<i>Hydrangea arborescens 'Annabelle'</i>	Annabelle Hydrangea	3'-4"	3'-4"	Medium	TBD
<i>Hydrangea arborescens 'Endless Summer'</i>	Endless Summer Hydrangea	3'-5"	3'-5"	Medium	TBD
<i>Penstemon angustifolius 'Little Spire'</i>	Little Spire Penstemon	2'-3"	2'-3"	Low	TBD
<i>Philadelphus v. vireg. 'Dwif. Minnesota Snow'</i>	Dwif. Minn. Snowflake Mockorange	3'-4"	2'-3"	Medium	TBD
<i>Prunus tenella</i>	Dwarf Prunus Almond	3'-5"	3'-4"	Low	TBD
<i>Rhus aromatica 'Gro-Low'</i>	Dwarf Fragrant Sumac	2'-3"	6'-6"	Low	TBD
<i>Ribes alpinum</i>	Alpine Currant	3'-6"	3'-6"	Low	TBD
<i>Rosa 'Charming 'Blair'</i>	Charming Blue Rose	5'-12"	6'-6"	Medium	TBD
<i>Rosa 'Nearly Wild'</i>	Nearly Wild Rose	2'-3"	2'-3"	Low	TBD
<i>Spiraea japonica 'Shirobana'</i>	Shirobana Spirea	2'-3"	2'-3"	Medium	TBD
<i>Syringa chinensis 'Hancock'</i>	Hancock Coriberry	2'-5"	3'-6"	Low	TBD
<i>Syringa patula 'Miss Kim'</i>	Miss Kim Dwarf Lilac	3'-5"	3'-5"	Low	TBD

ORNAMENTAL GRASS SCHEDULE:

Latin Name	Common Name	Ht.	Spread	Water use	Qty.
<i>Calamagrostis acutiflora 'Overdam'</i>	Variiegated Feather Reed Grass	2'-3"	2'-3"	Low	TBD
<i>Calamagrostis brachytricha</i>	Korean Feather Reed Grass	3'-4"	2'-3"	Low	TBD
<i>Impatiens cylindrica 'Red Banner'</i>	Red Banner Japanese Blood Grass	12'-18"	12'-18"	Medium	TBD
<i>Miscanthus sinensis 'Morning Light'</i>	Morning Light Miscanthus	4'-5"	2'-3"	Medium	TBD
<i>Panicum virgatum 'Heavy Metal'</i>	Heavy Metal Switch Grass	3'-4"	10'-18"	Low	TBD
<i>Pennisetum alopecuroides 'Hamaki'</i>	Dwarf Fountain Grass	18'-24"	10'-18"	Low	TBD
<i>Helictotrichon sempervirens</i>	Blue Aviana Grass	18'-24"	18'-24"	Low	TBD

PERENNIAL SCHEDULE:

Latin Name	Common Name	Ht.	Spread	Water use	Qty.
<i>Agastache aurantiaca 'Coronado'</i>	Coronado Hyssop	10'-18"	10'-18"	Low	TBD
<i>Agastache can. Sconson Sunset</i>	Sonoran Sunset Hyssop	10'-18"	10'-18"	Low	TBD
<i>Artemisia 'Powis Castle'</i>	Powis Castle Sage	2'-3"	2'-3"	Low	TBD
<i>Chamae x jacksonii</i>	Purple Clematis	vine	vine	Medium	TBD
<i>Delosperma 'Lavender Ice'</i>	Lavender Ice Iceplant	2'-4"	10'-18"	Low	TBD
<i>Delosperma 'Mesa Verde'</i>	Mesa Verde Iceplant	2'-4"	10'-18"	Low	TBD
<i>Delosperma 'Table Mountain'</i>	Table Mountain Iceplant	2'-4"	10'-18"	Low	TBD
<i>Delosperma 'rubrum'</i>	Yellow Iceplant	2'-4"	10'-18"	Low	TBD
<i>Eoumus fortunei 'Colorado'</i>	Purpleleaf Wintercreeper	2'-18"	3'-6"	Low	TBD
<i>Gaura bicolor</i>	Roseum Gaura	10'-18"	1'-2"	Medium	TBD
<i>Hauzoria microantha 'Palace Purple'</i>	Palace Purple Coral Bells	10'-18"	10'-18"	Medium	TBD
<i>Iris essata 'Royal Robes'</i>	Purple Japanese Iris	2'-3"	10'-18"	Medium	TBD
<i>Nepeta x Little Tudy'</i>	Little Tudy Catmint	10'-18"	10'-18"	Low	TBD
<i>Papaver orientale 'Brilliant'</i>	Orange Oriental Poppy	2'-3"	18'-24"	Low	TBD
<i>Sakki greggi 'Wild Thing'</i>	Hot Pink Sakai	10'-20"	10'-18"	Low	TBD
<i>Salvia verticillata 'Purple Rain'</i>	Purple Rain Salvia	10'-20"	18'-24"	Medium	TBD
<i>Sedum acre evergreen</i>	Goldmoss Utah Sedum	2'-4"	10'-18"	Low	TBD
<i>Thymus praenox 'Coccineus'</i>	Red Creeping Thyme	3'-4"	10'-18"	Low	TBD
<i>Thymus praenox 'Pneobotanoginosis'</i>	Woolly Thyme	1'-2"	10'-18"	Low	TBD
<i>Veronica 'Sunny Border Blue'</i>	Sunny Border Blue Veronica	2'-3"	10'-18"	Low	TBD
<i>Veronica Crystal River</i>	Crystal River Speedwell	10'-20"	10'-18"	Low	TBD
<i>Vinca minor 'Bowles Variety'</i>	Bowles Periwinkle	4'-6"	10'-18"	Low	TBD



- 1-Northern Red Oak
- 1-Greenspire Littleleaf Linden
- 1-European Larch
- 1-Radiant Crabapple
- 1-Montmorency Cherry
- 1-Bloodgood London Plane Tree
- 1-European Hornbeam
- 1-Spring Snow Crabapple
- 1-Chanticleer Pear
- 1-Fastigate English Oak
- 1-Sugar Maple 'Legacy'
- 1-Skyline Honeylocust
- 1-Autumn Blaze Pear
- 1-Japanese Tree Lilac
- 1-Columbar Purple Plum
- 1-Eastern Redbud
- 1-Red Barron Crabapple
- 1-Prairie Pride Hackberry
- 1-Yellow Buckeye
- 1-Autumn Gold Maidenhair Tree

NOTE: PLEASE REFER TO CIVIL SHEETS C4.8 - C4.9 "PRELIMINARY STREET SECTIONS" FOR DIMENSIONS OF PLANTING STRIPS / TREE LAWNS.

STREET TREE SCHEDULE:

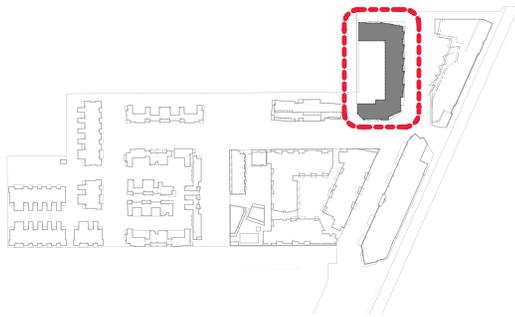
Latin Name	Common Name	Ht.	Spread	Water use	Qty.
<i>Quercus rubra</i>	Northern Red Oak	40'-60'	40'-60'	Medium	8
<i>Platanus occidentalis</i>	'Bloodgood' London Plane Tree	40'-60'	40'-60'	Medium	14
<i>Acer saccharum</i>	'Legacy' Sugar Maple	50'-60'	30'-40'	Medium	7
<i>Tilia cordata</i>	'Greenspire' Littleleaf Linden	30'-40'	20'-30'	Medium	12
<i>Carpinus betulus</i>	European Hornbeam	35'-45'	25-35'	Medium	11
<i>Gleditsia tri. inermis</i>	'Skyline' Honeylocust	40'-50'	30'-40'	Low	19
<i>Ginkgo biloba</i>	'Autumn Gold' Maidenhair Tree	35'-50'	25-30'	Medium	8
<i>Pinus calleryana</i>	'Chanticleer' Pear	20'-30'	12'-18'	Medium	7
<i>Aesculus flava</i>	'Yellow Buckeye'	50'-60'	30'-40'	Medium	8
<i>Ostrya occidentalis</i>	Western Hackberry	50'-60'	40'-50'	Low	21

LANDSCAPE TREE SCHEDULE:

Latin Name	Common Name	Ht.	Spread	Water use	Qty.
<i>Larix decidua</i>	European Larch	50'-60'	25'-30'	Medium	5
<i>Nalux sp.</i>	'Radiant' Crabapple	20'-25'	20'-25'	Medium	6
<i>Prunus cerasus</i>	Montmorency Cherry	15'-20'	12'-18'	Medium	8
<i>Pinus calleryana</i>	'Chanticleer' Pear	20'-30'	12'-20'	Medium	9
<i>Styrax reticulata</i>	Japanese Tree Lilac	15'-20'	10'-20'	Low	2
<i>Carpinus betulus</i>	'Fastigate' European Hornbeam	25'-35'	10'-15'	Medium	6
<i>Quercus canadensis</i>	Eastern Redbud	20'-25'	20'-25'	Medium	11
<i>Quercus robur</i>	'Fastigate' English Oak	40'-50'	10'-15'	Medium	8
<i>Pinus cerasera</i>	'Crimson Point' Plum	20'-25'	6'-10'	Medium	10
<i>Nalux sp.</i>	'Red Barron' Crabapple	15'-18'	7'-8'	Medium	13
<i>Gleditsia tri. inermis</i>	'Skyline' Honeylocust	40'-50'	30'-40'	Low	8
<i>Pinus calleryana</i>	'Autumn Blaze' Pear	25'-35'	20'-30'	Medium	18
<i>Ginkgo biloba</i>	'Autumn Gold' Maidenhair Tree	25'-50'	25-35'	Medium	3
<i>Nalux sp.</i>	'Spring Snow' Crabapple	20'-25'	20'-25'	Medium	6

STREET TREE CALCULATION

Street	Length	Spacing	Trees Req.	Trees Prov.
BLUFF (north side)	665 LF	40' O.C.	22	21
MEREDITH (north side)	470 LF	30' O.C.	16	19
MEREDITH (south side)	470 LF	30' O.C.	16	18
32nd (west side)	325 LF	30' O.C.	11	10
32nd (east side)	295 LF	20' O.C.	15	7
JUNCTION PL. (west side)	295 LF	30' O.C.	10	9
JUNCTION PL. (east side)	295 LF	30' O.C.	10	9
34th (west side)	330 LF	20' O.C.	17	11
34th (east side)	380 LF	20' O.C.	19	10
VALMONT	260 LF	30' O.C.	9	9
Aggregate Totals			145	123



Our intention was to showcase the maker/builder/crafter/bike. Working with the constraints of a site surrounded on three sides by the street, we decided that this constraint would in fact be our biggest opportunity. Ciclo provides a street frontage that will demonstrate and display the life of its occupants. The living units wrap around an elevated roof deck. Outdoor space to create and share.

The building is an exhibit that is transformed continuously, and everyone gets to watch.



Ciclo is a showcase for creativity. The intended commercial occupants will be maker/crafter/builders/bike enthusiasts/bike advocates. The design of the building is meant to showcase the occupants in a way that celebrates their ideas and the important place they occupy in the Boulder community. The ground level tenant space creates a continuous zone where proposed Community Cycles can occupy and utilize indoor and outdoor space concurrently. The building creates a space for people to ride up and work on their bikes outdoors, learn proper bike maintenance, and purchase bikes from the Community Cycles stock of two wheeled transport.

The two upper levels are 32 units of 100% Permanently Affordable Housing. The outdoor deck on the second level would provide an amenity deck that could be used for both outdoor play and deck zone. The internal circulation zone is wide enough to serve as the residents breakout area.

The overall form of the structure is intended to be a complement to the MARKET building across 34th Street, establishing a pedestrian experience that creates interest and variety along the Valmont/34th corridor.



CICLO

- Mixed Use Commercial Building
- 50,677 gsf
- 4 Story, including a Ground Floor Mezzanine; Code Height: 44'-6" (Perceived Height: 42'-0")
- Ground Floor - Proposed Community Cycles retail/workshop/education space, Leasable tenant area/Retail, Lobby area for residential, Covered parking
- Floors 2 & 3 - Apartments, Accessory Gallery, Common tenant areas
- Permanently Affordable Residential Units

SopherSparrn | Ciclo

August 21, 2015

SPARK

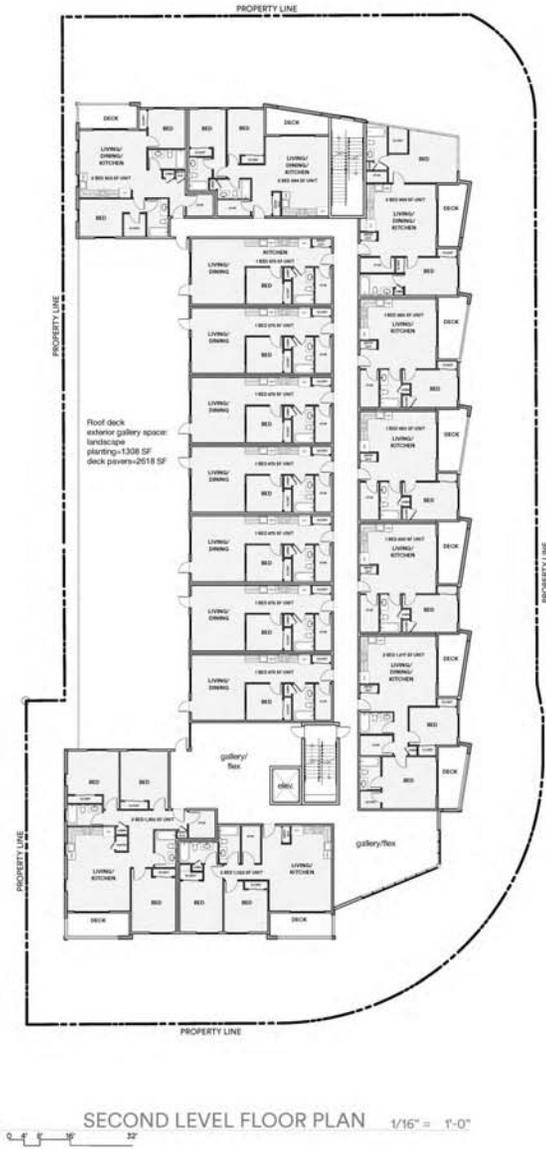
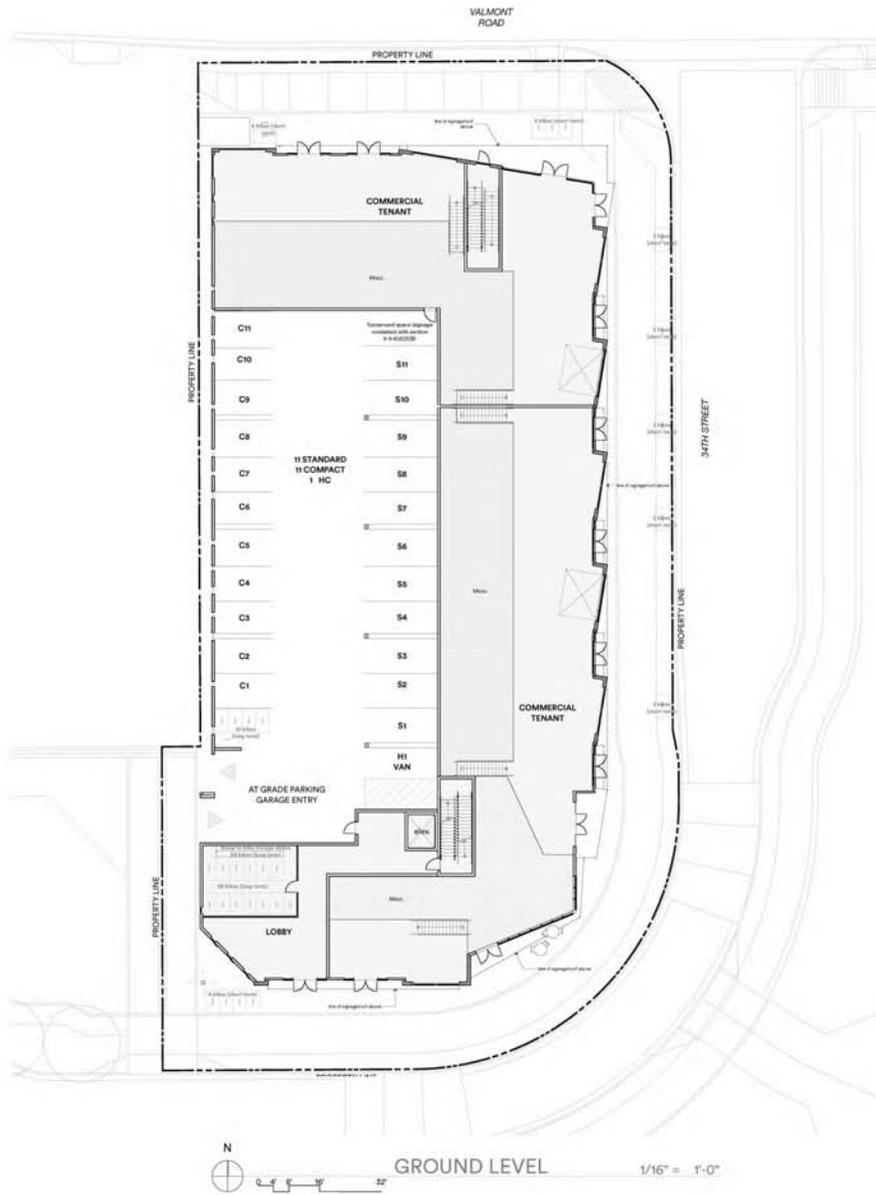


Scenario
a day in the life of...

"It's an amazing thing to be a resident in this place. Creative chaos is a good description. The people I meet when I walk downstairs can provide the inspiration or idea, or maybe it's the train...it could be the brew pub. I never thought this was attainable for me in Boulder. It's a nice village to be a part of."

SopherSparrn | **Ciclo**
S⁺PARK

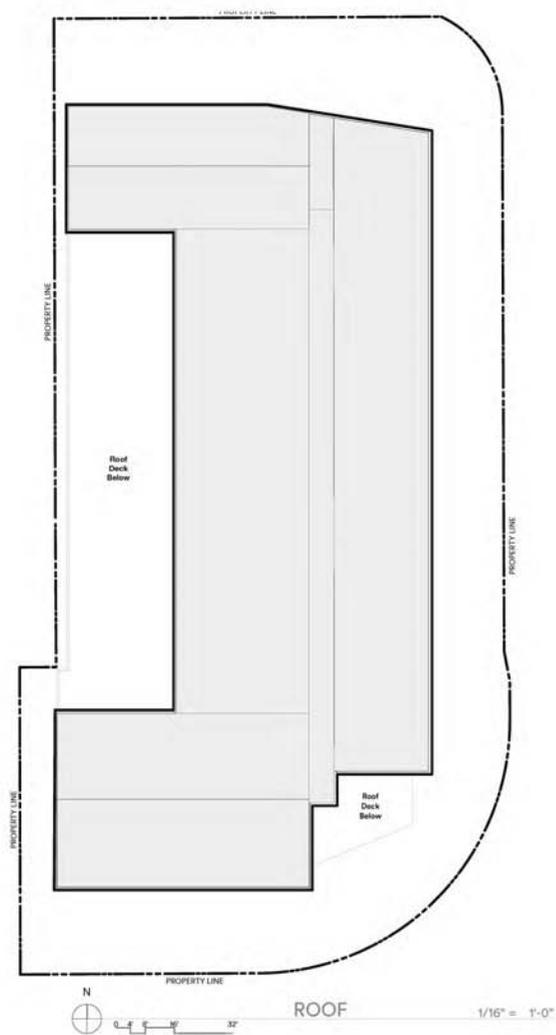
August 21, 2015



Floor Plans

SopherSparrn | Ciclo
S'PARK

August 21, 2015



Floor Plans

SopherSparrn | Ciclo
 August 21, 2015 **SPARK**



CICLO EAST ELEVATION 3/32" = 1'-0"



CICLO SOUTH ELEVATION 3/32" = 1'-0"



CICLO NORTH ELEVATION 3/32" = 1'-0"



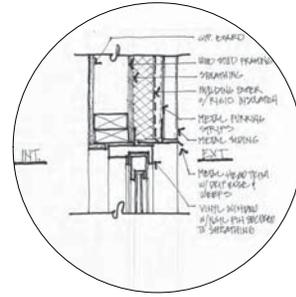
CICLO WEST ELEVATION 3/32" = 1'-0"



material composition: corrugated metal, powder coated, charcoal



material composition: cor-ten steel, masonry and storefront



window detail



Roll up garage doors in select locations around the ground floor blend the transition between interior and exterior creating a vibrant street front.



The entry canopy acts as a marker and a drop in scale that will continue seamlessly inside as an invitation to enter.



Corrugated metal: Chosen for architectural interest and verticality. This material is easy to maintain, durable and sustainable. The material can be painted and will provide a nice complement to the dark vertical wood siding.

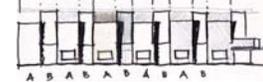
Residential Window Systems: Windows will be chosen for quality, ease of maintenance, high thermal value and ease of installation.

Stack Bond Masonry Veneer: Chosen for its modern aesthetic and prominent grid.

CORRUGATED METAL SIDING, POWDER COATED

COR-TEN STEEL PANEL

The bays of masonry engage with the bays of vertical corrugation/storefront, creating a rhythm that marches down 34th street. The play of shadows made from varied elevation planes provide depth and shadow to the facade.



Cor-Ten accents at the ground level give a pedestrian textural interest.



DARK STAINED CEDAR
VERTICAL GRAIN



VINYL WINDOWS
BRONZE



OVERHEAD STEEL AND GLASS
DOOR



ALUMINUM STOREFRONT



CORRUGATED METAL, POWDER COATED

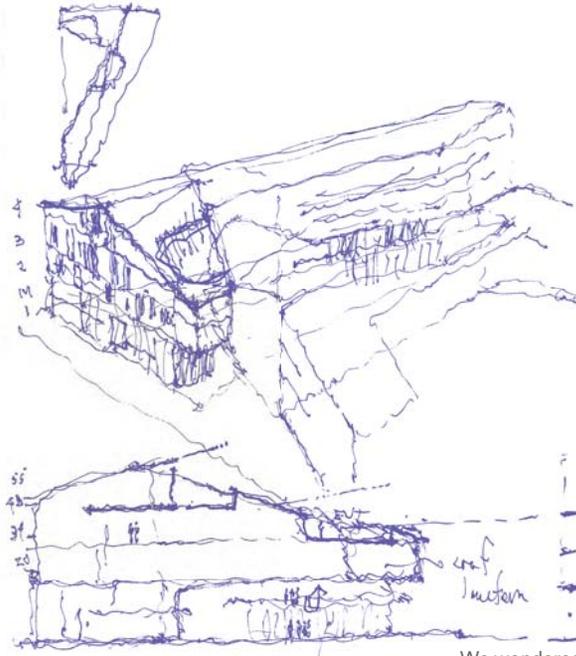
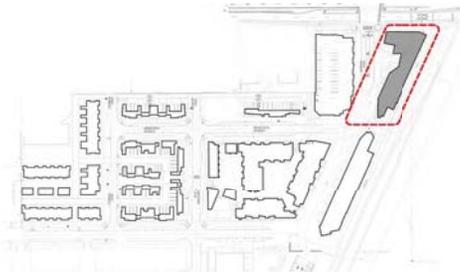


BUFF BRICK
STACKED BOND



CMU STACKED BOND





We wondered:
 What would happen if a creative Class A office building felt nothing like an office, and what if it floated over a glowing lantern-space full of people and great food and drink?

So we carved a gabled form that defers to neighborhood scale, wrapped in silvered wood, with kinetic rhythm and punches of color. It had to hover over glass. And on top, the 'lifted lid' at the event deck gives a gateway view to everyone.



Inspired by the simple lines and texture of mountain sheds and the kinetic rhythms of railroad cars, Markt is a modest gable form carved from a triangular city block. Erosion forms its plaza and arcade overhangs. On its roof, lifted flaps create the 3rd level Aerie event deck, upper office mezzanines and terraces, and trackside dormers with northeast views. Markt is designed to work at two scales and velocities: at the speed of the train or as seen from Foothills Parkway, and the slower pedestrian pace as one enters Junction Place from Valmont.

Composed in two shades of silver grey cedar, vertical and horizontal in sympathy with building proportions, the main body of Markt is completed by a soffit of reclaimed Sutherlands lumber. Windows similarly shift from vertical to horizontal, to the advantage of creative offices and views within. A standing seam galvanized roof wraps down the east (trackside) face, interrupted by wood wedges at the sawtooth dormers. This floating volume is underpinned by steel columns and vertically score-jointed stack bond concrete masonry along the tracks. On the west, steel columns and glass with dark mullions create an arcade walk—animated by a floating light/signage ribbon. The first story soffit celebrates sustainability, repurposing the warm wood decking rescued from the Sutherlands lumber yard.

At the north, a brewpub with taproom and mezzanine creates the transparent 'lantern' on Valmont. Several micro-restaurants stretching down the arcade toward S'PARK Place complete an indoor-outdoor dining scene.



1 Valmont gateway, frame and viewshed

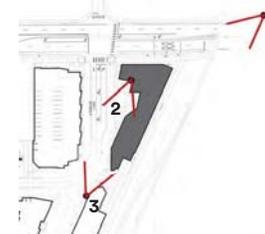
Mixed Use Commercial Building

- 5 Stories
- Actual Height 47'-7" to ridgeline on Valmont; 49'-1" to high point on east or west (from grade including mechanical)
- 53,350 gsf / 44,641 nsf
- Floor 1-2 – Restaurant /Retail
- Floors 3-5 – Creative Class A Office

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S'PARK



1 Valmont to tracks: rhythm and wrap-around



2 the Aerie, Flatirons viewshed from L3



3 from Woonerf to Markt Plaza

Scenario

a day in the life of...

"I bike to work, and I usually come early. I've got the best coffee at MARKET and I hang out in the plaza with my laptop, or stroll S'PARK Place while I get my gameplan on. Our studio is on 3rd; I work in the loft with sky and mountains, but we usually conference out on the deck...some of our best ideas are al fresco. It's either the tacqueria on the plaza for lunch, or they cater up for our clients.

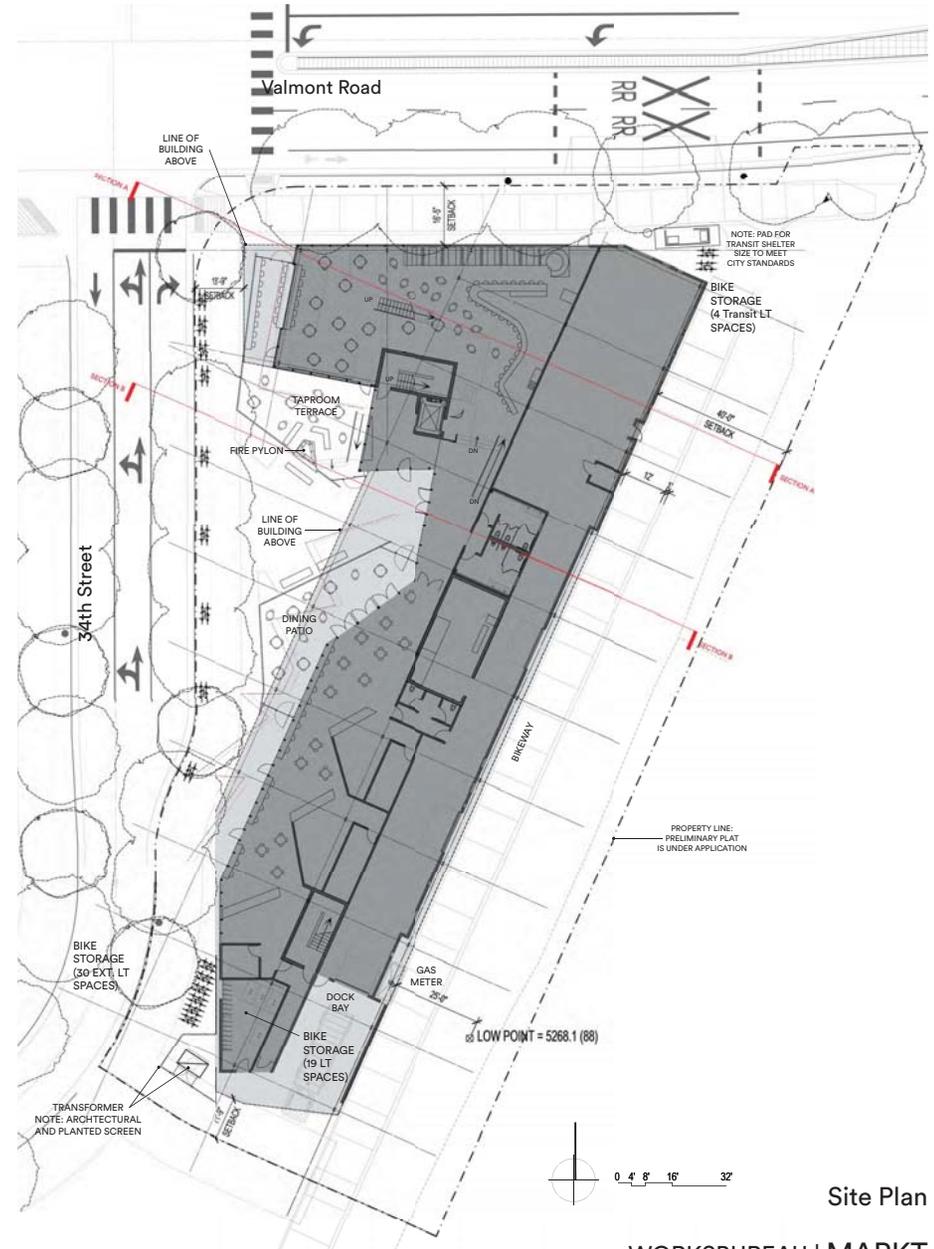
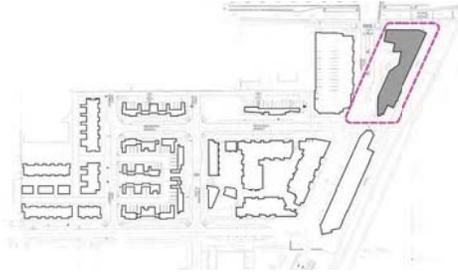
Most Thursdays some of us head down to the brewpub by 5:30. Tonight though we've got the Aerie reserved—celebrating our 4th year of solid growth, with our families. We'll eat and see the sunset up on the deck, and close the night over gelato downstairs.

The rumble of the freightliner cranks on once in a while. Clouds rip over the peaks and light and fresh air stream through our windows. Nothing is static here, it's alive and moving."

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S'PARK

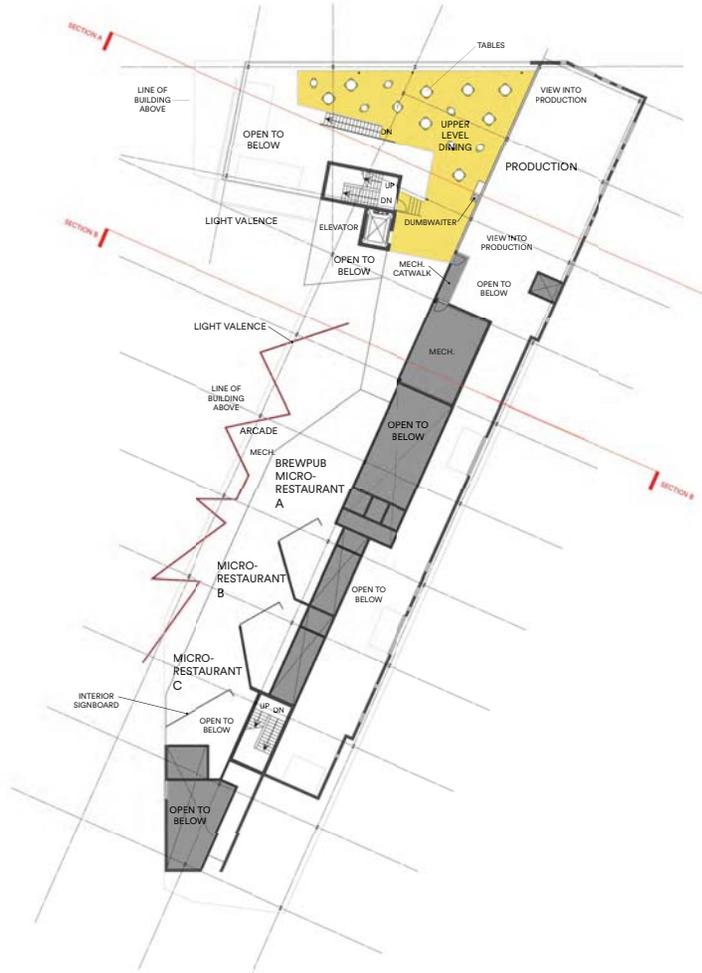


Site Plan

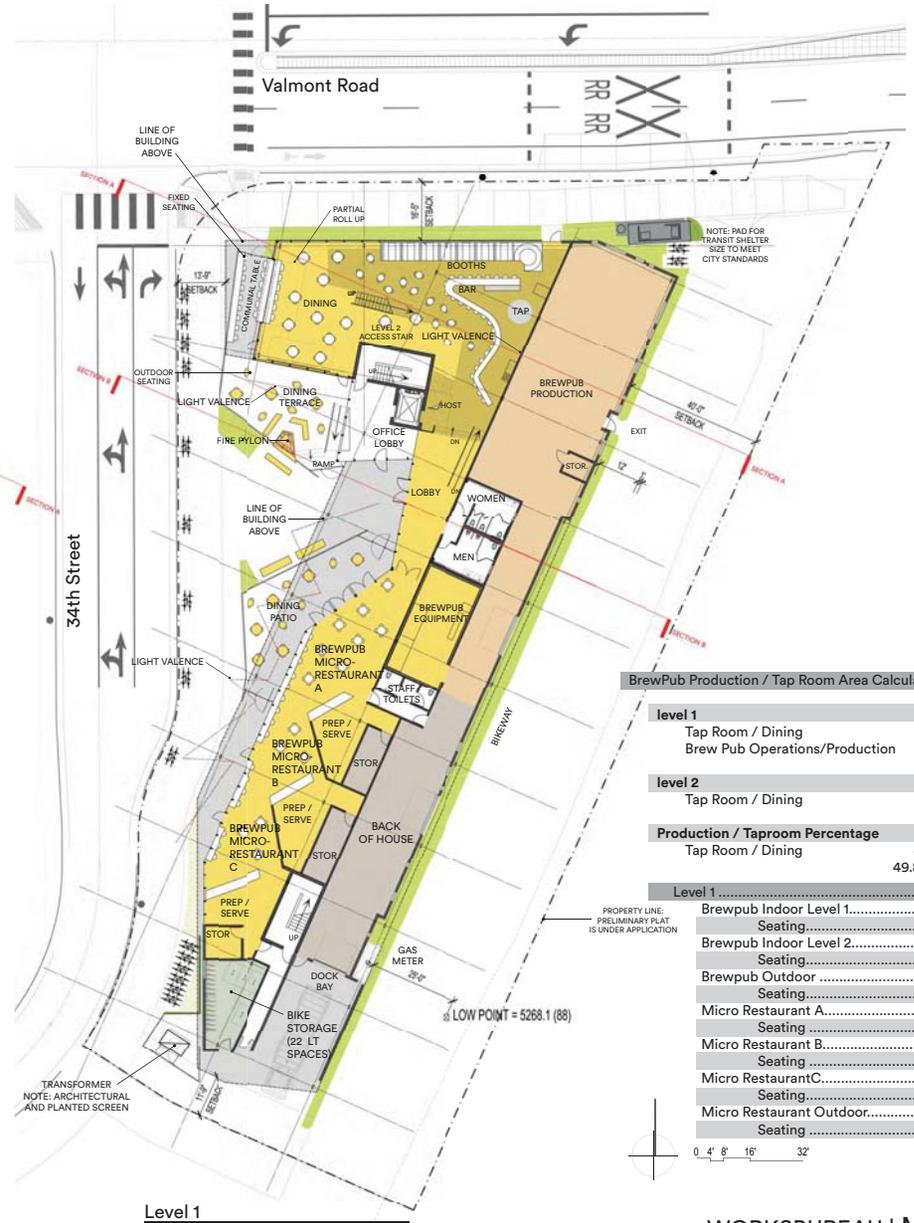
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SPARK



Level 1 Mezzanine



Level 1

BrewPub Production / Tap Room Area Calculations

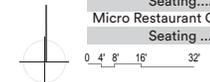
level 1		
Tap Room / Dining		3126 sf
Brew Pub Operations/Production		4869 sf

level 2		
Tap Room / Dining		1,715 sf

Production / Taproom Percentage		
Tap Room / Dining	4841 / 4869	
		49.85%/50.15%

Level 1		
Brewpub Indoor Level 1	.....	13998.6 sf
Seating	.....	.134
Brewpub Indoor Level 2	.....	1756 sf
Seating	.....	.48
Brewpub Outdoor	.....	1082 sf
Seating	.....	.50
Micro Restaurant A	.....	1326 sf
Seating	.....	.38
Micro Restaurant B	.....	983 sf
Seating	.....	.17
Micro Restaurant C	.....	866 sf
Seating	.....	.17
Micro Restaurant Outdoor	904 sf
Seating34

PROPERTY LINE:
PRELIMINARY PLAT
IS UNDER APPLICATION

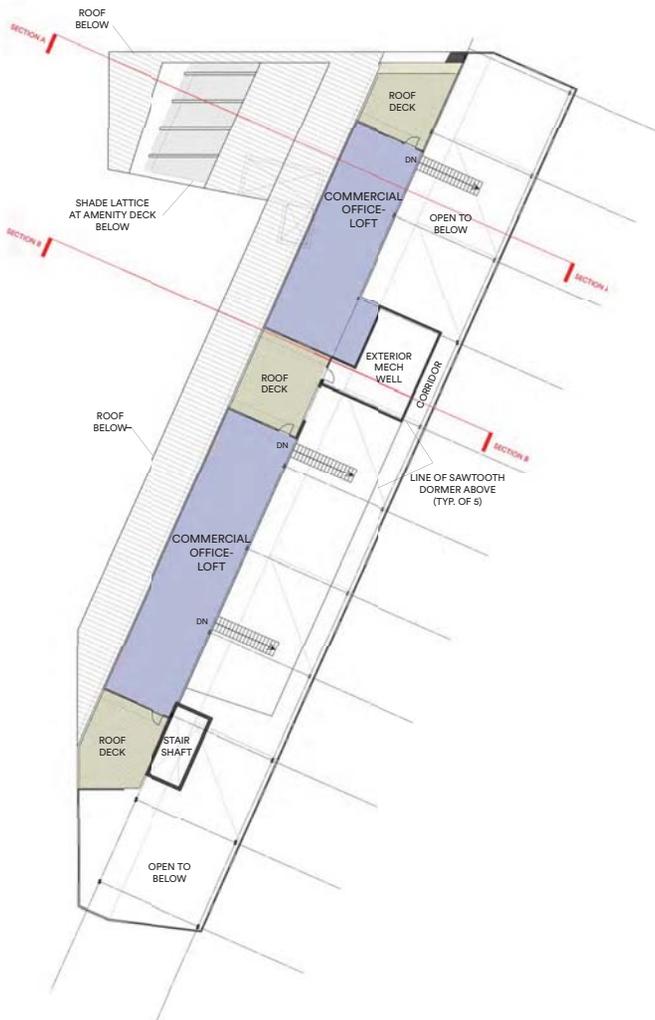


Plans

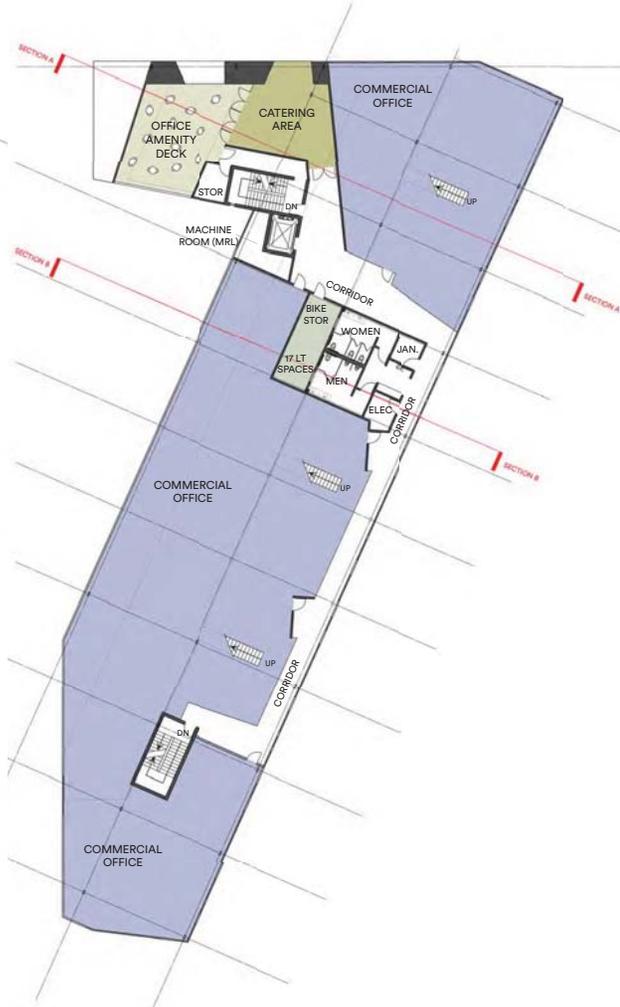
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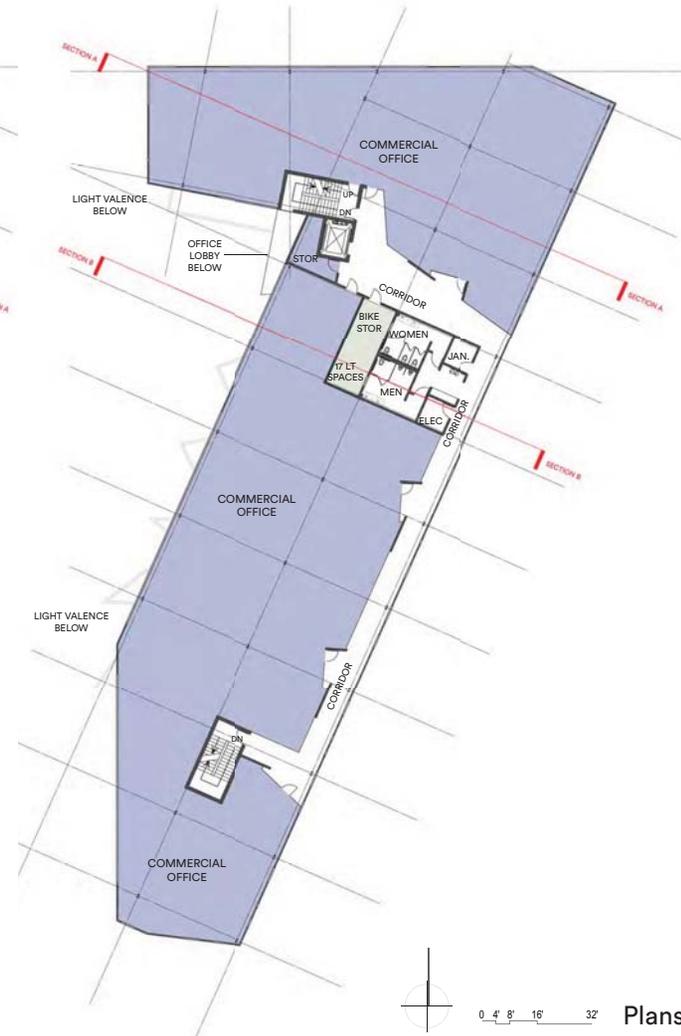
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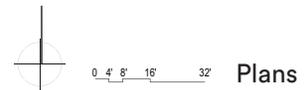
Level 3 Mezzanine



Level 3



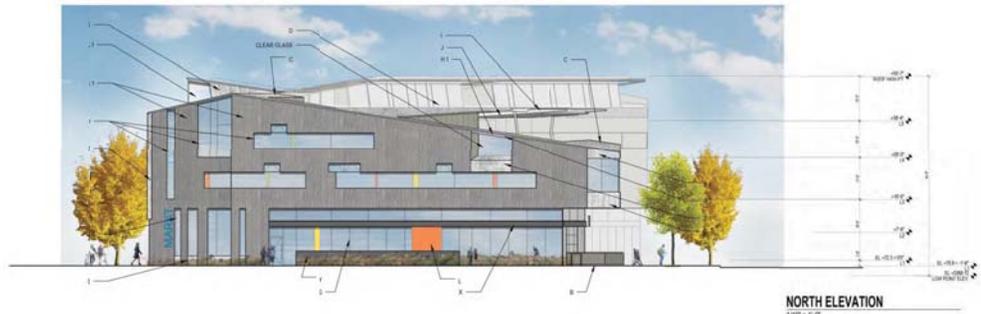
Level 2



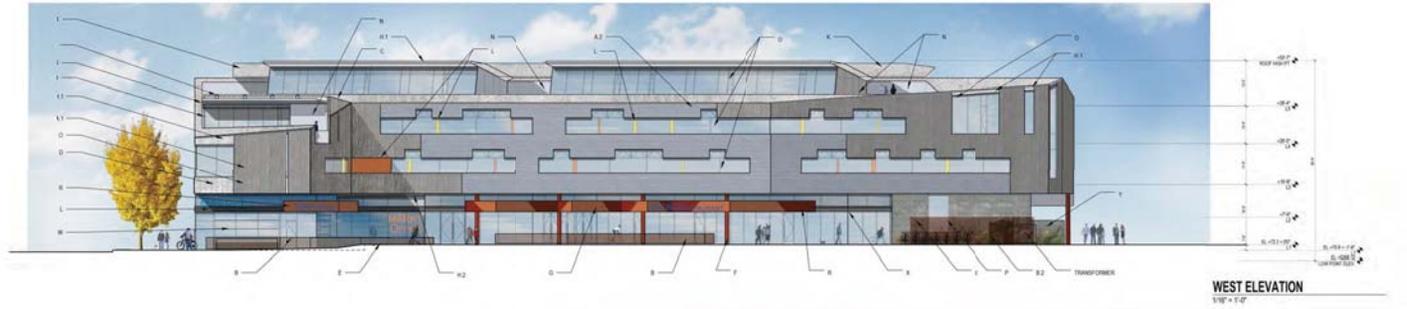
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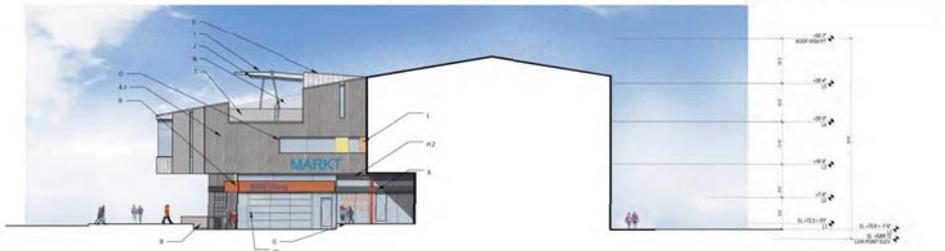
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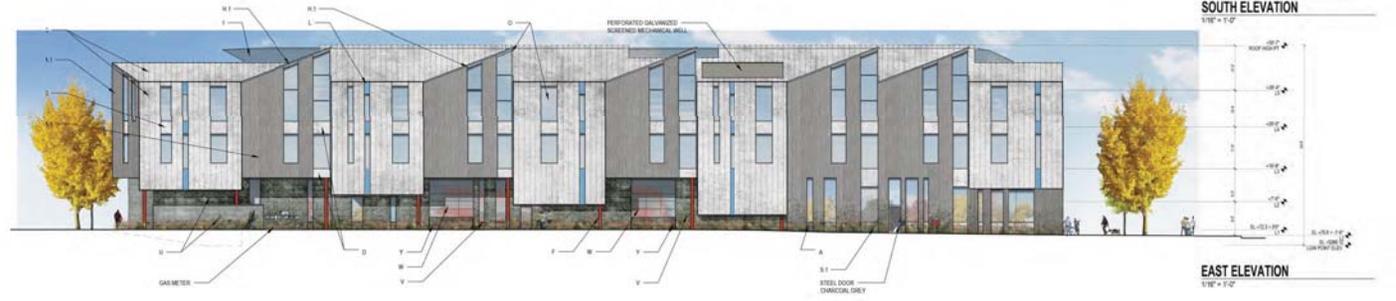
NORTH ELEVATION
1/8" = 1'-0"



WEST ELEVATION
1/8" = 1'-0"



SOUTH ELEVATION
1/8" = 1'-0"



EAST ELEVATION
1/8" = 1'-0"

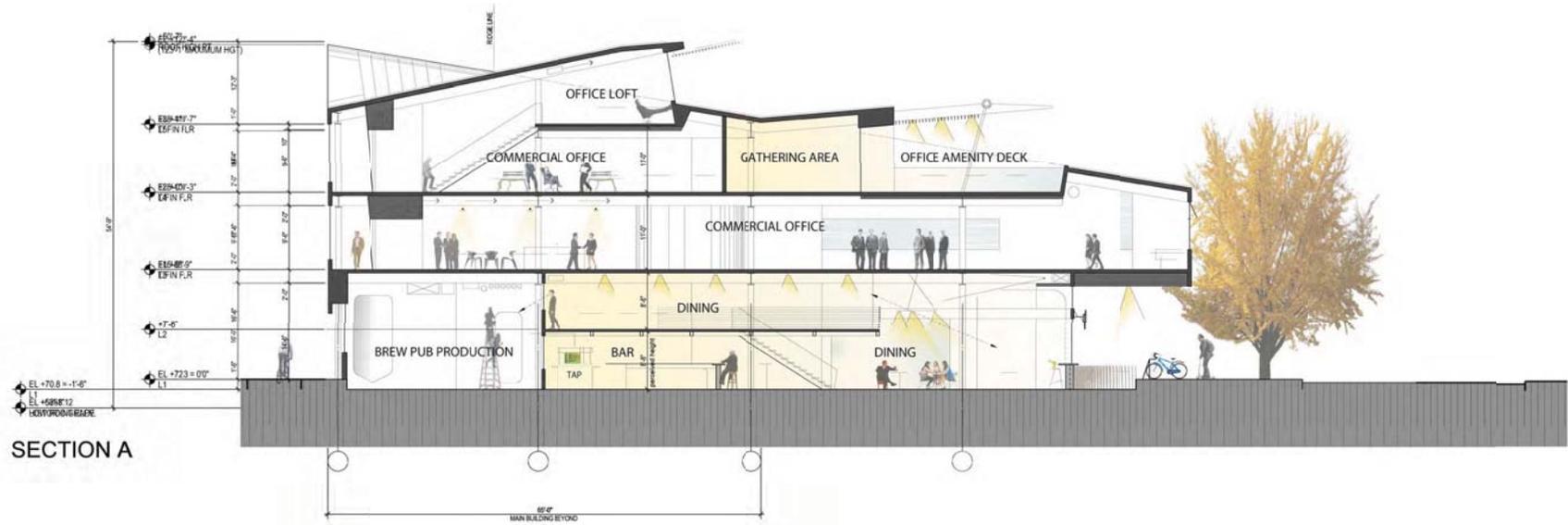
- A1 4" OXBT WOOD BEAM - FLAT, TRANSLUCENT SILVER GREY
- A2 8" OXBT WOOD BEAM - FLAT, TRANSLUCENT BLUE GREY
- B WEATHERING STEEL LATE 4" O.C. VERTICAL SCREEN FLAT PLATE OF RAIL
- B1 WEATHERING STEEL LATE 4" O.C. VERTICAL SCREEN WITH INTEGRATED CLIMBING VEGETATION PIN
- C 1/8" GALVANIZED STANDING SEAM
- D FLAT SEAM GALVANIZED CLADDING
- E LOCATION OF FIRE PLY (NOT SHOWN FOR BUILDING CLARITY)
- F STEEL COLUMNS RED OXIDE
- G ALUMINUM WINDOW WALL SYSTEM - CHARCOAL GREY
- H1 GALVANIZED FASCIA
- H2 CHARCOAL GREY ALUMINUM FASCIA
- I STEEL SHADE LATTICE FRAME, SILVER
- J GALVANIZED 7" Z-CHANNEL SHADE LATTICE
- K GALVANIZED 7" Z-CHANNEL SHADE BROW
- L POWDER COATED ALUMINUM PANEL
- M PERFORATED WEATHERING STEEL LIGHT VALENCE
- N SAND FINISH STUCCO
- O ALUMINUM WINDOW CLEAR ANODIZED AT UPPER LEVELS TYP.
- P BIKE ROOM WINDOW
- Q BOARD FORM CONCRETE STAGE ???
- R PERFORATED WEATHERING STEEL
- S1 GALVANIZED PANEL
- S2 GALVANIZED BELL
- T CLEAR GLASS GUARNIAE
- U STACK BOND CONCRETE MASONRY, RANDOM VERTICAL SCORE JOINTS - GREY
- V ALUMINUM WINDOW CHARCOAL GREY AT FLOOR TYP.
- W GLASS PANELED ALUMINUM GARAGE DOORS - CHARCOAL GREY
- X CHARCOAL GREY ALUMINUM CLAD HEADER
- Y CONCRETE MASONRY/CHARCOAL - HORNED
- Z1 ALUMINUM WINDOW CLEAR ANODIZED
- Z2 ANODIZED ALUMINUM WINDOW/FRONT SYSTEM - CHARCOAL

Elevations

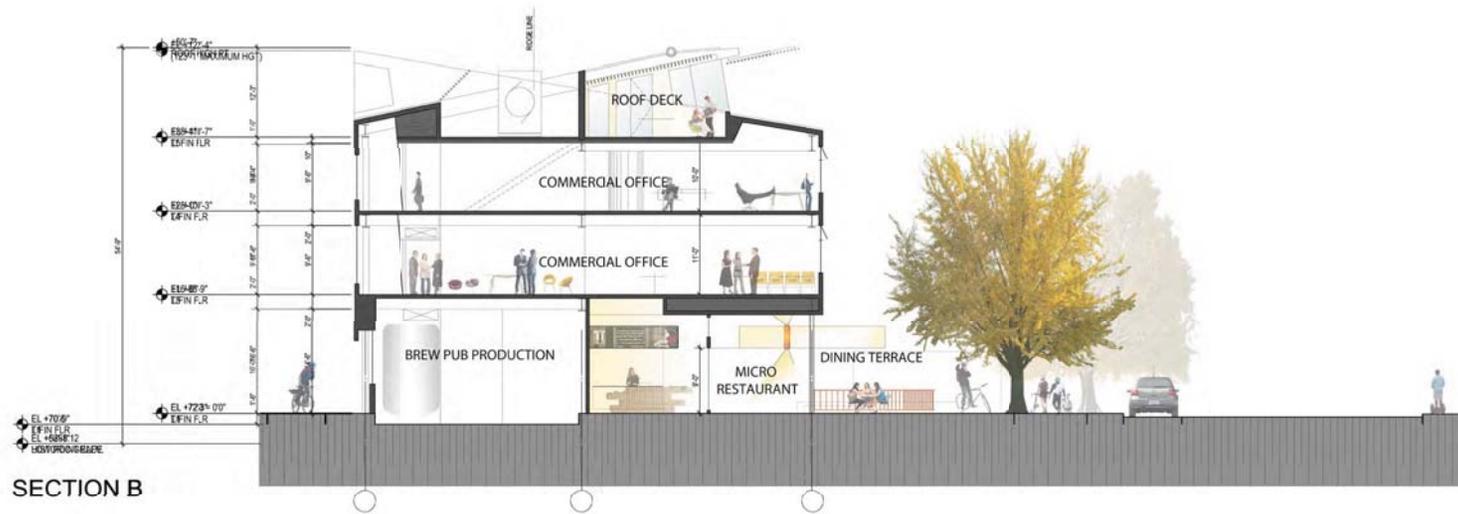
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SECTION A



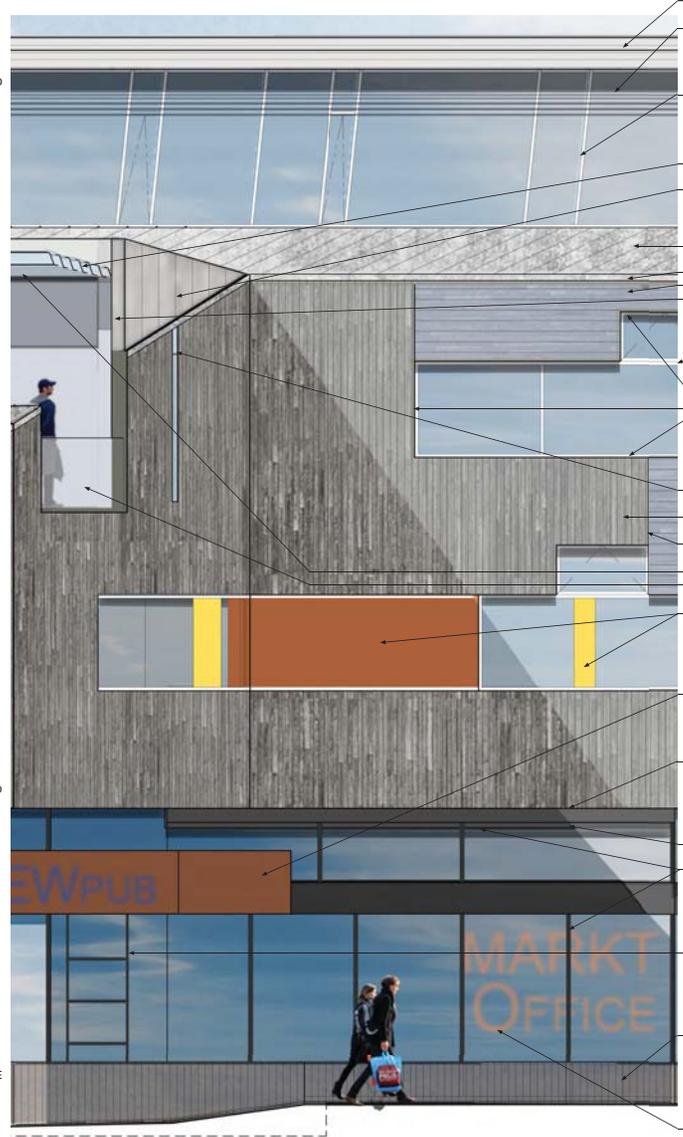
SECTION B

0' 2' 4' 8' 16' Sections

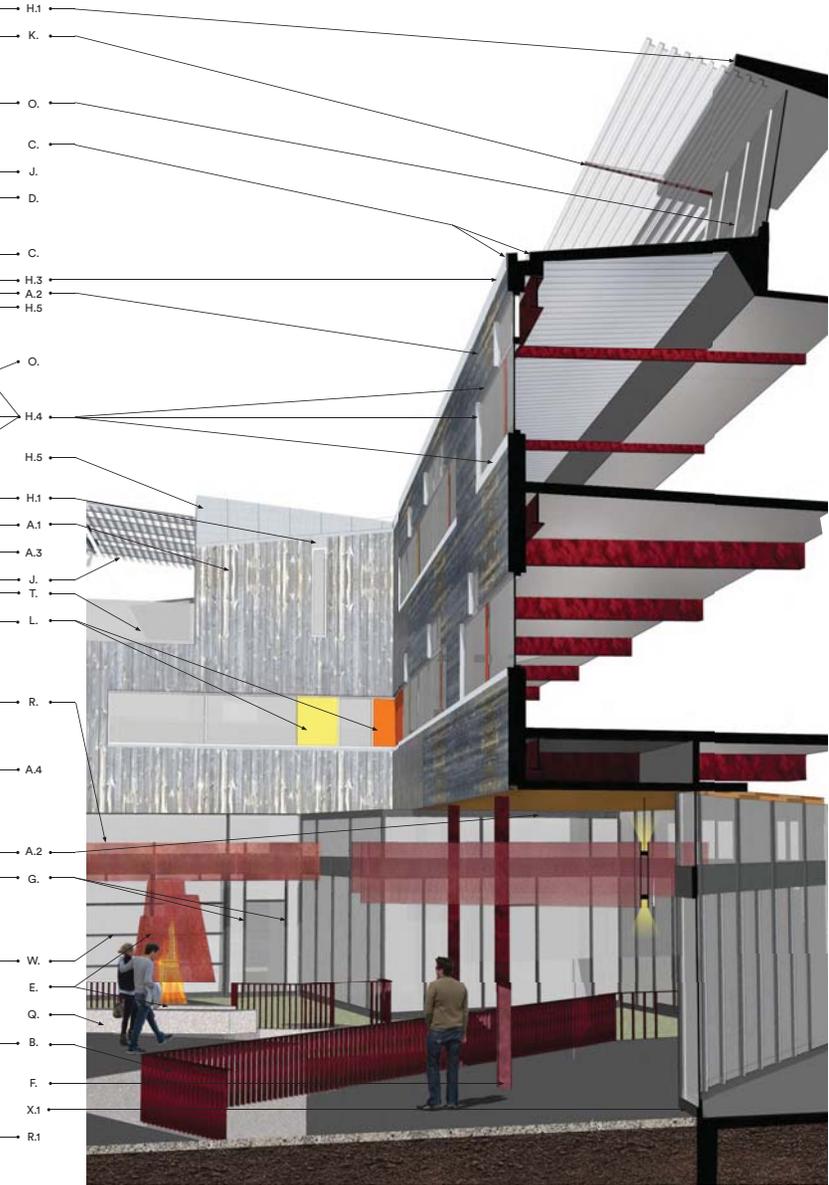
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August 21, 2015 **S'PARK**

- H.1 GALVANIZED FASCIA
- K. 3" PERFORATED GALVANIZED Z-CHANNEL LATTICE ON PAINTED STEEL ARM BRACKETS
- O. 1-3/4" CHAMPAGNE ANODIZED WINDOWS UNITS; OPERABLE AWNING STYLE UNITS AS SHOWN
- J. 6" PERFORATED GALVANIZED Z-CHANNEL ON PAINTED STEEL ARMATURE
- D. FLAT-SEAM GALVANIZED WALL CLADDING AT LIFTED ROOF
- C. GALVANIZED STANDING SEAM ROOF WITH RECESSED GUTTER AND CONCEALED RAIN LEADERS
- H.3 4" GALVANIZED FLASHING
- A.2 6" 'GHOST WOOD' PRE-TREATED SIDING, TREATMENT BLUE-GREY STAIN
- O. WINDOW UNITS MULLED TOGETHER FOR OPEN SIGHT LINE
- H.4 GALVANIZED DRIP FLASHING AT HEAD; L-BLADE @ JAMB; AND SILL FLASHING
- H.5 GALVANIZED WALL CAP
- A.1 4" 'GHOST WOOD' PRE-TREATED SIDING, SILVER-GREY
- H.1 GALVANIZED CLADDING AT HEAD
- A.4 ALUMINUM REGLET REVEAL AT TRANSITION
- T. CLEAR CLASS GUARDRAIL SET IN RECESSED METAL GLAZING SHOE
- G. POWDER COATED ALUMINUM PANELS APPLIED TO WINDOW SYSTEM - SIMILAR TO RAIN SCREEN, NOT PART OF WEATHER ENVELOPE
- E. PERFORATED WEATHERING STEEL LIGHT VALENCE; DOWN LIGHT PLAZA; UP LIGHT WOOD SOFFIT; DIMENSIONAL TEXT SIGNAGE SEPARATE FROM SITE REVIEW - SUPPORTED BY STEEL COLUMNS AND TENSION RODS TO SOFFIT
- A.4 ALUMINUM REGLET / DRIP MOLD AT BASE OF WOOD SIDING, TO PROTECT SIDING AND RECLAIMED WOOD SOFFIT
- A.2 CHARCOAL GREY METAL FASCIA / CAP AT ROOF OF OFFICE LOBBY
- G. 2 1/2" CHARCOAL ANODIZED ALUMINUM STOREFRONT SYSTEM WITH INTEGRAL HORIZONTAL GIRT AND RECESSED HEAD MULLION
- W. ROLL-UP GLAZED PANEL DOOR
- E. PERFORATED WEATHERING STEEL ON STEEL FRAME CONCEALED AT FIRE PYLON'; ON C.I.P. CONCRETE BASE WITH ROCK AGGREGATE TRAY
- Q. C.I.P. CONCRETE BENCH
- B. WEATHERING STEEL FENCE AT DINING PATIO
- F. RED OXIDE PAINTED STEEL COLUMN
- X.1 5" SILL MULLION FOR SCALE AND DURABILITY
- R.1 SIGNAGE UNDER SEPARATE PERMIT; INTENT TO BE APPLIED TO GLASS, INSIDE FACE.



Partial Elevation, 34th Street Scale 3/8" - 1' - 0"



Section Perspective Simplicity: Articulation of materiality and detail

- Shade lattice armature connects below fascia; to avoid penetration of roof or thermal envelope. Roof detailed with positive sill and concealed gutter and rain leaders, for a clean facade expression.
- Upper windows extend beyond structure for daylight penetration; operable awning windows intended.
- All windows detailed with galvanized perimeter flashings for depth of relief and protection of wood siding edges. Anodized aluminum window units will provide maximum durability. Glazing will employ relatively transparent (less tinted) low-E coatings for a natural effect. Color panels are powdercoated, applied to overlay mullions at spandrels.
- Reclaimed wood soffit will be protected by positive drip edges, and will appear simple and complete with a minimum of lighting penetrations: the perforated valence below will bring it alive with indirect light. The valence will also serve as signage armature, in lieu of flag mount signage and provide downlight to plaza and dining patio.
- Exposed steel columns in red oxide convey authentic tectonics and complement natural wood siding. Wood cladding is pre-weathered and treated for longevity.

Design Excellence
 Details: Tectonics and Materiality

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S⁺PARK

August 21, 2015

The use of mixed orientations of siding is specific to this building, and is based upon following the dominant proportion of the façade (for example: to reinforce the vertical ends, or work in sympathy with horizontal fenestration). This aligns with materiality: vertical boards meet raked roofs, avoiding long tapered cuts which are infeasible with wood siding. Further, the difference of siding is used to enhance the special quality of the plaza: board width changes the visual texture; translucent stain takes on a subtle blue, similar to beetle-kill pine appearance; and the 'shift' from vertical is composed for a sense of dynamic movement. This is tied to the trackside location and to the activated sense of the plaza.



TRANSLUCENT BLUE STAIN 'GHOST WOOD'
 -effect emulates Beetle-kill pine
 -provides increased longevity
 -horizontal application



Beetle-kill siding : precedent



silver-grey vertical siding



translucent blue-grey horizontal siding

aluminum reveal joint



K. GALVANIZED 3" Z-CHANNEL SHADE LATTICE



C. 16" GALVANIZED STANDING SEAM



A. 'GHOST WOOD' STANDING SPRUCE OR LODGEPOLE SIDING - FLAT TRANSLUCENT SILVER GREY



LUPINE



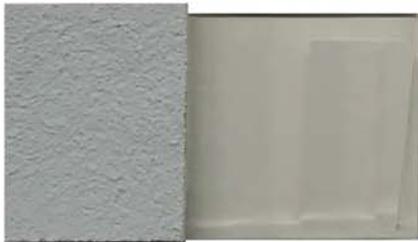
POPPY



SUNFLOWER

L. POWDERCOATED ALUMINUM PANELS

B. 'GHOST WOOD' STANDING SPRUCE OR LODGEPOLE SIDING - FLAT TRANSLUCENT BLUE GREY



N. SAND FINISH STUCCO

T. CLEAR GLASS GUARDRAIL



WEATHERED GALVANIZED METAL PANEL



R. PERFORATED WEATHERING STEEL LIGHT VALENCE



F. STEEL COLUMNS RED OXIDE



J. GALVANIZED 6" Z-CHANNEL SHADE LATTICE



ANODIZED ALUMINUM WINDOWS- CHARCOAL

ANODIZED ALUMINUM WINDOWS- LIGHT CHAMPAGNE



E. STACK BOND CONCRETE MASONRY - CHARCOAL



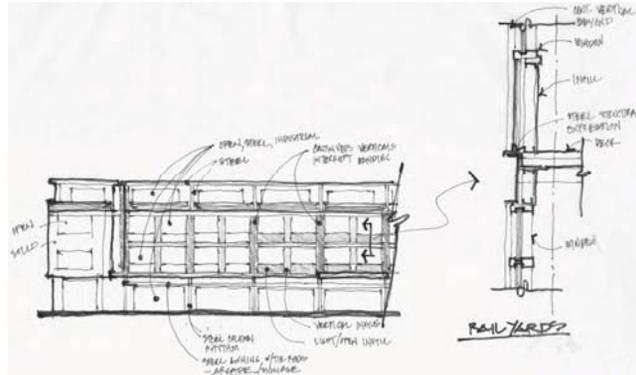
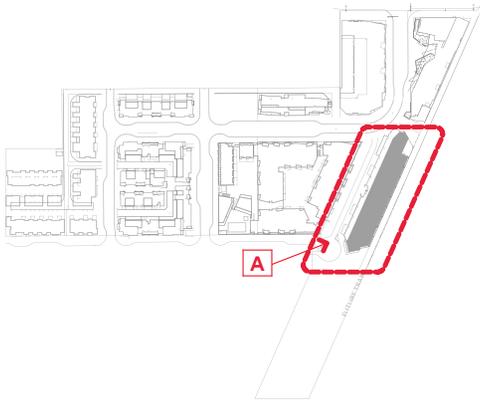
E. STACK BOND CONCRETE MASONRY

Material Palette

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We were inspired by the rhythm and movement found in the structure of the train tracks themselves, as well as historic warehouses. Both have a permeability and simple structure.

We wanted to create a modern interpretation of these elements. The design and functional intent is to be visually open to, and encourage interaction between, the activities occurring within the building and the activities and natural beauty occurring outside in the S'PARK neighborhood and along the Flatirons and Front Range.

For as long as there have been rail lines crossing this country, industry has located itself to take advantage of the unique opportunities available with proximity to the tracks.

Historically, the architecture associated with these typically industrial zones have been either very specific or it has been a simple multi-story accommodation that can allow a variety of light industries to flourish. Structures where the process itself defined the form of the building - the architecture built itself into the shape of the process

The buildings that inspired RAILYARDS essentially provided a simple space with good light and a simple internal layout of open space with a large column spacing and relatively tall space - tall enough and open enough to provide opportunity for many and differing industrial tenants. The space could then be fit-out to suit specific needs of the particular business, but the exterior form maintained a simple and clear form of multiple stories of typically concrete frame supporting concrete floors with masonry infill and glass walls.

Today, we still find these simple structures providing useful life. They may no longer house garment industry workers, or supply parts for the tool and dye industry, but often they have been retrofitted to provide the simple accommodation with good light and open space to office users, artist lofts, and residential condominium owners.

The RAILYARDS Buildings takes the same basic approach as the latter of these two types of trackside structures. The goal was to provide a structure that works at the scale of the tracks; a simple frame system, but now built from lightweight materials instead of the heavier concrete frame and masonry. The framework is steel with lightweight and highly insulated infill, glazing and translucent panel materials. The occupants will no longer be the same kinds of light industry as in decades past, but startups, office users, and small scale service companies who find that the open and small scale spaces best suit their business needs.

Also, by locating the non-residential structure along the tracks, we are able to protect the residential users who benefit from the mass and form that the trackside RAILYARDS structure can provide.

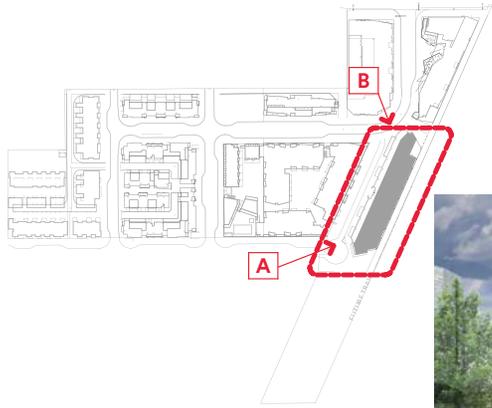
RAILYARDS

- Mixed Use Commercial Building
- 70,155 gsf
- 4 Story, Code Height: 54'-5" (Perceived Height: 50'-0")
- Ground Floor - Restaurant/ Commercial/ Retail
- Floors 2, 3 & 4 - Commercial Office

SopherSparrn | **RAILYARDS**

August 21, 2015

S'PARK



PRECEDENT IMAGE: WOONERF



PRECEDENT IMAGE: SEATING



PRECEDENT IMAGE: WOONERF

Scenario

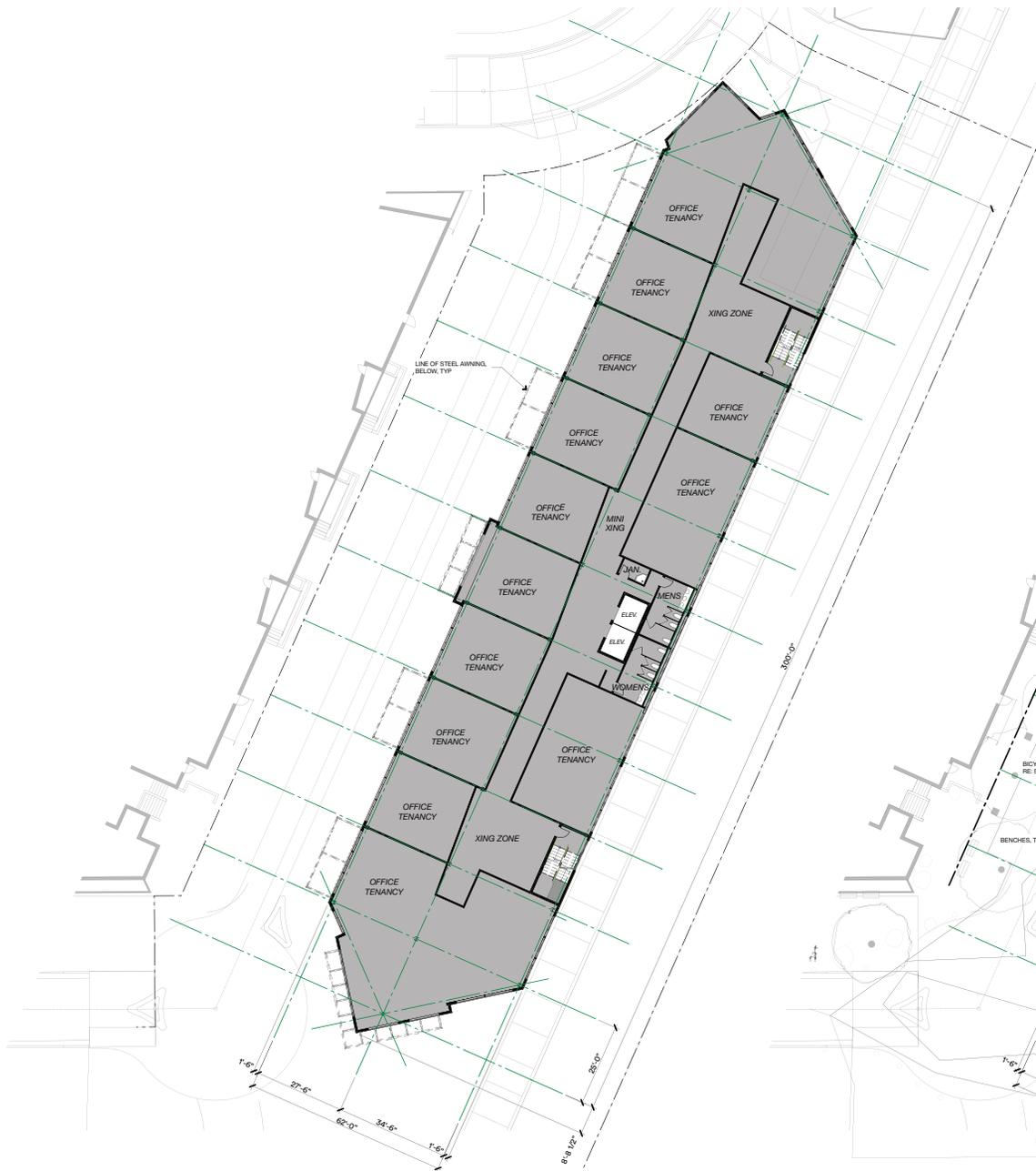
a day in the life of...

I love having my office in the Railyards building. Commuting in on my bike, I can grab a quick coffee in the lobby on my way up to the office. After work, I can walk up the block to get drinks at the tap room with co-workers, then have dinner on the patio of one of the great restaurants along S-PARK Place. Then, hopping back on the bike path right outside, I'm within 5-10 minutes of home and just about any place else in Boulder I want to go. It's great to be so connected to so many neighborhood amenities, and to be able to walk or ride to all of them.

SopherSparrn | **RAILYARDS**

August 21, 2015

S-PARK



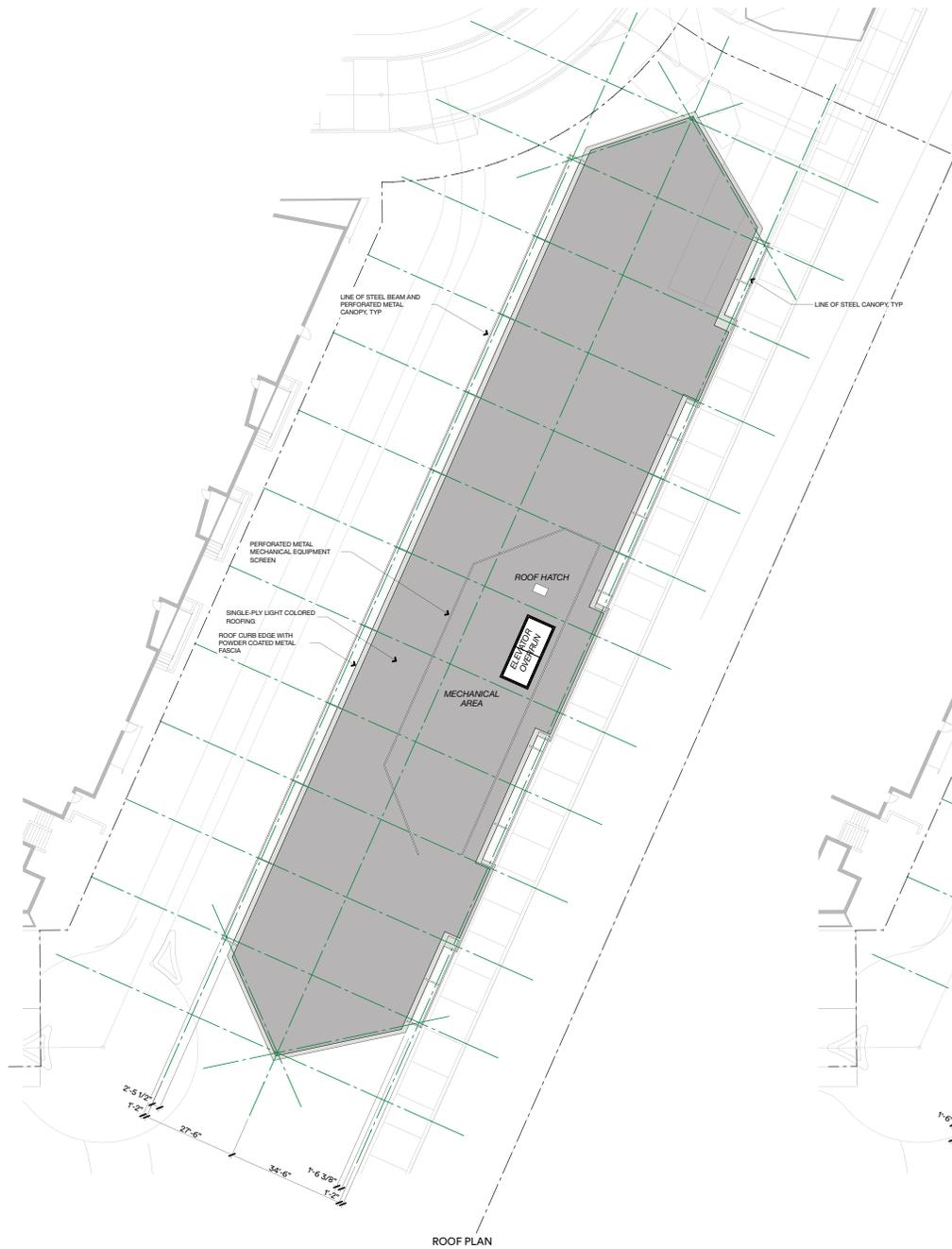
SECOND FLOOR PLAN (THIRD FLOOR SIM.)



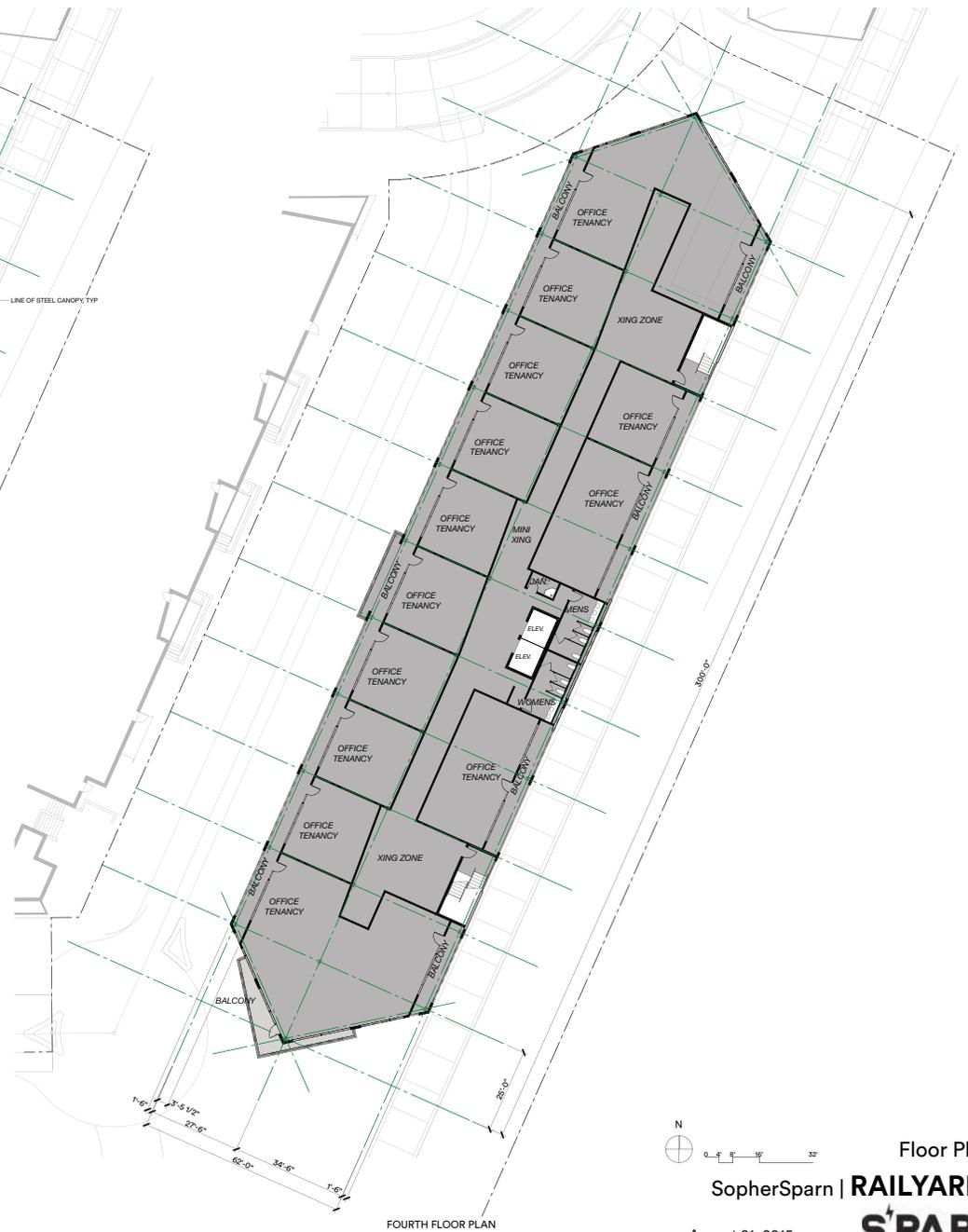
GROUND FLOOR PLAN

RE: LANDSCAPE DRAWINGS FOR PAVING PATTERN AND SITE AMENITIES
 RE: CIVIL DRAWINGS FOR STREET LAYOUT AND STREET SECTIONS

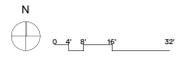


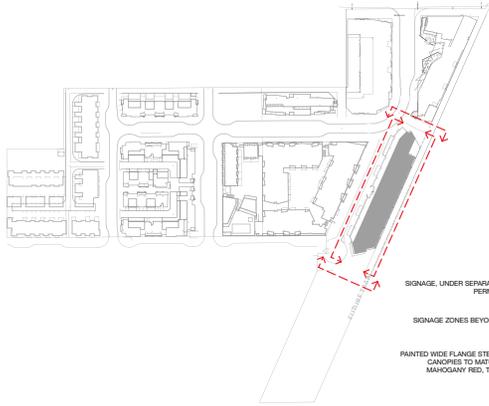


ROOF PLAN



FOURTH FLOOR PLAN





SOUTH ELEVATION



NORTH ELEVATION



EAST ELEVATION



WEST ELEVATION

0 4 8 16 32 Exterior Elevations

SopherSparrn | RAILYARDS

August 21, 2015

SPARK



MATERIALS RETURN WHERE APPLICABLE AND ARE CAPTURED IN A PRECISE MANNER



MATERIALS TRANSITION IN A WAY THAT IS THOUGHTFUL AND PURPOSEFUL TO THE ARCHITECTURE



WELDED CONNECTIONS AT STEEL MEMBERS AND POWDER COATED FINISHES PROVIDE DURABILITY



ROOF FLASHING AND DECORATIVE PARAPET

PAINTED STEEL

PERFORATED METAL SUN SHADE

RAINSCREEN PANEL
POWDER COATED STEEL RAILING

POWDER COATED METAL FINISHES FOR DURABILITY

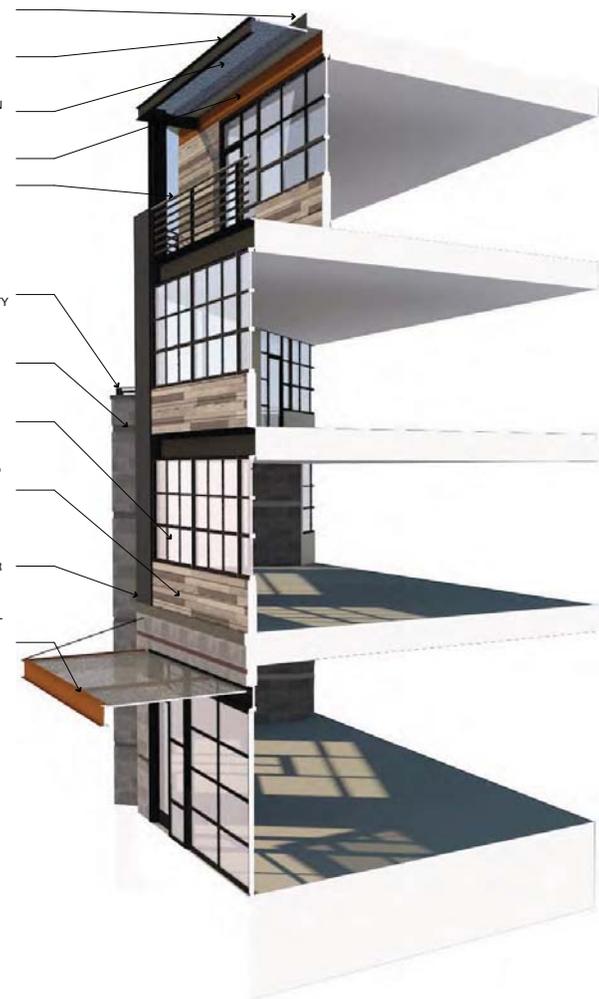
CONCRETE MASONRY UNITS WITH 4" REVEAL COURSES

INSULATING GLASS AND KALWALL TO INCREASE ENERGY EFFICIENCY

RAINSCREEN PLANKS TO ALLOW THE FACADE TO BREATHE AND SELF VENTILATE

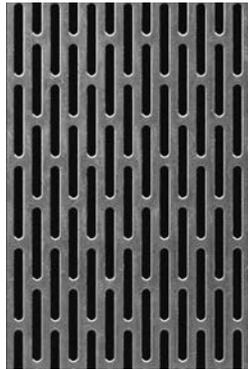
POWDER COATED COMPOSITE PANELS FOR DURABILITY

PAINTED EXPOSED STEEL STRUCTURAL AND ACCENT MEMBERS PROVIDE A LEVEL OF DETAIL AND DURABILITY





KALWALL INFILL AT WINDOWS



MT-2 PERFORATED METAL SCREEN
PAINT FINISH TO MATCH RS-2

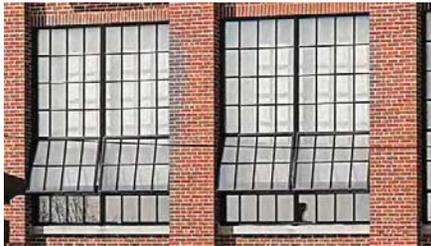


RS-1 RAINSCREEN PANEL
COLOR: MAHOGANY RED



TYPICAL ELEVATION

- MT-2 PERFORATED METAL MECHANICAL SCREEN
- BLACK PAINTED WIDE FLANGE STEEL CANOPY AND COLUMNS, TYP
- BLACK METAL RAILING
- SHEET METAL ROOF FASCIA, TYP FINISH TO MATCH MT-1
- RS-1 PANELS, TYP
- INTEGRAL COLOR CONCRETE WALL CAP TO MATCH RS-2
- MT-1 COMPOSITE ALUMINUM PANELS, TYP
- METAL WINDOWS WITH INSULATING TRANSLUCENT PANELS AND OPERABLE AWNING IN CENTER, TYP
- 6TH PLANKS OF RS-2 THRU RS-4 IN RANDOM PATTERN
- INTEGRAL COLOR CONCRETE WALL CAP TO MATCH RS-2
- CMU-2 RECESSED ACCENT COURSE @ 48" O.C., TYP
- CMU-1 FIELD COLOR, TYP
- PAINTED WIDE FLANGE STEEL CANPIES, COLOR TO MATCH RS-1
- OVERHEAD STEEL AND GLASS DOOR, TYP STANDARD ALUMINUM STOREFRONT SYSTEM, BLACK FINISH



METAL WINDOWS WITH AWNING OPENING
BLACK FINISH



MT-1 ALUMINUM COMPOSITE PANEL
COLOR: CHARCOAL



RS-2 RAINSCREEN PANEL
COLOR: STONE GREY



RS-3 RAINSCREEN PANEL
COLOR: MID BEIGE



RS-4 RAINSCREEN PANEL
COLOR: TOSCANA GREIGE



STEEL RAILING
POWDER-COATED BLACK FINISH



CMU-2, 4"x8"x16"
ACCENT COLOR: 663r1
GROUND FACE FINISH



CMU-1, 8"x8"x16"
FIELD COLOR: 807
GROUND FACE FINISH



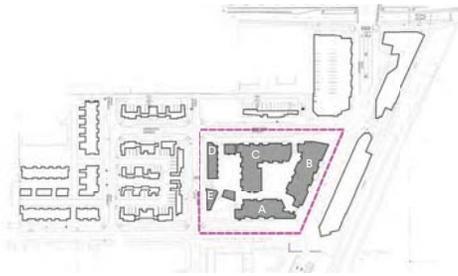
OVERHEAD STEEL AND GLASS DOOR
BLACK FINISH

Material Palette

SopherSparrn | Railyards

SPARK

August 21, 2015



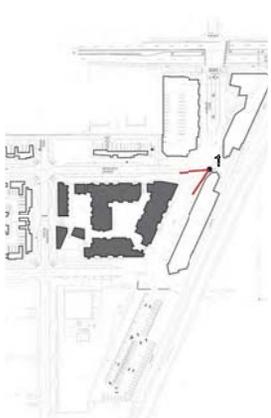
We wanted to build a place with pedestrian rhythm and front stoops between corner shops, that always feels like porch lights are on—people live here. And we can feel invited from the street to the terrace, and vice versa.

We wrapped an array of five buildings around structured parking, linked to pedestrian ways, capped by multiple gardens that 'breathe' with many ways in, many ways out.

TIMBERLOFTS is a framework for living within S'PARK. Its terraced scale of multi-family dwellings is split open at corners and several mid-block points, to allow residents a complete sense of connection to the courtyards and pool/fitness deck within, and to the fabric of streets, walks, offices and shops beyond. These apertures offer generous stair connections where breezes flow through. Sightlines tied into 'lines of desire' take residents and visitors through active or contemplative spaces, and to the S'PARK Place woonerf, Meredith Park, or MARKET Plaza connections.

As a supporting character in the urban play, TIMBERLOFTS is a backdrop to the woonerf, a veneer to structured parking, and a 'foil' in complement to neighboring buildings. In that sense Timber has been rendered in multiple materials and variants, as it shifts from predominant pewter grey V-rib cladding on Bluff and S'PARK Place, to light bronze (opposite the midnight blue Meredith House), charcoal on the Meredith and Junction townhomes, and finally warm wood on the southeast lobby area. This warm wood, coupled with buff concrete masonry and boardform concrete stoops, is wrapped around the first story, below the 'Timber Line' datum. An emphasis on warm, tactile, organic texture in the first 15 feet enhances the pedestrian horizon.

This rapidly renewable wood resource is used in the building frame. Yet the sustainable agenda is also overt, as bike rooms are glazed, visible to street, above the entrance to the garage and gardens. The rhythmic relief of the façade with thin walls and alcoved decks and stoops gives shade, a passive solar benefit to the larger glass—while introducing a play of light shadow and color, at a gentle cadence.



1 vibrant mixed-use at Meredith and the woonerf

TIMBER - 5 Buildings	Bldg A	-	TIMBER LOFTS 1	—27 units
	Bldg B	-	TIMBER LOFTS 2	—41 units
	Bldg C	-	TIMBER LOFTS 3	—53 units
	Bldg D	-	TIMBER TOWNHOMES	—8 units
	Bldg E	-	TIMBER COMMON	

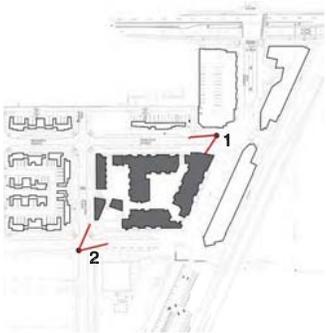
Mixed Use Buildings

- 4 Story, actual height 49'-8" at northeast corner; 51'-9 1/2" technical height
- 206,465 nsf
- Ground Floor – Live/Work units, Restaurant, Walk-up Apartments, Resident Amenities & Services
- Floors 2, 3 & 4 – Apartments
- Roof – Resident Amenities, including pool, deck and enclosed areas.
- 214 Structured parking spaces (shared, unbundled, proposed as part of Boulder Junction Access District)

WORKSBUREAU | **TIMBERLOFTS**

August 21, 2015





1 Meredith access, shortcut, and rhythm of stoops



2 residents' 'living room' lobby and terrace above

Scenario

a day in the life of..

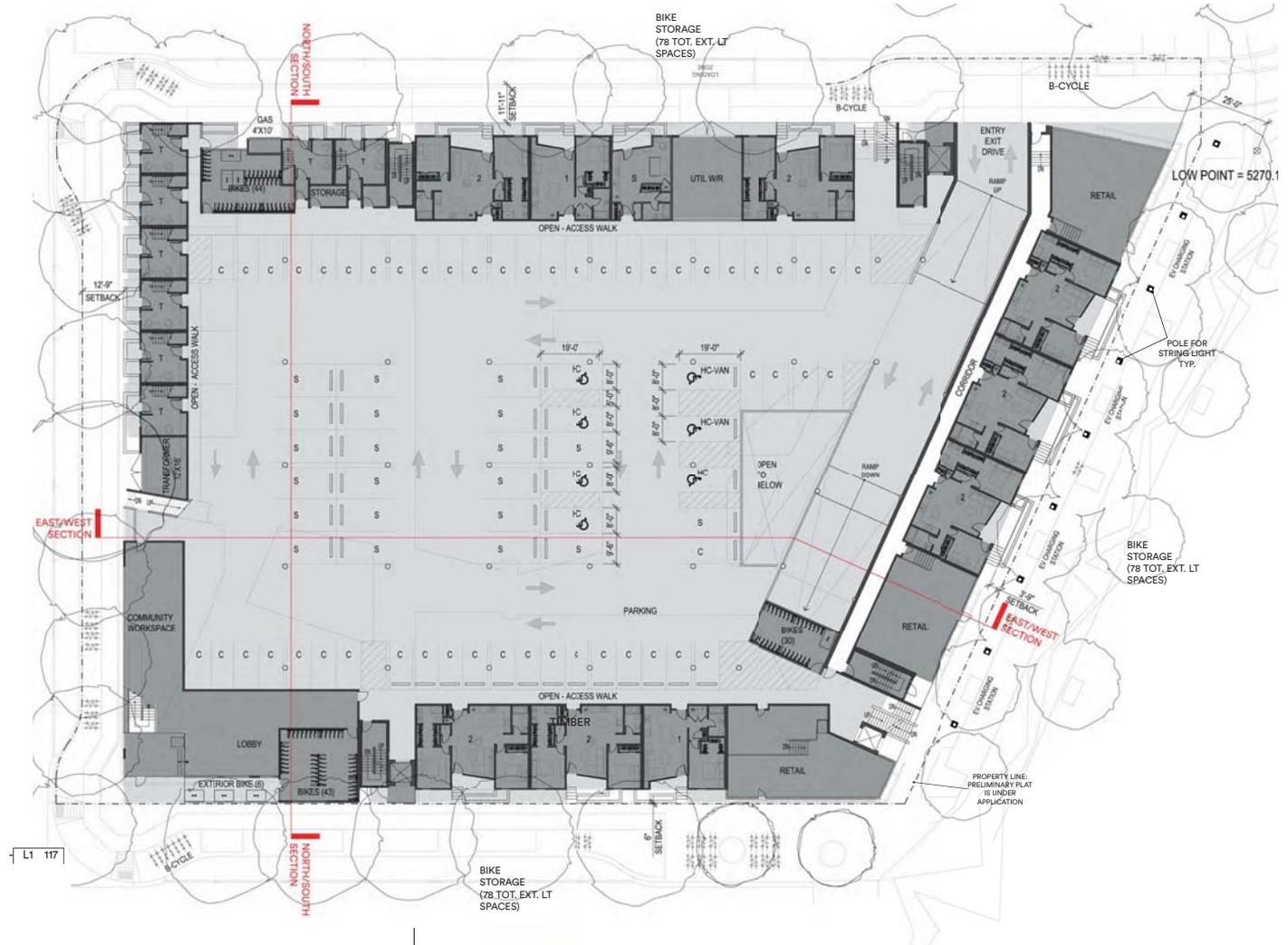
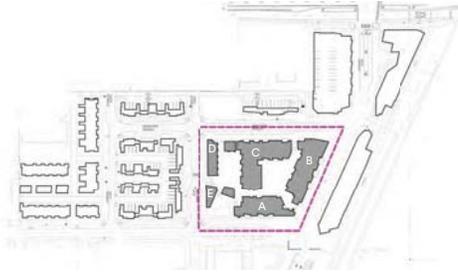
"My daughter and I shared breakfast on our balcony today. Our time allowed us to then stroll through the east court, on our way west before preschool. When I returned walking through Steelyards, I took the southwest lobby stair up to the Coworking suite on the pool deck and the west court. I'm an entrepreneur, and this is my space. I can take a conference call in the garden or tend my plot of rosemary and sprouts, and break to workout. Later we are in Meredith Park with friends.

Seasons pass easily here and we see the shadows shift across the months. My car is downstairs but I haven't used it in three weeks. We take our bikes, or just walk to the Bluff corner café, Whole Foods, anything we need. Fridays are sometimes dining on MARKET plaza, or an improv dance piece on S'PARK place—love those lights through the trees—it's always different."

WORKSBUREAU | **TIMBERLOFTS**

August 21, 2015

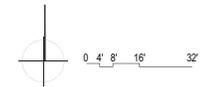
S'PARK



L1 117

RH-6 zoning

MU-4 zoning



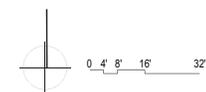
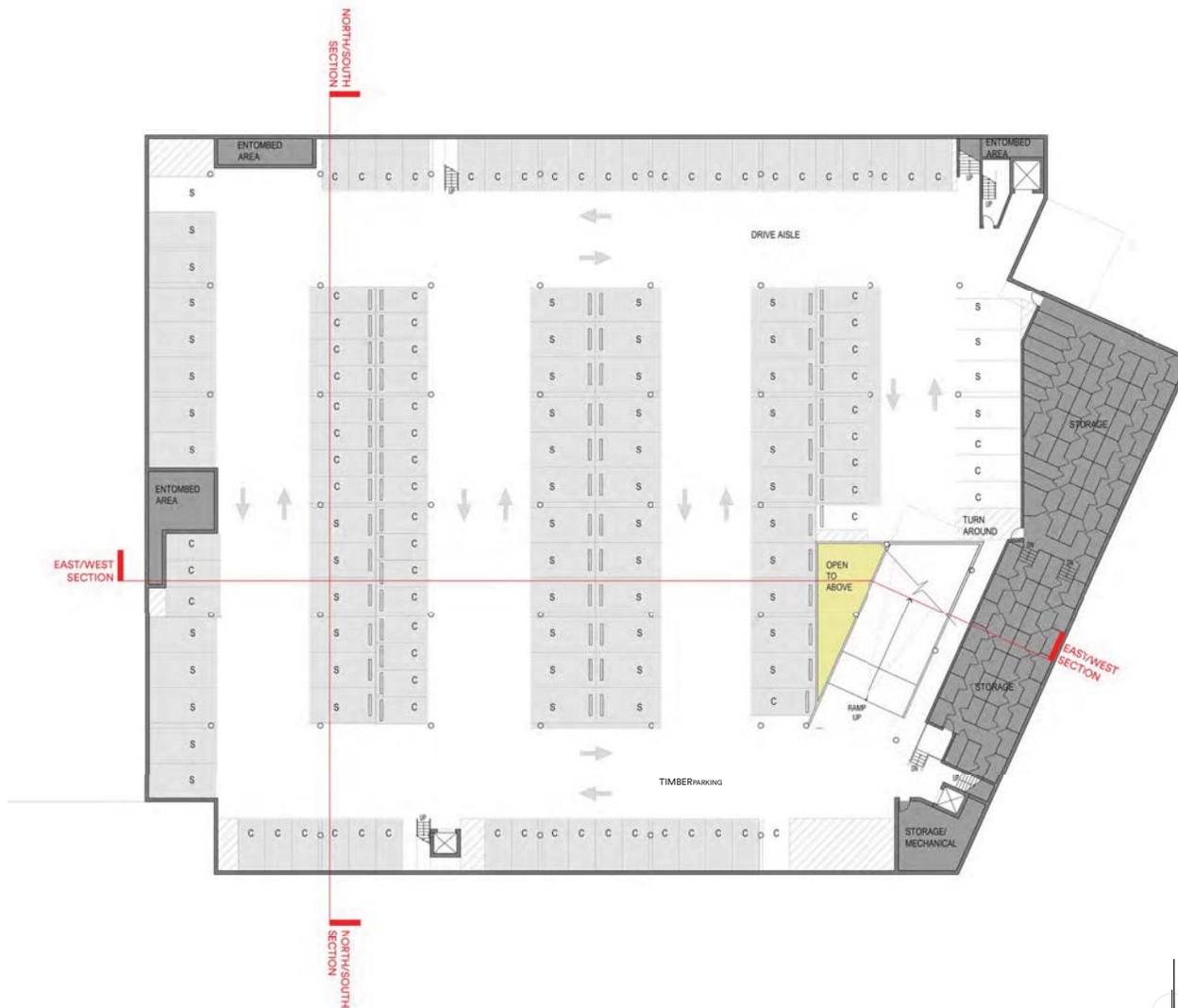
Site Plan

Figures for FAR and Open Space pertain to the portion of project in the MU-4 zone. For the portion in RH-6, refer to Project Fact Sheets, as Density and Open Space are calculated in the aggregate in this zone.

WORKSBUREAU | **TIMBER**LOFTS

August 21, 2015

SPARK



Basement Plan

WORKSBUREAU | **TIMBERLOFTS**

August 21, 2015

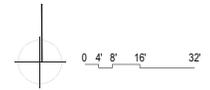
S'PARK



Level 1 Plan
PROJECT NOT PROPOSED TO BE PHASED

WORKSBUREAU | **TIMBERLOFTS**

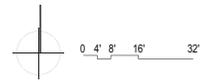
August 21, 2015 **SPARK**



Level 2 Plan
PROJECT NOT PROPOSED TO BE PHASED

WORKSBUREAU | **TIMBERLOFTS**

August 21, 2015 **S'PARK**



Level 4 Plan
 PROJECT NOT PROPOSED TO BE PHASED

WORKSBUREAU | **TIMBERLOFTS**

August 21, 2015

S'PARK



Elevations

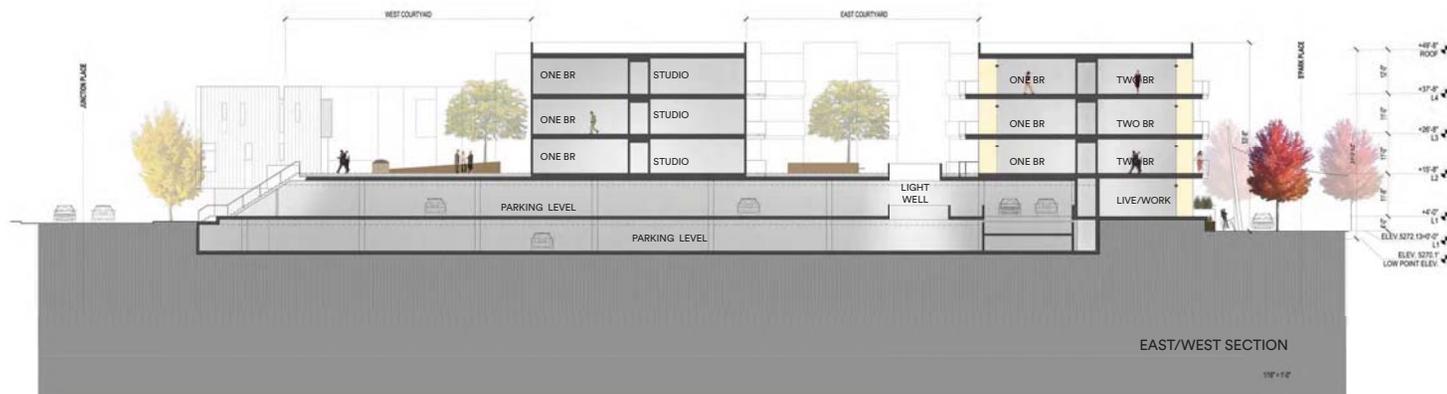
WORKSBUREAU | **TIMBERLOFTS**

August 21, 2015





SECTION AT WOONERF STOOP



Sections

WORKSBUREAU | **TIMBERLOFTS**

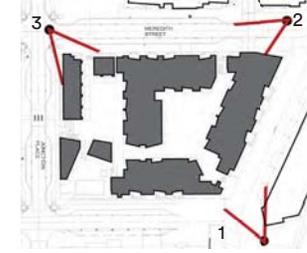
August 21, 2015





1. SE Corner of Bluff and S'PARK Place

- At the convergence of Bluff Street and the Woonerf, retail and balconies urn the corner. Shops are brought forward to align with S'PARK Place facades and brick stoops. The warmth of masonry folds in at the 'short cut' to the gardens. The stair and the landing project out to overlook the Woonerf like a bay window.



- A 'breathing building', Timber opens its garden courts to the street at three locations. In this case, all forms of transport converge: pedestrian, cyclist (bike room visible), auto (garage entrance; resident and visitor). The interlude between pewter and bronze-clad buildings furthers the sense of multiple, residentially scaled buildings. The break is treated as a cleft of garden colors.



2. Turning the Corner: Continuity and Rhythm



3. Public Invitation and Personal Domain

- At all corners, including townhomes at the northwest, Timber returns materials for a sense of wholeness. Here, corner windows extend the four-sided quality. Windows respond to program inside, and are expressed with similar character on each face.
- Key materials such as wood cladding extend to become screens or gates at utility zones—enhancing texture at the pedestrian horizon.



Partial Elevation at S'PARK Place Woonerf Scale 3/8" - 1' - 0"

- RETURN WALL, FLAT SEAM METAL CLAD AT INTEGRAL PARAPET/ MECHANICAL SCREEN
- CAP FLASHING, PEWTER GREY TO MATCH CLADDING
- FLAT SEAM METAL PANEL AT SPANDREL WALL ZONE
- PEWTER GREY V-RIB METAL CLADDING, WITH TERMINATION FIN AT WINDOW; ADDS DEPTH
- FIBERGLASS WINDOW UNITS RECESSED APPEARANCE GREY RECESSIVE COLOR
- PERFORATED GALVANIZED GUARDRAIL, WITH RANDOM V-RIB FOR STIFFNESS AND SHADOW TEXTURE
- WOOD FRAME BALCONY WITH IMPERVIOUS DECK SURFACE, PEWTER GREY METAL FASCIA, AND PROTECTED STUCCO SOFFIT
- STUCCO WALL AT BALCONY ALCOVE, PROTECTED AT CORNER / EDGE BY METAL CLADDING
- SLIDING DOOR AND TRANSOM UNIT PROVIDE DAYLIGHT AND SCALE TO BALCONY
- MULTIPLE WINDOW UNITS MULLED TOGETHER FOR SIMPLICITY, AND OPEN SIGHT LINE
- PEWTER GREY METAL FASCIA; CREATES POSITIVE DRIP, PROTECTION AT TOP OF WOOD SIDING
- 2 1/2" ANODIZED ALUMINUM STOREFRONT SYSTEM CHAMPAGNE COLOR, FULL HEIGHT GLAZING
- POWDER COATED STEEL SIGN FASCIA, SUPPORTED BY KNIFE PLATED BRACKETS FROM VERTICAL MULLIONS, WITH GALVANIZED BAR-GRATE SHADE (WHERE DEPTH ALLOWS); MAINTAINS CLEARANCE FROM GLASSING FOR CLEANING
- PAINTED STEEL PIPE HANDRAIL WITH CABLE GUARDRAIL
- DIMENSION TEXT-SIGNAGE INTENT; UNDER SEPARATE PERMIT NOT IN SITE REVIEW
- 4"X16" OCHRE BUFF BRICK; MODULE SCALE CONTRASTS WITH V-RIB CLADDING
- CONCEALED FLASHING OF WOOD-TO-BRICK AT TRANSITION
- DOOR AND TRANSOM UNIT, 5" SILL MULLION ADDS SCALE AND PROTECTION



Woonerf Transitions, depth and relief

- Color matched parapet coping, V-rib cladding, and metal spandrel panels, create relief, texture and shadow. Recessed window units will have amplified depth through perimeter flashing 'fins' which terminate V-rib profiles. Dark color window frames will appear more recessive and harmonious.
- Perforated galvanized balcony guardrails use random V-fold ribs to remain straight and true, with subtle light effects.
- Stucco is reserved to stoops and balconies, for the warmth of 'porchlight yellow' walls and soffits—terminated with edges protected behind metal claddings.
- The base course of Timber is clad in 'Accoya' wood, environmentally treated for 50 year durability, and finished to emulate the warmth and grain of natural cedar with clearcoat.
- The metal clad bottom 'belt' fascia transitions residential cladding to retail storefront—to be anodized aluminum, soft champagne finish. This is in complement to other warm finishes at the base of Timber.
- Stoops feature warm buff concrete masonry in honed finish for sense of urbane quality, in complement to board-form concrete steps and stoops. Scale has been limited to bench or waste-height, with plantings creating a sense of 'interior' for residents.

Design Excellence
Tectonics and Materiality
WORKSBUREAU | **TIMBERLOFTS**

August 21, 2015

S'PARK



B.2 V-RIB METAL CLADDING LIGHT BRONZE

B.1 V-RIB METAL CLADDING PEWTER GREY



C. 11 GA PERFORATED GALV GUARDRAIL. RANDOM V FOLD



N. PERFORATED GALVANIZED Z CHANNEL



O. STANDING SEAM CLADDING CHARCOAL GREY

BOARD FORM CONCRETE STOOP



J. 'ACCOYA' WOOD SIDING IN CLEAR SEALED CEDAR FINISH



I. SAND FINISH STUCCO 'PORCHLIGHT YELLOW'



F. POWDERCOATED STEEL CHANNEL / SIGN FASCIA. RETAIL



F. POWDERCOATED STEEL CHANNEL / SIGN FASCIA. RESIDENTIAL LOBBY

R. CLEAR GLASS GUARDRAIL



ANODIZED ALUMINUM WINDOWS - LIGHT CHAMPAGNE



DARK GREY FRP



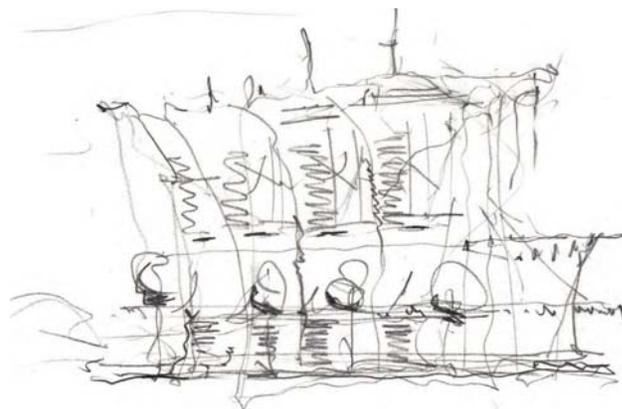
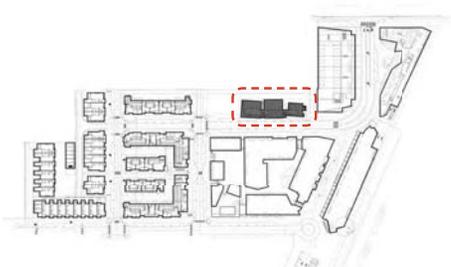
G. BUFF COLORED MASONRY BRICK

S. COR-TEN PLATE STEEL PLANTER

WORKSBUREAU | **TIMBERLOFTS**

August 21, 2015

SPARK



On a long narrow site, we wanted to create a place that did not feel so long and narrow. A building that shifts slightly. One that feels like it might be moving.

So we created three pieces that twist softly, and an elevation that moves; one that feels musical and looks like a dance, with the ground floor transparent, pulled back, revealing a glimpse of Meredith Park beyond.



Situated on a long and narrow East– West oriented site, Meredith House provides a key transition element from mixed use to pure residential within S’PARK.

A dynamic and creatively designed structure that shifts in both plan and elevation, Meredith House takes advantage of its orientation to harvest ample daylighting for the 15 loft style units within.

The length of the building is separated into three main volumes with the entrance oriented east, towards the woonerf, to capture the energy and activity that will be at the heart of S’PARK. On the west side, the community room opens to the S’PARK pocket park, acting as an expansion of the ground floor. An open program to vitalize the open space.

With smaller setbacks, abundant onsite bicycle parking, and generous glazing at the ground floor, Meredith House is intentionally urban. Meredith House blends textural concrete masonry units that are produced regionally with a modern rainscreen cladding to reduce maintenance and increase material longevity. Touches of timber warm the palette and make a material nod to the history of the site.

High performance windows and superior insulation aid in the completion of an energy efficient and sustainable building envelope. The low slope roofs allow for plenty of solar PV panels to be installed.

The project houses 15 two-bedroom dwelling units that are offered at market rate. Each unit has one parking space as well as private storage at the ground level; useful for secure bicycle storage. Each floor has five units and by creating the separated building volumes, each unit is, in spirit, a corner unit. Private balconies introduce cadence to the elevations, carefully placed to take advantage of views and connect the residents to several energy centers within S’PARK.

MEREDITH HOUSE

- Residential Condominiums
- 20,754 gsf
- 4 Story, 47’ - 2” (actual height from sidewalk)
- Ground Floor – Resident Parking, Amenities & Services
- Floors 2, 3 & 4 – Condos

Surround Architecture | Meredith House

August 21, 2015

S’PARK



Scenario
a day in the life of...

"I just love living so close to the transit center and using the Bus Rapid Transit to commute to and from work. In fact, just the other evening I left work late but was able to make quick time home because of the BRT. I arrived at the station on 30th, unlocked my bike and peddled up the bike path to my neighborhood, S'PARK. As expected, my girlfriends were still waiting for me in the woonerf and all of the food truck vendors were still there.

Food night is my favorite night of the week in the summer. Tons of local chefs sell great eats and there are always local musicians on the bricks. It is so fun to watch the kids dance in the street without a care in the world.

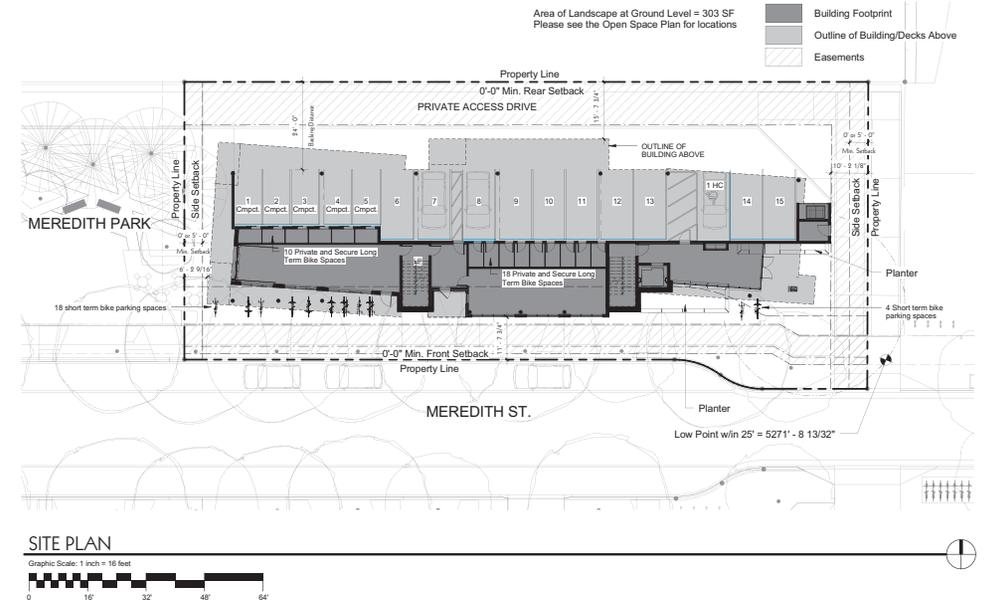
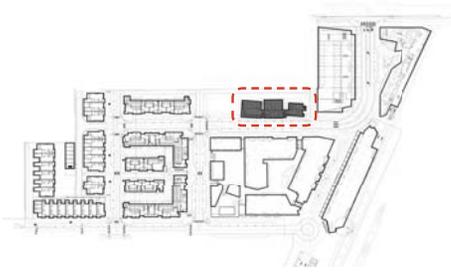
After dinner with my friends, I made my way back to Meredith. The pocket park was alive with kids reveling in a hot summer's night – capture the flag was on the agenda for this twilight. At the end of the day my respite is found staring out to the lights of Boulder and the faint glow of the flatirons beyond.

This will never get old."

Surround Architecture | **Meredith House**

August 21, 2015

S'PARK

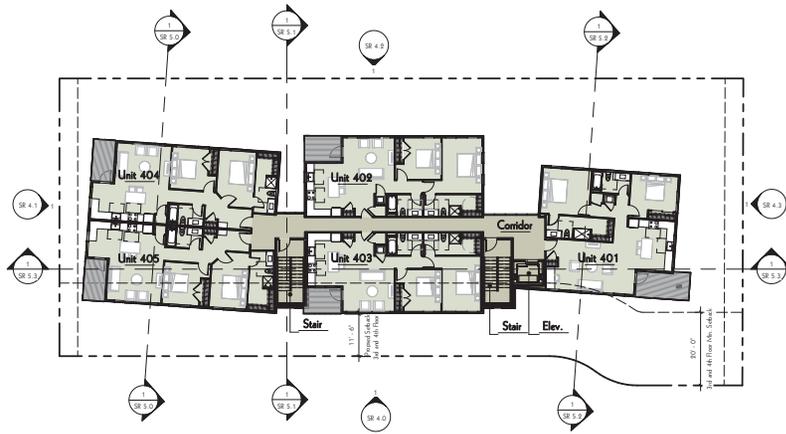


Site Plan

Surround Architecture | Meredith House

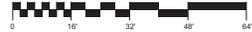
August 21, 2015





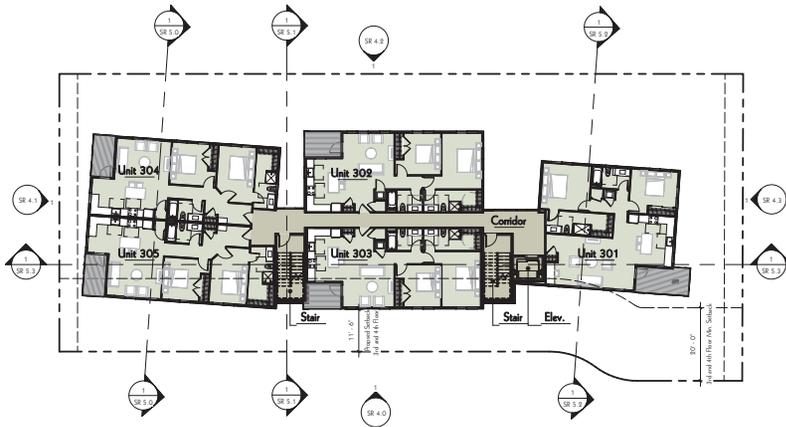
FOURTH LEVEL FLOOR PLAN

Graphic Scale: 1 inch = 16 feet



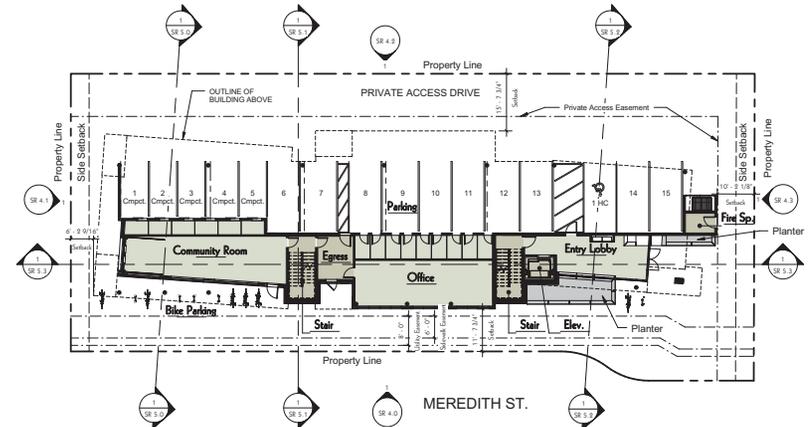
SECOND LEVEL FLOOR PLAN

Graphic Scale: 1 inch = 16 feet



THIRD LEVEL FLOOR PLAN

Graphic Scale: 1 inch = 16 feet



GROUND LEVEL FLOOR PLAN

Graphic Scale: 1 inch = 16 feet



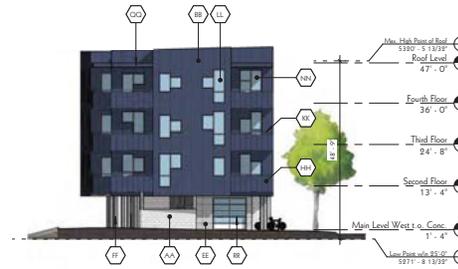
Plans

Surround Architecture | Meredith House

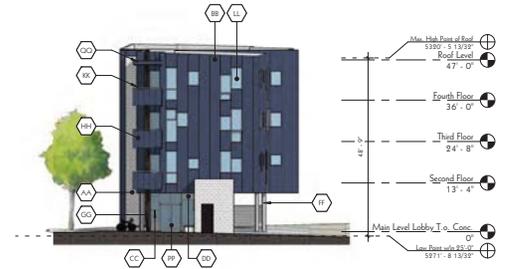
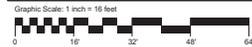
August 21, 2015



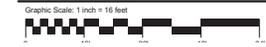
ELEVATION KEY NOTES		
NO.	DESCRIPTION	MANUF. COLOR/SPEC.
AA	CMU Veneer	BeWhite 8x16 CMU - ground face, color #720 - see photos
BB	3' Wide Rainscreen Panels	Trepa or Richlin Dark Blue
CC	Aluminum storefront	Aluminum/Silver
DD	Aluminum Spandrel Panels	Aluminum/Silver
EE	Aluminum Siding Panels	Aluminum/Silver
FF	Exposed Concrete Columns	--
GG	Timber Column	--
HH	Perforated Metal Railing Panels	Raised perforations - color to match rainscreen panels
JJ	Perforated Metal Fence Panels	Same as railing panels
KK	Steel guardrail & railing panel support	Pol. medium grey
LL	Metal Clad Wood Windows	Aluminum/Silver
MM	Hollow Metal Door	Pol. to match CMU
NN	Metal Clad Wood Doors w/ Glass	Aluminum/Silver
OO	Insulated Glazing Unit	--
PP	Storefront Door with Glass Lite	Aluminum/Silver
QQ	Metal Facs	Finished to Match Rainscreen
RR	Overhead Door - Insulated/Glazed	Aluminum/Silver
SS	CMU Veneer - Accent Color	8x16 CMU - ground face, color #807



BUILDING ELEVATION - WEST



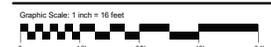
BUILDING ELEVATION - EAST



BUILDING ELEVATION - NORTH



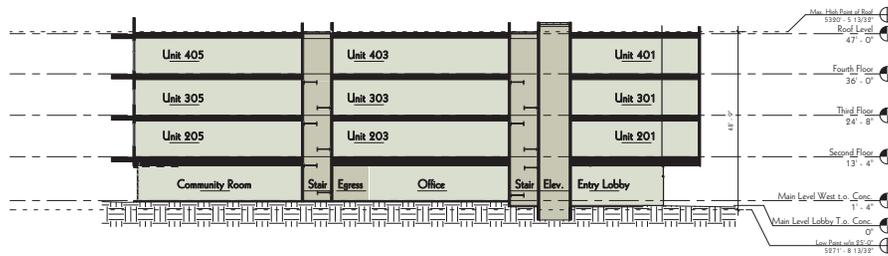
BUILDING ELEVATION - SOUTH



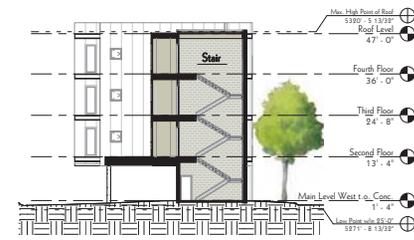
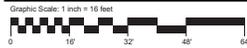
Elevations

Surround Architecture | Meredith House

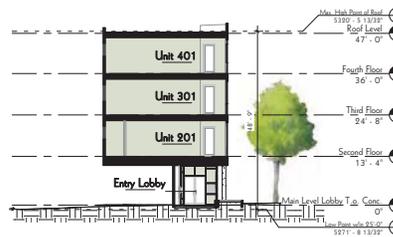
August 21, 2015 **SPARK**



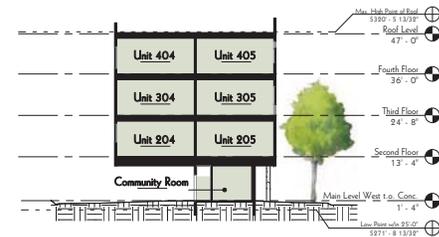
EAST WEST BUILDING SECTION



NORTH SOUTH BUILDING SECTION B



NORTH SOUTH BUILDING SECTION C



NORTH SOUTH BUILDING SECTION A



Sections

Surround Architecture | Meredith House

August 21, 2015



Rainscreen Panels: Chosen for their low maintenance, longevity, and performance. Open rainscreens provide pressure equalization, eliminate mold and mildew, and reduce maintenance as no grouting and minimal sealants are used. In addition, rainscreen panels can be easily removed for monitoring and inspection of the structure.

Residential Window Systems: High quality, low maintenance, high thermal value, low infiltration, and ease of installation.

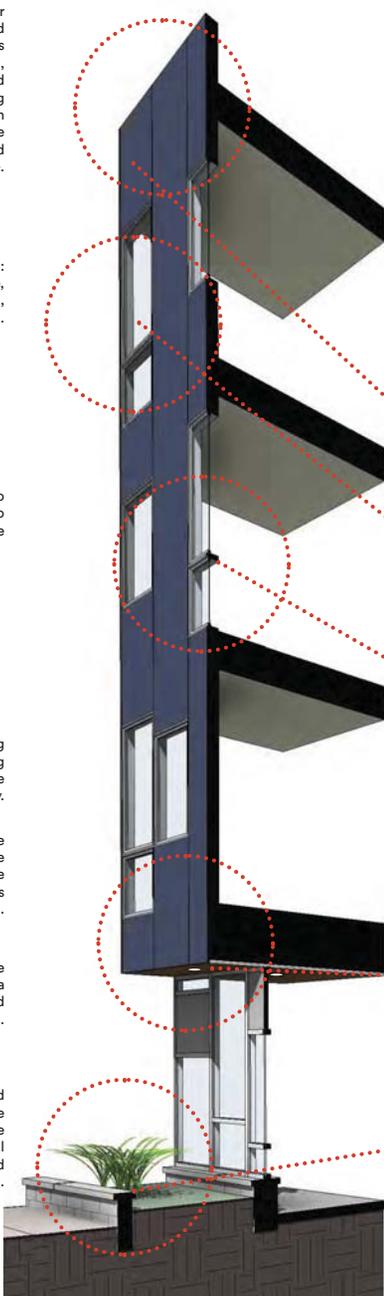
Windows that are nearly floor to ceiling provide light deeper into the space

Integrated recessed lighting provides important wayfinding and accentuates depth and texture of materiality.

Material and plane change at the ground floor level anchors the building, creates an experience at the human scale, and provides depth via shadow and lighting.

The location of planters and the use of organic material create a soft interface between ground plane and building.

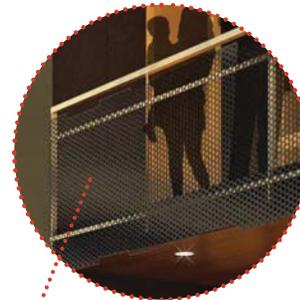
The top of the foundation is held below grade, eliminating exposure of the typical rough concrete foundation, so the material interface at grade is complete and unbroken.



Metal-clad wood windows are durable and maintenance free. The wood interior can be stained or painted. The exterior finish is factory applied to the aluminum profiles. Manufacturer provided sill and jamb extensions for rainscreen provide a complete installation.



Material & Transitions: Materials return around corners and complete the formal massing of the primary building elements. High quality materials are used throughout, without substituting budget materials on secondary elevations, or in negative spaces. Douglas fir tongue and groove soffits aesthetically warm the spaces frequently occupied by users.



Balcony railings provide privacy screening while allowing the passage of light into the spaces during day, and creating a unique texture at night.



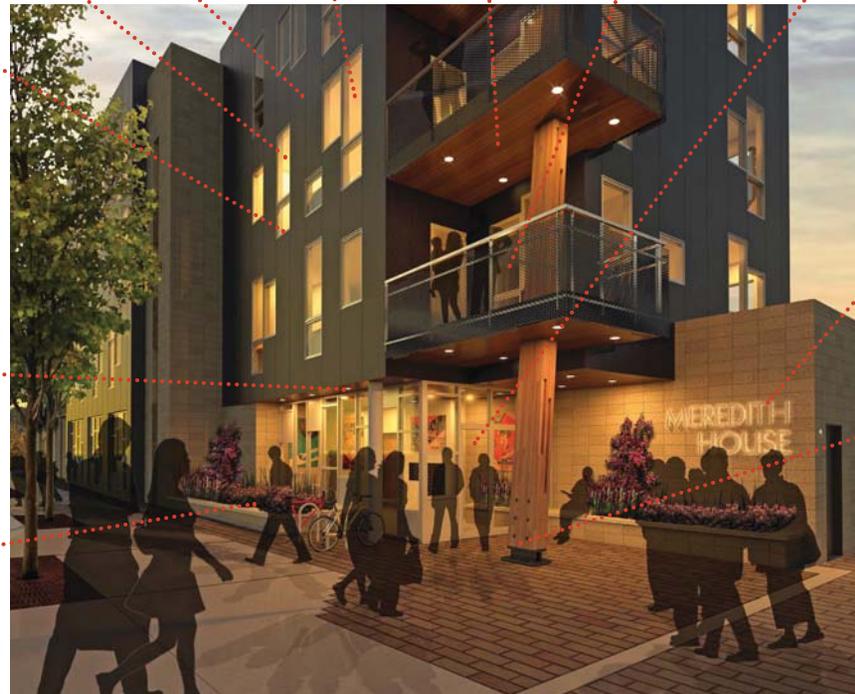
Blurring the Boundaries: Masonry transitions seamlessly inside to out, providing continuity and invitation to enter



Wayfinding through materiality and signage, obviating circulation paths, both inside and out.



Thoughtful detailing of structural elements and integrity of materials provide honesty in architectural expression.

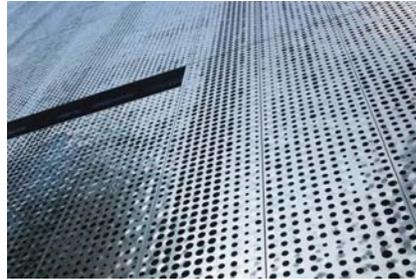


Design Excellence

Surround Architecture | Meredith House

SPARK

August 21, 2015



PERFORATED RAILING PANEL



CONCRETE MASONRY



SIDING PANEL



TIMBER



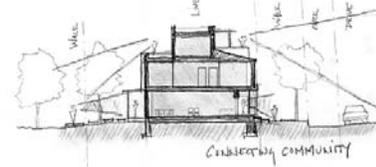
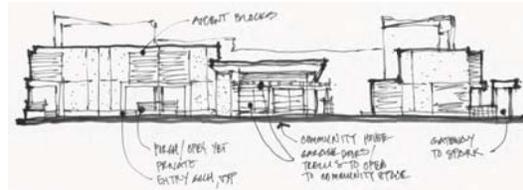
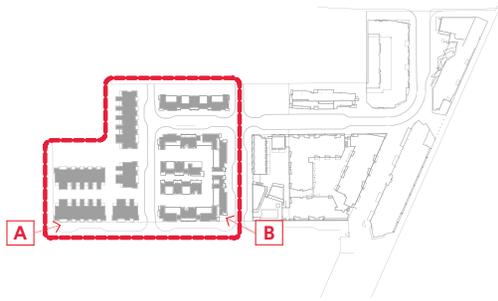
DOUG FIR SOFFIT

Materials

Surround Architecture | Meredith House

August 21, 2015





We started with a neighborhood vision, with front stoops and large porches and engaging upper decks that would connect the people with the ground, the sky, and the mountains. Knowing that there is an existing neighborhood fabric that can already be found adjacent to the site, but has not yet reached it's potential, we set about to add additional architecture that adds to the neighborhood in a positive and welcoming manner. Inclusive sites with welcoming permeability throughout.



The western zone of the S'PARK project is dedicated to family housing in primarily townhouse units, with a few flats at corners of blocks, in order to help maintain the continuous block face pattern as the building turns the street corners.

All units are brought close to the street, with individual porches that are raised from 12-21", maintaining a sense of separation from the public realm, while having a very direct connection to the pedestrian zone. The inspiration for this model comes from making a pedestrian friendly and engaging walking street along Bluff and all of the townhouse zone of the project.

Internally, from west to east, there is a shared common series of open spaces that tie the site back to TIMBER and crossing the various blocks. Each have their own character and each have residences fronting directly on green space. Central to the site is a Community Commons Building that mostly serves the affordable community, while the western site has an outdoor covered picnic area fronting onto its outdoor green space.



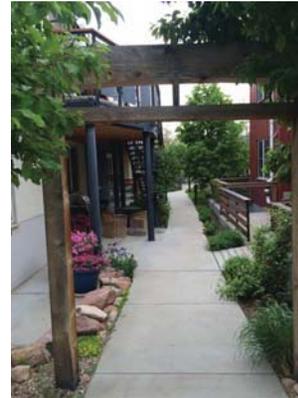
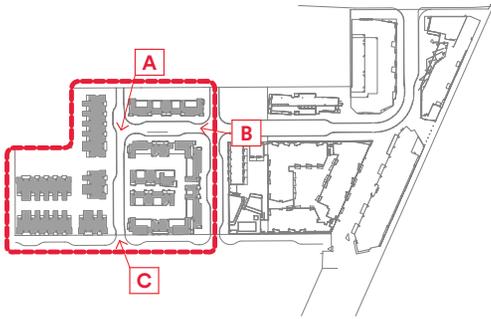
S'PARK_{west}

- Mixed Residential - 3085 Bluff Market Rate Townhomes and 3155 Bluff Permanently Affordable Townhomes and Flats
- 106,533 gsf
- 3 Story, 3085 Code Height: 34'-8", max. (Perceived Height: 30'-6"); 3155 Code Height: 37'-10", max. (Perceived Height: 34'-2")
- Ground Floor - Resident Parking, Flats, First Floor Townhomes, Community House
- Floors 2 & 3 - Flats and Townhomes
- 45 covered parking spaces & 24 private garages

SopherSparr | S'PARK_{west}

August 21, 2015

S'PARK



PRECEDENT IMAGE: CONNECTIONS



B



PRECEDENT IMAGE: PORCHES



PRECEDENT IMAGE: MASSING & MATERIALS



A



C

Scenario

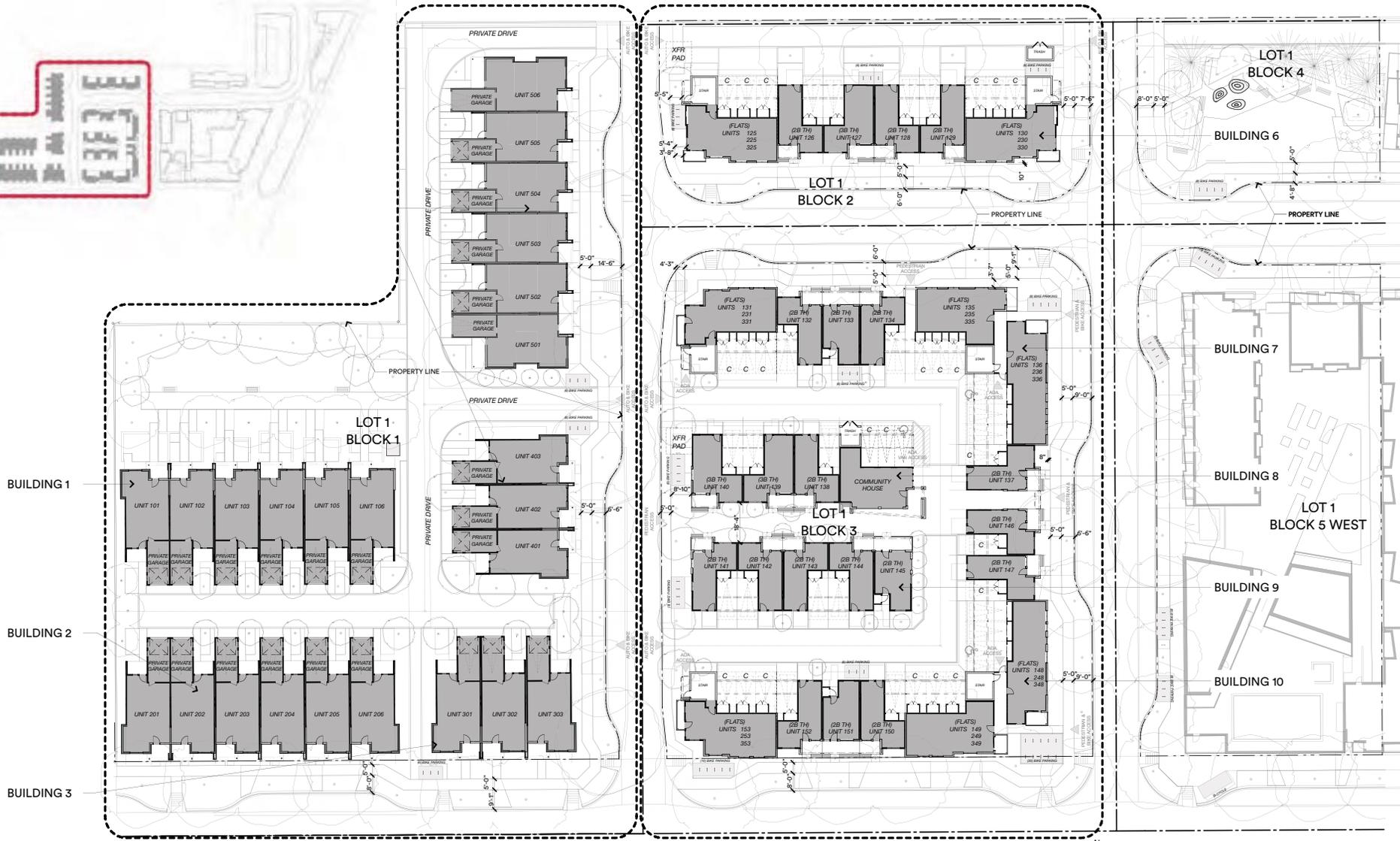
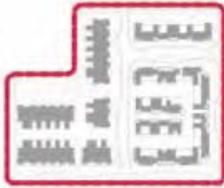
a day in the life of...

It's great to live in a place within walking distance to everything you need. This is the quintessential Boulder neighborhood; the exact reason people want to be here...good neighbors, great parks, nice markets/shops and plenty of access to multi-use trails. Everything I need is right here.

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August 21, 2015

S'PARK



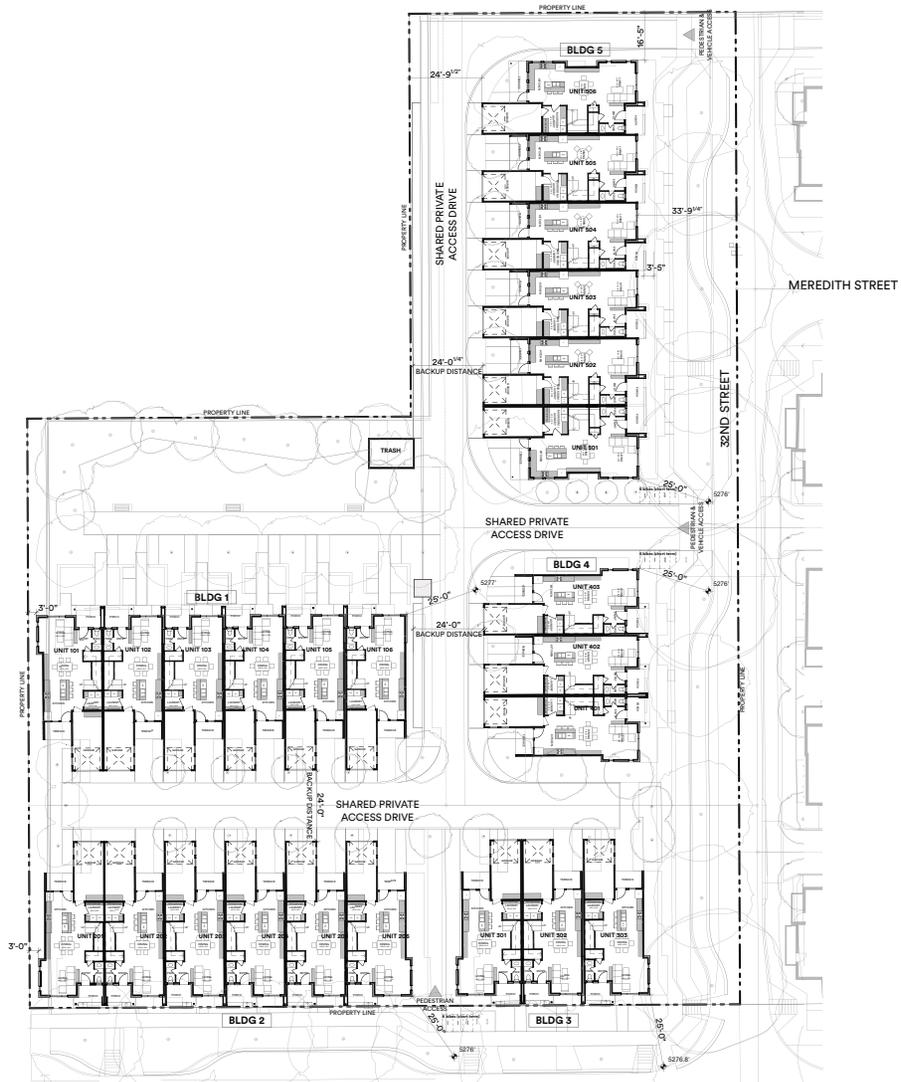
3085 BLUFF
(MARKET RATE TOWNHOMES)

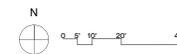
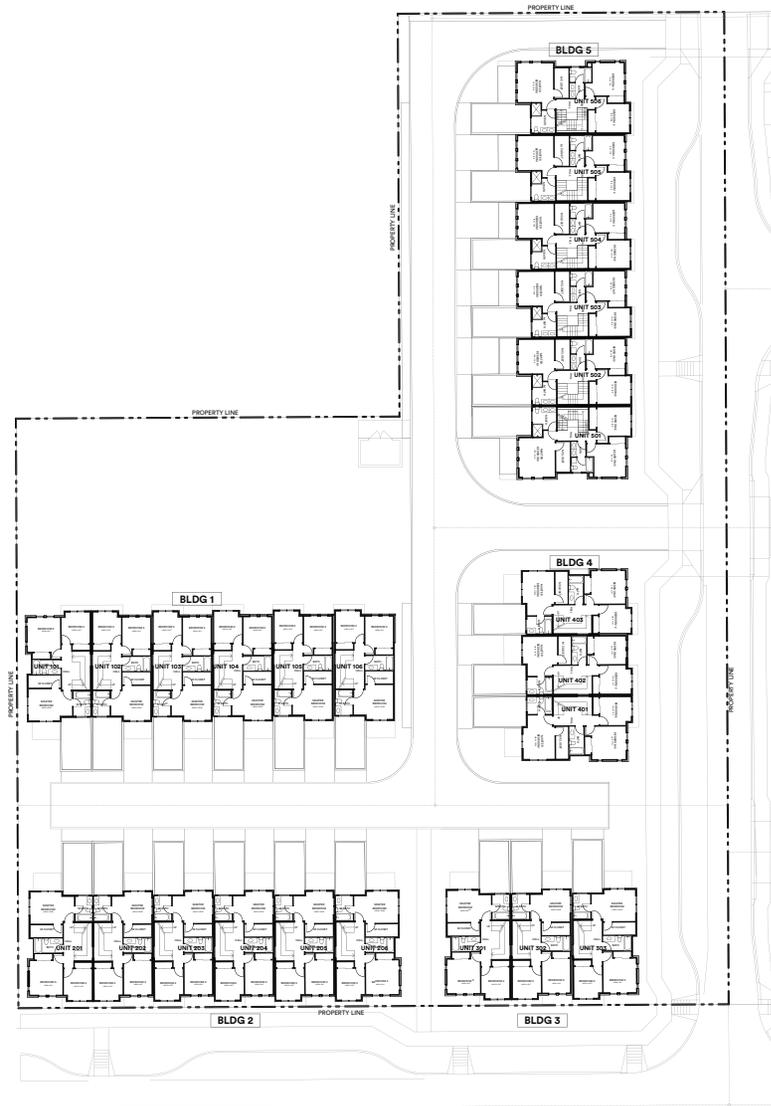
3155 BLUFF
(PERMANENTLY AFFORDABLE FLATS & TOWNHOMES)

S'PARK_west Site Plan
SopherSparrn | S'PARK_west

August 21, 2015





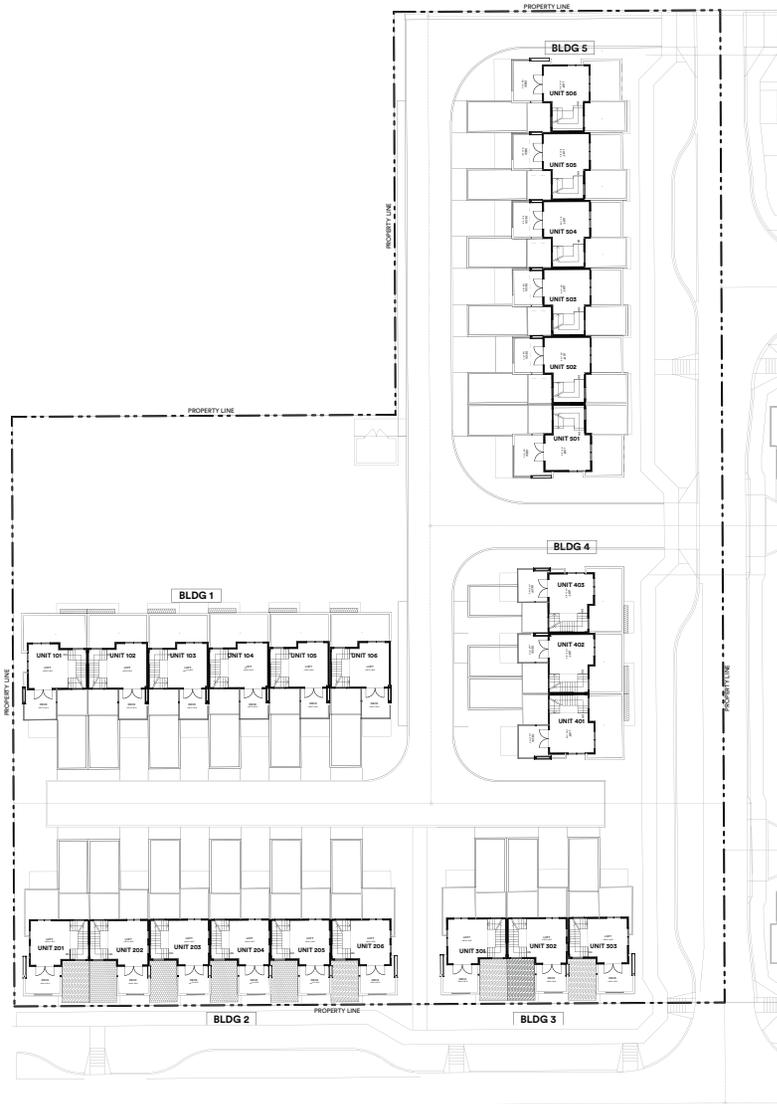


3085 Bluff Second Floor Plans

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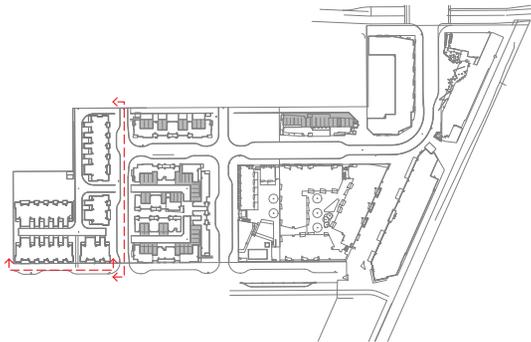




3085 Bluff Third Floor Plans
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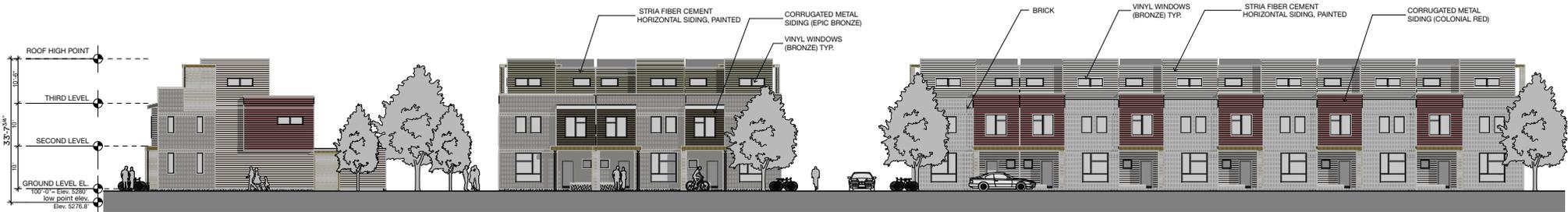




ENLARGED ELEVATION



BLUFF STREET ELEVATION



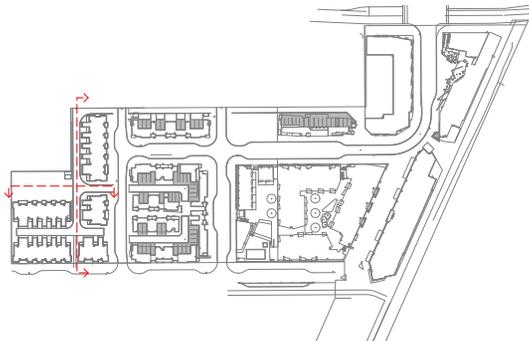
32nd STREET ELEVATION

3085 Exterior Elevations

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ENLARGED ELEVATION



GREEN SPACE ELEVATION_NORTH



PRIVATE DRIVE ELEVATION_WEST

3085 Exterior Elevations
 SopherSparn | S'PARK_west
 August 21, 2015
S'PARK



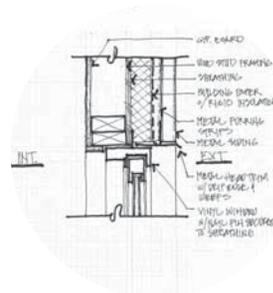
Horizontal entry detail



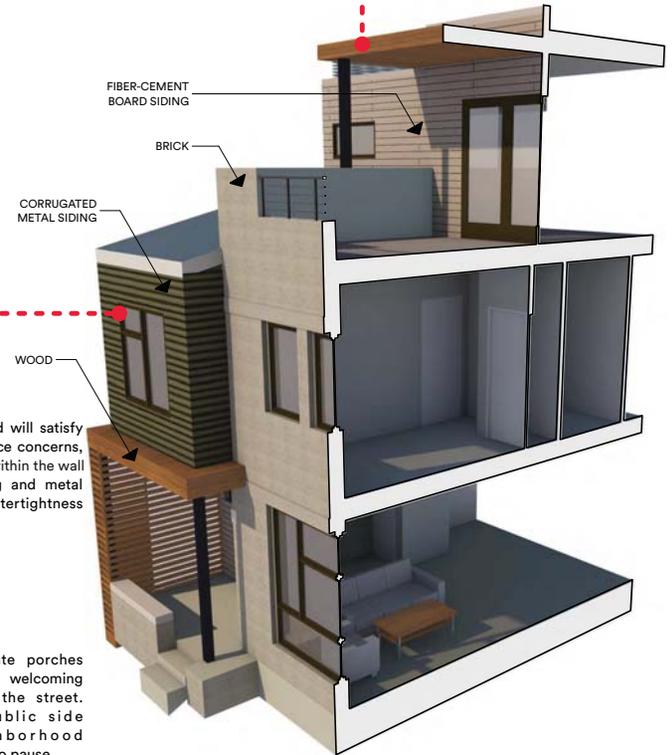
Metal railing



canopy detail



window detail



Vinyl windows selected will satisfy budget and maintenance concerns, and be detailed to set within the wall assembly. The flashing and metal trims will ensure watertightness and material capture.

Protected, semi-private porches designed to create a welcoming entry that engages the street. Stoops on the public side encourage neighborhood interaction and places to pause.





CORRUGATED METAL
COLONIAL RED



SIDING
CLAY BEIGE



BRICK
LIGHT PEWTER

BUILDING 3



CORRUGATED METAL
EPIC BRONZE



SIDING
SUSSEX GREEN



BRICK
LIGHT PEWTER

BUILDING 4



CORRUGATED METAL
WEATHERED COPPER



SIDING
SHENANDOAH TAUPE



BRICK
ALASKAN

BUILDING 2



CORRUGATED METAL
COLONIAL RED



SIDING
VINTAGE TAUPE



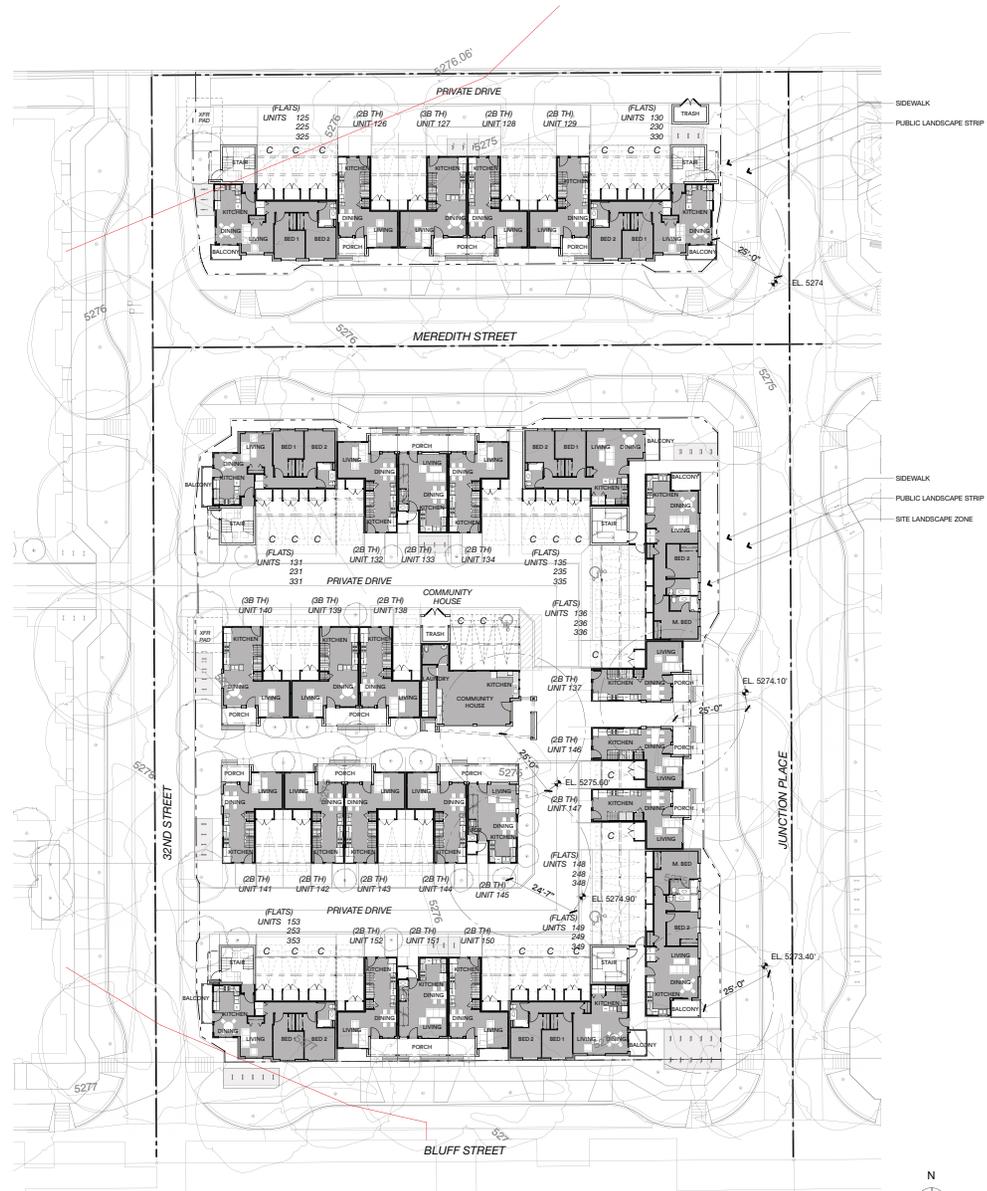
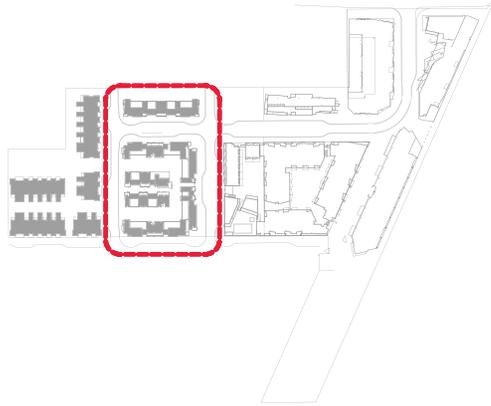
BRICK
BLACK DIAMOND

BUILDING 1 & 5

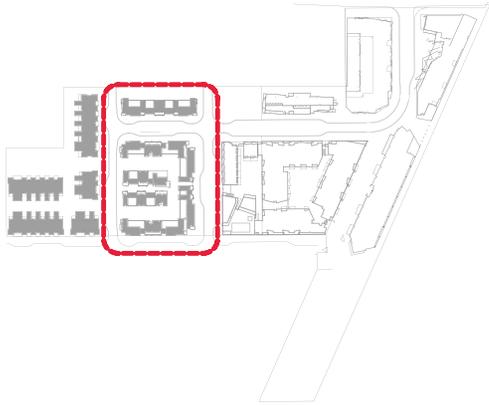


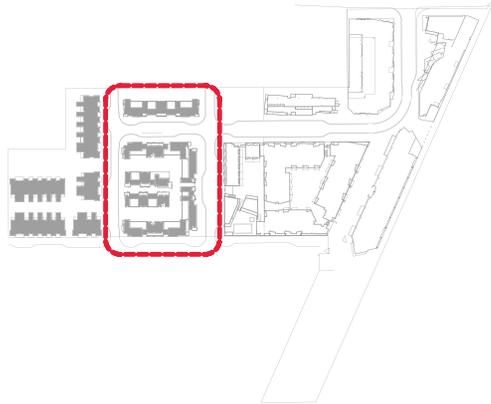
VINYL WINDOWS
BRONZE

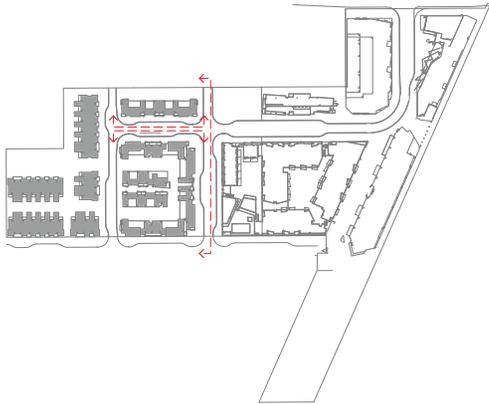
ALL BUILDINGS



N
 0 5 10 20 40'
 3155 Bluff Ground Floor Plans
 SopherSparrn | S'PARK_{west}
 August 21, 2015 **S'PARK**







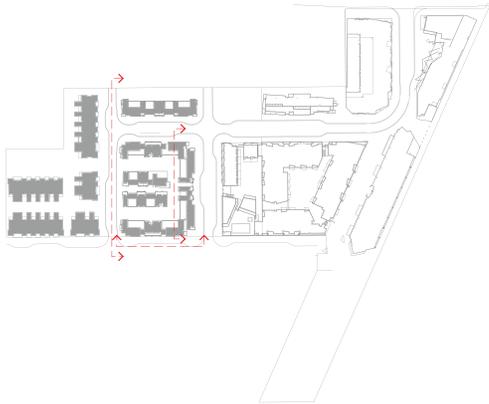
MEREDITH STREET - NORTH ELEVATION



MEREDITH STREET - SOUTH ELEVATION



JUNCTION PLACE - WEST ELEVATION



BLUFF STREET - NORTH ELEVATION



BUILDINGS 7 & 10 - PRIVATE DRIVE ELEVATION



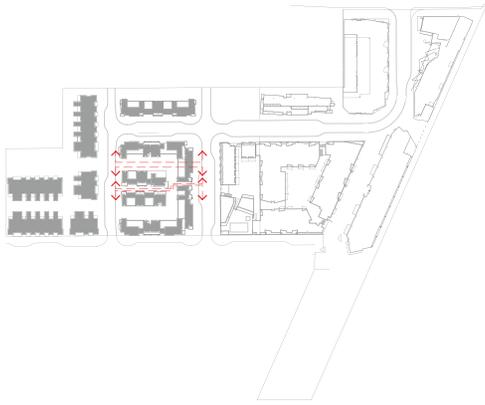
32nd STREET - EAST ELEVATION

0 5' 10' 20' 3155 Exterior Elevations

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August 21, 2015

S'PARK



BUILDING 7 - ELEVATION AT PRIVATE DRIVE



BUILDING 8 - ELEVATION AT PRIVATE DRIVE



SOUTH ELEVATION AT CENTRAL GREEN

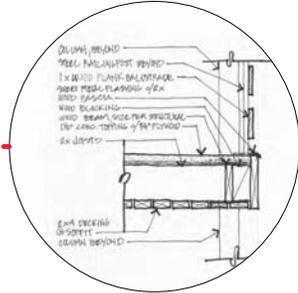


SOUTH ELEVATION AT CENTRAL GREEN



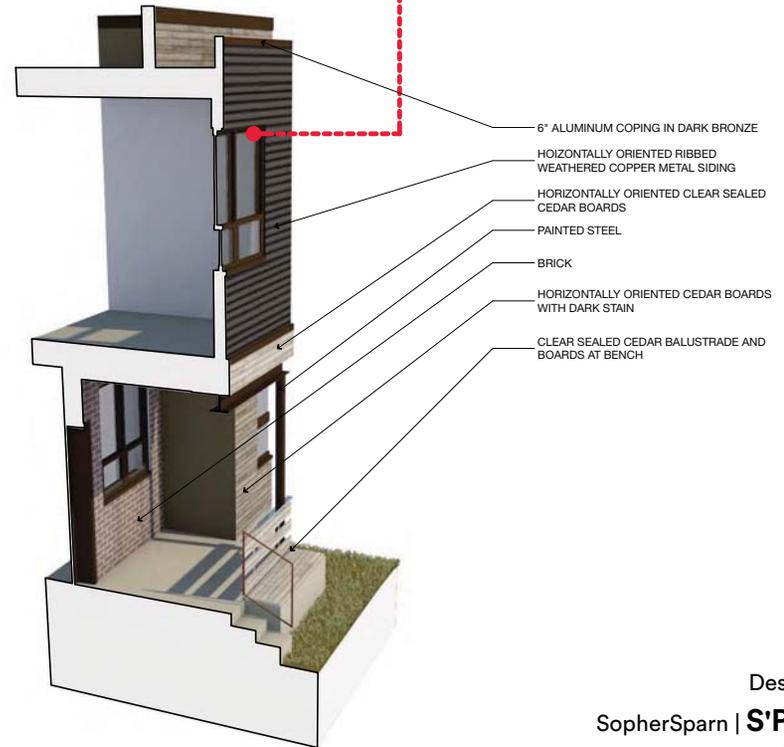
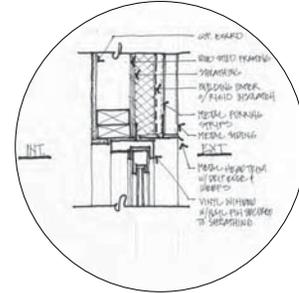


PORCHES ARE TREATED AS PRIVATE SPACES BUT ALLOW FOR ACTIVATION OF STREET PRESENCE



MATERIALS RETURN, OR ARE TRIMMED, IN A WAY TO ALLOW FOR CLEAN DETAILING OF TRANSITIONS AND TERMINATIONS

MATERIALS AND FINISHES WERE SELECTED FOR QUALITY OF APPEARANCE AND DURABILITY

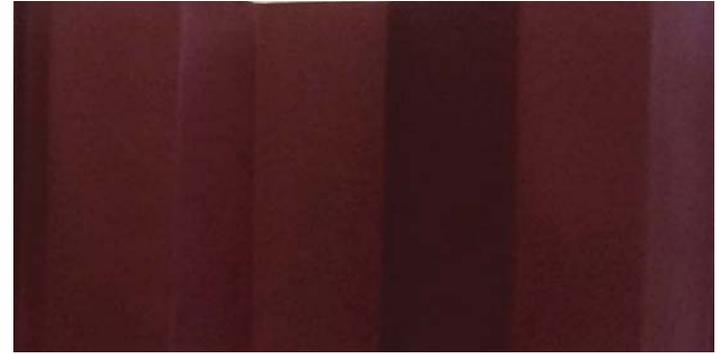




MT-3 METAL ROOFING - 7/8"D CORRUGATED
COLOR: CHAMPAGNE
*ALL COVERED PARKING



MT-2 METAL SIDING - 7.2 RIBBED
COLOR: WEATHERED COPPER



MT-1 METAL SIDING - 7.2 RIBBED
COLOR: COLONIAL RED



WOOD SLAT & STEEL RAILING



WD-2 WOOD PLANK
COLOR: DARK KNOTTY CEDAR



CEDAR PLANK RAINSCREEN



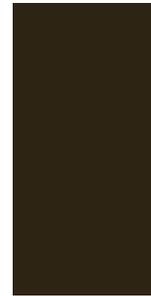
WD-1 WOOD PLANK
COLOR: CLEAR SEALED CEDAR



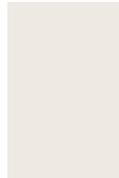
BR-2 BRICK
COLOR: ANDIRON



PAINT PT-1A
COLOR: IVY LEAGUE



ALL WINDOWS
VINYL, COLOR: BRONZE



PAINT PT-1B
COLOR: VINTAGE TAUPE



BR-1 BRICK
COLOR: PEWTER



TYPICAL TOWNHOUSE ELEVATION



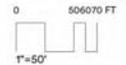
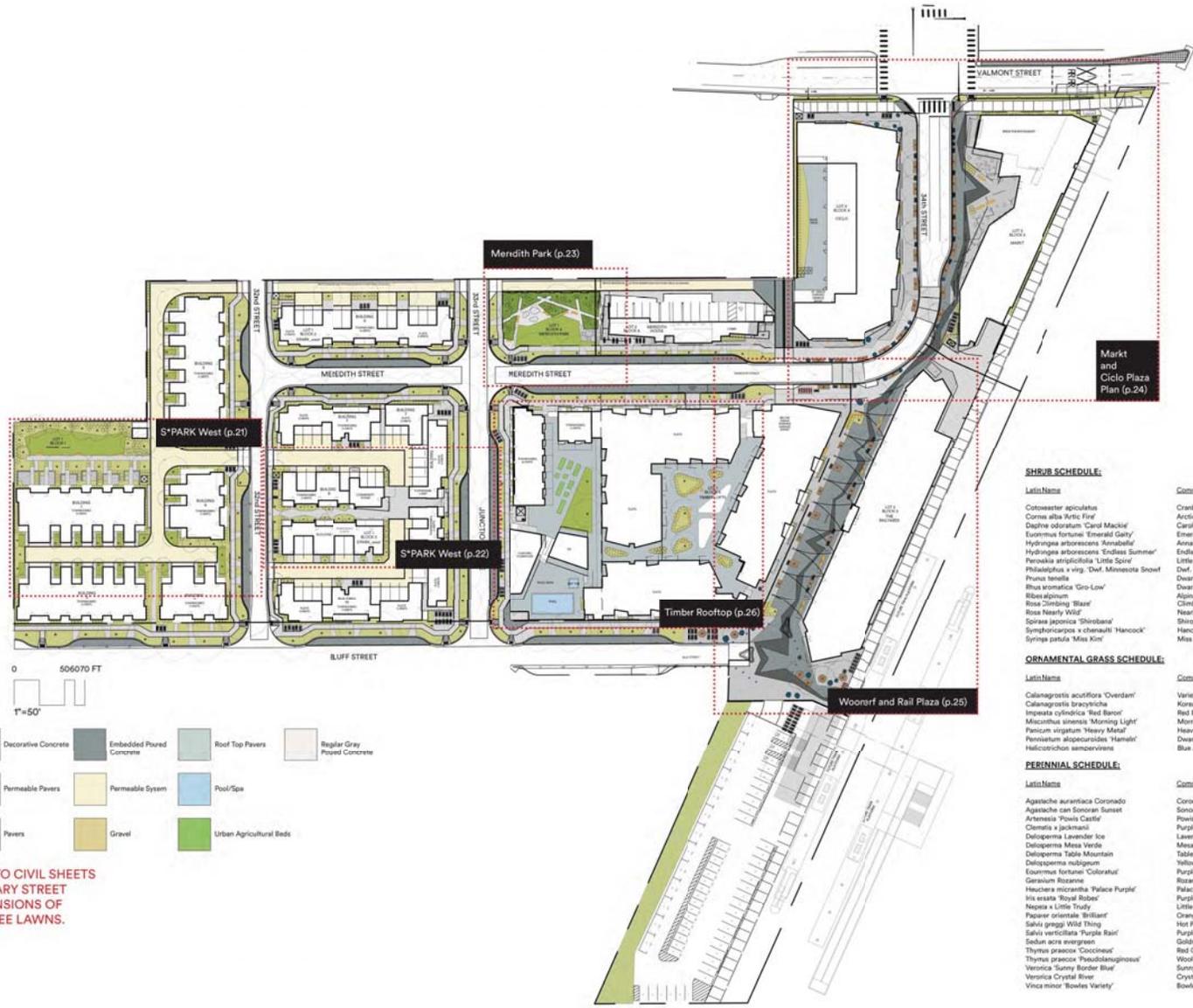
TYPICAL FLAT ELEVATION

3155 Material Palette

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S'PARK



- Rivelle Turf
- Decorative Concrete
- Embedded Poured Concrete
- Roof Top Pavers
- Regular Gray Poured Concrete
- Mixed Low Planting
- Permeable Pavers
- Permeable System
- Pool/Spa
- Shade Garden
- Pavers
- Gravel
- Urban Agricultural Beds

NOTE: PLEASE REFER TO CIVIL SHEETS C4.8 - C4.9 "PRELIMINARY STREET SECTIONS" FOR DIMENSIONS OF PLANTING STRIPS / TREE LAWNS.

SHRUB SCHEDULE:

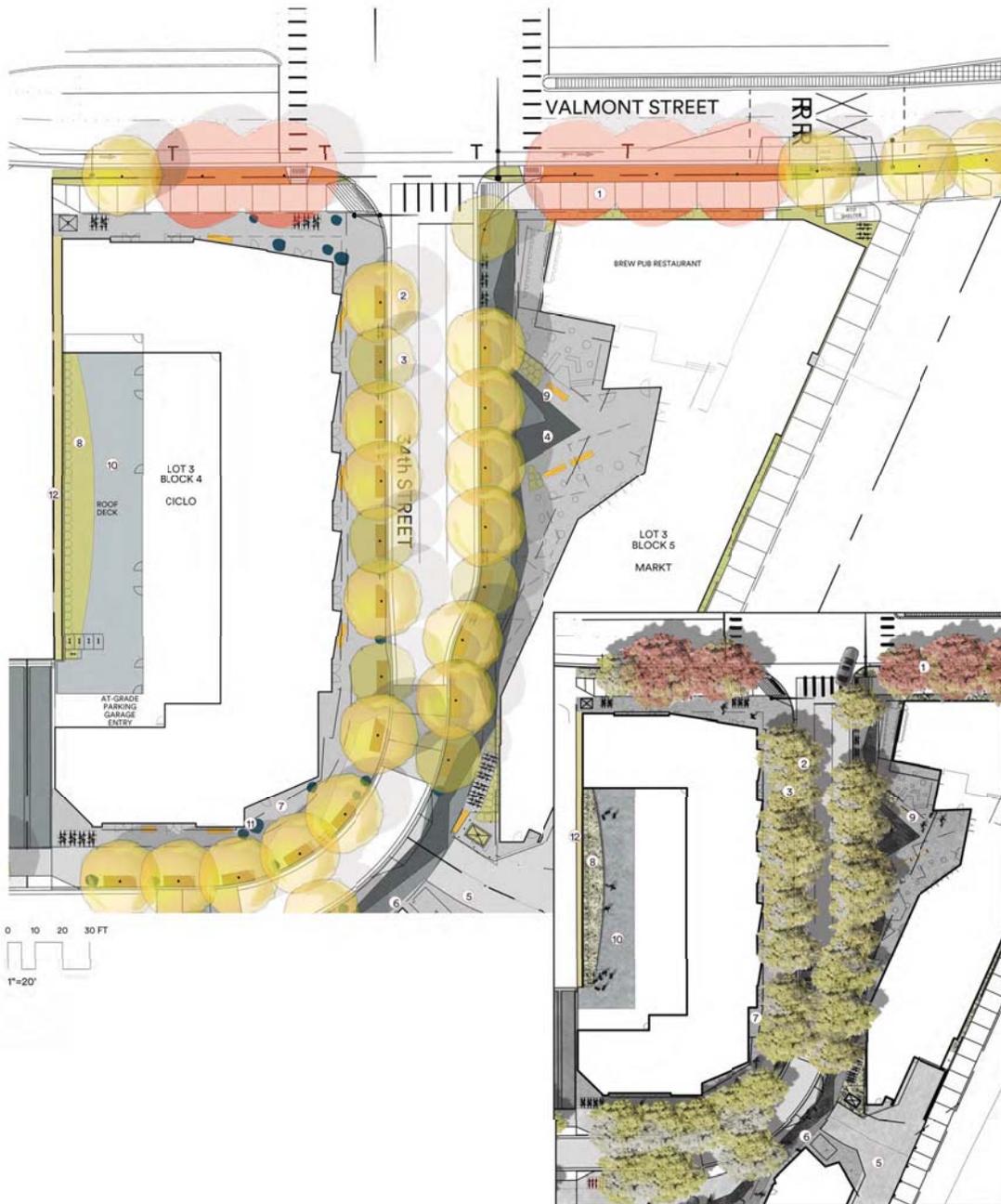
Latin Name	Common Name	Ht.	Spread	Water use	Qty.
<i>Cotoneaster stipulatus</i>	Cranberry Cotoneaster	18'-24"	4'-6"	Medium	TBD
<i>Cornus alba Arctic Fire</i>	Arctic Fire Dogwood	4'-6"	4'-6"	Medium	TBD
<i>Daphne odorum 'Carol Mackii'</i>	Carol Mackii Daphne	3'-5"	3'-5"	Medium	TBD
<i>Eucornis fortunei 'Emerald Gully'</i>	Emerald Gully Euonymus	18'-24"	3'-4"	Medium	TBD
<i>Hydrangea arborescens 'Annabelle'</i>	Annabelle Hydrangea	3'-4"	3'-4"	Medium	TBD
<i>Hydrangea arborescens 'Endless Summer'</i>	Endless Summer Hydrangea	3'-5"	3'-5"	Medium	TBD
<i>Penstemon angustifolius 'Little Spire'</i>	Little Spire Penstemon	2'-3"	2'-3"	Low	TBD
<i>Philadelphus v. vireg. 'Dwf. Minn. Snowflake'</i>	Dwf. Minn. Snowflake Mockorange	3'-4"	2'-3"	Medium	TBD
<i>Prunus tenella</i>	Dwarf Prunella Almond	3'-5"	3'-4"	Low	TBD
<i>Rhus aromatica 'Gro-Low'</i>	Dwarf Fragrant Sumac	2'-3"	6'-6"	Low	TBD
<i>Ribes alpinum</i>	Alpine Currant	3'-6"	3'-6"	Low	TBD
<i>Rosa 'Climbing Blue'</i>	Climbing Blue Rose	10'-12'	6'-6"	Medium	TBD
<i>Rosa 'Nearly Wild'</i>	Nearly Wild Rose	2'-3"	2'-3"	Low	TBD
<i>Spiraea japonica 'Shirobana'</i>	Shirobana Spirea	2'-3"	2'-3"	Medium	TBD
<i>Symphoricarpos x chinensis 'Hancock'</i>	Hancock Corberry	2'-3"	3'-6"	Low	TBD
<i>Syringa patula 'Miss Kim'</i>	Miss Kim Dwarf Lilac	3'-5"	3'-5"	Low	TBD

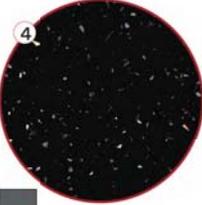
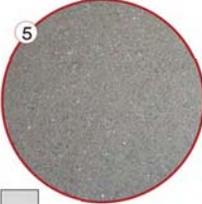
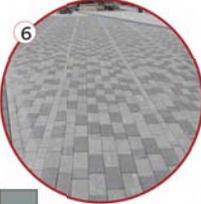
ORNAMENTAL GRASS SCHEDULE:

Latin Name	Common Name	Ht.	Spread	Water use	Qty.
<i>Calamagrostis acutiflora 'Overdam'</i>	Variiegated Feather Reed Grass	2'-3"	2'-3"	Low	TBD
<i>Calamagrostis brachytricha</i>	Korean Feather Reed Grass	3'-4"	2'-3"	Low	TBD
<i>Impatiens cylindrica 'Red Banner'</i>	Red Banner Japanese Blood Grass	12'-18"	12'-18"	Medium	TBD
<i>Miscanthus sinensis 'Morning Light'</i>	Morning Light Miscanthus	4'-5"	2'-3"	Medium	TBD
<i>Panicum virgatum 'Heavy Metal'</i>	Heavy Metal Switch Grass	3'-4"	10'-18"	Low	TBD
<i>Pennisetum alopecuroides 'Hamaki'</i>	Dwarf Fountain Grass	18'-24"	10'-18"	Low	TBD
<i>Helleborichon sempervirens</i>	Blue Aviana Grass	18'-24"	18'-24"	Low	TBD

PERENNIAL SCHEDULE:

Latin Name	Common Name	Ht.	Spread	Water use	Qty.
<i>Agastache aurantiaca 'Coronado'</i>	Coronado Hyssop	18'-18"	12'-18"	Low	TBD
<i>Agastache can. Sirocco Sunset</i>	Sirocco Sunset Hyssop	12'-18"	12'-18"	Low	TBD
<i>Artemisia 'Powis Castle'</i>	Powis Castle Sage	2'-3"	2'-3"	Low	TBD
<i>Chamaemelum nobile 'Jackmanii'</i>	Purple Chamomile	vine	vine	Medium	TBD
<i>Delosperma 'Lavender Ice'</i>	Lavender Ice Iceplant	2'-4"	12'-18"	Low	TBD
<i>Delosperma 'Mesa Verde'</i>	Mesa Verde Iceplant	2'-4"	12'-18"	Low	TBD
<i>Delosperma 'Table Mountain'</i>	Table Mountain Iceplant	2'-4"	12'-18"	Low	TBD
<i>Delosperma 'rubrum'</i>	Yellow Iceplant	2'-4"	12'-18"	Low	TBD
<i>Eoumna fortunei 'Colorado'</i>	Purpleleaf Wintercreeper	2'-18"	3'-6"	Low	TBD
<i>Gaura bicolor</i>	Roseum Gaura	12'-18"	1'-2"	Medium	TBD
<i>Hauzoria microantha 'Palace Purple'</i>	Palace Purple Coral Bells	12'-18"	12'-18"	Medium	TBD
<i>Iris essata 'Royal Robes'</i>	Purple Japanese Iris	2'-3"	12'-18"	Medium	TBD
<i>Limn. Little Trudy'</i>	Little Trudy Camellia	18'-24"	18'-24"	Low	TBD
<i>Papaver orientale 'Brilliant'</i>	Orange Oriental Poppy	2'-3"	18'-24"	Low	TBD
<i>Sakoi greggii 'Wild Thing'</i>	Hot Pink Sakoi	18'-20"	12'-18"	Low	TBD
<i>Salvia verticillata 'Purple Rain'</i>	Purple Rain Salvia	18'-20"	18'-24"	Medium	TBD
<i>Sedum acre evergreen</i>	Goldmoss Utah Sedum	2'-4"	12'-18"	Low	TBD
<i>Thymus praenox 'Coccineus'</i>	Red Creeping Thyme	3'-4"	12'-18"	Low	TBD
<i>Thymus praenox 'Pseudobalaustrinus'</i>	Woolly Thyme	1'-2"	12'-18"	Low	TBD
<i>Veronica 'Sunny Border Blue'</i>	Sunny Border Blue Veronica	2'-3"	12'-18"	Low	TBD
<i>Veronica Crystal Blue</i>	Crystal River Speedwell	18'-20"	12'-18"	Low	TBD
<i>Vinca minor 'Bowles Variety'</i>	Bowles Periwinkle	4'-6"	12'-18"	Low	TBD

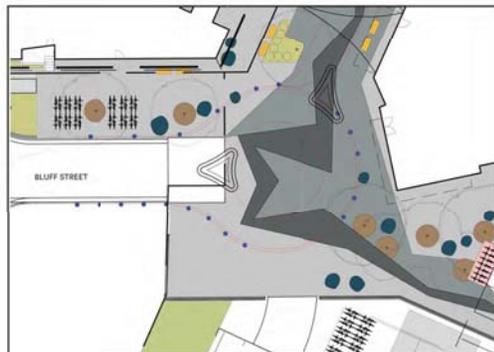


- 1  Bloodgood London Plane Tree
- 2  Skyline Honeylocust
- 3  Autumn Gold Madenhair Tree
- 4  Embedded Decorative Concrete
- 5  Decorative Concrete
- 6  Permeable Pavers
- 7  Non-Permeable Pavers
- 8  Mixed Low Planting
- 9  Reclaimed Wood Benches
- 10  Rooftop Pavers
- 11  Amorphic Seating
- 12  Gravel

Landscape: Markt and Ciclo Plaza Plan

August 21, 2015

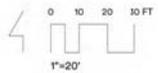




Turning Radius at the Rail Flaza

Functional Dimensions of Turning Area





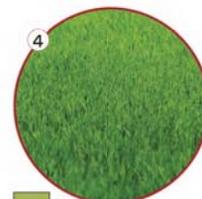
1 Autumn Blaze Pear



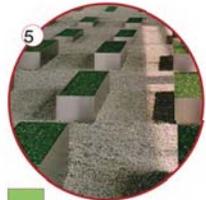
2 Rooftop Pavers



3 Mixed Low Planting



4 Reville Turf



5 Urban Agricultural Beds



6 Bermed Turf



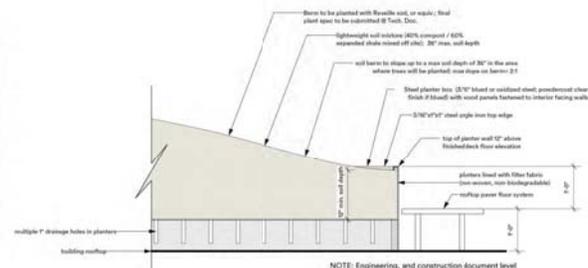
7 Raised Turf



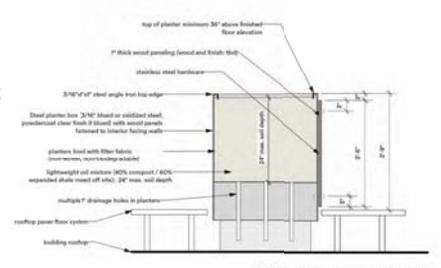
8 Pool



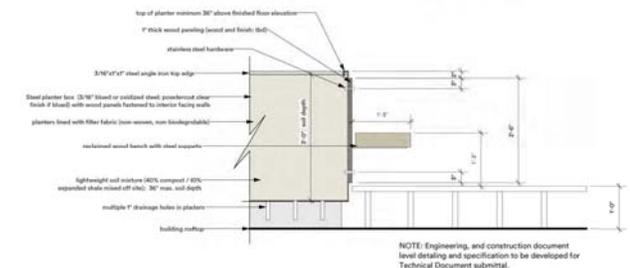
9 Reclaimed Wood Benches



1 DETAIL: Rooftop Bermed "Lawn" Planter Section Scale: 3/4" = 1'-0"



2 DETAIL: Rooftop Urban Agricultural Planter Section Scale: 3/4" = 1'-0"

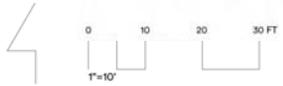


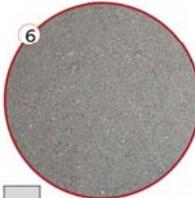
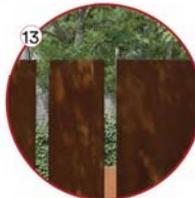
3 DETAIL: Rooftop Raised Planter w/ Seating Section Scale: 3/4" = 1'-0"

Landscape: Timber Rooftop

August 21, 2015



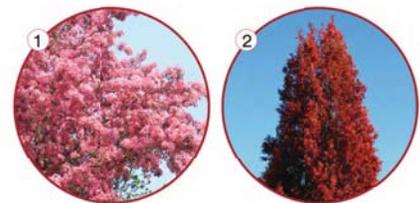
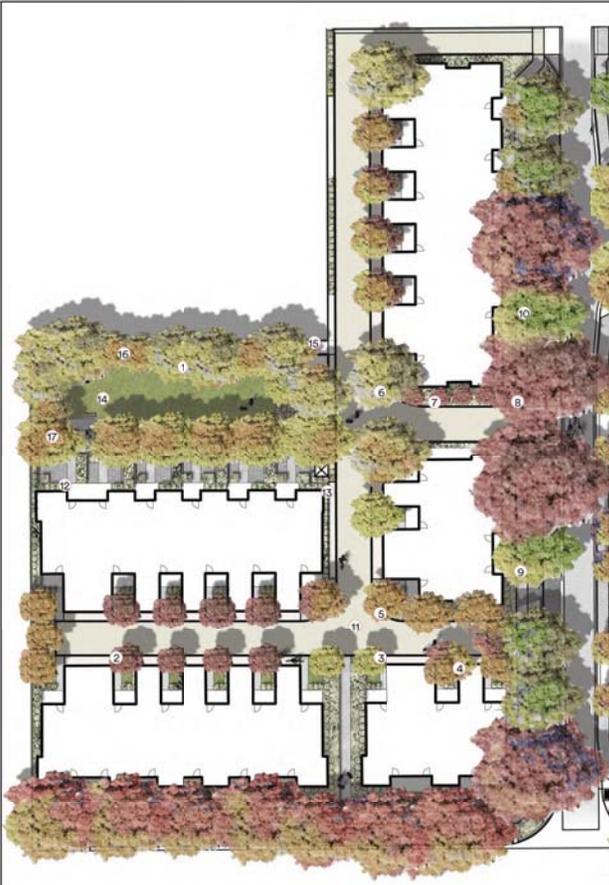


 European Larch	 Eastern Redbud	 Littleleaf Linden	 Hackberry
 Yellow Buckeye	 Decorative Concrete	 Permeable Pavers	 Non-Permeable Pavers
 Breez Crusher-bines	 Reclaimed Wood Benches	 Vinca minor and Mixed Shrub Planting	 Climbing Boulders
 Corren Steel Panel Fence	 Reveille Turf	 Mixed Low Planting	 Northern Red Oak

Landscape: Meredith Park Plan

August 21, 2015





1 Radiant Crapeple



2 Fastigate English Oak



3 Japanese Tree Lilac



4 Eastern Redbud



5 Chanticleer Pear



6 Skyline Honeylocust



7 Columnar Purple Plum



8 Northern Red Oak



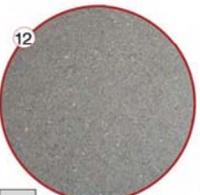
9 European Hornbeam



10 Littleleaf Linden



11 Permeable Paving System



12 Decorative Concrete



13 Mixed Low Planting



14 Reveille Turf



15 Wood Fence



16 Montmorency Cherry

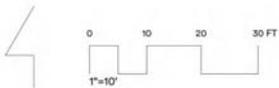
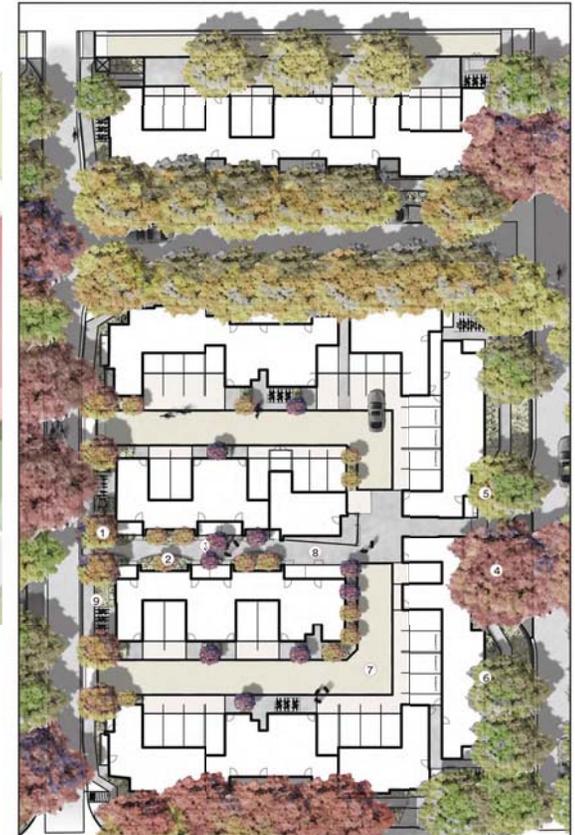
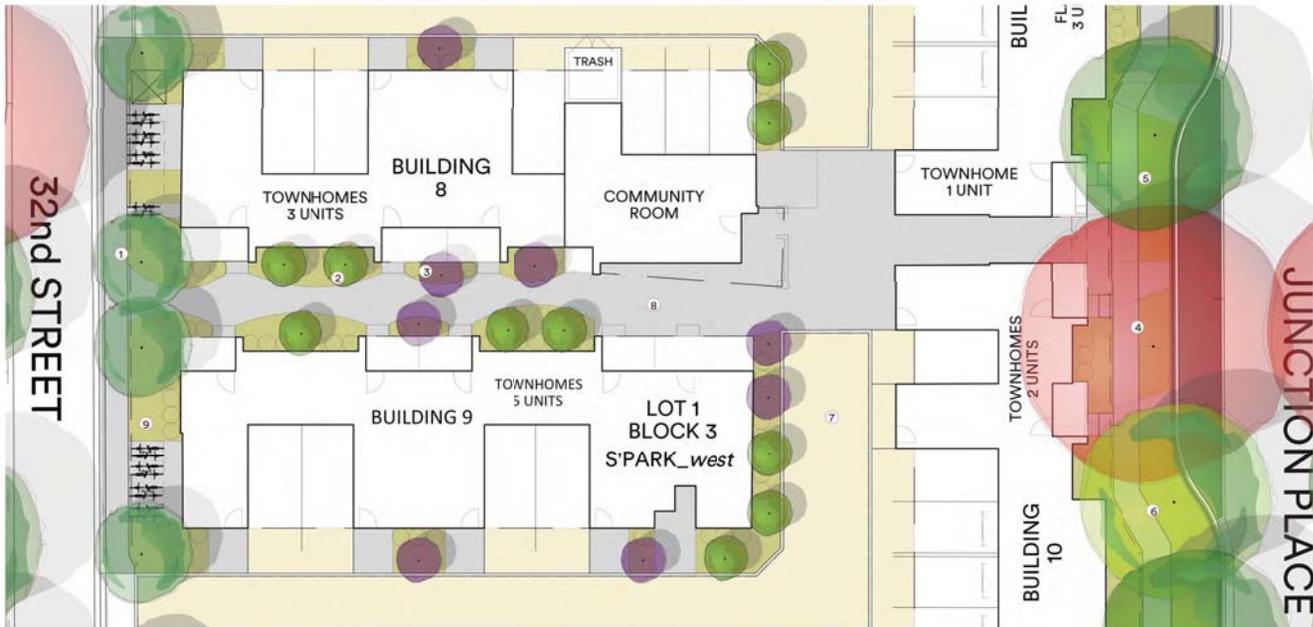


17 Spring Snow Crabapple

S'park west Landscape: 3085

August 21, 2015





1 Chanticleer Pear



2 Red Barron Crabapple



3 Columnar Purple Plum



4 Northern Red Oak



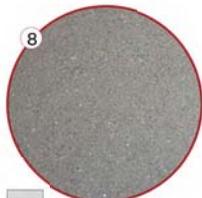
5 European Hornbeam



6 Littleleaf Linden



7 Permeable Paving System



8 Decorative Concrete

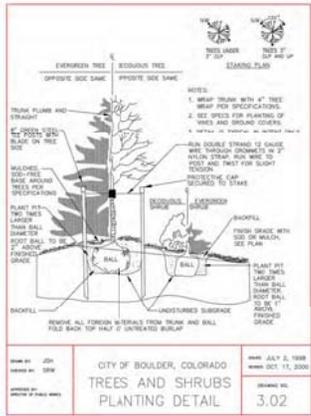


9 Mixed Low Planting

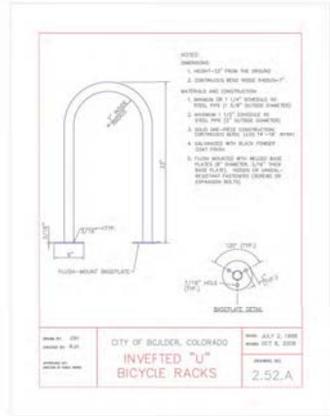
S'park west Landscape: 3155

August 21, 2015

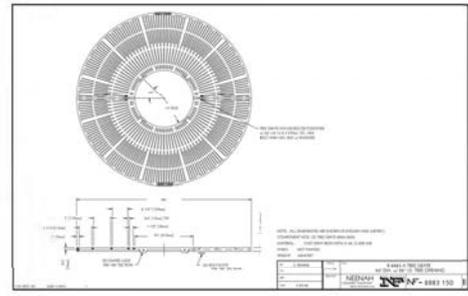
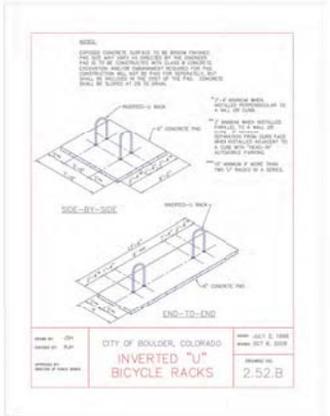




1. TREE PLANTING / STAKING



2. PUBLIC BIKE RACK



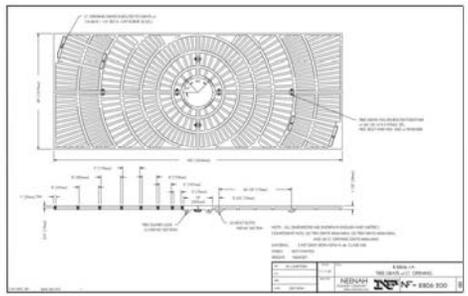
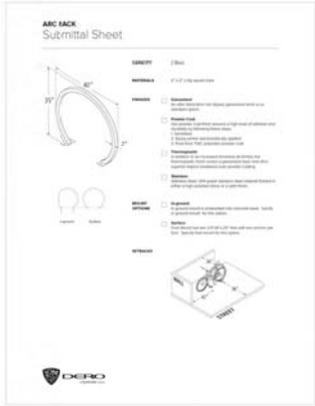
7. TREE GRATE - 8' Round
(located within Rail Plaza area)



10. BOLLARD
(Precedent Image)



3. PRIVATE BIKE RACK



8. TREE GRATE - 4'x12' Rectangular
(4'x8' rectangular grates to be used in Woonerf)



11. PERMEABLE / NON-PERMEABLE PAVERS
(Pavestone - Eco City Lock narrow modular pavers)



4. WOOD FENCING
(Precedent Image: S'PARK West)



5. STEEL PANEL FENCING
(Precedent Image: Meredith Park)



6. RECLAIMED WOOD BENCH
(Precedent Image)



9. STONE BENCH / TRAFFIC BOLLARD
(Precedent Image)



12. TREE GUARDS
(Precedent Image)

Landscape: Site Landscape Details

General, TVAP, Mass/Scale, Walkability, Density

Comment

1. Project plan is appropriate for Transit Oriented Development and provided excellent community benefit with affordable housing component and LEED Platinum status.

2. Appreciation for effort to create a "much loved place", TVAP consistency, authenticity, appropriate height-mass-scale in this location but recommendation to break up buildings and not look too monolithic.

3. Relate to Valmont and don't let it "become the back door of the project".

4. Appreciation for the office use along the train tracks to buffer noise and impacts, but consider adding residential on western side of woonerf to put "residents' feet on the ground" as well as bonafide townhomes on east side of Junction Place.

Response

The applicant has enhanced all of these aspects since Concept Plan with:

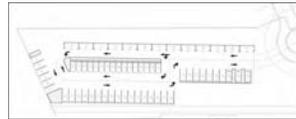
- Voluntary participation in Boulder Junction Access District and Travel Demand Management to leverage alt-modes subsidy as well as EcoPasses for all project occupants (commercial and residential) and is participating financially in efforts to create a "Quiet Zone" for the train activity
- New additional affordable housing planned at Valmont and 34th Street (additional 32 units bringing project total to 77 units) in place of the previously-planned hotel
- Project has registered with the U.S. Green Building Council and will achieve LEED Platinum status

The updated Site Review plan is much smaller in mass, scale, scope:

- Ciclo building replaces a 96,000sf, 5-story hotel at Valmont and 34th with (a) a 3-story mixed use building for non-profit space and 32 affordable housing units and (b) a small, boutique-style 15-unit condo flats building on Meredith Street (Meredith House)
- Eliminates the "Flex Building" at Junction Place/ Meredith Street and in its place provides a new pocket park named Meredith Park
- Breaks-up the main apartment building (Timber) from a previously 173,000sf single building with 255 units to now five (5) smaller buildings with 25, 42 and 56 units each, plus 8 townhome units and live-work units.
- Reduces the 34th/Valmont commercial building (Markt) from 4-stories and 77,000sf to a 2 to 3-story (plus mezzanine) 55,000sf building with an innovative roofline and abundant outdoor spaces
- Reduces central Railyard commercial building from an almost 80,000sf building to a 70,000sf building and no longer proposes a "cantilever" over the multi-use path to the east
- Entire southern portion of the site has been set aside to respond to community feedback, uses and needs that emerge and – to allow the future of Boulder Junction to grow more organically

Architecture now more appropriately addresses the activity of Valmont with "gateway scale", presents an innovative design welcoming visitors to S'PARK and creates a node of activity with the brewpub, micro-restaurants, and sun plaza of Markt, as well as tasteful landscape architecture and opportunities for public art.

The program now features several residential "live-work" units on the woonerf which are bookended by retail suites on the corners to attract pedestrian activity and the Junction Place/Meredith corner now contains eight (8) bonafide townhomes (consistent with RH-6 zoning) to accommodate families.



Comment

5. Appreciation for the plan "embracing" the train platform despite no train service anytime soon.

6. Consider parallel parking along woonerf.

7. Consider revisiting apartment courtyard view corridors

8. Appreciation for family-orientation

9. Appreciation for human scale.

Site Plan and Connections

10. Include many access points to the units to create a rich streetscape.

Response

The project continues to honor the location of the future potential train platform in TVAP/TMP, provides an easy turnaround at the Bluff terminus that benefits the project today for traffic calming as well as in the future if the train comes into service. Also continues to provide primarily pedestrian areas for people to gather from the platform area all the way to Valmont. And the Rail Plaza and underpass proposed in TVAP are able to be accommodated.

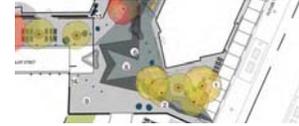
The woonerf is 55' wide with limited parallel parking for eight (8) cars to access street-level retail for short trips.

The architecture for the apartments has been developed specifically with these view sightlines, permeability and connectivity in mind for residents and visitors. There are multiple ways to connect from one site to another other than on traditional sidewalks.

Program has developed further to include substantial amenities for families such as additional storage, expansion and mindful programming of greenspace, inclusion of private yards and common areas at S'PARK_west and the addition of the park at Junction Place/Meredith Street.

Project has enhanced this with reductions in height, mindful harmonic dimensions in site and connection planning relative to adjacent building heights and additional care to pedestrian experience with planting beds, raised porches, and rhythm of commercial and residential uses at grade to maintain a lively and engaging place

- Timber creates a rhythm around all 4-sides of the building for commercial and residential pedestrian activity day and night, as well as visual interest.
- All of S'PARK and S'PARK_west are designed with multiple points of entry in each block
- Every ground floor unit in the entire project has a front porch entry and will help give Boulder Junction an indelibly "family neighborhood" feeling



Site Plan and Connections (cont.)

Comment

11. Create a community space for the apartment units.

Response

Timber will have both an active common amenity deck with a small pool, a co-working space for those who office at home, a workout facility, a gardening area, and great views, as well as a more passive area with plantings, seating, and contemplative areas – and both connected through an architecturally unique breezeway and access to both Junction Place and the woonerf.



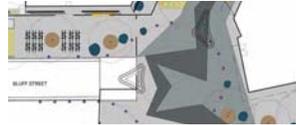
12. Woonerf should be as car unfriendly while still supporting businesses, break up parallel parking with landscape elements, and separate vehicle and pedestrian traffic along woonerf

The design team has made great strides to program this area for pedestrian safety, a lively area for programmed events or spontaneous play, all while maintaining limited auto access and nominal parking to support the retail and businesses there. The auto will be a "guest" in this area (when even open to vehicular traffic) and the area will often be restricted to pedestrian access only to accommodate a safe place for regular cultural programming and events.



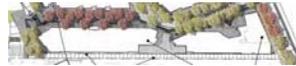
13. Previous "teardrop" shaped turnaround wastes space and may not be necessary

The design team has significantly shrunk the size of the turnaround from a 72' DIA. x 120'L teardrop configuration to a 60' DIA roundabout, while utilizing the woonerf on S'PARK Place for emergency vehicle access which creates a calmed traffic condition and more pedestrian friendly place



14. Appreciation for bike path along tracks and important to connect people but not convinced of building overhead

This feature has been streamlined and the 14' multi-use path will connect users unimpeded from Goose Creek to Valmont and also invites cyclists and pedestrians into the project in a fun and safe manner.



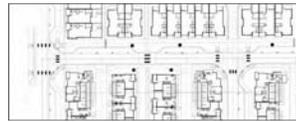
15. Office space would be better served as residential

Approximately 10,000sf of previous office space has been re-appropriated to residential units along S'PARK Place and Bluff Street.65



16. Consider traffic calming strategies as much as possible, maintain narrow streets and neck-downs at intersections

The design team has strongly maintained the neck-downs, has reduced street width on 32nd to 17' (not including curbs, sidewalks and parking), and added raised crosswalk sections on three (3) street locations – Meredith connection Timber and Ciclo, Junction Place connecting Timber and S'PARK_west and running east-west in the middle of 32nd Street.



17. Consider active uses on ground level to attract people

The project has developed the "node" of activity at the Markt building with the brewpub and micro-restaurants, used architecture at the Railway building through a promenade and locating the office entrance towards the center of the building, and added retail at the corners of Timber – all to create intersection points and promote positive pedestrian "collisions".



18. Choreograph garage entry and exit points on the site and maintain as much underground parking as possible

The underground parking garage improves the ground level experience by reducing the visual impact of surface parked cars. The strategic garage also has the benefit of quickly converting auto-traffic to pedestrian activity. The pedestrian exit points encourage street interaction and access to S'PARK Place and Markt.

Comment

19. Appreciation of connection of a spine of greenway and pedestrian connectivity across the SPARK_west project to Junction Place

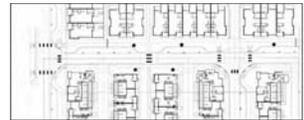
Response

The project was able to enhance the greenspace in S'PARK_west for play and enjoyment while still maintaining a continuous greenspace connection longitudinally through the project. S'PARK_west residents seeking to access S'PARK Place or the multi-use path to the east, will have the equally enjoyable choices of whether to go around the block on the widened sidewalks along the shops and stoops or to take the scenic route through Timber and its garden areas.



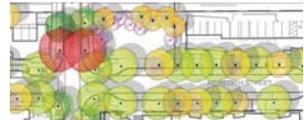
20. Provide leakages and through the buildings to promote permeability

The design team has added this where appropriate – from the community center located within S'PARK_west to its additional residential units north of Meredith, across and through Timber from Junction Place to S'PARK Place, and from Meredith Street to the amenity deck. Each building site now has multiple points of entry for pedestrian convenience and to break-up building masses.



21. Consider a terminus at the north end of 32nd

The design team has responded by making efforts to discourage speed and a "through-way" for vehicles within S'PARK_west with raised walks, signage and wayfinding, and pavement materials



22. Make the alleys and stub-in streets spaces for children and families, not cars

The design team has responded by making efforts to discourage speed and a "through-way" for vehicles within SPARK_west with raised walks, signage and wayfinding, and pavement materials.



Architecture

22. Important to not be able to distinguish affordable from market rate units

The project team is using authentic materials throughout S'PARK, has cut no "corners" anywhere especially for affordable housing and, yet, sees the affordable housing opportunities as some of the project's most valuable attributes to create a culturally and ideologically diverse place that this community values.



23. A variety of architectural styles and elements will make the project appear as if it were built by different people at different times

With a roster of three distinct and very different architecture firms – almost a dozen different architects working on various aspects – and an additional 12 engineers and consultants, S'PARK is achieving unprecedented design diversity in Boulder to make "creative chaos" and a lively and engaging place.



24. Assure that the front porches are large enough to accommodate people

Within S'PARK_west, the vast majority of residences have both oversized front porches and private elevated deck spaces. Wood has been added to every front porch to provide a softer and more human scaled material to these spaces



25. Provide some vertical separation at ground level from residential units and street

The design team has ensured that all front porch spaces have vertical separations that simultaneously create a barrier for defensible space, but also have some transparency to engender interaction

26. Consider natural materials when possible

Natural materials have been used as much as possible in conjunction with an aggressive landscape architecture program. The emphasis has been brick, wood, and steel.

Architecture (cont.)

Comment

27. Avoid concrete slab porches

Response

While porches are still concrete, the design team has added horizontal wood elements to soften and humanize these spaces. In addition, porches along the greenspaces have integrated benches on the outside of the porch to promote neighborly interaction and activity.



28. Avoid all 55' rectangular buildings

This has been specifically avoided and the project has gone to great lengths to adhere to this comment as well as many comments in the community about design excellence. The focus has been on great buildings that first serve a noble purpose of performing for the needs of the occupants and community, then appropriately serve the pedestrian experience in a TOD place and finally feature architecturally interesting statements and gestures that help make Boulder Junction an inspiring, fun and pleasant place.



Summary

29. Pay attention to the neighbors across Valmont

The project has worked carefully with all available stakeholders to learn how to make SPARK the best place it can be. Practically speaking, the project has dramatically reduced its visual and operational impacts – real or perceived – in direct response to several neighbor comments and requests. This includes replacing the previously-proposed 5-story hotel with the modest 3-story Ciclo and “chiseling” the Markt building down to 2-stories at the corner of 34th/Valmont – where the views may matter the most to neighbors



30. If there is public benefit, the board would support the proposed mass and scale

SPARK's priority from inception has been to provide leadership in the Boulder community by demonstrating that a socially-responsible project is possible and that new development can serve people and their needs and aspirations. SPARK will be a monument to environmental and economic sustainability and how the two can and must work hand-in-hand.

- Enumerated “direct benefits” are SPARK's:
- dedication and construction of over two (2) acres rights-of-way for the public for both autos and pedestrians,
 - financial participation in the creation of a Quiet Zone,
 - “self-taxing” with participation in the Boulder Junction Access District to minimize traffic and carbon impacts,
 - commitment as the state's 1st LEED-ND Platinum project (2nd in U.S.), and
 - commitment to providing all required affordable housing on-site plus an additional voluntary amount resulting in 32% permanently affordable housing (covenants already recorded)
 - example that development can not only “pay its way” but provide leadership in helping solve community challenges



Written Statement

August 21, 2015

SPARK

Response to TVAP Guidelines

VISION

TVAP Criteria

1. A Neighborhood and a Destination...a lively, mixed-use, pedestrian-oriented place where people will live, work, shop and access regional transit...A place that attracts a broad spectrum of the community...A place that emphasizes and provides for alternative energy, sustainability, walking, biking.

2. Urban Character...low-density, automobile-oriented environment will gradually transform into a higher-density, more urban environment. Most new buildings will range in height from two to four stories, and many will have a mixture of different uses. Much of the new parking will be in structures, underground or tucked under the first floor of buildings. This will create a more attractive streetscape and pleasant pedestrian environment.

3. Alternative Transportation...To reduce the traffic impacts of higher-density development and capitalize on the new transit services... program incentives and managed, paid parking will encourage area residents, employees and shoppers to choose transit, walking, bicycling, ride-sharing and telecommuting over driving.

4. Diverse Housing...New development in the area is expected to be predominately residential, both as standalone residential development and as mixed-use development. New housing will provide an opportunity for workers who currently commute into Boulder to live in Boulder...and creating a new neighborhood.

5. New Retail - Neighborhood-serving retailers will tend to be in more interior, but also highly visible locations, and will be interwoven with new housing and offices.

Response

S'PARK has worked to create a place that emphasizes and enhances this ambitious vision for an incredibly well-loved, pedestrian and transit-oriented place, that is economically, culturally, and socially diverse, with cutting edge sustainability.

The project has worked carefully to balance appropriate mass, scale and density where appropriate and with purpose and seized opportunities to create real and meaningful community benefits wherever possible.

S'PARK has emphasized the pedestrian experience with buildings remarkably smaller than other new buildings in the Boulder Junction area, much smaller than originally proposed in Concept Review and inline with TVAP's mandate to hide parking and create a vibrant streetscape.

The project's participation in the Boulder Junction Access District will ensure that this public-private-partnership will actively reduce impacts associated with new mixed-use development, promote alt-modes of transportation to project users, and help the project operate in a manner consistent with the community's vision.

S'PARK features both "standalone residential development" and "mixed-use development" – both with market-rate affordable options through smaller unit sizes and through partnerships to create permanently affordable housing (31.2% of the entire housing inventory will be permanently affordable at S'PARK). This will lead to an economically and culturally diverse place and an opportunity for current in-commuters to finally live in Boulder and enjoy the benefits of highly energy-efficient homes and utilize our community's investment in active (RTD) and passive (multi-use path) transit throughout Boulder and the Front Range.

The project will complement the Twenty Ninth Street commercial areas by providing the neighborhood with more locally-focused and affordable retail options, such as micro-restaurants, a brewpub, commercial services, and boutique-style shops. All of these will be enjoyed in a more authentic and neighborhood setting.

Written Statement

August 21, 2015

S'PARK

Response to TVAP Guidelines
GOALS AND OBJECTIVES

‘Create a well-used and well-loved, pedestrian-oriented place’

- 6. Places with special character that signal that you are in Boulder and just “Anywhere, USA”

The S’PARK team has embraced this, working to integrate our community’s values of sustainability and inclusiveness into the program, as well as worked to promote design diversity through utilizing the site’s unique history, context, and aspirations

- 7. A mixture of uses, including housing, to enliven the area

Residential stakeholders are key to creating a place in Boulder. S’PARK will satisfy a variety of housing demand that will result in families, professionals and seniors through market-rate townhomes, apartments, and condos, as well as permanently affordable townhomes and flats.

- 8. Sufficient amount of housing to create a neighborhood

With 247 total residential units across a diverse spectrum of housing types, S’PARK will succeed in providing the critical mass for a successful new neighborhood.

- 9. Engaging, convenient and safe pedestrian and bike connections. Active, walkable streets in a fine grain grid pattern

S’PARK will provide approximately 20% of its gross site area to public right-of-way and development-paid improvements. This will result in not only key automobile connections contemplated in the Transportation Master Plan, but critical bike and pedestrian infrastructure such as the multi-use path along the tracks that will connect Goose Creek and Valmont Road, the woonerf and plaza and the future rail plaza. In addition, the project appropriates a substantial portion of private property to important pedestrian connections from site-to-site throughout S’PARK to make a beautiful and inclusive place.

- 10. A variety of community gathering spaces

The project team believes that people are what make a place great. Providing ample places for play, casual convening, contemplation, commerce and gathering are all equally important. These manifest in the future rail plaza, the active woonerf, Markt plaza, and Meredith Park.

Private outdoor spaces are equally as important to occupant well being for access to sunlight, views, and fresh air. These manifest in every single unit in the project having a private porch and/or balcony, water-saving small yard areas, the Timber courtyard and amenity deck, and various office balconies to create health work environments.

‘Support Diversity’

- 11. A variety of housing types at a range of prices from market rate to affordable (including housing for very low, low, moderate and middle income households)

S’PARK will create the following housing types and quantities:
 S’PARK_west: (24) Market-rate, for-sale, 3-bedroom townhomes and (45) Permanently affordable, for-rent, 2-3 bedroom townhomes at 50% and 60% AMIs

Timber: (129) Market-rate, for-rent, studio, 1,2 and 3-bedroom flats, live-work units, and townhomes

Meredith House: (15) Market-rate, for-sale, 2-bedroom flats

Ciclo: (32) Permanently affordable, for-rent, 1,2 and 3-bedroom flats at 50 and 60% AMI

- 12. Services that support residents, adjacent neighbors and businesses, Support for locally owned and minority businesses...and non-profits

Much of the retail and office is being designed with small, local business and non-profits in mind. Furthermore, S’PARK is working in partnership with Community Cycles to create a new headquarters, retail shop, and community space that will not only provide neighborhood employment, but services to those living in S’PARK wishing to become less auto-dependent. S’PARK is also working to explore creative partnerships that the Boulder Junction Access District may utilize with Community Cycles for administration of the EcoPass program, providing day-time bike concierge services to S’PARK occupants and neighbors and affordable bike sales and repair to all residents and office users. Community Cy S’PARK envisions a continual management presence into the future to create exciting and relevant programming for residents, neighbors, visitors in the community such as a farmers’ market annex to better serve east Boulder residents and a variety of regular arts events in the woonerf to bring people together and celebrate life. S’PARK’s public spaces – as well as private businesses – are designed to be just a short and inviting walk away for residents of Orchard Grove and San Juan del Centro across Valmont Road to help establish a neighborhood center for the greater Boulder Junction area.

- 14. Affordable retail and office

The project has programmed a number of smaller retail at-grade spaces that serve the dual purposes of maintaining a lively pedestrian experience at the street-level and in the woonerf area, and by being smaller (such as 500-1000sf), provide entrepreneurs an opportunity for a reasonably-priced space to set up a shop without “breaking the bank” for real estate costs. Compact spaces promote the ability to achieve this important aspect of affordability.

‘Enhance Economic Vitality’

- 15. Neighborhood-serving retail uses that complement Twenty Ninth Street

Whereas Twenty Ninth Street features larger, big box tenants, S’PARK will feature authentic local purveyors and artisans in a neighborhood setting

- 16. Convenient and safe connections to downtown and Twenty Ninth Street

Workers, commuters, children and residents will have the ability – via S’PARK’s public and private multi-use connections – to navigate the immediate and larger Boulder area without crossing a single major thoroughfare. This begins with safe access to Goose Creek from S’PARK’s woonerf and multi-use path.

- 17. Office in locations close to future transit

With S’PARK’s participation in the EcoPass program in perpetuity through the Access District, office workers will be able to leverage the project’s close proximity to RTD’s Bus Rapid Transit station to access local bus routes to downtown Boulder and the CU Campus, express routes to Denver’s Union Station, and SkyRide routes to Denver International Airport.

- 18. Realistic plan including implementation of public-private partnerships

S’PARK’s partnerships as this time are not only realistic, but a reality.

The Access District (BJAD) partnership will help the project and community leverage public transportation investments to minimize development impacts and provide stakeholders access to the subsidies associated with living or working in a transit-oriented-development.

The project’s partnership with the City of Boulder at S’PARK_west to provide deep and diverse affordable housing to families and workers is in place through existing permanent affordability covenants (which are already recorded and in place) and a strong working relationship between the development team and city staff to seize opportunities to provide access to affordable housing for those in need in the community.

‘Connect to the natural and built environment’

- 19. Innovative “green” energy-efficient site planning, architecture and urban design

While many of the TMP road connections promote a north-south placement for building sites, the design team has been able to orient many of the buildings to take advantage of passive solar availability on an east-west axis.

Furthermore, with a LEED Platinum for Neighborhood Development program, the S’PARK plan considers many more “holistic” aspects of sustainability, in addition to energy consumption, such as healthy living, affordable housing, economic diversity, and active and passive transit access.

The project team carefully evaluated district-wide energy generation and ultimately found that there are not enough residential units across the site to make this financially feasible. However, as part of the project’s LEED Platinum profile, the project’s engineers have recommended an aggressive insulation package combined with cutting-edge HVAC systems and possibly photo-voltaic systems to achieve energy-efficiency more cost-efficiently.

- 20. An overall stormwater management plan

The engineering and landscape architecture team have excelled at this aspect of TVAP and been able to create an innovative drainage plan that minimizes runoff, utilizes porous surfaces to accept and naturally treat surface water – all without the use of a single detention pond or vault. Surface water is treated on a site-wide basis to promote efficiency and equity.

- 21. Connections to existing natural amenities

The site’s circulation pattern allows for easy bike and pedestrian access to Goose Creek – in addition to new natural features that the S’PARK plan creates, such as the Meredith Park and active and passive greenspaces

- 22. Taking advantage of viewsheds

The design team has addressed this criterion with great care and emphasized this in public places. It will be as much an amenity for visitors as residents to be able to be able to maintain excellent view corridors from the Markt plaza, Meredith Park or S’PARK Place

Response to TVAP Guidelines

'Maximize the community benefit of the transit investment'

23. Improving the balance of jobs and housing in the community through new mixed-use neighborhoods in areas close to multiple transit facilities

S'PARK will offer market-rate housing, affordable housing, and commercial tenants unprecedented energy-efficiency leading to lower utility costs and real savings combined with the additional subsidy of cost-saving proximate access to the entire RTD mass-transit system through project-wide EcoPasses in perpetuity. These two subsidies – in combination – are extraordinary subsidies for the community that will be a laudable and exportable example for how cities that are sensitive to growth can plan low-impact, high-performance incremental growth.

24. Managed parking strategies

The project is working closely with City of Boulder staff, the Boulder Junction Access District, GoBoulder and Community Cyles on Travel Demand Management (TDM) strategies that include unbundling, sharing and managing parking access.

25. Multimodal access within area and to rest of Boulder

S'PARK will have unprecedented access for commuters, children and families to the City of Boulder multi-modal path system from the project without having to cross a major thoroughfare or utilize a traffic signal.

26a. Lively and engaging transit locations

The project has provided for the potential that the RTD Commuter Rail service will one day travel the Northwest Corridor and to Boulder. While this is not likely until after 2040 (RTD's current projection), the project has provided an area for future train platform build out by the City of Boulder or RTD – while not sacrificing an excellent site plan in the meantime at S'PARK for residents or visitors

26b. Create a plan that will adapt to and be resilient for Boulder's long-term future

The project's site plan has evolved since Concept Plan Review to adapt to this criterion by leaving a substantial area of the subject and adjacent property for future proposals.

The design and development team see the value to the project and the community to scaling back the initial phasing plan, allowing for a build out that responds to community demand, and then allows additional phasing to adapt more organically to community needs and interests once the initial phase has been established.

27. Build in flexibility and allow for serendipity and changes in use over time

S'PARK's commercial plan allows for very flexible uses at ground level. With a total of 43,000sf of street-level commercial, the development team anticipates a greater amount of retail and restaurant as critical mass develops in S'PARK over time and the project "seasons".

29. Provide for density in targeted locations

The project's two zones achieve density inline with TVAP's recommendations, with the MU-4 zone achieving an FAR of 1.62 and the RH-6 zone achieving approximately 20 units per acre. The MU-4 zone is adjacent to the tracks and transit, while the RH-6 zone becomes less intense to the west and more compatible with the existing Steelyards land use typology

URBAN DESIGN

Land Use Prototypes

High Density Residential - 1

29. Urban townhomes and garden apartments with individual garages, surface parking lots, or underground parking. Mainly two to three stories.

Project architects have carefully followed this recommended typology, with townhomes, detached garages or tuckunder parking (not a single uncovered surface-parked vehicle), in two to three stories, as well as provided greenspace amenities for outdoor play and social space, a community center and a gazebo gathering area for S'PARK_west – all to appeal to families.

High-Density Residential -1-	15-24 Dwelling Units per Acre	Who would live here?
		Middle and upper incomes. <ul style="list-style-type: none"> • Garden apartments tend to attract moderate income singles, couples, families and some seniors. Often have facilities for small children. • With their vertical arrangement, urban town homes may not be suited for people with disabilities or seniors.
Urban townhomes and garden apartments with individual garages, surface parking lots, or underground parking. Mainly two to three stories.		

Mixed Use - 2

30. Three-to-four story mixed-use buildings. Predominant use may be business or residential. Mostly structured or first-floor parking; may have some surface parking.

The S'PARK plan allows for these taller and broader structures in the MU2 land use zone to provide a buffer from the high impact train activity on the east for the benefit of the residential properties to the west in HDR1. Other than some street parking in the public right-of-way, all parking is contained in a structured garage within and below the Timber building.

Mixed Use -2-	1.5 - 2.0 Floor Area*	Who would live here?	Who would work here?
		Mixed use can range in price and type of house-holds served, from affordable to higher-end. <ul style="list-style-type: none"> • These prototypes are more likely to be attractive to singles, couples and workers who like proximity to urban services, transit and employment centers. • These prototypes may also appeal to empty-nesters and active seniors. 	Retail workers would be those associated with small- and medium-sized retail stores, restaurants, and personal services. Office workers would be those associated with professional, corporate or technical fields.
Three- to four-story mixed-use buildings. Predominant use may be business or residential. Mostly structured or first-floor parking; may have some surface parking.			

Response to TVAP Guidelines

‘Building Placement and Design’

31. Orient the main façade to the street and provide an entrance on the street side of the building

Project architects have considered and implemented this recommendation. Furthermore, the Timber building provides multiple points of entry on all four sides of the building as well as “break points” for permeability and pedestrian interest.

32. Design buildings with pedestrian-scale materials and architectural articulation, particularly on the first floor. Avoid large blank walls.

The various S’PARK team members have designed buildings based on how they will be experienced in the built environment by the primarily pedestrian stakeholders. Therefore, the greatest importance has been given to the first 12-18’ of the elevation – the range of pedestrians’ main view. These zones of the façade have human-scale and authentic materials such as brick, wood and metal. Articulation has been done carefully to promote an interesting street experience, while balancing defensible space for safety.

Also, all project common stairwells within the project have access to light through mindful placement of windows. This not only breaks up the wall façade as TVAP contemplates, but promotes stair usage to circulate within buildings which reduces energy consumption and contributes to healthy living.

33. Consider opportunities to frame or preserve views

Boulder Junction has incredible views of the Flatirons and Front Range and has taken great care to “frame” these views using CAD modeling combined with Google Earth mapping. S’PARK Place – almost coincidentally – is oriented directly towards the 2nd Flatiron and offices and many of the two and three story residences maintain unobstructed mountain views to the west and prairie views to the east. Sometimes the best “views” are also in providing mindful access to clean air and sunlight and S’PARK provides this in both residential and commercial spaces.

‘Usable Open Space’

34. Incorporate well-designed, functional open spaces with tree, quality landscaping and art, and access to sunlight and places to sit comfortably

The open space profile for S’PARK has always been to provide a mix of active and passive greenspaces – places for interactivity and play and places for quiet contemplation.

The S’PARK_west residences provide each unit with private open space on balconies plus many units with private yard spaces. S’PARK_west also has two open greens for activity, barbecues, playing ball or creative gatherings and celebrations. S’PARK_west also has adjacent access to the Meredith Park at the corner of Meredith/Junction Place.

The main S’PARK sites include the woonerf, S’PARK Place, with trees, sunlight access, seating, open play area and limitless possibilities for programming and activities for the public and S’PARK inhabitants. The Markt building also has a lively Markt Plaza for activities,

programming and outdoor seating for the brewpub and micro-restaurants. Also, all residences within S’PARK at Ciclo and Timber have private balconies and an abundance of light filled courtyard and common area amenity space outdoors.

‘Permeability’

35. Also provide opportunities to walk within the interior between abutting properties

There are numerous areas within the project that the design team has incorporated interesting passageways for permeability and to enliven the pedestrian experience within S’PARK. There are two main “off ramp” points from the multi-use path along the tracks into S’PARK Place and the Markt Plaza that safely sneak between buildings, a break in the north and west edge of the Timber building with access to the amenity deck from Meredith Street and Junction Place, with the Junction Place access aligning with the “green spine” of pedestrian circulation of S’PARK_west and continues all the way through Timber to S’PARK Place. And, finally, there is a neat little connection from the interior drive space of eastern S’PARK_west north across Meredith to neighboring units. These passages, along with a fine-grained street pattern that share cars, bikes and pedestrians, will all contribute to a lively and connected pedestrian experience at S’PARK that TVAP envisions

‘Parking Structures’

36. Design the ground level of parking structure to be interesting and appealing for pedestrians, for example, by wrapping the ground level with active uses, such as retail

The design team has carefully followed this important aspect of TVAP. No parking garage structure is exposed at any point in the project, which aligns with the team’s goal of creating a place that looks, feels and operates as a primarily non-auto-centric neighborhood. There is a careful balance between residential and retail in the MU2 zone that mostly puts “feet on the ground” but also creates pedestrian interest and attracts people to the nodes of activity.

37. If tuck-under parking or podium parking is provided, locate it at the rear of the property or wrap with active uses if feasible

This has been also followed carefully, particularly in S’PARK_west and Meredith Lofts.

‘Mixed-Use Buildings’

38. The potential for conflicts between residential and non-residential uses within mixed-use buildings should be minimized through careful design

At S’PARK in its entirety, only two buildings, Timber and Ciclo, have actual residential-commercial mixed-use combination. The commercial uses at Timber are expected to be lower impact commercial services or coffee shop type uses, where the uses at Ciclo are likely for non-profits with primarily only daytime operation. Both buildings will conform to important code-mandated sound transmission ratings, ventilation and fire separations.

‘Stormwater Guidelines’

39. Use of permeable materials...to maximize infiltration and minimize surface runoff

The project is utilizing permeable paving solutions on S’PARK_west private driving surfaces, on the S’PARK Place woonerf and behind Meredith Lofts.

40. Surface detention ponds should be minimized and avoided

Indeed, the project’s care to provide many greenspaces assists in the treatment and management of stormwater – a win-win for engineering, sustainability and livability in S’PARK.

41. Surface detention ponds should be minimized and avoided

There are no surface (or underground vault style) detention ponds at S’PARK.

‘Rail Plaza District’

42. Place active uses on the ground level...stores, restaurants, cafes, or commercial services

There is a careful balance between residential and retail in the MU land use areas that mostly puts “feet on the ground” but also creates pedestrian interest and attracts people to the nodes of activity. The development team expects the micro-restaurants, brewpub, coffee shop, commercial services and boutiques to provide a vibrant retail offering and a more local, authentic experience for S’PARK inhabitants and visitors.

43. Orient buildings to Junction Place...consider making...a car-free zone with pedestrian amenities

The project team has routed Junction Place as the likely higher impact bus route to the west and retained S’PARK Place as a thoroughfare where pedestrian circulation is the principal mode of transport and a curated place with amenities, commerce and activities.

44. A new traffic signal with crosswalks at Valmont Road and 34th Street will help tie the Transit Village area to the neighborhoods to the north

The development team plans to participate with transportation staff to provide this community benefit and to minimize traffic impacts on Valmont Road, all the while allowing for safe access to the site for cars, bikes and pedestrians.

45. A multi-use path along the west side of the tracks will provide easy bicycle and pedestrian access between Valmont, the rail stop and Goose Creek Greenway

This pathway is a key aspect of S’PARK and TVAP and the development team is pleased to participate in bringing it to reality.

46. “Having a plaza or Mercado would be a great way to bring in the folks from across Valmont.” –Rosemary Rodriguez, former Denver City Council member

S’PARK will invite all surrounding neighbors to enjoy Boulder Junction and what will hopefully be a new neighborhood center for all. Markt is not only a direct – and literal – application of this TVAP suggestion, but a sincere gesture to ensure that S’PARK provides an attractive place for people to visit and enjoy.

ARTS IN TVAP

47. Look for opportunities to incorporate art into built elements such as paving, railings, signage, seating, or overhead structures

One of the ethics for the design team has been to integrate the history of the site into the programming and design, which includes repurposing many of the elements of the former lumberyard at the site. This will include steel racks, rail and actual lumber that have been integrated into lobby ceilings, Meredith Park and S’PARK Place. The inclusion of these materials -- a preservation of the past in a new interpretation – is what will help make S’PARK feel authentic and artistic.

48. Gateway opportunities exist at the experiential edge of character areas, while the densest groupings of art opportunities occur where the environment is most urban

The development team is exploring an arts partnership within the Ciclo building at the Valmont Gateway where TVAP encourages public art. The team expects that the artistic building designs at that gateway, along with actual artists occupying the space and playful landscape architecture will indicate to project inhabitants and visitors that S’PARK is a creative place that embraces diversity and “maker” culture.

Written Statement



August 21, 2015

BOULDER VALLEY COMPREHENSIVE PLAN (BVCP) GUIDELINES SUPPORTED BY S'PARK PLAN

The following aspects of the BVCP are readily supported by the S'PARK plan and provided here for review quick reference.

1. Core Values, Sustainability Framework and General Policies

Many of the key policies in the Boulder Valley Comprehensive Plan stem from long-standing community values and represent a clear vision of our community and our commitment to:

- Sustainability as a unifying framework to meet environmental, economic and social goals
- A welcoming and inclusive community
- Culture of creativity and innovation
- Strong city and county cooperation
- Our unique community identity and sense of place
- Compact, contiguous development and infill that supports evolution to a more sustainable urban form
- Open space preservation
- Great neighborhoods and public spaces
- Environmental stewardship and climate action
- A vibrant economy based on Boulder's quality of life and economic strengths
- A diversity of housing types and price ranges
- An all-mode transportation system to make getting around without a car easy and accessible to everyone
- Physical health and well-being

Applying a sustainability framework to decision-making in Boulder means considering the issues of environment, economy and social equity together.

1.02 Principles of Environmental Sustainability c) Reducing and minimizing the use of non-renewable resources.

1.03 Principles of Economic Sustainability

The city and county will strive to develop and maintain a healthy, adaptable economy that is vital to the community's quality of life and high level of services and amenities by:

- a) Promoting a diverse economy that supports the needs of all community members;
- b) Promoting a qualified and diversified work force that meets employers' needs and supports a range of jobs; and
- c) Providing for and investing in a quality of life, unique amenities, and infrastructure that attracts, sustains, and retains businesses and entrepreneurs.

1.04 Principles of Social Sustainability

The city and county will strive to promote a healthy community and address social and cultural inequities by:

- a) Respecting and valuing cultural and social diversity;
- b) Ensuring the basic health and safety needs of all residents are met; and
- c) Providing infrastructure and services that will encourage culturally and socially diverse communities to both prosper within and connect to the larger community.

1.05 Community Engagement

The city and county recognize that environmental, economic and social sustainability are built upon full involvement of the community. The city and county therefore support the right of all community members to play a role in governmental decisions, through continual efforts to maintain and improve public communication and the open conduct of business. The city and county will continue to support programs and provide opportunities for public participation and neighborhood involvement. Efforts will be made to use effective technologies and techniques for public outreach and input, remove barriers to participation and involve community members not usually engaged in civic life. Emphasis will be placed on notification and engagement of the public in decisions involving large development proposals or major land use decisions that may have significant impact on or benefits to the community.

1.07 Leadership in Sustainability

The city and county will act as leaders and role models for others in striving to create a sustainable community. Through their master plans, regulations, policies and programs, the city and county will strive to create a healthy, vibrant and sustainable community for future generations.

2. Built Environment

Boulder's compact, interconnected urban form helps ensure the community's environmental health, social equity and economic vitality. It also supports cost-effective infrastructure and facility investments, a high level of multimodal mobility, and easy access to employment, recreation, shopping and other amenities, as well as a strong image of Boulder as a distinct community.

2. Individual Character Areas - Different parts of Boulder are distinguished by their individual character. Boulder's city structure is also defined by the individual character and distinctive qualities of its different areas, drawing on each area's unique history, development pattern, land uses, amenities and other factors.

3. Activity Centers - Activity centers concentrate activities into nodes at a variety of scales. Boulder's commercial, entertainment, educational and civic centers are focused in concentrated nodes of activities at a variety of scales distributed throughout the community. The next tier of intensity is neighborhood activity centers. In addition to serving as neighborhood gathering places, these centers also provide goods and services for the day-to-day needs of nearby residents, workers and students, and are easily accessible from surrounding areas by foot, bike and transit.

4. Mobility Grid - Boulder's 'mobility grid' interconnects the city. Boulder's 'mobility grid'—the system of streets, alleys, transit corridors, multi-use and greenway paths—interconnects the city and both serves and reflects the city's land use pattern. Networks for vehicles, bicycles, pedestrians and transit—sometimes shared, sometimes separate—overlay the city and create a lacework of movement between and within regional centers, neighborhood centers, and residential and employment areas.

5. The Public Realm - The public realm provides key functions and strongly influences character and aesthetics. The public realm includes the city's streets, sidewalks and paths, ditches, parks, plazas and other urban outdoor spaces. It comprises a large portion of Boulder's land and represents a substantial public investment. The design of the public realm plays a major role in defining the character, identity and aesthetic quality of the city overall and individual neighborhoods. It also serves a variety of important functions: transportation, passive and active recreation, gathering places, opportunities to connect to nature, enhancement of air and water quality, and mitigation of urban heat island effects.

Sustainable Urban Form Definition

The city's urban form is shaped by the location and design of streets, paths and open spaces; the mix of uses and activities that are allowed in each area of the city; and the design and intensity of development and public improvements. The city's goal is to evolve toward an urban form that supports sustainability. This "sustainable urban form" is defined by the following components:

Compact:

- A compact development pattern with density in appropriate locations to create and support viable, long term commercial opportunities and high frequency public transit.

Connected:

- An integrated multimodal system with abundant, convenient and pleasant ways to get around on foot, by bike, and by local and regional transit service.
- Opportunities for people to connect to nature and natural systems.

Complete:

- Daily needs within easy access from home, work or school without driving a car.
- A quality of life that attracts, sustains and retains diverse businesses, creative entrepreneurs and investment in the local economy.

Green, Attractive and Distinct:

- Comfortable, safe, and attractive places to live, work, learn and recreate that have a distinct, memorable character and high-quality design and that promote healthy, active living.
- A public realm that is beautiful, well-used and enriched with art, trees and landscaping.
- Buildings, streets, utilities and other infrastructure that protect natural systems, minimize energy use, urban heat island effects and air and water pollution, and support clean energy generation.
- Preservation of agriculturally significant lands, environmentally sensitive areas and historic resources. Inclusive:
 - A diversity of employment, housing types, sizes and prices, and other uses to meet the needs of a diverse community.
 - Welcoming, accessible public gathering spaces for interaction among people of all ages, walks of life and levels of ability.

Written Statement



August 21, 2015

Mixed Use and Higher Density Development

2.16 Mixed Use and Higher Density Development

The city will encourage well-designed mixed use and higher density development that incorporates a substantial amount of affordable housing in appropriate locations, including in some commercial centers and industrial areas and in proximity to multimodal corridors and transit centers. The city will provide incentives and remove regulatory barriers to encourage mixed use development where and when appropriate. This could include public-private partnerships for planning, design or development; new zoning districts; and the review and revision of floor area ratio, open space and parking requirements.

Urban Design Linkages

2.19 Urban Open Lands

Open lands within the fabric of the city constitute Boulder's public realm and provide recreational opportunities, transportation linkages, gathering places and density relief from the confines of the city, as well as protection of the environmental quality of the urban environment. The city will promote and maintain an urban open lands system to serve the following functions: active and passive recreation, environmental protection, flood management, multimodal transportation, enhancement of community character and aesthetics.

2.21 Commitment to a Walkable and Accessible City

The city and county will promote the development of a walkable and accessible city by designing neighborhoods and business areas to provide easy and safe access by foot to places such as neighborhood centers, community facilities, transit stops or centers, and shared public spaces and amenities. The city will consider additional neighborhood-serving commercial areas where appropriate and supported by the neighbors they would serve.

2.22 Improve Mobility Grid

The walkability, bikeability and transit access should be improved in parts of the city that need better connectivity and mobility, for example, in East Boulder. This should be achieved by coordinating and integrating land use and transportation planning and will occur through both public investment and private development.

2.23 Trail Corridors/Linkages

In the process of considering development proposals, the city and county will encourage the development of paths and trails where appropriate for recreation and transportation, such as walking, hiking, bicycling or horseback riding. Implementation will be achieved through the coordinated efforts of the private and public sectors.

2.31 Design of Newly-Developing Areas

The city will encourage a neighborhood concept for new development that includes a variety of residential densities, housing types, sizes and prices, opportunities for shopping, nearby support services and conveniently sited public facilities, including roads and pedestrian connections, parks, libraries and schools.

2.33 Environmentally Sensitive Urban Design

For capital improvements and private development, the city and county will strive to ensure that buildings, streets, utilities and other infrastructure are located and designed to protect natural systems, minimize energy use, urban heat island effects and air and water pollution, and support clean energy generation.

2.34 Importance of Street Trees and Streetscapes

The city and county will develop regulations and programs to encourage the planting and maintenance of attractive, healthy street trees and streetscapes, which act as the primary connection between the private and public realm and provide aesthetics, comfort and environmental benefits for the public realm.

2.37 Enhanced Design for Private Sector Projects

Through its policies and programs, the city will encourage or require quality architecture and urban design in private sector development that encourages alternative modes of transportation, provides a livable environment and addresses the elements listed below.

a) The context. Projects should become a coherent part of the neighborhood in which they are placed. They should be preserved and enhanced where the surroundings have a distinctive character. Where there is a desire to improve the character of the surroundings, a new

character and positive identity as established through area planning or a community involvement process should be created for the area. Special attention will be given to protecting and enhancing the quality of established residential areas that are adjacent to business areas.

b) Relationship to the public realm. Projects should relate positively to public streets, plazas, sidewalks, paths, ditches and natural features. Buildings and landscaped areas—not parking lots—should present a well-designed face to the public realm, should not block access to sunlight, and should be sensitive to important public view corridors. Future strip commercial development will be discouraged.

c) Transportation connections. Projects should provide a complete network of vehicular, bicycle and pedestrian connections both internal to the project and connecting to adjacent properties, streets and paths, including dedication of public rights-of-way and easements where required.

d) Human scale. Projects should provide pedestrian interest along streets, paths and public spaces.

e) Permeability. Projects should provide multiple opportunities to walk from the street into projects, thus presenting a street face that is permeable. Where appropriate, they should provide opportunities for visual permeability into a site to create pedestrian interest.

f) On-site open spaces. Projects should incorporate well-designed functional open spaces with quality landscaping, access to sunlight and places to sit comfortably. Where public parks or open spaces are not within close proximity, shared open spaces for a variety of activities should also be provided within developments.

g) Buildings. Buildings should be designed with a cohesive design that is comfortable to the pedestrian, with inviting entries that are visible from public rights of way. Design innovation and the use of high quality building materials are encouraged.

4. Energy and Climate

4.04 Energy-Efficient Land Use

The city and county will encourage energy conservation through land use policies and regulations governing placement, orientation and clustering of development.

4.05 Energy-Efficient Building Design

The city and county will pursue efforts to improve the energy and resource efficiency of new and existing buildings. The city and county will improve regulations ensuring energy and resource efficiency in new construction, remodels and renovation projects and will establish energy efficiency requirements for existing buildings. Energy conservation programs will be sensitive to the unique situations that involve historic preservation and low-income homeowners and renters and will ensure that programs assisting these groups are continued.

5. Economy

Boulder recognizes the need to revitalize its older commercial and industrial areas, renovate "tired buildings" and support renovation and business growth in these areas. As a mature,

compact city with little remaining vacant land, the city must engage in strategic planning for redevelopment areas and ensure that economic benefit is a primary outcome. The city also recognizes that economic growth can bring many benefits to the community, including greater tax revenues, local job opportunities, increased diversity, a variety of services and business types and physical improvements. The city will collaborate with the business community to facilitate growth, development, and infrastructure improvements that benefit residents and businesses alike.

5.01 Revitalizing Commercial and Industrial Areas

The city will develop specific strategies to optimize redevelopment opportunities, partner with the private sector and proactively support redevelopment of commercial and industrial areas. Examples of areas for revitalization that have been identified are Diagonal Plaza, University Hill Commercial district and the East Boulder Industrial area. The city will use a variety of tools to create public/private partnerships that lead to successful redevelopment. These tools may include, but are not limited to, area planning, infrastructure improvements, changes to zoning or development standards and incentives including financial incentives, increased development potential or urban renewal authority.

5.03 Diverse Mix of Uses and Business Types

The city and county will support a diversified employment base within the Boulder Valley, reflecting labor force capabilities and recognizing the community's strengths in scientific, professional, technological and related industries. The city will identify areas that should be protected for industrial, service and office uses and will evaluate areas with non-residential zoning to ensure that the existing and future needs of a rapidly changing and technologically-oriented global economy and employment base are adequately accommodated. Where appropriate, mixed use development will be encouraged incorporating residential uses and support services for the employment base.

5.05 Support for Local Business and Business Retention

The city and county recognize the significant contribution of existing businesses in the local economy and will work to nurture and support established businesses and maintain a positive climate to retain businesses. Business retention and expansion is a primary focus for the city. The existing jobs that are in Boulder are the city's most important jobs.

5.09 Role of Arts and Cultural Programs

The city and county will support and encourage further development of arts and cultural programs that can serve as attractors for new business investment and visitors to the city. The city values the arts within the public realm and will work to enhance the capacity of arts and culture to act as an economic generator.

6. Transportation

The Boulder Valley Comprehensive Plan and the city's Transportation Master Plan (TMP) have the fundamental premise that the transportation system be developed and managed in conjunction with land use, social and environmental goals. A mature community like Boulder has little opportunity or desire to add road capacity as widening streets and building new roads would have significant negative environmental, community character and financial impacts. Consequently, the strategies of the TMP center on maintaining and developing a balanced transportation system that supports all modes of travel, on making the system more efficient in carrying travelers, maintaining a safe system and on shifting trips away from the single-occupant vehicle.

6.01 All-Mode Transportation System

The Boulder Valley will be served by an integrated all-mode transportation system, developed cooperatively by the city and county. This transportation system will include completed networks for each mode, make safe and convenient connections between modes, and provide seamless connections between the city and county systems. Improvements to the travel corridors network will be made in a manner that preserves or improves the capacity or efficiency of all modes and recognizes pedestrian travel as a component of all trips.

6.02 Reduction of Single Occupancy Auto Trips

The city and county will support greater use of alternatives to single occupancy automobile travel. It is the city's specific objective to continue progress toward 'no long-term growth in traffic' from 1994 levels through the year 2025 within the Boulder Valley. Both the city and county are committed to reductions in green house gas emissions. These efforts will include other communities and entities and will include developing and implementing integrated travel demand management programs and new services. Within the city, new developments will be required to include travel demand management to reduce the vehicle miles traveled produced by the development.

6.08 Transportation Impact

Traffic impacts from a proposed development that cause unacceptable community or environmental impacts or unacceptable reduction in level of service will be mitigated. All development will be designed and built to be multimodal, pedestrian-oriented and include strategies to reduce the vehicle miles traveled generated by the development. New development will provide continuous pedestrian, bike and transit systems through the development and connect these systems to those surrounding the development. The city and county will provide tools and resources to help businesses manage employee access and mobility and support public-private partnerships, such as transportation management organizations, to facilitate these efforts.

6.09 Integration with Land Use

Three intermodal centers will be developed or maintained in the downtown, Boulder Junction and on the university's main campus as anchors to regional transit connections and as hubs for connecting pedestrian, bicycle and local transit to regional services. The land along multimodal corridors will be designated as multimodal transportation zones when transit service is provided on that corridor. In these multimodal transportation zones, the city will develop a highly connected and continuous transportation system for all modes, identify locations for mixed use and higher density development integrated with transportation functions through appropriate design, and develop parking maximums and encourage parking reductions. The city will complete missing links in the transportation grid through the use of area transportation plans and at the time of parcel redevelopment.

6.10 Managing Parking Supply

Providing for vehicular parking will be considered as a component of a total access system of all modes of transportation - bicycle, pedestrian, transit and vehicular - and will be consistent with the desire to reduce single occupant vehicle travel, limit congestion, balance the use of public spaces and consider the needs of residential and commercial areas. Parking demand will be accommodated in the most efficient way possible with the minimal necessary number of new spaces. The city will promote parking reductions through parking maximums, shared parking, unbundled parking, parking districts and transportation demand management programs.

6.11 Transportation Facilities in Neighborhoods

The city and county will strive to protect and improve the quality of life within neighborhoods while developing a balanced transportation system. Improving access and safety within neighborhoods by controlling vehicle speeds or providing multi-modal connections will be given priority over vehicle mobility. The city and county will design and construct new transportation facilities to minimize noise levels. Neighborhood needs and goals will be balanced against the community benefit of a transportation improvement.

6.12 Neighborhood Streets Connectivity

Neighborhood streets and alleys will be developed in a well connected and fine grained pattern to facilitate public access, to effectively disperse and distribute vehicle traffic and promote bike and pedestrian travel.

6.13 Improving Air Quality

The city and county will design the transportation system to minimize air pollution by promoting the use of non-automotive transportation modes, reducing auto traffic, encouraging the use of fuel efficient and alternatively fueled vehicles that demonstrate air pollution reductions and maintaining acceptable traffic flow.

7. Housing

The range of available housing opportunities helps to define a community. The social, economic and environmental well-being of the community is enhanced when individuals and families are retained, workforce housing is available, and existing residents with changing or special housing needs are served.

7.01 Local Solutions to Affordable Housing

The city and county will employ local regulations, policies, and programs to meet the housing needs of their low and moderate income households and workforce. Appropriate federal, state and local programs and resources will be used locally and in collaboration with other jurisdictions. The city recognizes that affordable housing provides a significant community benefit and will continually monitor and evaluate its policies, programs and regulations to further the city's affordable housing goals.

7.02 Permanently Affordable Housing

The city will increase the proportion of permanently affordable housing units to an overall goal of at least ten percent of the total existing housing stock through regulations, financial subsidies and other means. City resources will also be directed toward maintaining existing permanently affordable housing units and securing replacements for lost low and very low income units.

7.04 Strengthening Community Housing Partnerships

The city will create and preserve partnerships dedicated to the community's housing needs by supporting private and nonprofit agencies that create and maintain permanently affordable

housing in the community, and fostering nonprofit and private sector partnerships. The city recognizes the role of the university in the housing market and will encourage the University of Colorado and other post-secondary institutions in their efforts to increase the amount of on-campus housing.

7.06 Mixture of Housing Types

The city and county, through their land use regulations and housing policies will encourage the private sector to provide and maintain a mixture of housing types with varied prices, sizes and densities, to meet the housing needs of the full range of the Boulder Valley population.

7.09 Housing for a Full Range of Households

The city and county will encourage preservation and development of housing attractive to current and future households, persons at all stages of life and to a variety of household configurations. This includes singles, couples, families with children and other dependents, extended families, non-traditional households and seniors.

7.10 Balancing Housing Supply with Employment Base

Expansion of the Boulder Valley housing supply should reflect to the extent possible current employer locations, projected industrial/commercial development sites, variety of salary ranges, and the demand such developments bring for housing employees. Key considerations include housing type, mix, and affordability. The city will explore policies and programs to increase housing for Boulder workers by fostering mixed-use and multi-family development proximate to transit, employment or services and by considering the conversion of commercial and industrial zoned or designated land to residential use.

7.13 Integration of Permanently Affordable Housing

Permanently affordable housing, whether publicly, privately or jointly financed will be designed as to be compatible, dispersed, and integrated with housing throughout the community.

8. Community Well-Being

Boulder, like all communities, is much more than its physical form. It is composed of people as well as the places where they live and work; it is as much a social environment as it is a physical environment. Boulder is a center of active living, attracting residents, businesses and visitors who value community and individual health. The city is committed to continuing to be a national leader in promoting the physical health and welfare of the community as well as promoting civil and human rights.

8.04 Addressing Community Deficiencies

The city will identify barriers to provision of important basic human services and work to find solutions to critical social issues such as lack of housing options for very low income and special needs populations, access to and affordability of basic services, and limited availability of affordable retail products.

8.05 Diversity

The community values diversity as a source of strength and opportunity. The city and county will support the integration of diverse cultures and socio-economic groups in the physical, social, cultural and economic environments; promote opportunities for community engagement of diverse community members; and promote formal and informal representation of diverse community members in civic affairs.

8.07 Physical Health

The city and county strive to ensure that this community continues to be a leader in promoting physical health and welfare of community members. The city recognizes that physical activity is essential to health and well-being. The city will support opportunities for people to exercise. Neighborhood and community design will encourage physical activity by establishing easy access to parks and trails, and locating activity centers close to where people live, work and attend school. The city will support community health programs such as: obesity prevention, outdoor education, safe routes to school, and healthy eating.

8.18 The Arts

The city and county recognize and support the arts. They are central to the cultural life for children, youth and adults of the Boulder community and a clean industry that contributes significantly to the Boulder economy. They present significant quality of life advantages to the Boulder community through education, entertainment and the aesthetic environment and provide a vehicle to bring together people of all walks of life and diverse ages, genders, religions, abilities, opinions, races, ethnicities, classes, and economic means for better communication and mutual understanding.

8.19 Public Art

The city and county will incorporate artistic elements in public projects whenever possible.

8/21/15

MANAGEMENT PLAN – S’PARK BREWPUB

Introduction

The proposed Brewpub will be a full service Brewpub specializing in tavern/american/pub fare and craft beer located at 3390 Valmont Road, Boulder, Colorado that will be open daily from 11:00 AM and no later than 11:00 PM. At the S’PARK location, the Brewpub will offer a wide selection of locally crafted beers to complement its lunch and dinner service to serve the neighborhood and its visitors. We expect a maximum interior capacity of 182 seats, a maximum of 50 patio seats, and a maximum of 38 employees at one time in the brewpub and production area.

Licensing

This establishment will hold a Brew pub license class liquor license pursuant to C.R.S. 12-47-415, which provides for and allows for food sales. The Brewpub will operate alongside three (3) additional micro-restaurants in an eclectic and family atmosphere.

A Neighborhood Center

The Brewpub will provide an important social gathering place for the surrounding S’PARK neighborhood for residents and employees, as well as draw visitors from beyond the surrounding neighborhood. The Brewpub will be an “anchor” for the project in location, visibility and activity. The Brewpub will be actively involved in the S’PARK (and surrounding community) providing a family-friendly venue for special events, charity functions and seasonal happenings.

Activity

The Brewpub expects approximately three hundred (300) people to patronize the business daily. We expect more business around lunch and dinner, operating from 11:00 AM to 11:00 PM. The majority of these patrons will walk from home/work or park in the public garages nearby. S’PARK is providing through the Boulder Junction Access District ample parking for daytime and nighttime patrons in adjacent parking garage, though we will provide incentives for people who ride their bikes or take public transportation as many other Boulder businesses do. Our hope is that this is a way for our business to promote the goals of the Transit Village Area Plan (TVAP) but also help minimize traffic impacts on the surrounding neighborhoods.

Outdoor Activity and Noise

The Brewpub is located along Valmont Road, a four-lane road with 28,000 cars per day and typical daytime and nighttime noise levels in excess of 80dB mostly due to auto-traffic. The brewpub would play indoor background music during all hours of operation and is purposely monitored so patrons can hold conversations at normal levels. We expect periodic weekly indoor live music that would have no effect on outdoor noise levels. We are proposing an outdoor patio for seasonally appropriate use of approximately 1,300sf and accommodating up to 50 people. We are not proposing any outdoor live music at this time unless under a special event permit. We also will not take garbage or recycling out any later than 9:00pm and will “bank” these items for the follow morning out of respect for our neighbors. Any deliveries associated with our brewing activities will be during normal daytime business hours and we are providing a loading dock with turnaround on private property so deliveries will not impede traffic and impact our neighbors. Despite an already high impact area due to traffic noise, we believe that these efforts can minimize – if not eliminate – noise impact to new and existing residents.

Training

All employees will be TIPS certified for responsible vending and are trained to check IDs for everyone who appears 35 years old or younger. The Brewpub will maintain the exterior of its premises in a neat and clean manner at all times, including sweeping up cigarette butts and other garbage. All employees are instructed to pick up any trash and litter within the outdoor dining area and the adjacent sidewalks as it is discovered throughout the day with a final cleaning immediately after closing.

Additional Outreach

As part of its initial opening procedures, the Brewpub will communicate with neighborhood residents, business users, and property owners and provide contact information of the general manager in the event there are complaints or issues that need to be resolved. In the event that there are complaints about late night noise from neighborhood residents, the Brewpub will work with the neighborhood in good faith, including if necessary, the use of mediation services recommended by the City of Boulder.

8/21/15

MANAGEMENT PLAN – Railyard North Restaurant Suite

Introduction

The proposed restaurant will be located at 3390 Valmont Road, Boulder, Colorado in the Railyard building in the North Suite and will be open daily from as early as 8:00 AM and no later than 11:00 PM. The restaurant suite is proposed as 2500sf of interior space with a maximum of 82 seats and a proposed outdoor dining area of 500sf with a maximum of 30 seats. We expect a maximum of 12 employees at one time. It will be a restaurant that will cater in the daytime to area businesses and visitors and more a family-oriented restaurant in the evening.

Licensing

This establishment will hold a Hotel and Restaurant license class liquor license pursuant to C.R.S. 12-47-415, which requires food sales to account for at least 25% of all revenue.

Training

All employees will be TIPS certified for responsible vending and are trained to check IDs for everyone who appears 35 years old or younger. The restaurant will maintain the exterior of its premises in a neat and clean manner at all times. All employees are instructed to pick up any trash and litter within the outdoor dining area and the adjacent sidewalks as it is discovered throughout the day with a final cleaning immediately after closing.

Additional Outreach

As part of its initial opening procedures, the restaurant will communicate with neighborhood residents, business users, and property owners and provide contact information of the general manager in the event there are complaints or issues that need to be resolved. In the event that there are complaints about late night noise from neighborhood residents, the restaurant will work with the neighborhood in good faith, including if necessary, the use of mediation services recommended by the City of Boulder.

8/21/15

MANAGEMENT PLAN – Railyard South Restaurant Suite**Introduction**

The proposed restaurant will be located at 3390 Valmont Road, Boulder, Colorado in the Railyard building in the South Suite and will be open daily from as early as 8:00 AM and no later than 11:00 PM. The restaurant suite is proposed as 3500sf of interior space with a maximum occupancy of 120 seats and a proposed outdoor dining area of 500sf with a maximum of 30 seats. We expect a maximum of 12 employees at one time. It will be a restaurant that will cater in the daytime to area businesses and visitors and more a family-oriented restaurant in the evening.

Licensing

This establishment will hold a Hotel and Restaurant license class liquor license pursuant to C.R.S. 12-47-415, which requires food sales to account for at least 25% of all revenue.

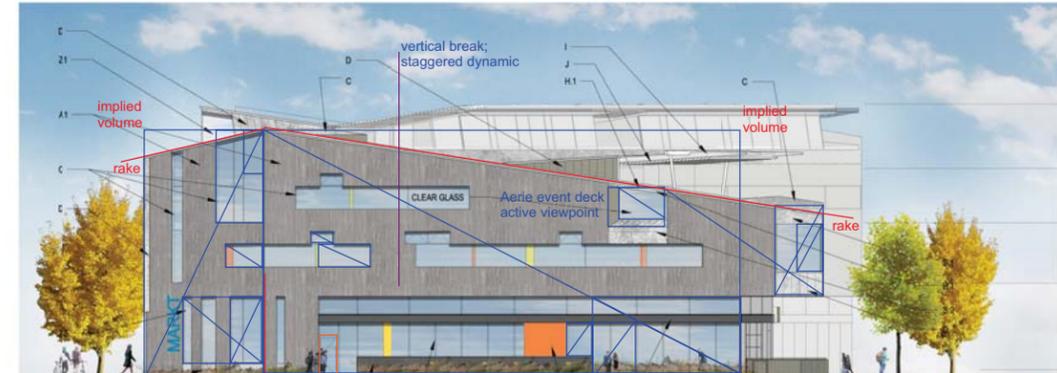
Training

All employees will be TIPS certified for responsible vending and are trained to check IDs for everyone who appears 35 years old or younger. The restaurant will maintain the exterior of its premises in a neat and clean manner at all times. All employees are instructed to pick up any trash and litter within the outdoor dining area and the adjacent sidewalks as it is discovered throughout the day with a final cleaning immediately after closing.

Additional Outreach

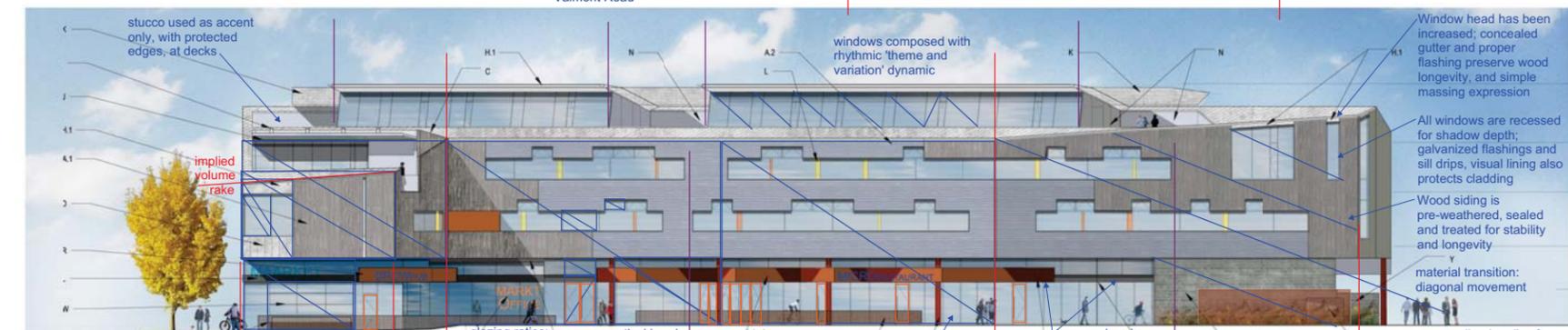
As part of its initial opening procedures, the restaurant will communicate with neighborhood residents, business users, and property owners and provide contact information of the general manager in the event there are complaints or issues that need to be resolved. In the event that there are complaints about late night noise from neighborhood residents, the restaurant will work with the neighborhood in good faith, including if necessary, the use of mediation services recommended by the City of Boulder.

- A.1. 4" 'GHOST WOOD' SIDING - FLAT, TRANSLUCENT SILVER GREY
- A.2. 6" 'GHOST WOOD' SIDING - FLAT, TRANSLUCENT BLUE GREY
- B. WEATHERING STEEL PLATE 4" O.C. VERTICAL SCREEN FLAT PLATE TOP RAIL
- C. 16" GALVANIZED STANDING SEAM
- D. FLAT SEAM GALVANIZED CLADDING
- E. CAST IN PLACE CONCRETE BENCH / WALL
- F. STEEL COLUMNING REDOXIDIC
- G. ALUMINUM WINDOW WALL SYSTEM - CHARCOAL GREY
- H.1. GALVANIZED FASCIA
- H.2. CHARCOAL GREY ALUMINUM FASCIA
- I. STEEL SHADE LATTICE FRAME, SILVER
- J. GALVANIZED 6" Z-CHANNEL SHADE LATTICE
- K. GALVANIZED 3" Z-CHANNEL SHADE BROW
- L. POWDER COATED ALUMINUM PANEL
- M. PERFORATED WEATHERING STEEL LIGHT VALENCE
- N. SAND FINISH STUCCO
- O. ALUMINUM WINDOW CLEAR ANODIZED AT UPPER LEVELS TYP.
- P. C.I.P. CONCRETE BENCH CHARCOAL
- Q. BOARD FORM CONCRETE STAGE ???
- R. PERFORATED WEATHERING STEEL
- S.1. GALVANIZED PANEL
- S.2. GALVANIZED SILL
- T. CLEAR GLASS GUARDRAIL
- U. STACK BOND CONCRETE MASONRY, RANDOM VERTICAL SCORE JOISTS - GREY
- V. ALUMINUM WINDOW CHARCOAL GREY AT FLOOR TYP.
- W. GLASS PANELED ALUMINUM GARAGE DOORS - CHARCOAL GREY
- X. CHARCOAL GREY ALUMINUM CLAD HEADER
- Y. CONCRETE MASONRY CHARCOAL - HONED
- Z.1. ALUMINUM WINDOW CLEAR ANODIZED
- Z.2. ANODIZED ALUMINUM STOREFRONT SYSTEM - CHARCOAL



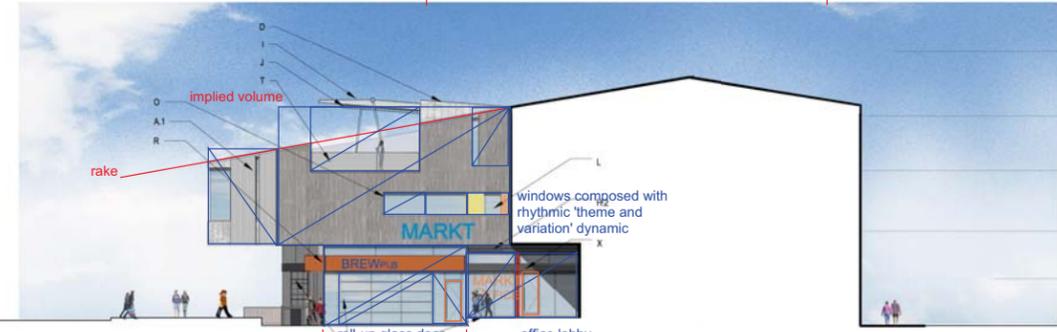
corner engagement of wood cladding; 'grounding' of the mass on the prow of Valmont Road
 window groups showcase brewpub production aspect
 Valmont entry door
 4' honed charcoal masonry 'anchor' wall at dining booths
 MARKET is intentionally narrow in plan here, for 'lantern' transparency and view to Plaza
 Level 1 building width
NORTH ELEVATION
 1/16" = 1'-0"

COMPOSITIONAL KEYS:
 Proportional resonance: harmonics of similarity
 Fenestration respects dominant proportion of each facade facet
 Valmont elevation is visually reduced in length; modulated by breaks in ribbon glazing, and transom windows.
 Scaling elements of transom head, booth wall, and terrace fence add human dimension
 Aerie and 'lantern window' punctuate the gateway.
 Undercut shades terrace and expands pedestrian horizon



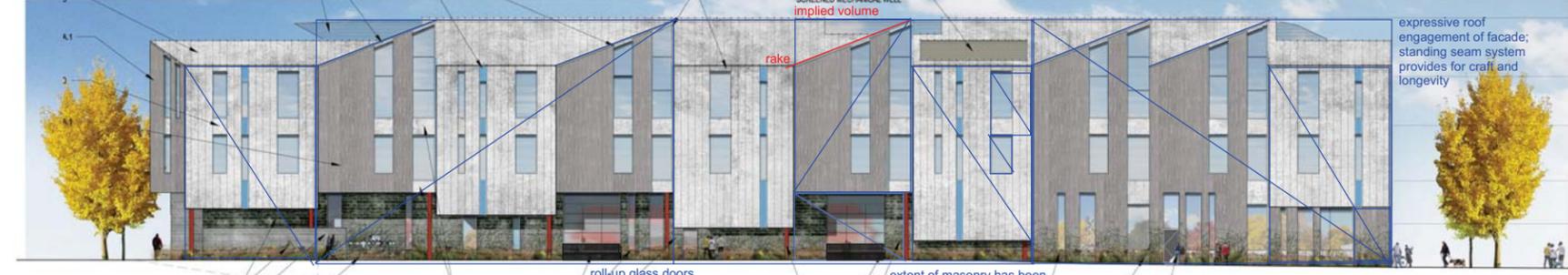
roll-up glass door plaza engagement
 West Face
 oblique
 West Plaza facade (parallel to 34th Street)
 glazing ratios; entries marked by frame treatment and valence 'vector'
 vertical break; staggered dynamic
 note multiple entry doors
 arcade is 9' wide; 14' high; light and airy with shade for outdoor dining
 transom head and light valence provide human scale
 WEATHERING STEEL SCREEN WITH CLIMBING PLANTS, IN FAMILY OF DINING TERRACE FENCES
 vertical break; staggered dynamic
 masonry 'bookend' softened by landscape screen; note transom face angles back from sidewalk
WEST ELEVATION
 1/16" = 1'-0"

COMPOSITIONAL KEYS:
 Building length is composed in much smaller segments; proportions resonant to structural bay. Best seen in 3D views as the facade is not flat.
 human scale of the pedestrian story: glazing ratios, transom head; light valence; dining terrace fences; fire pylon
 Clarity of material vocabulary; layering and depth
 Horizontality plays with verticality: syncopated ribbon window and transoms; segmented dormers; material transition points
 Recessed 4th level reduces scale



roll-up glass door plaza engagement
 Level 1 south
 office lobby entry door
 office entry volume
SOUTH ELEVATION
 1/16" = 1'-0"

COMPOSITIONAL KEYS:
 On each face, major fields and significant elements compose in sympathy (proportional resonance)
 Large aperture roll-up doors treated with importance: plaza engagement
 Aerie event deck is composed considering proportion defined by its shade lattice
 Rake provides slender diminishing scale effect



roll-up glass doors exterior engagement plantings below sill
 extent of masonry has been reduced, by increased wood cladding and fenestration; reads as recessive base layer
 window groups showcase brewpub production aspect
 corner engagement of wood cladding; 'grounding' of the mass on the prow of Valmont Road
EAST ELEVATION
 1/16" = 1'-0"

COMPOSITIONAL KEYS:
 Proportional resonance: harmonics of similarity
 Rhythmic movement of sawtooth skyline, roof panels, structure and fenestration at the speed of the bike or the train
 Clarity of material vocabulary; layering and depth
 Grounding of corner, as wood engages to grade level on 2 sides
 Expression of roof, engaging the facade

Elevations



COMPOSITIONAL KEYS:

Proportional resonance: harmonics of similarity

TIMBER intentionally foregoes the classical notion of a 'cap' tier: it runs warm tactile materials in the pedestrian horizon (or base) of Level 1; then uses simple metallic forms with shadow profiles and deep relief (at balconies), so the roof is expressed only at these zones between parapets

Elevations wrap at all corners, as four-sided architecture

Massing breaks foster the reading of multiple buildings (rather than monolithic block), punctuated by semi-public 'short cut' amenity garden access

COMPOSITIONAL KEYS:

Varied heights step in response to program. Limited parapet zones extend only as needed to provide integral mechanical screening, and foster a sense of rhythm and movement

The tri-partite composition along Meredith Street enhances the residential scale, stepping down to S'Park West townhome height

Verticality creates rhythmic alternation of cladding, fenestration, and 'porchlight yellow' alcoves when seen oblique along the street

The recess of level one allows room for stoops and stepped planters, enhanced by light yellow soffits for a sense of invitation. This counters the visual weight above

COMPOSITIONAL KEYS:

Townhomes and TIMBER Common terrace down toward Steelyards, providing mountain views from the Level Two garden and pool deck

Wood base provides tactile warmth, and returns into the 'short cut' stair, consistent with its cousins

Transparency of inside-outside connection are provided at lobby, clubhouse, coworking and fitness spaces

COMPOSITIONAL KEYS:

Proportional resonance: harmonics of similarity

Elevated stoops for personal domain; front approach; framed by brick planters to create residential scale

Extended perforated balcony rails at Level two 'bridge' the datum, offer sense of interior to stoops below

Bike storage and bike rooms are glazed, overt as part of the ethos

Fitness and clubhouse volumes are set back for open, active corner terrace and viewshed

Elevations