



CITY OF BOULDER
PLANNING BOARD MEETING AGENDA
DATE: September 4, 2014
TIME: 6 p.m.
PLACE: Council Chambers, 1777 Broadway

1. CALL TO ORDER

2. APPROVAL OF MINUTES

The [July 31, 2014](#) Planning Board minutes are scheduled for approval.

3. PUBLIC PARTICIPATION

4. DISCUSSION OF DISPOSITIONS, PLANNING BOARD CALL-UPS/CONTINUATIONS

A. Call Up Item: Wetland Permit (LUR2014-00056): Expires: Sept. 9, 2014

B. Call Up Item: 2250 Pearl Street (LUR2014-00022): Expires: Sept. 2, 2014

5. PUBLIC HEARING ITEMS

- A. Public hearing and consideration of a Site Review Minor Amendment (LUR2013-00059) and Final Plat (TEC2013-00073) for the Boulder Municipal Airport** to subdivide the existing 123.5-acre lot into two new lots: Lot 1C (2.6 acres) and Lot 2C (120.8 acres). Lot 1C will be removed from the existing Airport PUD, and Lot 2C will contain the existing Boulder Municipal Airport. The site is located at 3300 Airport Rd and is within the P and IG zone districts.

Applicant: City of Boulder

Owner: City of Boulder

- B. Public hearing and consideration of a Site and Use Review application, no. LUR2014-00036, to amend the existing Iris Hollow PUD** to allow for a new two-story, 3,131 sq. ft. office building on Lot 39. The proposed office building would be an expansion of the existing “Blue Sky Bridge” facility located on the adjacent site to the west. The proposal includes a request for a 20% parking reduction to allow for 8 off-street parking spaces where 10 parking spaces are required.

Applicant: Blue Sky Bridge c/o Peter Weber

Owner: Mark L. Polster

- C. CONCEPT PLAN REVIEW AND COMMENT: Request for citizen, staff and Planning Board comment on a proposal to redevelop the existing properties located at 3085, 3155 and 3195 Bluff Street** totalling approximately 4.25 acres into 77 dwelling units consisting: 24 three-bedroom, for-sale townhomes; 45 two and three-bedroom permanently affordable rental townhomes; and eight standard townhomes. Total of 84,534 square feet of habitable area on three lots: 3085, 3155 and 3195 Bluff Street. Review case number LUR2014-00050.

Applicant: Adrian Sopher

Property Owner: 1240 Cedar, LLC

6. MATTERS FROM THE PLANNING BOARD, PLANNING DIRECTOR, AND CITY ATTORNEY

7. DEBRIEF MEETING/CALENDAR CHECK

8. ADJOURNMENT

**CITY OF BOULDER PLANNING BOARD
MEETING GUIDELINES**

CALL TO ORDER

The Board must have a quorum (four members present) before the meeting can be called to order.

AGENDA

The Board may rearrange the order of the Agenda or delete items for good cause. The Board may not add items requiring public notice.

PUBLIC PARTICIPATION

The public is welcome to address the Board (3 minutes* maximum per speaker) during the Public Participation portion of the meeting regarding any item not scheduled for a public hearing. The only items scheduled for a public hearing are those listed under the category PUBLIC HEARING ITEMS on the Agenda. Any exhibits introduced into the record at this time must be provided in quantities of ten (10) to the Board Secretary for distribution to the Board and admission into the record.

DISCUSSION AND STUDY SESSION ITEMS

Discussion and study session items do not require motions of approval or recommendation.

PUBLIC HEARING ITEMS

A Public Hearing item requires a motion and a vote. The general format for hearing of an action item is as follows:

1. Presentations

- a. Staff presentation (5 minutes maximum*)
- b. Applicant presentation (15 minute maximum*). Any exhibits introduced into the record at this time must be provided in quantities of ten (10) to the Board Secretary for distribution to the Board and admission into the record.
- c. Planning Board questioning of staff or applicant for information only.

2. Public Hearing

Each speaker will be allowed an oral presentation (3 minutes maximum*). All speakers wishing to pool their time must be present, and time allotted will be determined by the Chair. No pooled time presentation will be permitted to exceed ten minutes total.

- Time remaining is presented by a Green blinking light that means one minute remains, a Yellow light means 30 seconds remain, and a Red light and beep means time has expired.
- Speakers should introduce themselves, giving name and address. If officially representing a group, homeowners' association, etc., please state that for the record as well.
- Speakers are requested not to repeat items addressed by previous speakers other than to express points of agreement or disagreement. Refrain from reading long documents, and summarize comments wherever possible. Long documents may be submitted and will become a part of the official record.
- Speakers should address the Land Use Regulation criteria and, if possible, reference the rules that the Board uses to decide a case.
- Any exhibits introduced into the record at the hearing must be provided in quantities of ten (10) to the Secretary for distribution to the Board and admission into the record.
- Citizens can send a letter to the Planning staff at 1739 Broadway, Boulder, CO 80302, two weeks before the Planning Board meeting, to be included in the Board packet. Correspondence received after this time will be distributed at the Board meeting.

3. Board Action

- d. Board motion. Motions may take any number of forms. With regard to a specific development proposal, the motion generally is to either approve the project (with or without conditions), to deny it, or to continue the matter to a date certain (generally in order to obtain additional information).
- e. Board discussion. This is undertaken entirely by members of the Board. The applicant, members of the public or city staff participate only if called upon by the Chair.
- f. Board action (the vote). An affirmative vote of at least four members of the Board is required to pass a motion approving any action. If the vote taken results in either a tie, a vote of three to two, or a vote of three to one in favor of approval, the applicant shall be automatically allowed a rehearing upon requesting the same in writing within seven days.

MATTERS FROM THE PLANNING BOARD, DIRECTOR, AND CITY ATTORNEY

Any Planning Board member, the Planning Director, or the City Attorney may introduce before the Board matters which are not included in the formal agenda.

ADJOURNMENT

The Board's goal is that regular meetings adjourn by 10:30 p.m. and that study sessions adjourn by 10:00 p.m. Agenda items will not be commenced after 10:00 p.m. except by majority vote of Board members present.

*The Chair may lengthen or shorten the time allotted as appropriate. If the allotted time is exceeded, the Chair may request that the speaker conclude his or her comments.

CITY OF BOULDER
PLANNING BOARD ACTION MINUTES
July 31, 2014
1777 Broadway, Council Chambers

A permanent set of these minutes and a tape recording (maintained for a period of seven years) are retained in Central Records (telephone: 303-441-3043). Minutes and streaming audio are also available on the web at: <http://www.bouldercolorado.gov/>

PLANNING BOARD MEMBERS PRESENT:

Aaron Brockett
Bryan Bowen
Crystal Gray
John Gerstle
Leonard May
Liz Payton
John Putnam

PLANNING BOARD MEMBERS ABSENT:

STAFF PRESENT:

Charles Ferro, Development Review Manager for CP&S
Hella Pannewig, Assistant City Attorney
Susan Meissner, Administrative Assistant III
Sam Assefa, Senior Urban Designer
Peggy Bunzli, Budget Officer, Finance
Chris Meschuk, Flood Recovery Coordinator – Community Services
Lesli Ellis, Comprehensive Planning Manager, CP&S
Jean Gatza, Sustainability Planner, CP&S
Milford John-Williams, Budget Analyst, Finance
Joe Castro, Facilities and Asset Management
Jeff Haley, Parks & Recreation
Bob Harberg, Public Works / Utilities
Douglass Sullivan, Public Works / Utilities
Tim Head, Public Works / Airport
Don Ingle, Information Technology
Annie Noble, Public Works / Utilities & Greenways
Kurt Bauer, Public Works / Utilities & Greenways
Mike Orosel, Open Space and Mountain Parks
Stephany Westhusin, Public Works / Transportation
Molly Winter, DUHMD

1. CALL TO ORDER

Chair, **A. Brockett**, declared a quorum at 6:03 p.m. and the following business was conducted.

2. APPROVAL OF MINUTES

There were no minutes scheduled for approval.

3. PUBLIC PARTICIPATION

1. **Richard Harris, 2645 Briarwood Drive**, spoke in opposition to the Comprehensive Housing Strategy to be discussed at the August 7th Planning Board meeting. He did not think that there had been sufficient public process.
2. **Steve Pomerance, 335 17th Street**, asked the city to put a moratorium on growth and to allow for more public input. He spoke in opposition to the proposed CHS.

The board asked both applicants to send their comments via email.

4. DISCUSSION OF DISPOSITIONS, PLANNING BOARD CALL-UPS/CONTINUATIONS

There were no items for discussion.

5. PUBLIC HEARING ITEMS

A. Compliance of Proposed Changes to the 9th and Canyon Urban Renewal Plan to the Boulder Comprehensive Plan

Staff Presentation:

M. Winter and **S. Assefa** presented the item.

Board Questions:

M. Winter and **S. Assefa** answered questions from the board.

Public Hearing:

No one from the public spoke.

Board Comments:

L. Payton expressed disappointment that meeting space was proposed to be the primary programmatic function of the civic use pad. She had hoped that the site would provide a venue for different ethnic and socioeconomic groups downtown.

A. Brockett agreed with **L. Payton's** disappointment that meeting space is the only proposed civic use. However, given that this effort has taken 16 years and that there are only a few years remaining, he would support staff's efforts and recommendation.

C. Gray also agreed with **L. Payton**. She hoped that the management agreement would allow for different groups and organizations to use the space on a sliding scale.

J. Putnam felt uncertain whether the amount of money that the city would invest warranted the value that it would get out of the space. However, he thought it was worth pursuing the option.

B. Bowen noted that the city has invested a lot of time in this process and should salvage value from it. Assure that it be used well for good purposes and connect the architecture to the Civic Area, St. Julien and downtown.

C. Gray served on some of the committees and noted that there were many proposals that never came to fruition for a variety of reasons. She was concerned that this space would revert to the St. Julien. She thought that the integration with the Civic Area plan made sense and that it was be important to allow this to move forward.

Motion:

On a motion by **C. Gray**, seconded by **J. Putnam**, the Planning Board found 7-0 that the modifications to the to the two sections of the 9th and Canyon Urban Renewal Plan to the two sections of thean made s2, including the amendment by the Boulder Urban Renewal Board, as a whole, conform to the general plan for the development of the municipality of the city which is the Boulder Valley Comprehensive Plan.

On a motion by **C. Gray**, seconded by **J. Putnam**, the Planning Board voted 7-0 to recommend that City Council ensure that this space is available and welcoming all members of the community, including the low income community and minority community, and that City Council look at different rate structures to accomplish that.

J. Putnam noted that it is hard for nonprofits to justify spending a lot of money to rent nice spaces. He therefore questioned whether this was the highest and best use for the space. The city should consider this when looking at the cost benefit.

L. Payton reiterated for City Council that the Planning Board was not excited by the prospect of the proposed program because meeting space did not meet the need for low income and minority populations.

B. 2015-2020 Capital Improvement Program

Staff Presentation:

J. Gatza and **P. Bunzli** presented the item.

Board Questions:

J. Gatza, P. Bunzli, S. Richstone, B. Harberg, J. Castro, K. Bauer and **D. Sullivan** answered questions from the board.

Public Hearing:

Board Comments:

J. Putnam thought the plan was solid and found the information helpful. He recommended that future reports include a dashboard that shows where we are and where we're going, as well as a snapshot of resilience and maintenance. Understand what we're trying to solve and address. He thought that the plan should be approved. Some scope, location and design issues will need to be addressed in the future; these items are already highlighted in the CIP.

A. Brockett thought this was a dynamic document and was impressed by the depth of effort and cross departmental work.

C. Gray liked that the city plans to raise water and sewage fees, and appreciated resiliency efforts surrounding agricultural uses, Emerald Ash Borer, mitigation carbon reduction and energy efficiency. She encouraged staff to remind residents about the city's efforts to improve energy efficiency in its own buildings and to communicate how funds are spent to this end. She thought that the CIP had improved over the years and was pleased with this document.

L. Payton recommended that Boulder protect, enhance and amplify its existing special places; it does not need to focus solely on the creation of new ones. Consider partnering with BVSD to make school yards special places that are available to community members, especially low income families.

Motion:

On a motion by **J. Putnam**, seconded by **B. Bowen**, the Planning Board voted 7-0 to recommend to City Council the 2015-2020 proposed Capital Improvement Program, including the list of CIP projects to undergo a Community and Environmental Assessment Process, as outlined in the staff memorandum dated July 25, 2014.

C. Public hearing to consider a recommendation to City Council on an ordinance amending Title 9, “Land Use Code,” B.R.C. 1981, to add a process for review of Concept Plans by City Council and to relax housing occupancy limitations for persons 62 years of age and older, implementing measures recommended as part of the city’s Comprehensive Housing Strategy.

Staff Presentation:

K. Guiler presented the item.

Board Questions:

K. Guiler answered questions from the board.

Public Hearing:

1. **Ruth Blackmore, 205 S. 41st Street**, raised some concerns about the proposed occupancy levels and parking impacts. Seniors will have cars. She thought that the housing stock should be preserved for young families.
2. **Jordan Mann, 710 31st Street**, supported the higher occupancy codes. He thought that the number of occupants, age and parking restrictions were arbitrary.
3. **Neshama Abraham, 1460 Quince Avenue (pooled with Nina Hyde Huoself, Mary Kirk and Douglas Thompson)**, supported the proposed ordinance but thought that 10 – 12 occupants was a more reasonable and realistic number.
4. **Cha Cha Spinrad, 710 31st Street**, thought that it was important to for seniors to be able to live in community. She encouraged the age limit to 55 to allow for a wider variety of ages.
5. **Harry R. Moody, 3870 Broadway #16**, the retired VP of AARP noted that Boulder will be dealing with this issues for the next 20 years. He provided a report from AARP and thought that this could be an opportunity to create a model.
6. **Lincoln Miller, 744 Marine Street**, spoke in favor of the ordinance but would like to see an age limit of 55 and a cap of at least 10 people. He felt that the proposed changes were too small.
7. **Ian Basinger, 430 45th Street**, would like to have more opportunities for affordable housing and spoke in support of the ordinance.
8. **Will Toor, 3032 10th Street**, spoke in support of the ordinance and on behalf of Better Boulder. This would provide the benefit of greater density without making large changes to Boulder’s neighborhoods. Seniors would have a low impact. He encouraged lowering the age to 55 and increasing the occupancy levels. The most functional group homes have around ten people. He noted that other states do not allow occupancy limits.

9. **Michelle Seipp, 906 McIntire Street**, works with aging demographics and noted that this will have huge impacts on the community. It is important to allow people to age in their own communities.

Board Comments:

Concept Plan:

C. Gray supported the change but was disappointed that neighborhoods had not been informed or involved in the process.

J. Gerstle supported it in concept but thought that there were other issues that needed to be addressed. He thought it would have significant impacts on the character of neighborhoods, Boulder's population and the general housing scheme.

B. Bowen supported this whole heartedly. He thought that the benefits to the communities and neighborhoods far outweighed any concerns over parking, etc.

A. Brockett agreed with **B. Bowen** and thought it was important to have more housing options. He did not think there would be many in the next five years, but that it would be important to have them as a tool. It would further the city's sustainability goals of affordability and GHG reduction through lowered energy use. It would also help seniors to avoid entering assisted living.

L. May agreed with the previous comments and thought there is an element of urgency to act in some fashion. The financial crisis has affected many seniors and it is important to provide options for that demographic.

J. Putnam strongly agreed with the concept. He thought it should be explored but may need some tailoring to determine how and at what rate to act, and how to handle the pushback from the neighborhoods. He did not want to threaten progress while trying to figure out how to do it right. He asked about the existing housing stock and noted that many people are being forced out because they do not have options.

L. Payton agreed with the notion of shared senior housing but thought that neighbors needed to be informed. Currently it is possible to have roommates but most seniors live alone. She thought that the cohousing housing option with greater than 6 people could be attractive. She expressed concern that this could inadvertently burden seniors by creating an opportunity for people to take advantage of them or by increasing their housing prices. She did not for this to become something marketed nationally; she suggested that the owner be required to live in the house.

C. Gray would like to see an evaluation at the end of the year, suggesting that staff create a database and include allowed occupancy rates on rental licenses. She recommended creating a separate zoning category to make it possible to build housing for larger groups. Assure that ADUs be considered separate units and allow for additional occupancy. One large house could be used for senior housing while a couple lives in the ADU.

J. Gerstle thought **L. Payton's** recommendation for owner occupancy could be a good way to address the commercialization of senior housing. He thought it would be good to address whether occupancy regulations are necessary.

L. May thought that occupancy limits would change over time to become more liberal. He was interested in better understanding the timeline.

J. Putnam encouraged the board to keep this ordinance change simple. The more expansive it becomes, the more complicated it will be. He cautioned that requiring the owner occupancy could create unintended consequences.

B. Bowen agreed with **J. Putnam**. He noted that many different people handle finances differently with age. It could get thorny quickly to tie occupancy to ownership.

Age Limitation of 62 Years:

B. Bowen noted that the ratio of women to men increases with age and found benefit in having a variety of age groups to provide different skill sets. He would advocate for lowering the age limit to 55 years, but thought it would still be worth doing if it would be too burdensome to lower the age from 62 years.

L. May explained that a more refined ordinance will be worked out; this is an intermediary step in the process.

J. Gerstle agreed that this is an evolutionary process. He would prefer a 55 year age limit, but would accept 62 years to ease the burden on staff.

J. Putnam did not think that the Fair Housing Act was intended to deal with this. It will need to stay on the agenda because a 62 year limit will not work in the long run.

B. Bowen thought that the occupancy rates should be removed in general to allow people to live in the way that they want to live. It would be much more natural.

C. Cray noted that CU houses few of their students. If occupancy limitations were eliminated, it could limit opportunities for seniors. She was inclined to change the age to 55, but would prefer that staff focus on other quick win code changes.

L. Payton would prefer a 55 year limit, but also supported 62 years because it would reduce the burden on staff. She thought it would be important to provide oversight or licensing to prevent people from taking advantage of seniors.

A. Brockett also felt troubled by the possibility of senior exploitation. Consider adding some form of oversight via the rental licensing for this category. He would prefer a 55 year limit but would rather have a quick win. He requested that City Council look into the possibility of 55.

Occupancy Limitation of 6:

L. May thought an occupancy limit of 8 or 10 was compelling but that it would be too big of a step to take without getting input from neighborhoods. He recommended devising a mechanism to ensure affordability for expanded occupancy. Consider adding a provision to revisit this in a year.

J. Gerstle thought that it would be important to include an assurance of quality if the occupancy numbers were relaxed. Assure that there is adequate room for occupants.

C. Gray would like to keep the allowed occupancy at 6 in the RL-1 zones and require owner occupancy. For new builds in the 14,000 sf lots, she would be willing to consider 10 – 12 occupants.

B. Bowen did not think that going up to 10 – 12 occupants would be a problem; it would create a very different format and pro forma. This allows for a different way of approaching life than just getting roommates. The more people there are, the more likely residents are to find the connections and community. He thought it was better to encourage more people to live in existing homes.

A. Brockett was in favor of allowing 10 to 12 occupants. He appreciated **C. Gray's** request for outreach to neighborhoods, but thought the fundamental outcry would be over parking. The advantages to the senior population would be so great that it would outweigh parking difficulties for neighbors.

J. Putnam would like to find some standards or limits such as limiting it to a subset of zoning districts. This applies to existing as well as new homes and he thought it would generate some pushback. He would rather start from a more modest base to ensure success.

B. Bowen noted that co-ops are dependent on having enough people to work; they need at least ten to be successful. Capping it at 6 will reduce our success.

L. Payton agreed with **C. Gray** that there should be a public process to vet this. She would support six because the original intention is for people who are in their own home to bring in roommates. She thought that co-ops should be dealt with separately.

L. May recommended that the board address the occupancy limits for co-ops in the near future and consider whether senior co-ops could be its own category. That would allow staff to do public outreach.

A. Brockett suggested that different zones have different limits.

C. Gray recommended that staff hold an open house to ask for public input on this process before it goes to City Council.

L. Payton feared that this could allow for a commercial operation in someone's neighborhood.

A. Brockett did not think commercial operations would be a viable option. He saw this as a bottom up opportunity.

L. May noted that this would allow for affordability. Even if it were a commercial operation, he would like to see a mechanism for ensuring affordability.

Neshama Abraham spoke at the board's request. Her project would dedicate 20% of the home to affordable rentals and 80% would be owned. This would qualify as owner occupied. The model would not work with six people.

C. Gray would feel comfortable with this if there were an owner occupied contingent.

A. Brockett noted that the owner occupied criteria could be difficult in the event that the owner would pass away while others were living in the property.

J. Putnam feared that the owner occupied requirement would force tenants to move out if the owner passed away. He did not think that this was a healthy outcome.

L. May suggested that the allowed occupancy rates be based on a factor of people per bedroom.

B. Bowen cited some concerns with this approach and requested that Council grapple with the other details. He would like to vote on the motion. If problems arise in the future, the board can address them then.

J. Gerstle expressed concern about living and amenities. He was amenable to **B. Bowen's** recommendation that standards be tied to the city of Boulder's liveability standards for affordable housing.

S. Richstone thought it would be reasonable to consider adding some form of liveability standard.

L. May thought affordability was important but thought it could be addressed in later Comprehensive Strategy planning.

B. Bowen explained that some people may want to have a high end shared house. People should not be excluded based on income because sharing provides affordability and sustainability.

A. Brockett doubted that seniors with economic means would choose to live in this type of scenario but noted that it would still provide relative affordability.

Motion:

On a motion by **A. Brockett**, seconded by **J. Putnam**, the Planning Board voted 6-1 (**L. Payton** opposed) to recommend approval to the City Council of an ordinance amending Title 9, "Land Use Code," B.R.C. 1981, to relax occupancy limitations for housing for persons 62 years of age or older and specifically to allow up to 6 persons 62 years of age or older in RL, RR and RE zones and up to 10 persons 62 years of age or older in the RR and RE zones provided that an owner of the home is a resident in the house.

A friendly amendment by **C. Gray**, accepted by **A. Brockett** requested that the planning staff perform outreach to neighborhoods and stakeholders and the that the results of that outreach be reported to City Council.

L. Payton supported the motion and amendment, but thought that all instances should be owner occupied.

C. Gray agreed with **L. Payton** but wanted to vote for it for an early win. She requested that a zoning map be included in Council's packet.

Part 2: Concept Plan:

Board Questions:

K. Guiler answered questions from the board.

Public Hearing:

1. **Adrian Sopher**, 1919 14th Street, did not think that the Council call up process took too long. If Council is concerned and interested, they should take over the review process from the Planning Board at Site Review. Do not subject applicants to four reviews.

Board Comments:

C. Gray understood Mr. Sopher's recommendation but did not think that it was appropriate for Council to take on the Site Review process. They rely on Planning Board's discretion.

L. Payton recommended that Council and Planning Board have joint meetings for some items, or that they appoint members to co-hear projects with the Planning Board.

B. Bowen liked **L. Payton's** idea to have joint meetings.

L. May thought there was an issue with that process. He did not think that Council rehearing a concept plan would create greater predictability.

A. Brockett agreed with **L. May**. If the boards had divergent opinions it would create a difficult scenario.

C. Gray would prefer joint meetings because it would create a more transparent process. Developers currently meet with Council members to discuss projects before they go before Council.

A. Brockett liked **L. Payton's** suggestion to allow Council to appoint members to co-hear certain projects with the Planning Board.

L. May recommended that applicants be given the opportunity to determine whether they would prefer whether Council would rehear a project.

C. Gray noted that Council counts on the Planning Board's expertise when reviewing projects.

B. Bowen noted that the board is trying to encourage applicants to have Concept Reviews but the expectations are getting more intense. This could make concept review harder and more expensive for applicants.

Motion:

On a motion by **C. Gray**, seconded by **L. Payton**, the Planning Board voted 5-2 (**A. Brockett** and **B. Bowen** opposed) to support the proposed change to allow City Council to review Concept Plans as amended by **L. Payton**.

On an amendment by **L. Payton**, seconded by **J. Gerstle**, the board voted 6-1 (**C. Gray** opposed) to consider joint Concept Plan hearings.

6. MATTERS FROM THE PLANNING BOARD, PLANNING DIRECTOR, AND CITY ATTORNEY

A. Envision East Arapahoe- Draft Vision Elements and Scenario Concepts was moved to a future meeting.

7. DEBRIEF MEETING/CALENDAR CHECK

The August 21st meeting will start at 5 p.m. **A. Brockett** cannot be there early so **B. Bowen** will chair the first hour.

8. ADJOURNMENT

The Planning Board adjourned the meeting at 11:50 p.m.

APPROVED BY

Board Chair

Date

DRAFT

MEMORANDUM

TO: Planning Board

FROM: Heidi Hansen, Floodplain and Wetlands Administrator

DATE: August 26, 2014

SUBJECT: **Call Up Item:** Wetland Permit (LUR2014-00056)
28th Street Multi-Use Path

This decision may be called up before Planning Board on or before **September 9, 2014**.

A wetland permit was approved by Public Works Development Review staff on August 25, 2014 for the construction of a multi-use trail along 28th Street between Iris and Yarmouth Avenues.

The applicant is applying for a standard wetland permit for the construction of a multi-use trail along 28th Street between Iris and Yarmouth Avenues. The proposed project will minimize impacts and provide restoration including reseeding and weed removal for any temporary impacts to city regulated wetlands at Fourmile Canyon Creek and Wonderland Creek.

Work will consist of constructing a new multi-use path along 28th Street in the wetlands buffer areas of Wonderland Creek and Fourmile Canyon Creek. Impacts to wetlands areas will be minimized by construction stormwater management practices and any temporary disturbances will be reseeded following construction. The applicant has demonstrated that wetland impacts have been minimized and the project meets the requirements of the city's Stream, Wetlands and Water Body Protection ordinance.

The wetland permit was approved by Public Works Development Review staff on August 25, 2014 and the decision may be called up before Planning Board on or before September 9, 2014. There is a Planning Board meeting within the 14 day call up period on **September 4, 2014**. A copy of the wetland permit is attached.

Questions about the project should be directed to the Floodplain and Wetlands Administrator, Heidi Hansen at 303-441-3273 or by e-mail at hansenh@bouldercolorado.gov.

Attachments:

A. [Wetland Permit](#)



CITY OF BOULDER
Planning and Development Services

1739 Broadway, Third Floor • P.O. Box 791, Boulder, CO 80306-0791
 phone 303-441-1880 • fax 303-441-4241 • web boulderplandevlop.net

Wetland Permit

Date Issued: 8/25/2014 **Expiration Date: August 24, 2017**
 (Pursuant to Subsection 9-3-9(k), B.R.C. 1981)

Permit Number: LUR2014-00056

Contact Information

DEBBIE RITTER
 CITY OF BOULDER PO BOX 791
 BOULDER, CO 80306
 303 441 3253

Project Information

Location: 3975 28TH ST

Legal Description: LOT 1 ELK PARK SUB

Description of Work: Construction of a 10' wide concrete multi-use path and pedestrian bridge on the west side of 28th street from Iris to Fourmile Creek. Wetland impacts limited to the intersetion of Fourmile Creek and 28th street.

Conditions of Approval

- The proposed project/activity is approved on the basis that it satisfies applicable requirements of Chapter 9-3-9, "Wetlands Protection," Boulder Revised Code 1981. Other wetland requirements as set forth in Chapter 9-3-9 which are not specifically outlined in the conditions of approval below remain applicable to this project/activity.
- The improvements shall be constructed to minimize and mitigate impacts to the existing wetlands in conformance with the conditions of the City of Boulder Wetland Permit issued for this project.
- The applicant shall obtain a site inspection and approval from the City of Boulder Floodplain and Wetlands Coordinator upon completion of the projects.
- Impacts to wetlands shall be minimized through the construction Best Management Practices outlined in the Wetland Report and Mitigation Plan submitted by Pinyon Environmental and dated July 22, 2014.

Inspections

To schedule an inspection, call 303-441-3280 and refer to your permit number (LUR2014-00056).

MEMORANDUM

TO: Planning Board
FROM: Elaine McLaughlin, Case Manager
DATE: August 25, 2014

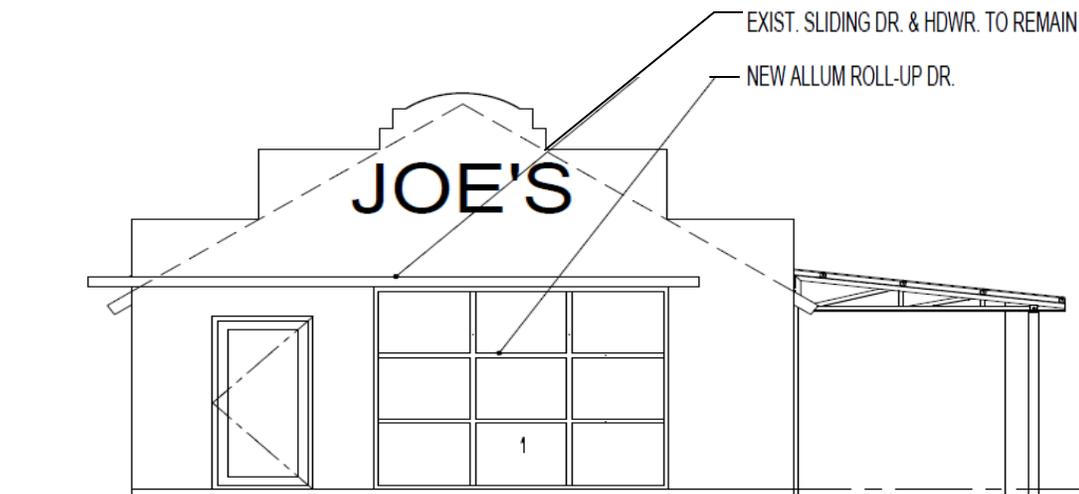
SUBJECT: **Call Up Item: 2250 Pearl Street.** Approval of an administrative Site Review and Use Review for the adaptive reuse of an existing single story building into a mixed use building with one residential unit at the rear and a juice shop within the Mixed Use – 3 (MU-3) zoning district. Site Review approval includes a 45 percent parking reduction and Use Review approval for restaurant over 1,000 square feet. LUR2014-00022 and LUR2014-00029



Background: The existing vacant building was built in 1924 as an auto repair and paint shop. Sometime between 1931 and 1960, the building was extended to the south to its current configuration. The building has been vacant for a number of years.

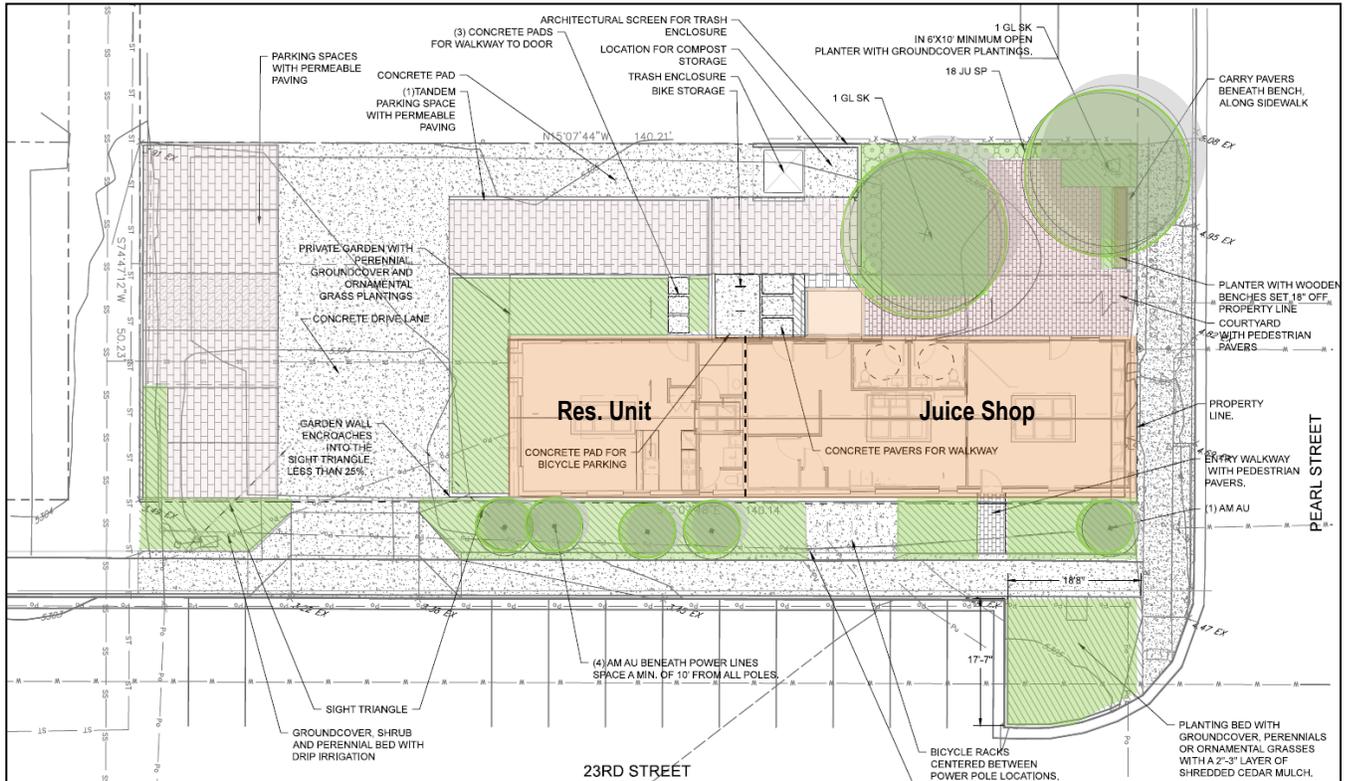


Proposed Project: The applicant is proposing to add 58 square feet onto the rear of the building; remodel the building and convert it into a juice shop in the front along and a small residential unit in the back. A small outdoor patio with a shed roof covering is proposed on the west side yard. The total floor area proposed is 1,969 square feet: 710 square feet for the residential and 1,260 square feet for the juice shop. The proposal also includes a request for a 45 percent parking reduction for the juice shop with a total of 6 parking spaces provided where 11 spaces are per standard. Because of the parking reduction request, Site Review is required, and because the juice shop (considered a restaurant use under the code) is in excess of 1,000 square feet in the MU-3 zoning district, a Use Review is required



Front (Pearl Street) Elevation of the Proposed Juice Shop

Address: 2250 Pearl Street



PROPOSED SITE PLAN

Project Analysis: The proposed 45 percent parking reduction was found to be consistent with the Site Review criteria, in particular with regard to land use code section 9-2-14(h)(2)(K), B.R.C. 1981, "Additional Criteria for Parking Reductions." Within the criteria is a specific criterion as follows (c) *The parking needs of any nonresidential uses will be adequately accommodated through on-street parking or off-street parking.*

To determine "adequate accommodation" of on- and off-street parking, the applicant provided a Trip Generation and Parking Study. The parking study concluded that there is an "abundance" of parking in the area and that,

"based on the parking utilization observations, there are typically anywhere between 10 and 11 on-street perpendicular parking spaces along the site frontage on 23rd Street. This should adequately accommodate the parking needs for this project. On the rare occasion that all of these parking spaces are filled, there are many other on-street parking areas within a block of the site that would likely be available."

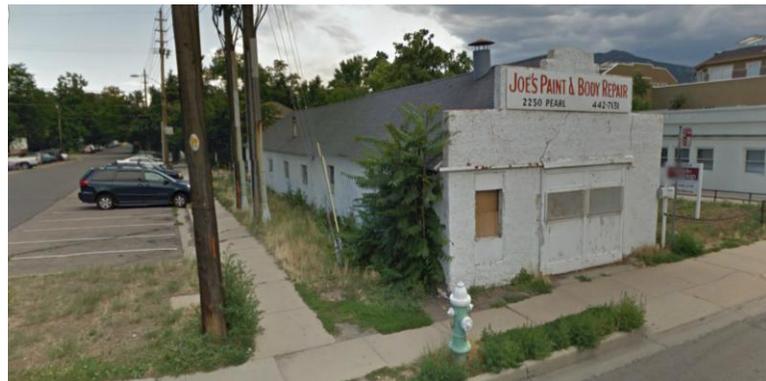
Shown to the right is an aerial photo illustrating the parking spaces on 23rd and Pearl streets. There are 11 on-street parking spaces on Pearl Street to the west of the site. The 11 on-street parking spaces located adjacent to the site have two-hour parking limits. The additional 11 across 23rd Street don't have a limit. Staff notes that with 51 on-street parking spaces located along 23rd

Address: 2250 Pearl Street



Street, and six parking spaces provided on-site; and the traffic study and staff observations that many of those spaces are not fully occupied at any given time, staff finds the application meets this criterion. With regard to the Use Review, findings were made that the juice shop meets the Use Review criteria in that a juice shop in this location “provides a direct service or convenience to the surrounding uses or neighborhood” as well as a “compatible transition between higher intensity and lower intensity uses.” Similarly, a finding was made that the proposed juice shop’s location, size, design, and operating characteristics will be reasonably compatible with, and have minimal negative impact on, the use of nearby properties.

Because of an existing overhead electrical utility and the narrowness of the right-of-way adjacent to the building on both 23rd and Pearl streets, the applicant is requesting modification of the streetscape standards. Similarly, due to the narrowness of the site and the size of the existing building, the applicant is requesting a modification to parking lot landscaping. The parking will be screened from the public right-of-way along Pearl Street, but the interior parking won’t be screened from the adjacent loading area and parking lot to the west or the alley. Despite the requested modifications, the applicant is proposing landscaping in other areas of the site that exceeds the standards, including along the east side of the site, adjacent to the building, and along the west side of the site within the proposed new patio area.



The applicant initially proposed to increase the height of the roof by approximately one foot, which would have necessitated a demolition permit given the age of the structure, built in 1923. Working with landmarks staff in an effort to preserve the building’s historic character, the applicant amended the plans limiting them to an interior retrofit of the roof’s structural support and a 58 square foot addition on the west. Considering that the proposed changes to the site are relatively minor and the proposed design respects and preserves the historic character of the building, staff found that the project overall meets the Site Review criteria without requiring that the applicant pursue designation of the building as a landmark. Any future modifications will be subject to the review standards for approved Site Review plans; and any potential for increase in height or size of the building will be limited due to lack of on-site parking.

Public Comment: Required public notice was provided in the form of written notifications of the application for Site Review to property owners within 600 feet of the subject property. In addition, a public notice sign was posted on the property. Therefore, all public notice requirements of section 9-4-3, “Public Notice Requirements,” B.R.C. 1981 were met. No comment letters or phone calls were received regarding the proposed project. A neighborhood meeting was held on-site on May 19, 2014 with five attendees all from the surrounding neighborhood. All of the attendees articulated support for the project, and one neighbor articulated concern about the parking reduction.

Conclusion: Staff finds that the application for a Minor Amendment meets the criteria of section 9-2-14(l), B.R.C. 1981. The proposal was *approved* by staff on **Aug. 25, 2014** and the decision may be called up before Planning Board on or before **Sept. 2, 2014**. There is one Planning Board hearings scheduled during the required 14 day call-up period on **August 28, 2014**. Questions about the project or decision should be directed to the Case Manager, Elaine McLaughlin at (303) 441-4130 or at the following email: mclaughline@bouldercolorado.gov.

Address: 2250 Pearl Street



CITY OF BOULDER
Community Planning & Sustainability

1739 Broadway, Third Floor • P.O. Box 791, Boulder, CO 80306-0791
phone 303-441-1880 • fax 303-441-3241 • web www.bouldercolorado.gov

CITY OF BOULDER PLANNING DEPARTMENT
NOTICE OF DISPOSITION

You are hereby advised that the following action was taken by the Planning Department based on the standards and criteria of the Land Use Regulations as set forth in Chapter 9-2, B.R.C. 1981, as applied to the proposed development.

DECISION:	APPROVED WITH CONDITIONS
PROJECT NAME:	2250 PEARL MIXED USE
DESCRIPTION:	SITE AND USE REVIEW to add a 58 square foot additon to an existing structure, remodel of existing structure to convert an existing auto garage into a 710 square foot residential unit and 1,260 square foot commercial space for a restaurant (juice shop). Site redeveloment includes the establishment of six parking spaces (request for 45 percent parking reduction), enclosure for trash and recycling, a bike parking area, and new landscaping.
LOCATION:	2250 PEARL ST
COOR:	N03W05
LEGAL DESCRIPTION:	Lot 1, Block 77, EAST BOULDER, City of Boulder, County of Boulder, State of Colorado
APPLICANTS:	James Truitt/Glenn Rappaport
OWNER:	PASHASANA LLC
APPLICATION:	Site Review No. LUR2014-00022 and Use Review No. LUR2014-00029
ZONING:	MU-3
CASE MANAGER:	Elaine McLaughlin
VESTED PROPERTY RIGHT:	NO; the owner has waived the oppportunity to create such right under Section 9 2-19, B.R.C. 1981.

APPROVED MODIFICATIONS FROM LAND USE REGULATIONS:

- 9-9-6, B.R.C. Parking Standards
- 9-9-13, B.R.C. Streetscape Design Standards
- 9-9-14, B.R.C. Parking Lot Landscape Standards

FOR CONDITIONS OF APPROVAL, SEE THE FOLLOWING PAGES OF THIS DISPOSITION.

Approved on: AUG. 25, 2014
Date

By: [Signature]
David Driskell, Executive Director of Community Planning and Sustainability

This decision may be appealed to the Planning Board by filing an appeal letter with the Planning Department within two weeks of the decision date. If no such appeal is filed, the decision shall be deemed final fourteen days after the date above mentioned.

Appeal to Planning Board expires: SEPT. 8, 2014

Address: 2250 Pearl Street

IN ORDER FOR A BUILDING PERMIT APPLICATION TO BE PROCESSED FOR THIS PROJECT, A SIGNED DEVELOPMENT AGREEMENT AND FINAL PLANS FOR CITY SIGNATURE MUST BE SUBMITTED TO THE PLANNING DEPARTMENT WITH DISPOSITION CONDITIONS AS APPROVED SHOWN ON THE FINAL PLANS, IF THE DEVELOPMENT AGREEMENT IS NOT SIGNED WITHIN NINETY (90) DAYS OF THE FINAL DECISION DATE, THE PLANNING DEPARTMENT APPROVAL AUTOMATICALLY EXPIRES.

Pursuant to Section 9-2-12 of the Land Use Regulations (Boulder Revised Code, 1981), the applicant must begin and substantially complete the approved development within three years from the date of final approval. Failure to "substantially complete" (as defined in Section 9-2-12) the development within three years shall cause this development approval to expire.

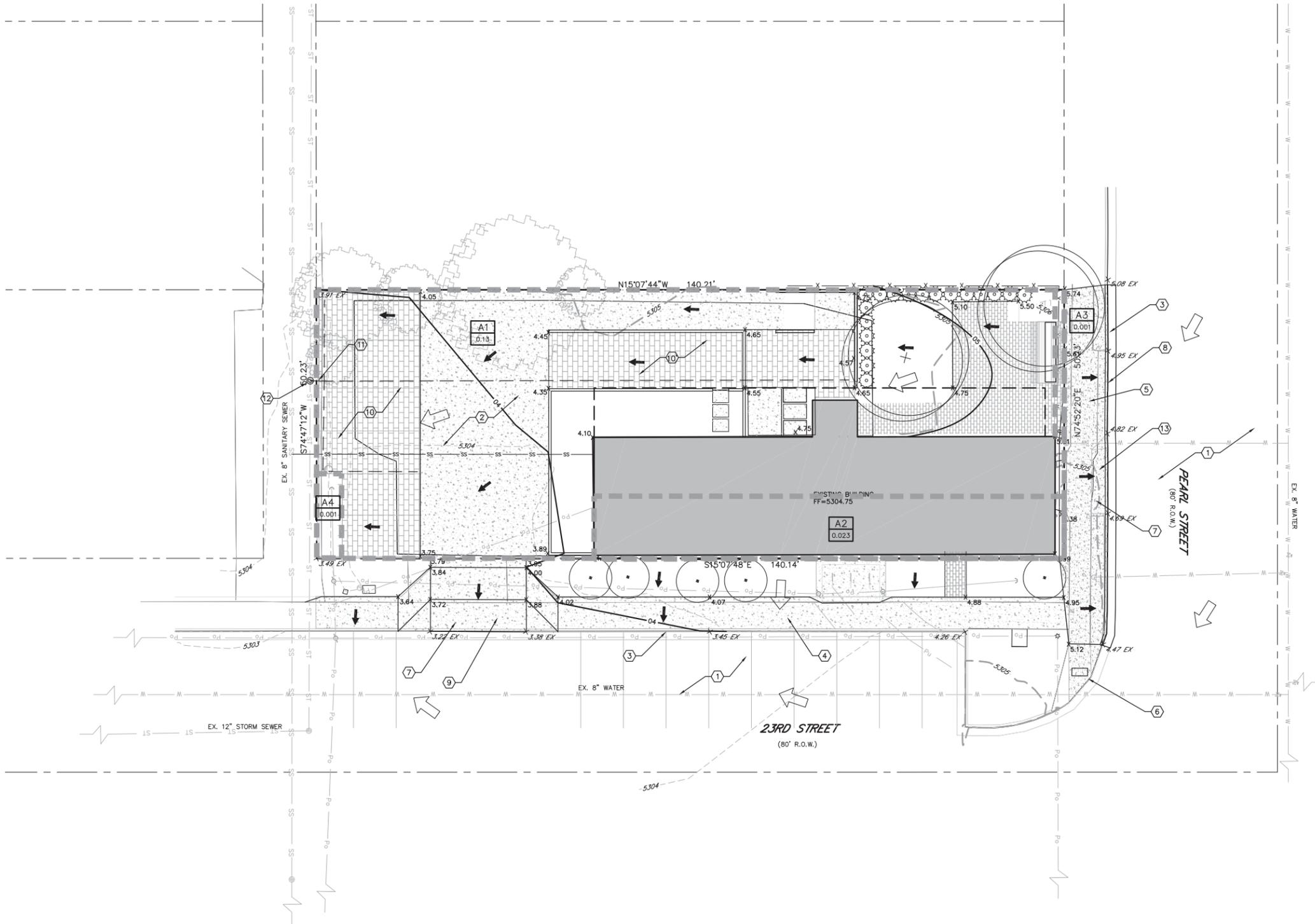
USE REVIEW - CONDITIONS OF APPROVAL

1. The Applicant shall ensure that the **development shall be in compliance with all final plans dated August 4, 2014** and the Management Plan dated June 14, 2014 on file in the City of Boulder Planning Department, except to the extent that the development may be modified by the conditions of this approval. Further, the Applicant shall ensure that the approved use is operated in compliance with the following restrictions:
 - a. Size of the restaurant shall be limited to 1,260 square feet. The seating area in the restaurant shall have a maximum of 24 indoor seats and a maximum of 12 exterior patio seats.
 - i. The restaurant shall be closed from 10:00 p.m. to 6:00 a.m., seven days per week.
 - b. Trash and bottles shall not be removed to outside trash containers between the hours of 10:00 p.m. and 8:00 a.m.
 - c. No outdoor speakers or outdoor amplified sound shall be permitted.
2. The Applicant **shall not expand or modify the approved use**, except pursuant to Subsection 9-2-15(h), B.R.C. 1981.

SITE REVIEW - CONDITIONS OF APPROVAL

1. The Applicant shall ensure that the **development shall be in compliance with all final plans dated August 4, 2014** on file in the City of Boulder Planning Department, except to the extent that the development may be modified by the conditions of this approval.
2. Prior to a building permit application, the Applicant shall submit a Technical Document Review application for the following items, subject to the approval of the City Manager:
 - a. A **final storm water report and plan** meeting the City of Boulder Design and Construction Standards.

- b. **Final transportation plans** meeting the City of Boulder Design and Construction Standards and CDOT Access Code Standards, for all transportation improvements. These plans must include, but are not limited to: street plan and profile drawings, signage and striping plans in conformance with Manual on Uniform Traffic Control Devices (MUTCD) standards, transportation detail drawings, and geotechnical soils report.
 - c. A **detailed landscape plan**, including size, quantity, and type of plants existing and proposed; type and quality of non-living landscaping materials; any site grading proposed; and any irrigation system proposed, to insure compliance with this approval and the City's landscaping requirements. Removal of trees must receive prior approval of the Planning Department. Removal of any tree in City right of way must also receive prior approval of the City Forester.
3. Prior to building permit application, the Applicant shall submit a **financial guarantee, in a form acceptable to the Director of Public Works**, in an amount equal to the cost of providing eco-passes to the employees of the development for three years after the issuance of a certificate of occupancy.



LEGEND

- E — E — E — EXISTING UNDERGROUND POWER LINE
- OW — OW — OW — EXISTING OVERHEAD UTILITY LINE
- G — G — G — EXISTING GAS LINE
- SS — SS — SS — EXISTING SANITARY SEWER W/MANHOLE
- W — W — W — EXISTING WATER W/FIRE HYDRANT
- ST — ST — ST — EXISTING STORM SEWER W/MANHOLE
- ST — ST — ST — PROPOSED STORM SEWER W/MANHOLE
- 5.305 — — — EXISTING CONTOUR
- ⊗ EXISTING WATER VALVE
- ⊗ EXISTING WATER METER
- ⊗ EXISTING POWER POLE
- x 25.8 PROPOSED SPOT ELEVATION
ADD 5300 TO ALL SPOT ELEVATIONS
- x 25.8 EX EXISTING SPOT ELEVATION
- FF FINISHED FLOOR ELEVATION
- ➔ PROPOSED FLOW DIRECTION
- ➔ HISTORIC SHEET FLOW

KEYED NOTES

1. EXISTING ASPHALT PAVEMENT TO REMAIN.
2. EXISTING CONCRETE PAVEMENT TO BE REMOVED AND REPLACED.
3. EXISTING CURB AND GUTTER TO REMAIN.
4. EXISTING CONCRETE SIDEWALK TO BE REMOVED AND REPLACED WITH 6" WIDE ATTACHED SIDEWALK.
5. EXISTING CONCRETE SIDEWALK TO BE REMOVED AND REPLACED WITH A VARIABLE WIDTH ATTACHED SIDEWALK FROM THE BACK OF CURB TO THE FACE OF THE BUILDING TO MATCH WIDTH AT ADJACENT PROPERTY TO THE WEST.
6. PROPOSED STANDARD CITY OF BOULDER CURB RAMP.
7. EXISTING DRIVE RAMP TO BE REMOVED.
8. PROPOSED STANDARD CITY OF BOULDER CURB AND GUTTER. REMOVE AND REPLACE APPROXIMATELY 47 FEET FROM THE WEST PROPERTY LINE.
9. PROPOSED STANDARD CITY OF BOULDER DRIVE RAMP.
10. PROPOSED PERMEABLE PAVERS. PAVERS WILL BE PLACED WITH 2" BEDDING COURSE, 12" RESERVOIR COURSE, AND 6" FILTER MATERIAL, ALONG WITH GEOMEMBRANE LINER, PER UDFCD STANDARDS.
11. PROPOSED STORM SEWER.
12. PROPOSED STORM SEWER MANHOLE.
13. EXISTING CONCRETE PAN TO BE REMOVED.

GRADING NOTES

1. GRADE AWAY FROM BUILDINGS AT A MINIMUM 10% SLOPE IN THE FIRST 10 FEET AT LANDSCAPE AREAS AND AT A MINIMUM 2% SLOPE IN THE FIRST 10 FEET AT IMPERVIOUS AREAS, EXCEPT AS NOTED.
2. EXISTING SITE IMPERVIOUSNESS IS 69.52%, PROPOSED SITE IMPERVIOUSNESS IS 51.38%
3. TOP OF FOUNDATION ELEVATION SHALL BE SET AT LEAST 0.8' ABOVE THE PROPOSED GRADING AT THE EXTERIOR OF THE FOUNDATION AT LANDSCAPE AREAS.
4. THE FEMA FLOOD INSURANCE RATE MAP, PANEL 0801300394J, DATED DECEMBER 18, 2012, INDICATES THAT THE ENTIRE SITE IS OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN.
5. ALL CITY UTILITIES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF BOULDER STANDARD SPECIFICATIONS.
6. THE LOCATION OF THE ABOVE GROUND UTILITIES SHOWN HEREON ARE BASED ON THE TOPOGRAPHIC SURVEY BY SCOTT, COX AND ASSOCIATES, ON 11/06/13. THE LOCATIONS OF THE UNDERGROUND UTILITIES SHOWN HEREON ARE BASED ON SAID SURVEY AND INFORMATION PROVIDED BY OTHERS (WHICH MAY INCLUDE THE UTILITY OWNER OR UTILITY LOCATING SERVICES). SCOTT, COX & ASSOCIATES, INC. IS NOT RESPONSIBLE FOR UTILITY INFORMATION PROVIDED BY OTHERS. SCOTT, COX & ASSOCIATES, INC. RECOMMENDS THAT THE LOCATION OF THE UTILITIES BE FIELD VERIFIED PRIOR TO ANY DIGGING ON, OR ADJACENT TO THE SUBJECT PROPERTY.
7. BENCH MARK: CITY OF BOULDER B-1-1; FOUND CHISELED "X" AT THE INTERSECTION OF THE SIDEWALK AND WESTERLY FIRE LANE, LOCATED AT THE NORTHWEST CORNER OF THE INTERSECTION OF 22ND STREET AND WALNUT STREET. ELEVATION = 5307.42 NAVD88.

**PRELIMINARY
GRADING, DRAINAGE AND
EROSION CONTROL PLAN
2250 PEARL STREET
BOULDER, COLORADO**

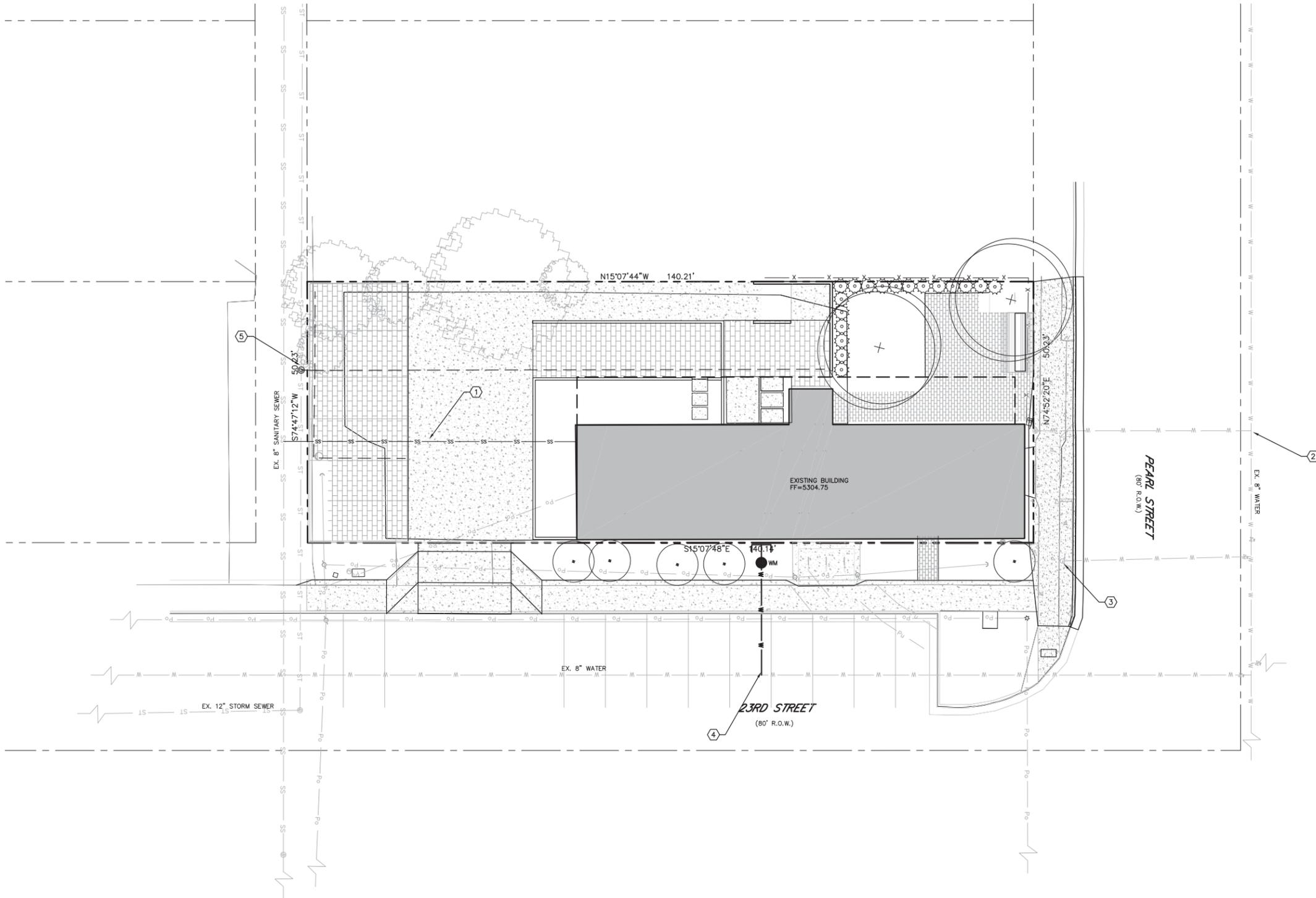
SCOTT, COX & ASSOCIATES, INC.
consulting engineers • surveyors
1530 55th Street • Boulder, Colorado 80303
(303) 444 - 3051



Scale: 1" = 10'



Designed by	MRF	Date	6/16/14	Scale	1"=10'	Drawing no.	13550B-2	Sheet	C1.01
Drawn by	JAS	Revision	1	Description	CITY COMMENTS	Date	7/28/14	Project no.	13550B
Checked by	DPA	Revision	1	Description	CITY COMMENTS	Date	7/28/14	Project no.	13550B



LEGEND

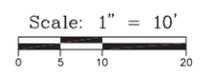
- Fo — Fo — EXISTING FIBER OPTIC
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- - - - - EXISTING CONTOUR
- ⊕ EXISTING WATER VALVE
- ⊕ EXISTING WATER METER
- ⊕ EXISTING POWER POLE
- ⊕ PROPOSED WATER VALVE
- ⊕ PROPOSED WATER METER

KEYED NOTES ①

1. EXISTING SANITARY SEWER SERVICE TO REMAIN.
2. EXISTING 3/4" DOMESTIC WATER SERVICE TO REMAIN AND BE USED AS IRRIGATION SERVICE.
3. EXISTING FIRE HYDRANT TO REMAIN.
4. PROPOSED 1" DOMESTIC WATER SERVICE.
5. PROPOSED STORM SEWER AND MANHOLE FOR UNDERDRAIN SYSTEM.

UTILITY NOTES

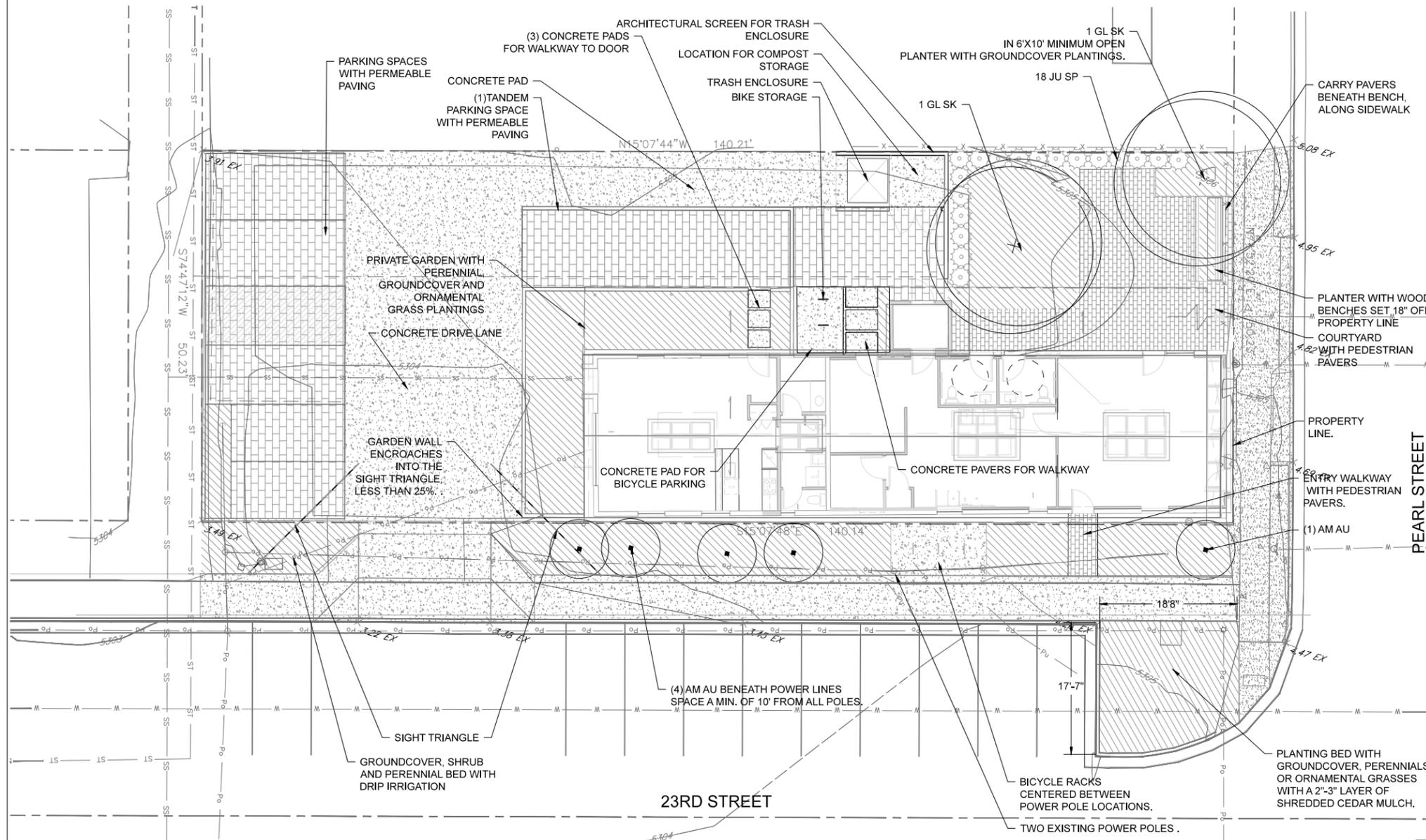
1. ALL CITY UTILITIES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF BOULDER STANDARD SPECIFICATIONS.
2. WATER, FIRELINE, SEWER TAPS, AND SERVICE LINE SIZES SHALL BE DETERMINED AT THE TIME OF BUILDING PERMIT APPLICATION.
3. ALL NEW WATER AND SANITARY SEWER SERVICE TAPS TO EXISTING MAINS SHALL BE MADE BY CITY CREWS AT THE DEVELOPER'S EXPENSE.
4. ALL CONNECTIONS TO EXISTING UTILITIES SHALL BE DONE IN A WAY SO AS TO MINIMIZE DISRUPTION IN SERVICE TO EXISTING USERS.
5. THE LOCATION OF THE ABOVE GROUND UTILITIES SHOWN HEREON ARE BASED ON THE TOPOGRAPHIC SURVEY BY SCOTT, COX AND ASSOCIATES, ON 11/06/13. THE LOCATIONS OF THE UNDERGROUND UTILITIES SHOWN HEREON ARE BASED ON SAID SURVEY AND INFORMATION PROVIDED BY OTHERS (WHICH MAY INCLUDE THE UTILITY OWNER OR UTILITY LOCATING SERVICES). SCOTT, COX & ASSOCIATES, INC. IS NOT RESPONSIBLE FOR UTILITY INFORMATION PROVIDED BY OTHERS. SCOTT, COX & ASSOCIATES, INC. RECOMMENDS THAT THE LOCATION OF THE UTILITIES BE FIELD VERIFIED PRIOR TO ANY DIGGING ON, OR ADJACENT TO THE SUBJECT PROPERTY.
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**PRELIMINARY
UTILITY PLAN
2250 PEARL STREET
BOULDER, COLORADO**

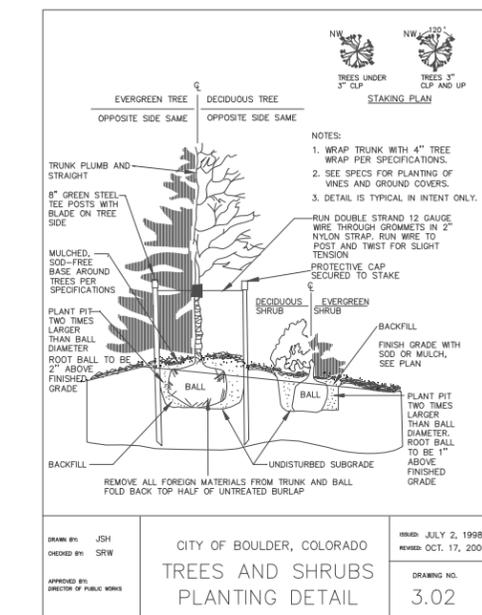
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Designed by	MRF	Date	6/16/14	Scale	1"=10'	Drawing no.	13550B-2	Sheet	C1.02
Drawn by	JAS	Revision	1	Description	CITY COMMENTS	Date	7/28/14	Project no.	13550B
Checked by	DPA								



MATERIAL LEGEND

- CONCRETE UNIT PAVER 1 : ECO-PRIORA PAVER FROM PAVESTONE OR APPROVED EQUAL WITH 40% EFFECTIVE IMPERVIOUSNESS
- CONCRETE UNIT PAVER 2 : VERONA PLANK PAVERS FROM PAVESTONE OR APPROVED EQUAL
- PERENNIAL, SHRUB OR GROUNDCOVER PLANTING BED WITH MULCH AND DRIP IRRIGATION TO EACH PLANT.
- CAST IN PLACE CONCRETE



GENERAL & IRRIGATION NOTES

1. ALL TREES SHALL BE PLANTED AWAY FROM UTILITY LINES IN ACCORDANCE WITH CITY OF BOULDER UTILITY SPACING REQUIREMENTS.
2. ALL PLANT BEDS TO HAVE DRIP IRRIGATION WITH EMITTERS RUN TO ALL PERENNIALS, GROUNDCOVER, TREES, AND SHRUBS.
3. ALL PLANT BEDS TO HAVE A 2-3" LAYER OF SHREDDED CEDAR MULCH UNLESS OTHERWISE NOTED ON THE PLAN. WEED BARRIER FABRIC SHALL NOT BE USED.
4. USE A PLANTERS MIX SOIL BLEND IN ALL BEDS CONTAINING A TOP SOIL, HUMUS, COMPOST AND BARK FINES BLEND.
5. ALL PLANT MATERIAL SHALL MEET OR EXCEED CURRENT AMERICAN STANDARD FOR NURSERY STOCK ANSI Z60.1 AND THE COLORADO NURSERY ACT AND ACCOMPANYING RULES AND REGULATIONS.
6. PRIOR TO ANY HARDSCAPE CONSTRUCTION, PROVIDE SLEEVING FOR IRRIGATION CONDUIT TO ALL STREET TREES AND PLANTERS.
7. ALL PLANT BEDS TO HAVE 100% IRRIGATION COVERAGE WITH EMITTERS RUN TO EACH PLANT.
8. ALL PLANT MATERIAL TO BE APPROVED BY A REPRESENTATIVE OF R DESIGN A MINIMUM OF 48 HOURS PRIOR TO INSTALLATION.
9. FOLLOW MANUFACTURERS INSTALLATION INSTRUCTIONS ON ALL PAVERS.
10. REFER TO CIVIL PLANS FOR ALL GRADING AND DRAINAGE DESIGN.

RECOMENDED SOILS TESTING NOTES

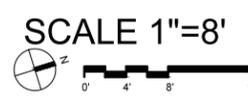
- SOIL ANALYSIS, BULK DENSITY TESTING AND REMEDIATION REQUIRED WHEN REMOVING HARDSCAPE TO PLANT TREES, INCLUDING BUT NOT LIMITED TO CONCRETE, ASPHALT, PAVERS AND BRICK.
- I. DEEP 24" SOIL ANALYSIS REQUIRED FROM CREDITED, LOCAL SOIL ANALYSIS LABORATORY WITH EXPERIENCE IN LOCAL URBAN SOILS.**
1. ANALYSIS MUST DETERMINE SOIL TEXTURE AND TYPE, PH BALANCE, FREE LIME, SOIL SALINITY, ORGANIC MATTER (OM) CONTENT AND AVAILABLE PLANT NUTRIENTS.
 2. SOIL REMEDIATION BASED UPON SOIL ANALYSIS.
 - a. ADD SOIL AMENDMENTS PER SOIL ANALYSIS RESULT AND TILL TO A DEPTH OF 18"
 - b. ADD ORGANIC MATTER TO ENSURE A MINIMUM OF AT LEAST 3% FOR NATIVE SOIL.
- II. SOIL BULK DENSITY TESTING REQUIRED FROM CREDITED, LOCAL SOIL ANALYSIS LABORATORY WITH EXPERIENCE IN LOCAL URBAN SOILS. REMEDIATION MAY BE REQUIRED AS DETERMINED BY SOILS TYPE.**

LANDSCAPE PLAN

NUMBER OF EXISTING TREES : 0
NUMBER OF TREES REMOVED : 0
NUMBER OF PROPOSED R.O.W. TREES : 2
NUMBER OF PROPOSED SITE TREES : 7

PLANT LEGEND

SYMBOL	ABBREV.	#	COMMON NAME	BOTANICAL NAME	SIZE	SPACING
	AM AU	5	AUTUMN BRILLIANCE SERVICEBERRY	<i>Amelanchier grandiflora</i>	10'12" MULTI-STEM	SEE PLAN
	GL SK	2	SKYLINE HONEYLOCUST	<i>Gleditsia triacanthos inermis</i>	2.5" CALIPER	SEE PLAN
	JU SP	18	SPARTAN JUNIPER	<i>Juniperus chinensis 'Spartan'</i>	8' TALL	30" O.C.



CRITICAL NOTE:

THIS PLAN IS FOR SUBMITTAL PURPOSES ONLY. R DESIGN IS NOT RESPONSIBLE FOR ANYTHING CONSTRUCTED USING THESE DRAWINGS AS A REFERENCE.

LANDSCAPE CALCULATIONS

TOTAL LOT AREA	7004 SF
BUILDING FOOTPRINT	1900 SF
TOTAL LANDSCAPE AREA	1813 SF
TOTAL PARKING LOT SIZE	2475 SF
USABLE OPEN SPACE	2267 SF
TOTAL NUMBER OF PARKING STALLS REQUIRED	11
TOTAL NUMBER OF PARKING STALLS PROVIDED	6
TOTAL INTERIOR PARKING LOT LANDSCAPED AREA REQUIRED	0
TOTAL INTERIOR PARKING LOT LANDSCAPED AREA PROVIDED	0
TOTAL PERIMETER PARKING LOT LANDSCAPING REQUIRED	0
TOTAL PERIMETER PARKING LOT LANDSCAPING PROVIDED	0
TOTAL NUMBER OF STREET TREES REQUIRED	5
TOTAL NUMBER OF STREET TREES PROVIDED	1 LARGE AND 5 SMALL TREES PROVIDED, DUE TO EXISTING UTILITY CONDITIONS, WE ARE REQUESTING A MODIFICATION TO THE STREET TREE REQUIREMENTS
TOTAL QUANTITY OF PLANT MATERIAL PROVIDED	25 TREES, 1818 SF OF GROUNDCOVER AND PERENNIAL BEDS.

ORGANIC SOIL ADMENDMENT SPECIFICATIONS

PLANTERS MIX SUPPLIED FROM PERMAGREEN ORGANICS 303.424.7291

TOP SOIL = 10%
HUMUS = 30%
COMPOST = 40%
BARK FINES = 20%

2250 PEARL STREET
BOULDER, CO

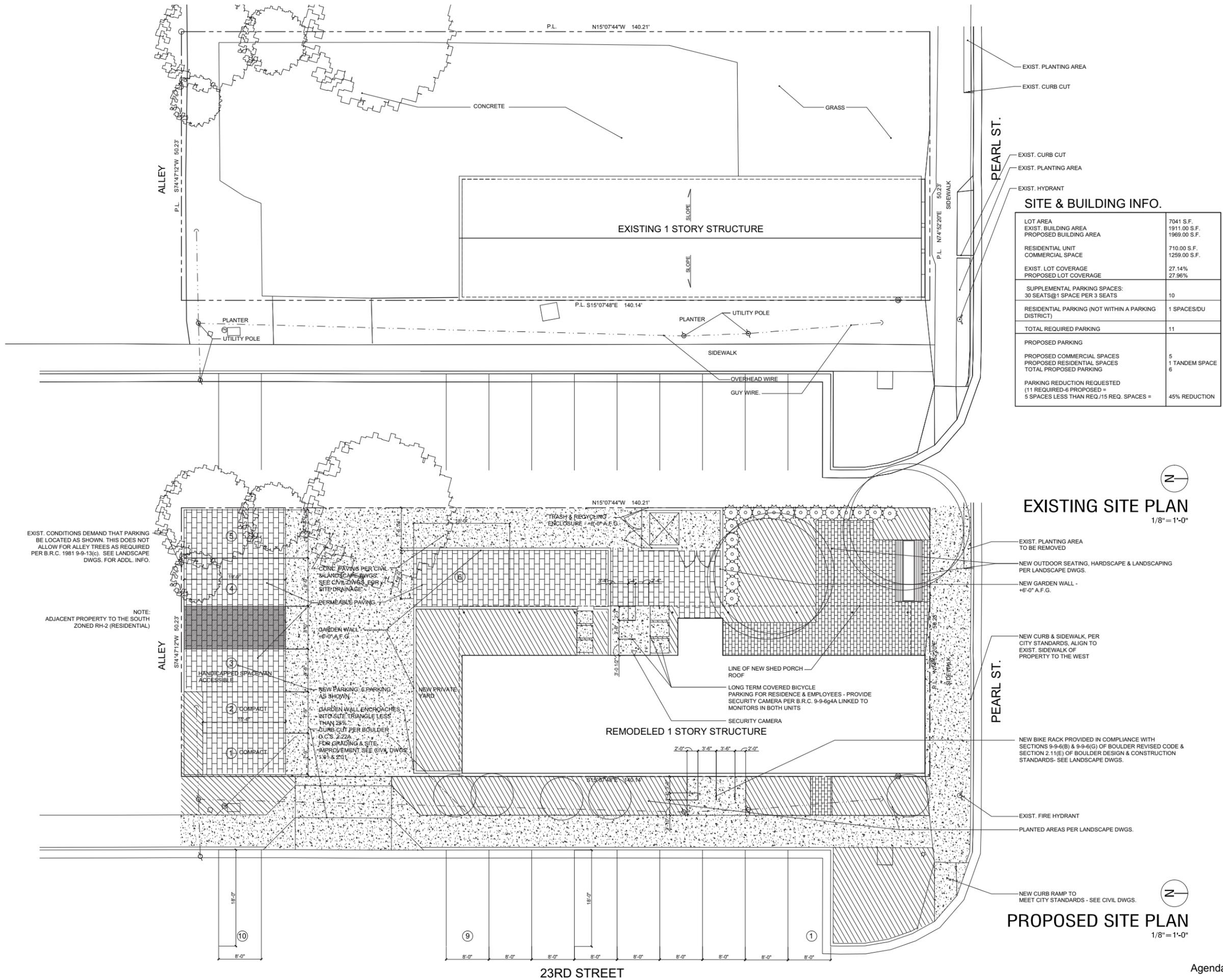
DRAWN BY: RB
CHECKED BY: RM

DATE: JULY 24, 2014
AUGUST 4, 2014 RESUBMITTAL #1

SHEET TITLE: LANDSCAPE PLANTING PLAN

SHEET NO.:

JOE'S
 2250 PEARL ST.
 BOULDER, CO
 80302



SITE & BUILDING INFO.

LOT AREA	7041 S.F.
EXIST. BUILDING AREA	1911.00 S.F.
PROPOSED BUILDING AREA	1969.00 S.F.
RESIDENTIAL UNIT	710.00 S.F.
COMMERCIAL SPACE	1259.00 S.F.
EXIST. LOT COVERAGE	27.14%
PROPOSED LOT COVERAGE	27.96%
SUPPLEMENTAL PARKING SPACES: 30 SEATS@1 SPACE PER 3 SEATS	10
RESIDENTIAL PARKING (NOT WITHIN A PARKING DISTRICT)	1 SPACES/DU
TOTAL REQUIRED PARKING	11
PROPOSED PARKING	
PROPOSED COMMERCIAL SPACES	5
PROPOSED RESIDENTIAL SPACES	1 TANDEM SPACE
TOTAL PROPOSED PARKING	6
PARKING REDUCTION REQUESTED (11 REQUIRED-6 PROPOSED = 5 SPACES LESS THAN REQ./15 REQ. SPACES =	45% REDUCTION

EXISTING SITE PLAN
 1/8" = 1'-0"

PROPOSED SITE PLAN
 1/8" = 1'-0"

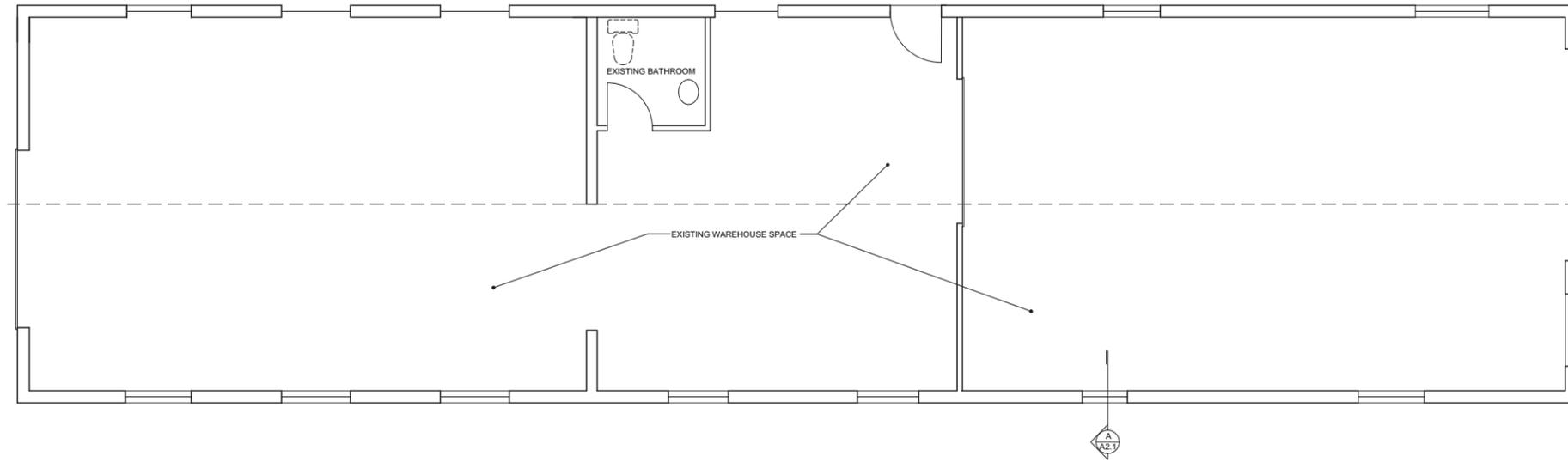
date	issue
6-16-14	PLAN REVIEW
8-4-14	CITY COMMENTS

project no:
 drawn by:
EXISTING SITE PLAN
PROPOSED SITE PLAN

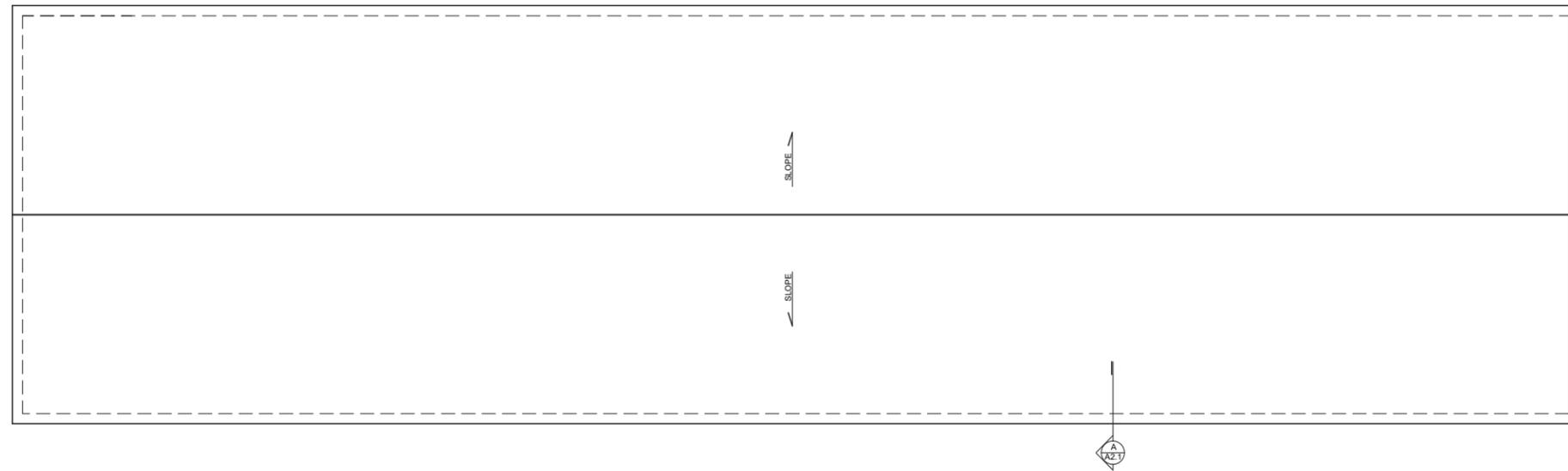
black shack office
 glenn rapaport
 po box 1847 boulder, co 81621
 970 976 0663 f
 970 379 3084 t

A1.0

JOE'S
 2250 PEARL ST.
 BOULDER, CO
 80302



AS-BUILT FLOOR PLAN
 1/4"=1'-0"



AS-BUILT ROOF PLAN
 1/4"=1'-0"

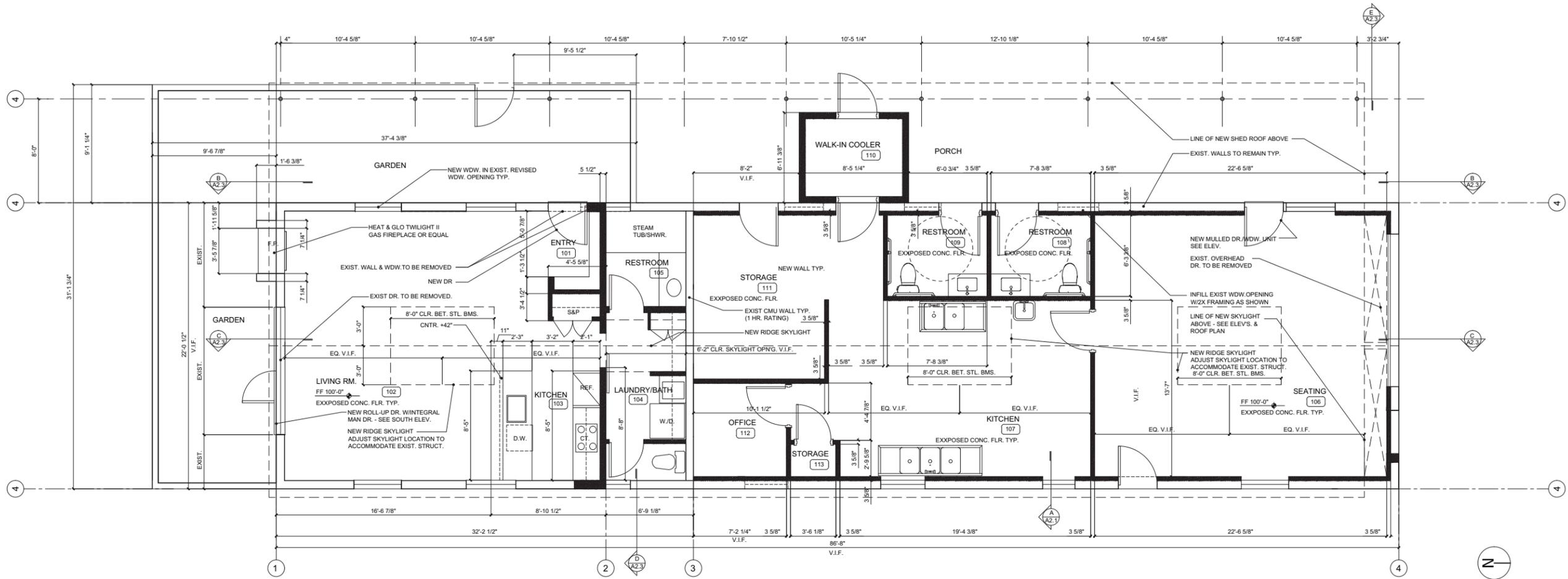
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9-4-13	REVIEW
9-13-13	REVIEW
9-19-13	REVIEW
2-27-14	PLANNING REVIEW
8-4-14	CITY COMMENTS

project no:
 drawn by:

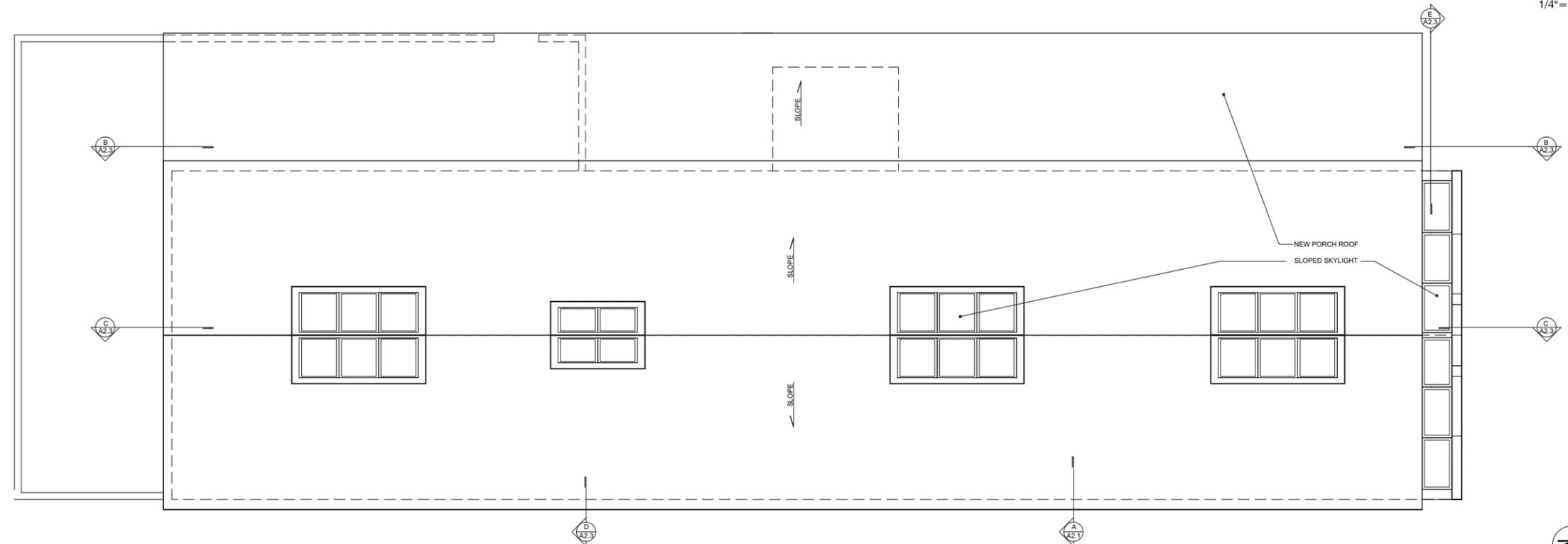
AS-BUILT PLANS

black shack office
 500 Pearl St.
 800 Box 1847 Boulder, CO 81621
 970 927 0635 T
 970 379 3698 F

A1.1



PROPOSED FLOOR PLAN
 1/4" = 1'-0"



PROPOSED ROOF PLAN
 1/4" = 1'-0"

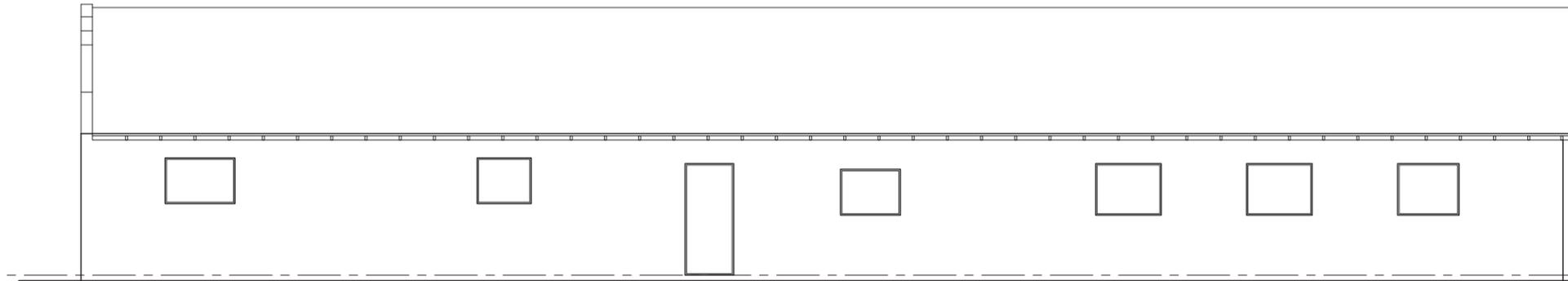
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9-13-13	REVIEW
9-19-13	REVIEW
2-27-14	PLANNING REVIEW
3-17-14	SKYLIGHT RELOCATION
8-4-14	CITY COMMENTS

project no:
 drawn by:

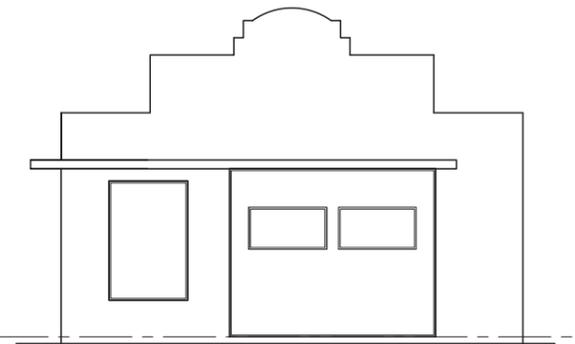
PROPOSED PLANS

black shack office
 500 Pearl St. Suite 100
 Boulder, CO 80502
 970.927.0635
 970.379.3698

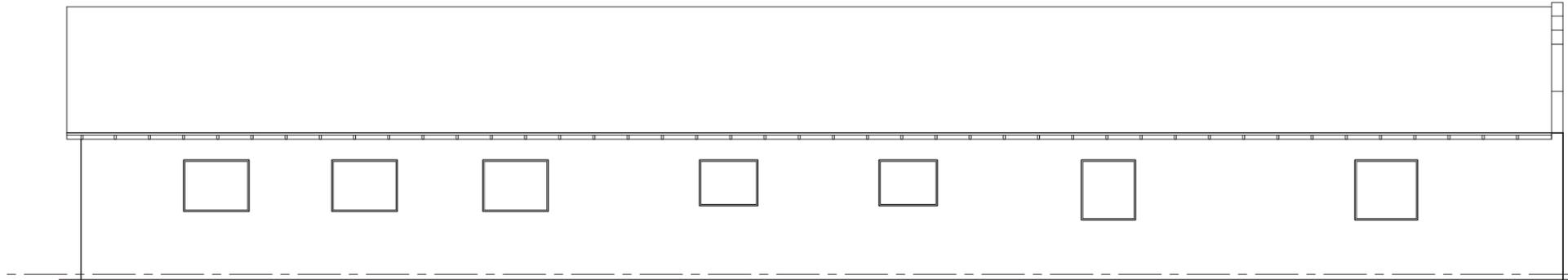
JOE'S
 2250 PEARL ST.
 BOULDER, CO
 80302



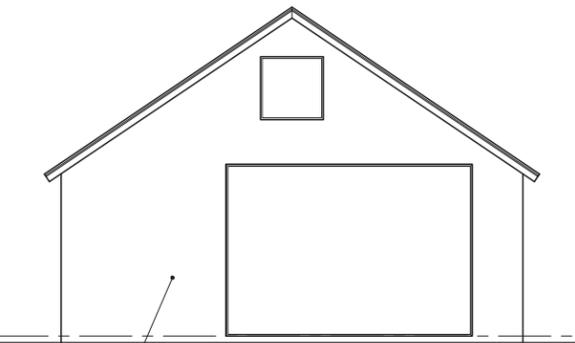
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NORTH

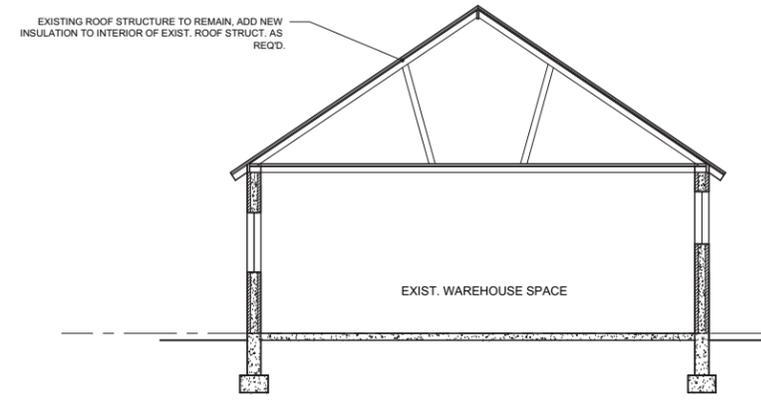


EAST



SOUTH

STUCCO TYP.



EXISTING ROOF STRUCTURE TO REMAIN, ADD NEW INSULATION TO INTERIOR OF EXIST. ROOF STRUCT. AS REQD.

EXIST. WAREHOUSE SPACE

SECTION A

date	issue
9-4-13	REVIEW
9-13-13	REVIEW
9-19-13	REVIEW
2-27-14	PLANNING REVIEW
8-4-14	CITY COMMENTS

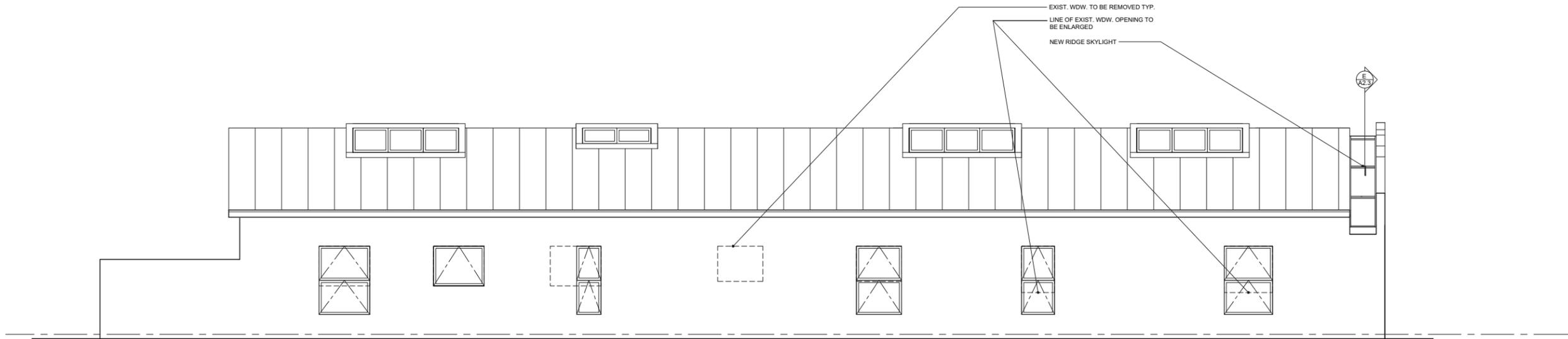
project no:
 drawn by:

AS-BUILT
 ELEVATIONS
 SECTION

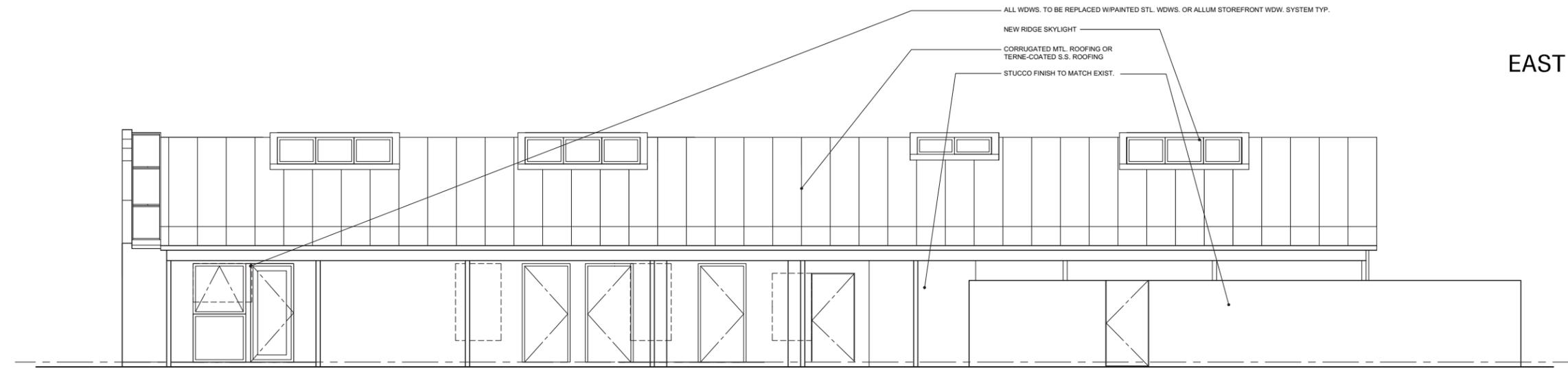
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 970 927 0635 t
 970 379 3698 f

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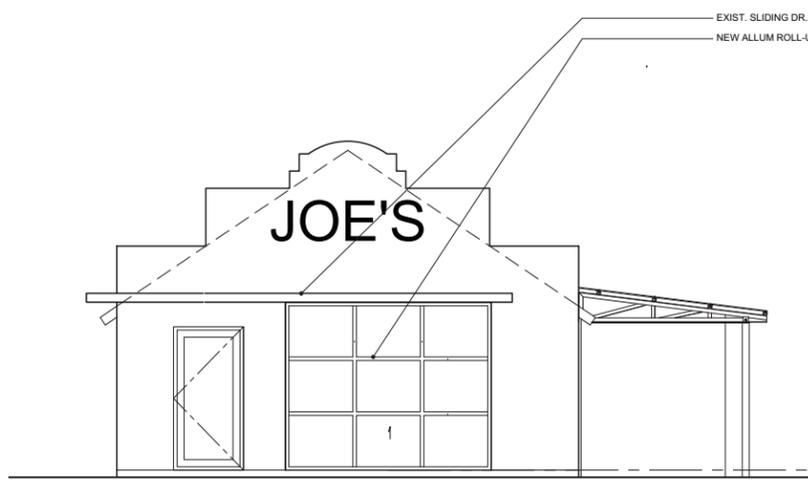
JOE'S
 2250 PEARL ST.
 BOULDER, CO
 80302



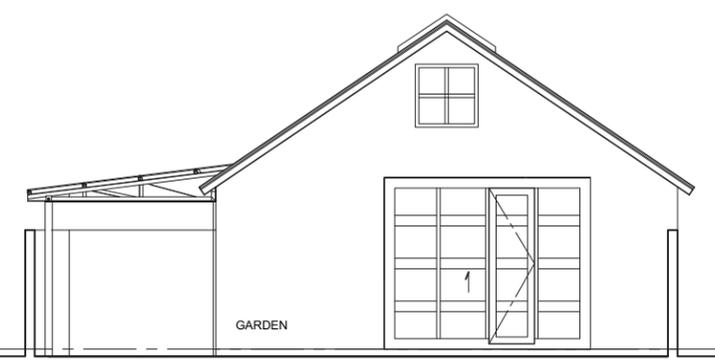
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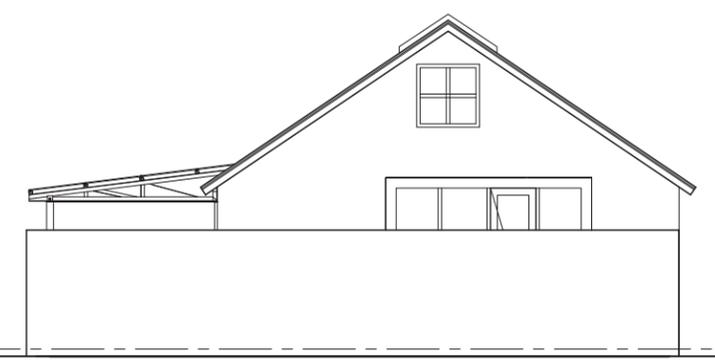
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NORTH



SOUTH



SOUTH

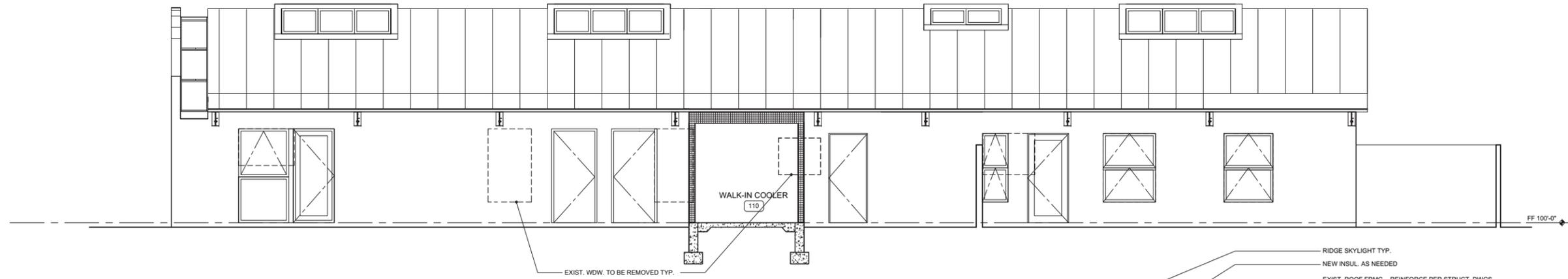
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9-19-13	REVIEW
2-27-14	PLANNING REVIEW
8-4-14	CITY COMMENTS

project no:
 drawn by:

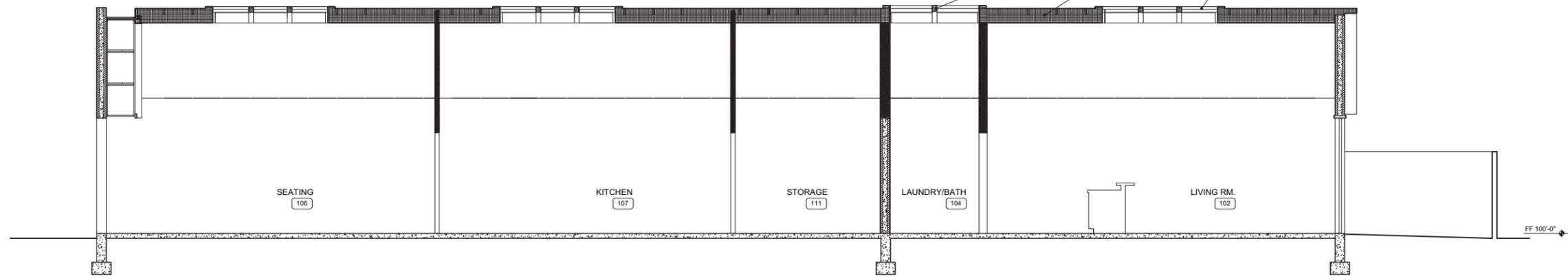
PROPOSED ELEVATIONS

black shack office
 5000 Pearl St., Suite 100
 Boulder, CO 80504
 970.927.0635
 970.379.3698

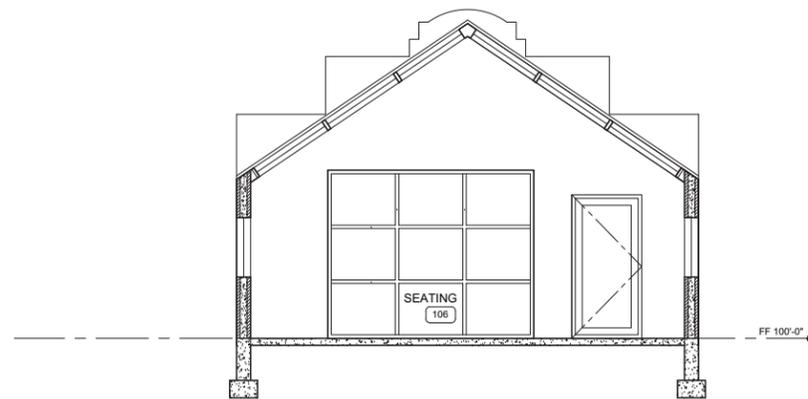
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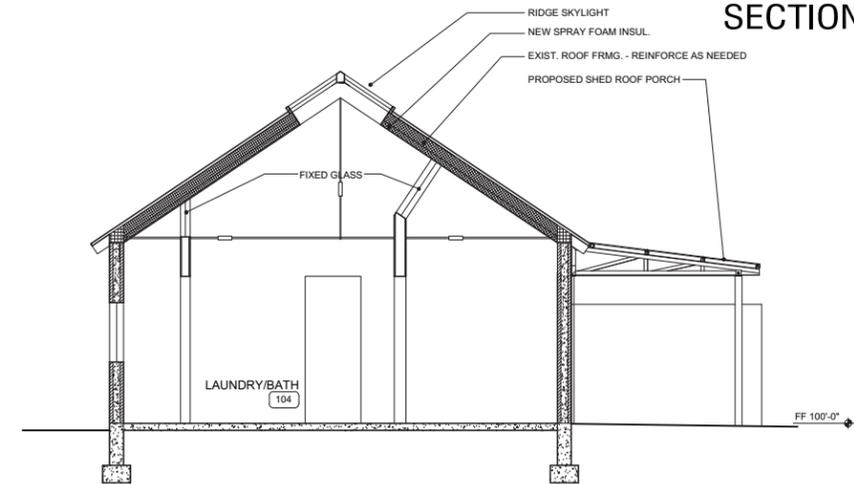
SECTION B



SECTION C



SECTION E



SECTION D

date	issue
9-4-13	REVIEW
9-13-13	REVIEW
9-19-13	REVIEW
2-27-14	PLANNING REVIEW
8-4-14	CITY COMMENTS

project no:
drawn by:

PROPOSED SECTIONS

black shack office
1000 Pearl St. Suite 100
Boulder, CO 80502
970.927.0635 T
970.379.3698 F

**CITY OF BOULDER
PLANNING BOARD AGENDA ITEM
MEETING DATE: September 4, 2014**

AGENDA TITLE:

Public hearing and consideration of a Site Review Amendment (LUR2013-00059) and Final Plat (TEC2013-00073) for the Boulder Municipal Airport to subdivide the existing 123.5-acre lot into two new lots: Lot 1C (2.6 acres) and Lot 2C (120.8 acres). The site is located at 3300 Airport Rd and is within the P and IG zone districts.

Owner: City of Boulder

REQUESTING DEPARTMENT:

Community Planning & Sustainability

David Driskell, Executive Director

Susan Richstone, Deputy Director

Charles Ferro, Development Review Manager

Chandler Van Schaack, Planner I

OBJECTIVE:

Define the steps for Planning Board consideration of this request:

1. Hear Applicant and Staff presentations
2. Hold Quasi-Judicial Public Hearing
3. Planning Board discussion
4. Planning Board action to approve, approve with conditions or deny

SUMMARY:

Proposal:	LAND USE REVIEW AND TECHNICAL DOCUMENT REVIEW: Site Review Amendment and Final Plat for the subdivision of one developed lot into two lots and the removal of the new lot from the existing P.U.D.
Project Name:	Airport South Replat C
Location:	3300 Airport Rd.
Size of Tract:	123.5 acres
Zoning:	P (Public) and IG (Industrial- General)
Comprehensive Plan:	Public and Light Industrial

KEY ISSUES:

1. Is the proposed Site Review Amendment consistent with the criteria for Amendments to Approved Site Plans as set forth in section 9-2-14(m), B.R.C. 1981?
2. Is the proposed Final Plat consistent with the Final Plat Subdivision criteria set forth in Section 9-12-8(b), B.R.C. 1981?

3. Is the proposed Final Plat consistent with the lot standards set forth in Section 9-12-12(a)(1), B.R.C. 1981?

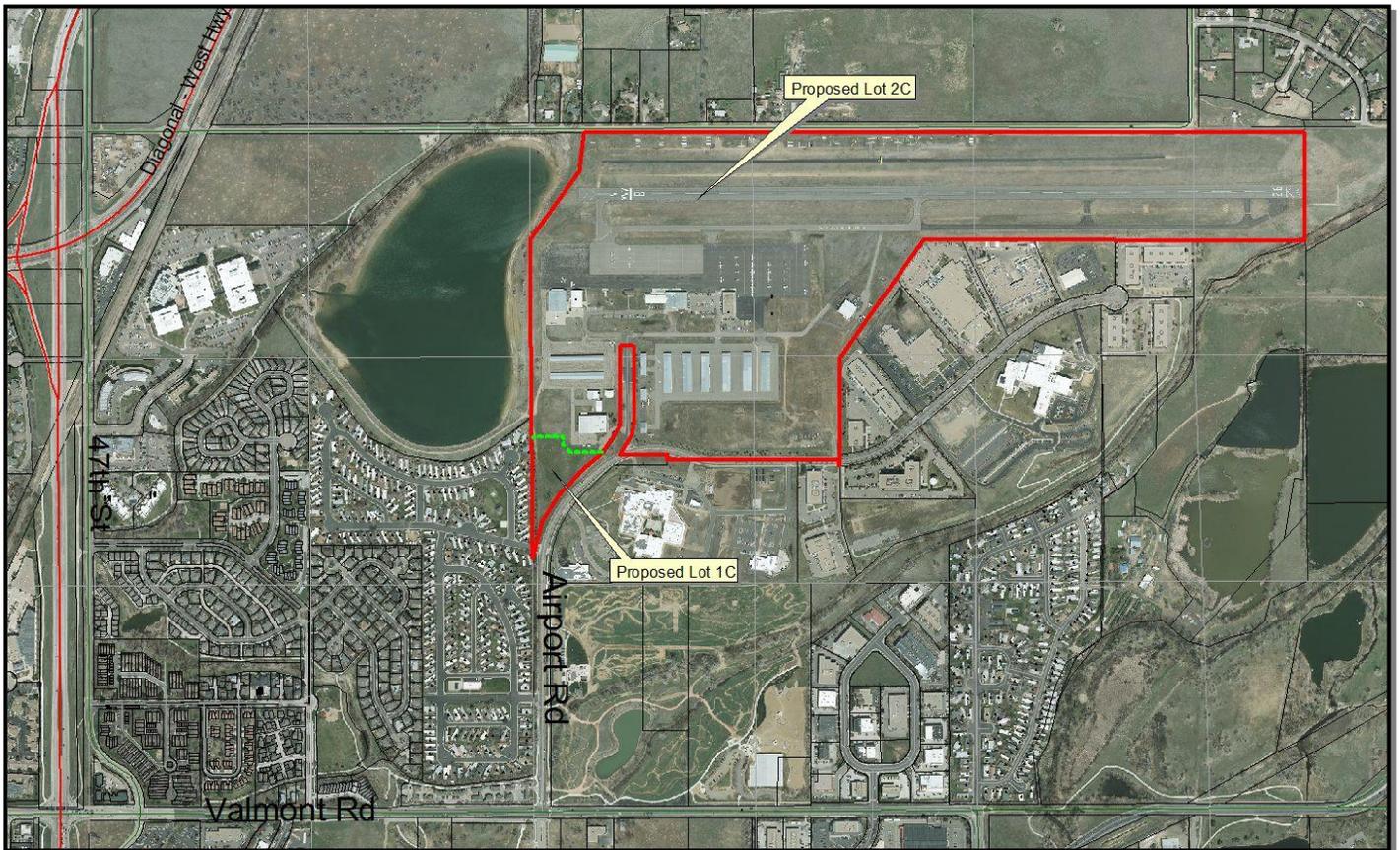


Figure 1: Vicinity Map Showing Proposed Subdivision

BACKGROUND:

Project Description

The current proposal is to subdivide the existing single-lot Airport South Subdivision in order to create a new 2.6-acre lot to be sold to fund other airport improvements. No changes to the existing airport facilities or the planned facilities as outlined in the 2007 Boulder Municipal Airport Master Plan are proposed as part of this amendment, and the applicant is not requesting any modifications to the land use regulations. The Site Review Amendment is required in order to reconfigure the existing Planned Unit Development (P.U.D.) boundary and remove the proposed Lot 1C from the PUD so that it may be developed in the future. Depending on the type and size of the future development proposed, a discretionary review process may or may not be required.

Zoning Description

The Land Use Designation and Zoning for the subject site were changed as part of the 2010 Boulder Valley Comprehensive Plan (2010 BVCP) update process. Currently, the site is split-zoned, with the majority of the site (120.8-acres) zoned P (Public) and a small (2.6-acre) area at the southwest corner of the site zoned IG (Industrial-General). See **Figure 2** below for a Zoning Map. Per section 9-5-2, B.R.C. 1981, the P zone district is defined as “Public areas in which public and semi-public facilities and uses are located, including without limitation, governmental and educational uses,” and the IG zone is defined as

“General industrial areas where a wide range of light industrial uses, including research and manufacturing operations and service industrial uses, are located. Residential uses and other complementary uses may be allowed in appropriate locations.” Please see [Attachment C](#), 2010 Staff Land Use Memo, for additional information.

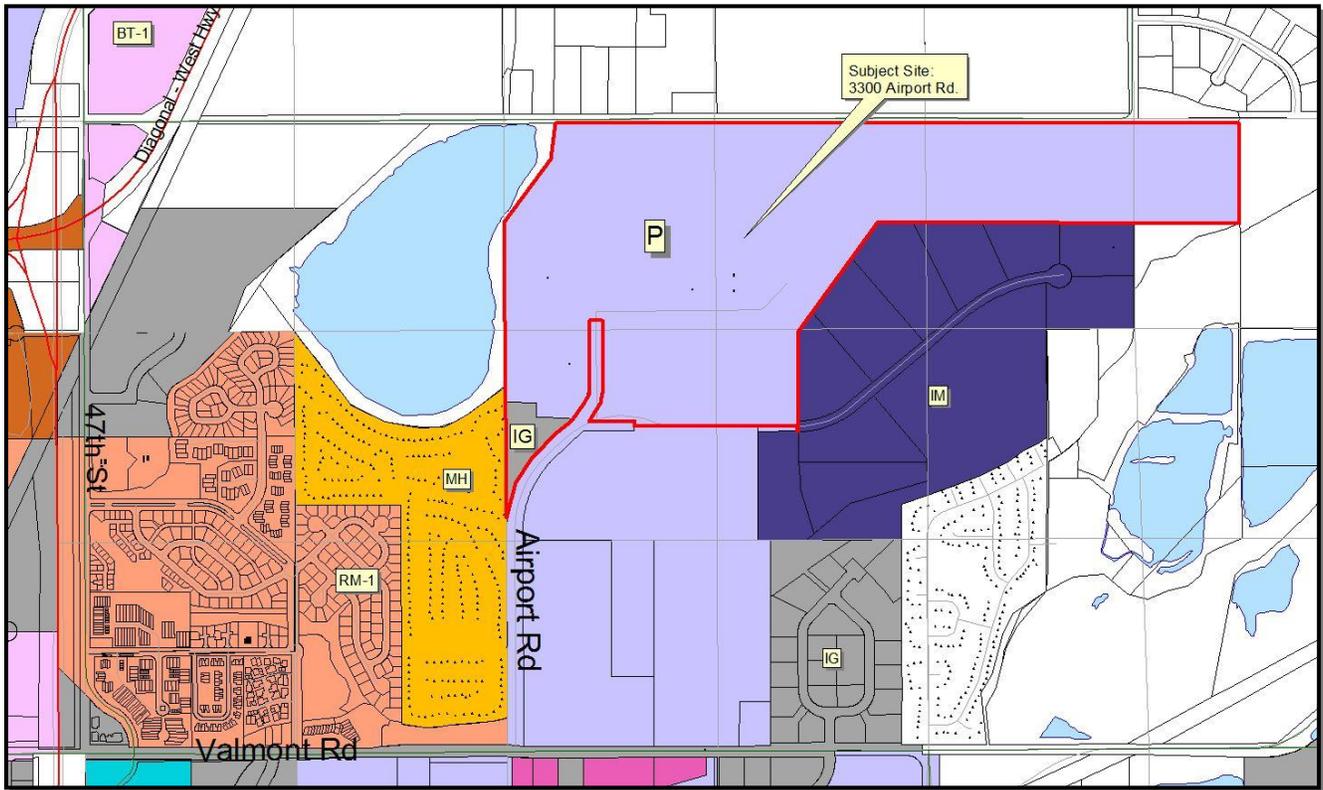


Figure 2: Zoning Map

Existing Site/Site Context

The Boulder Municipal Airport is owned and operated by the city of Boulder, and is located on Airport Blvd. north of Valmont Rd. at the northeast boundary of the main city limits, as shown below in **Figure 1**. The 123.5-acre site is bordered by Airport Boulevard on the southeast, with the Boulder County Jail across the street and Vista Village mobile home park on the west, with an entrance to the park just south of the site. Hayden Lake (owned by the Boulder and Left Hand Ditch Company) lies northwest of the site; Valmont City Park is southeast of the jail, and Lakecentre Business Park is farther east and north. A large sculpture by the late Kim Field is located at the southern end of the site. The sculpture was funded in 1973 by the Parks and Recreation Department's Art in the Park program and moved to this location from the corner of Baseline and Broadway in 1986.

The airport serves the general aviation needs of the community by providing business-related flying; personal and recreational flying; flight training; law enforcement, fire and rescue flying services; air charters for medical support; transport of mail and newspapers; and other aviation-related activities.

Project History

On January 16, 2007, council adopted the 2007 Airport Master Plan Update for inclusion in the Boulder Valley Comprehensive Plan. The 2007 Airport Master Plan identifies the subject portion of the Boulder Municipal Airport proposed to be subdivided, (proposed Lot 1C), for possible sale. The site is a small triangle of land with significant slopes (up to 9 percent) located on the southwest corner of the airport and

not accessible to the taxiway/apron (proposed new lot line shown in green in **Figure 1** above). The airport intends to sell the site for redevelopment to fund other airport improvements. The site's significant slope and lack of taxiway access are the primary reasons that the Airport Master Plan did not identify airport uses for the property and recommended considering it for future sale.

As part of the 2010 Boulder Valley Comprehensive Plan (BVCP) update, city staff performed a detailed analysis of possible alternative land uses for the site and recommended the most appropriate land use designation for the site to be Light Industrial (please see **Attachment C**, 2010 Staff Land Use Memo, for additional information). The change in the BVCP Land Use Designation from Public to Light Industrial for the portion of the Boulder Municipal Airport proposed to become Lot 1C was approved by Planning Board on May 24, 2011, and City Council on June 7, 2011. Public comment was solicited on the land use change at a neighborhood public meeting on Oct. 25, 2010 and at the May 24, 2011 hearing. On August 7, 2012, council approved an ordinance rezoning that portion of the site from Public to Industrial-General.

On July 8, 2014, following staff review and approval of a Preliminary Plat and Final Plat for consistency with the city's Final Plat Subdivision criteria and lot standards, city staff approved the Airport South Replat C Subdivision as well as an Amendment to the Airport PUD to allow the new Lot 1C to be removed from the existing PUD. Final plat and Site Review Amendment approvals may be called up by the board or by the public within 14 days of staff's decision. Three members of the Planning Board voted to call up the decision, indicating that they wished to discuss the project further in the context of the zoning and land use designation for the site.

KEY ISSUES:

Staff has identified the following key issues for the board's consideration:

1. Is the proposed Site Review Amendment consistent with the criteria for Amendments to Approved Site Plans as set forth in section 9-2-14(m), B.R.C. 1981?

Section 9-2-14(m), "Amendments to Approved Site Plans," B.R.C. 1981 includes the procedures and review criteria for approval of an amendment to an approved site review development. The proposal was found to be consistent with the criteria for Amendments to Approved Site Plans found in section 9-2-14(m), B.R.C. 1981. Please refer to **Attachment B** for staff's complete analysis of the review criteria.

2. Is the proposed Final Plat consistent with the Final Plat Subdivision criteria set forth in Subsection 9-12-8(b), B.R.C. 1981?

Subsection 9-12-8(b), B.R.C. 1981 lists all of the information that is required to be placed on a final plat. Staff has reviewed the plat and determined that the applicant has included all of the required information on the plat document.

3. Is the proposed Final Plat consistent with the lot standard criteria set forth in Section 9-12-12, B.R.C. 1981?

Section 9-12-12, "Standards for Lots and Public Improvements," B.R.C. 1981 includes all of the substantive regulatory requirements that need to be met in order to have an approvable final plat. The proposed subdivision meets all of the necessary lot standards set forth in Section 9-12-12,

B.R.C. 1981. [Attachment B](#) includes a detailed analysis of the subdivision standards.

PUBLIC COMMENT AND PROCESS:

The required public notice was given in the form of written notification mailed to all property owners within 600 feet of the subject property and a sign posted on the property for at least 10 days. All notice requirements of Section 9-4-2, B.R.C. 1981 have been met. Staff initially received questions from several neighboring residents regarding the potential future development of the site. One neighbor expressed a desire to preserve the existing trees between the subject site and the mobile home park at the time of redevelopment.

STAFF FINDINGS AND RECOMMENDATION:

Planning staff finds that the application meets the requirements of the Boulder Revised Code, City of Boulder Design and Construction Standards and other ordinances of the city.

Therefore, staff recommends that Planning Board approve Land Use Review # LUR2013-00059 for an Amendment to the Boulder Municipal Airport PUD and Technical Document Review # TEC2013-00073 for the Airport South Replat C Subdivision incorporating this staff memorandum and the Site Review Amendment and Final Plat Subdivision Review Criteria as findings of fact.

Approved By:



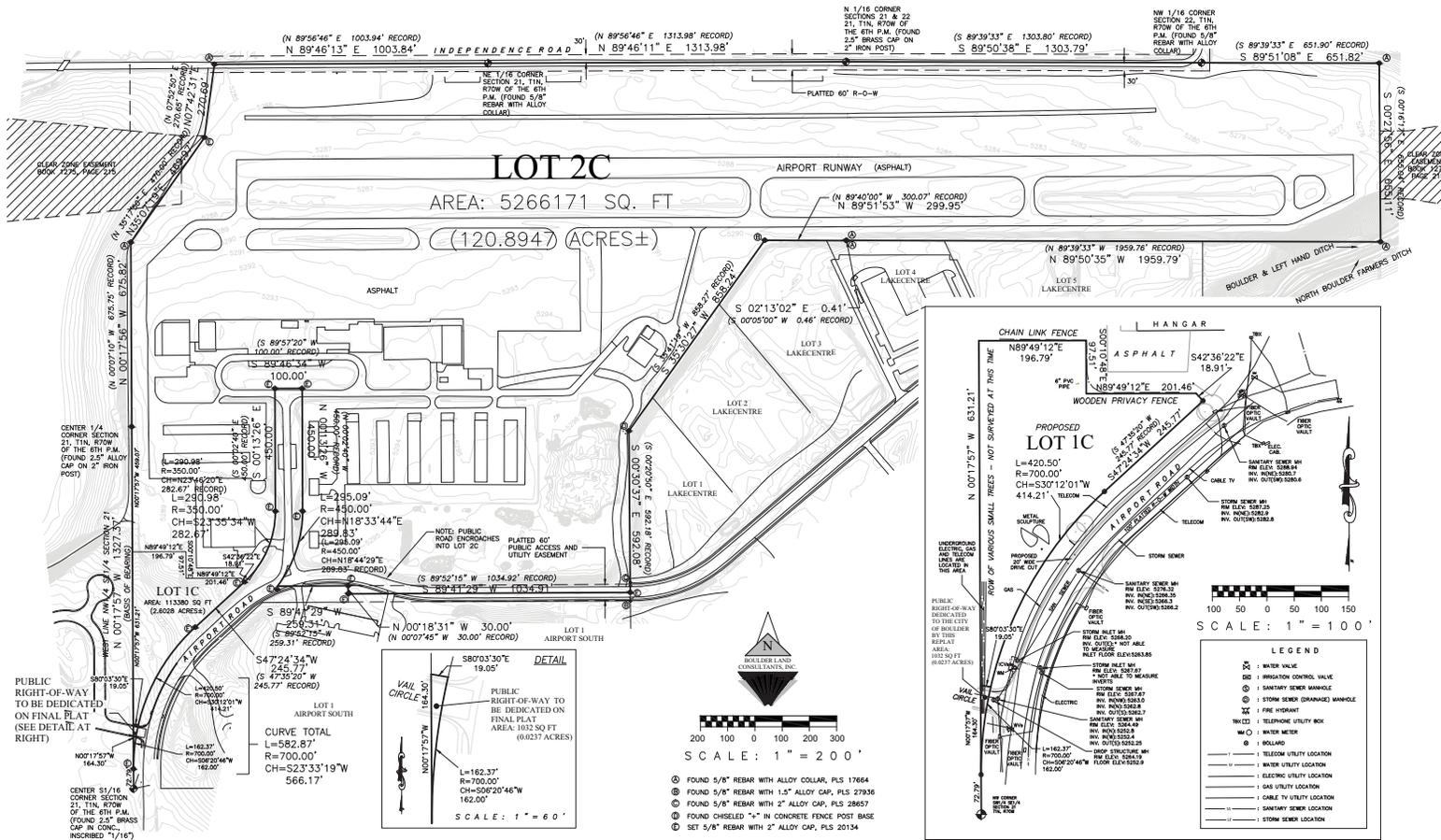
David Driskell, Executive Director
Department of Community Planning and Sustainability

ATTACHMENTS:

- A: [Proposed Final Plat and Site Plan](#)
- B: [Staff Analysis of Review Criteria](#)
- C: [2010 Staff Land Use Memo](#)

THE PRELIMINARY PLAT OF AIRPORT SOUTH REPLAT C

A REPLAT OF LOT 2, AIRPORT SOUTH, A SUBDIVISION LOCATED IN THE NE1/4 AND THE SE1/4 OF SECTION 21 AND THE NW1/4 OF SECTION 22, ALL IN T1N, R70W OF THE 6TH P.M., CITY OF BOULDER, COUNTY OF BOULDER, STATE OF COLORADO
TOTAL AREA (AS SURVEYED): 123.5212 ACRES



- NOTES
- THIS MAP IS BASED ON FIDELITY NATIONAL TITLE INSURANCE COMPANY COMMITMENT NO. 915-F045845-70-TWO, AMENDMENT NO. 1, DATED JULY 22, 2013, AND PREPARED BY FIDELITY NATIONAL TITLE COMPANY. THIS MAP DOES NOT REPRESENT A TITLE SEARCH PERFORMED BY BOULDER LAND CONSULTANTS, INC. OR THE UNDERSIGNED.
 - BASIS OF BEARINGS SHOWN HEREIN IS THE WEST LINE OF THE NORTHWEST 1/4 OF THE SOUTHEAST 1/4 OF SECTION 21, T1N, R70W OF THE 6TH P.M., MONUMENTED AS SHOWN HEREON, AND DETERMINED TO BEAR N 001°57' 00" E BY GPS OBSERVATION.
 - ACCORDING TO THE ABOVE DESCRIBED TITLE COMMITMENT, THE PROPERTY DESCRIBED HEREON IS SUBJECT TO THE FOLLOWING RECORDED DOCUMENTS:
 - EXCEPTION #1: ALL TAXES AND ASSESSMENTS, NOW OR HERETOFORE ASSESSED, DUE OR PAYABLE.
 - EXCEPTION #2: RIGHT OF WAY, WHETHER IN FEE OR EASEMENT ONLY, AS SET FORTH BELOW: PURPOSE: BOULDER A LEFT HAND DITCH AND NORTH BOULDER FARMERS DITCH; EXCEPTION #10: BOOK 59 AT PAGE 255 RECORDED DECEMBER 21, 1981 AND BOOK 1084 AT PAGE 546 RECORDED SEPTEMBER 2, 1958 (RESERVATIONS CONTAINED IN THE PATENTS FROM THE UNITED STATES OF AMERICA WHICH AMONG OTHER THINGS RECITES AS FOLLOWS: "THE RIGHT OF THE PROPRIETOR OF A VEIN OR LODE TO EXTRACT AND REMOVE HIS ORE THEREFROM SHOULD THE SAME BE FOUND TO EXIST OR TO INTERFERE WITH THE PREMISES HEREBY GRANTED AS PROVIDED BY LAW");
 - EXCEPTION #11: BOOK 1089 AT PAGE 200 RECORDED NOVEMBER 1, 1958 (TERMS, CONDITIONS, PROVISIONS, AGREEMENTS, OBLIGATIONS AND EASEMENTS CONTAINED IN AGREEMENT);
 - EXCEPTION #12: BOOK 1275 AT PAGE 215 RECORDED APRIL 16, 1943 (CITY OF BOULDER CLEAR ZONE EASEMENTS FOR THE PURPOSE(S) AND RIGHTS INCIDENTAL THERETO);
 - EXCEPTION #13: RECEPTION NO. 936582 RECORDED JANUARY 28, 1983 (TERMS, CONDITIONS, PROVISIONS, AGREEMENTS AND OBLIGATIONS CONTAINED IN ORDINANCE NO. 4734);
 - EXCEPTION #14: RECEPTION NO. 79027 RECORDED SEPTEMBER 22, 1986 (TERMS, CONDITIONS, RESTRICTIONS, PROVISIONS, NOTES AND EASEMENTS BUT OMITTING ANY COVENANTS OR OBLIGATIONS CONTAINED IN ORDINANCE NO. 4734);
 - EXCEPTION #15: RECEPTION NO. 1026283 RECORDED FEBRUARY 1, 1990 (TERMS, CONDITIONS, PROVISIONS, AGREEMENTS AND OBLIGATIONS CONTAINED IN ORDINANCE NO. 5254);
 - EXCEPTION #16: RECEPTION NO. 3206962 RECORDED MARCH 13, 2013 (A PENDING COURT ACTION AS DISCLOSED BY A RECORDED NOTICE OF LIS PENDENS - PLAINTIFF: THE CITY OF BOULDER; DEFENDANTS: BARRY M. BARROW, ET AL; COUNTY: BOULDER; COURT DISTRICT, CASE NO.: 2012CV0174; NOTE: RECEPTION NO. 3391978 RECORDED JUNE 12, 2013 (RULE AND ORDER IN CONNECTION WITH THE ABOVE LIS PENDENS);
 - EXCEPTION #17: ANY EXISTING LEASES OR TENANCIES, INCLUDING, BUT NOT LIMITED TO AIRPORT HANGER LEASES, ASSIGNMENTS OF LEASES, BILLS OF SALE, DEEDS OF TRUST, ASSIGNMENTS OF RENTS AND UCC FINANCING STATEMENTS THEREOF, AND ANY AND ALL PARTIES CLAIMING BY THROUGH OR UNDER SAID LEASES.

TITLE COMMITMENT NOTE: IF SCHEDULE B OF YOUR COMMITMENT FOR AN OWNERS TITLE POLICY REFLECTS AN EXCEPTION FOR MINERAL INTEREST OR LEASES, PURSUANT TO C.R.S. 10-11-123 (1980-1989), THIS IS TO ADVISE:

- THAT THERE IS RECORDED EVIDENCE THAT A MINERAL ESTATE HAS BEEN SEVERED, LEASED, OR OTHERWISE CONVEYED FROM THE SURFACE ESTATE AND THAT THERE IS A SUBSTANTIAL LIKELIHOOD THAT A THIRD PARTY HOLDS SOME OR ALL INTEREST IN OIL, GAS, OTHER MINERALS, OR GEOTHERMAL ENERGY IN THE PROPERTY; AND
- THAT SUCH MINERAL ESTATE MAY INCLUDE THE RIGHT TO ENTER AND USE THE PROPERTY WITHOUT THE SURFACE OWNERS PERMISSION.

THIS PLAT AND THE FIELD SURVEY ON WHICH IT IS BASED REFLECTS LINEAL UNITS IN U.S. SURVEY FEET.

- ANY PERSON WHO KNOWINGLY REMOVES, ALTERS OR DEFILES ANY PUBLIC LAND SURVEY MONUMENT OR LAND BOUNDARY MONUMENT OR ACCESSORY COMBATS CLASS TWO (2) MONUMENTOR PURSUANT TO COLORADO STATE STATUTE 18-4-508 C.R.S.
- CERTIFICATION DEFINED: THE USE OF THE WORDS "CERTIFY" OR "CERTIFICATION" BY A REGISTERED PROFESSIONAL LAND SURVEYOR CONSTITUTES AN EXPRESSION OF PROFESSIONAL OPINION REGARDING THE FACTS AND THINGS WHICH ARE THE SUBJECT OF THE CERTIFICATION, AND DOES NOT CONSTITUTE A WARRANTY OR GUARANTEE, EITHER EXPRESSED OR IMPLIED. THE CERTIFICATION SHOWN HEREON EXTENDS ONLY TO THOSE PARTIES SPECIFICALLY MENTIONED THEREIN (PER COLORADO STATE BOARD RULE 6.2.2).
- IN ACCORDANCE WITH C.R.S. 13-80-105: NOTICE: ACCORDING TO COLORADO LAW YOU MUST COMMENCE ANY LEGAL ACTION BASED UPON ANY DEFECT IN THIS SURVEY WITHIN THREE YEARS AFTER YOU FIRST DISCOVER SUCH DEFECT. IN NO EVENT MAY ANY ACTION BASED UPON ANY DEFECT IN THIS SURVEY BE COMMENCED MORE THAN TEN YEARS FROM THE DATE OF CERTIFICATION SHOWN HEREON.

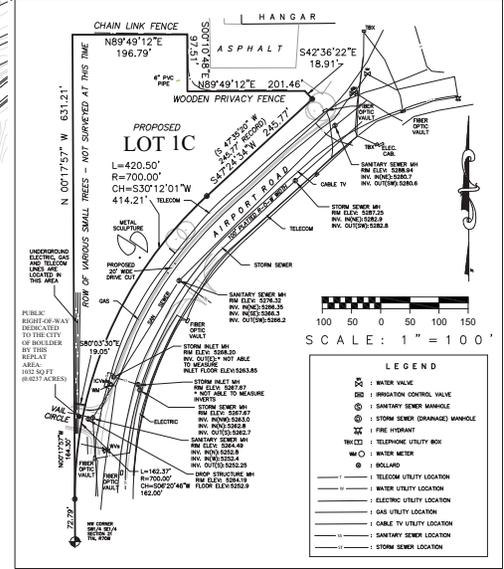
SURVEYOR'S CERTIFICATION

I, JASON EMERY, DO HEREBY CERTIFY TO THE CITY OF BOULDER, A COLORADO HOME RULE CITY AND FIDELITY NATIONAL TITLE INSURANCE COMPANY THAT, SUBJECT TO THE NOTES SHOWN HEREON, A SURVEY OF THE BOUNDARY OF THE PROPERTY COMPRISING AIRPORT SOUTH REPLAT C, A PROPOSED SUBDIVISION OF A PART OF THE CITY OF BOULDER, COLORADO, AS DESCRIBED HEREON, WAS MADE UNDER MY DIRECT SUPERVISION AND CONFORMS TO ALL APPLICABLE STATE LAWS AS DEFINED IN TITLE 38, ARTICLES 50, 51 AND 53, COLORADO REVISED STATUTES AND THAT THIS FINAL PLAT OF AIRPORT SOUTH REPLAT C ACCURATELY REPRESENTS THE RESULTS OF SAID SURVEY.

JASON EMERY
COLORADO LICENSED
PROFESSIONAL LAND SURVEYOR NO. 20134
DATE: OCTOBER 24, 2013

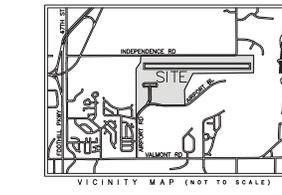
APPROVED BY THE CITY OF BOULDER

PLANNING DIRECTOR _____ DATE _____



LEGAL DESCRIPTION

LOT 2, AIRPORT SOUTH, A SUBDIVISION LOCATED IN THE NE1/4 AND SE1/4 OF SECTION 21 AND THE NW1/4 OF SECTION 22, ALL IN T1N, R70W OF THE 6TH P.M., CITY OF BOULDER, COUNTY OF BOULDER, STATE OF COLORADO.

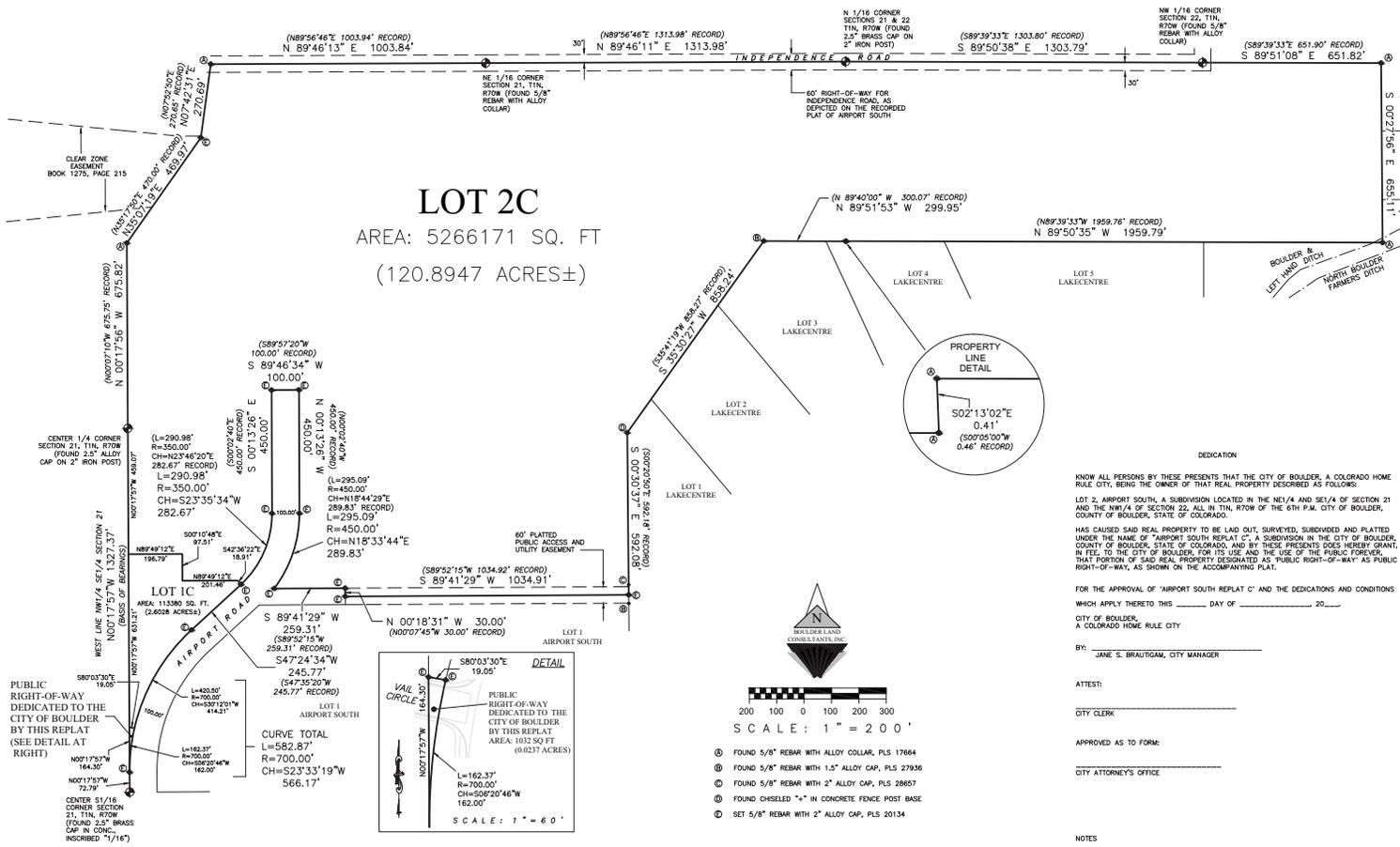


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THE FINAL PLAT OF AIRPORT SOUTH REPLAT C

A REPLAT OF LOT 2, AIRPORT SOUTH, A SUBDIVISION LOCATED IN THE NE 1/4 AND THE SE 1/4 OF SECTION 21 AND THE NW 1/4 OF SECTION 22, ALL IN T1N, R70W OF THE 6TH P.M., CITY OF BOULDER, COUNTY OF BOULDER, STATE OF COLORADO
TOTAL AREA (AS SURVEYED): 123.5212 ACRES

LOT 2C
AREA: 5266171 SQ. FT
(120.8947 ACRES±)



- NOTES (CONTINUED)
- ACCORDING TO THE ABOVE DESCRIBED TITLE COMMENT, THE PROPERTY DESCRIBED HEREON IS SUBJECT TO THE FOLLOWING RECORDED DOCUMENTS:
 - EXCEPTION #1: ALL TAXES AND ASSESSMENTS NOW OR HERETOFORE ASSESSED, DUE OR PAYABLE.
 - EXCEPTION #2: RIGHT OF WAY, WHETHER IN FEE OR EASEMENT ONLY, AS SET FORTH BELOW:
 - PURPOSE: BOULDER & LEFT HAND DITCH AND NORTH BOULDER FARMERS DITCH.
 - EXCEPTION #10: BOOK 50 AT PAGE 250 RECORDED DECEMBER 27, 1981 AND BOOK 1094 AT PAGE 546 RECORDED SEPTEMBER 2, 1958 (RESERVATIONS CONTAINED IN THE PATENTS FROM THE UNITED STATES OF AMERICA WHICH AMONG OTHER THINGS SECURE TO THE PATENTEE THE RIGHT OF THE PROPRIETOR OF A VEN OR LOSE TO EXTRACT AND REMOVE HIS ORE THEREFROM SHOULD THE SAME BE FOUND TO INTERFERE WITH THE PREMISES HEREBY GRANTED AS PROVIDED BY LAW).
 - EXCEPTION #11: BOOK 1029 AT PAGE 320 RECORDED NOVEMBER 1, 1958 (TERMS, CONDITIONS, PROVISIONS, AGREEMENTS, OBLIGATIONS AND EASEMENTS IN AGREEMENT).
 - EXCEPTION #12: BOOK 1272 AT PAGE 215 RECORDED APRIL 16, 1983 (CITY OF BOULDER CLEAR ZONE EASEMENTS FOR THE PURPOSE(S) AND RIGHTS INCIDENTAL THERETO).
 - EXCEPTION #13: RECEPTION NO. 530632 RECORDED JANUARY 26, 1983 (TERMS, CONDITIONS, PROVISIONS, AGREEMENTS AND OBLIGATIONS CONTAINED IN ORDINANCE NO. 47345).
 - EXCEPTION #14: RECEPTION NO. 791027 RECORDED SEPTEMBER 22, 1986 (TERMS, CONDITIONS, PROVISIONS, AGREEMENTS, NOTES AND EASEMENTS BUT OMITTING ANY COVENANTS OR RESTRICTIONS, IF ANY, INCLUDING BUT NOT LIMITED TO THOSE BASED UPON RACE, COLOR, RELIGION, SEX, SEXUAL ORIENTATION, FAMILIAL STATUS, MARITAL STATUS, DISABILITY, HANDICAP, NATIONAL ORIGIN, ANCESTRY, OR SOURCE OF INCOME, AS SET FORTH IN APPLICABLE STATE OR FEDERAL LAWS, EXCEPT TO THE EXTENT THAT SAID GOVERNMENT OR RESTRICTION IS PERMITTED BY APPLICABLE LAW, AS SET FORTH ON THE PLATE(S) OF SAID SUBDIVISION).
 - EXCEPTION #15: RECEPTION NO. 1028263 RECORDED FEBRUARY 1, 1982 (TERMS, CONDITIONS, PROVISIONS, AGREEMENTS AND OBLIGATIONS CONTAINED IN ORDINANCE NO. 52543).
 - EXCEPTION #16: RECEPTION NO. 3028063 RECORDED MARCH 13, 2012 (A PENDING COURT ACTION AS DISCLOSED BY A RECORDED NOTICE OF US PENDENS - PLAINTIFF: THE CITY OF BOULDER; DEFENDANTS: BARRY M. BARNOW, ET AL; COUNTY: BOULDER; COURT: DISTRICT; CASE NO.: 2012CV74; NOTE: RECEPTION NO. 3319179 RECORDED JUNE 12, 2013 (RULE AND ORDER IN CONNECTION WITH THE ABOVE US PENDENS)).
 - EXCEPTION #17: ANY EXISTING LEASES OR EASEMENTS, INCLUDING, BUT NOT LIMITED TO, BOULDER HANGER LEASES, ASSIGNMENTS OF LEASES, BILLS OF SALE, DEEDS OF TRUST, ASSIGNMENTS OF RENTS AND LIC, FINANCING STATEMENTS THEREOF, AND ANY AND ALL PARTIES CLAIMING BY, THROUGH OR UNDER SAID LESSEES.
 - TITLE COMMENT NOTE: IF SCHEDULE B OF YOUR COMMITMENT FOR AN OWNER'S TITLE POLICY REFLECTS AN EXCEPTION FOR MINERAL INTEREST OR LEASES, PURSUANT TO CRS 10-11-123 (1983-1985), THIS IS TO ADVISE:
 - THAT THERE IS RECORDED EVIDENCE THAT A MINERAL ESTATE HAS BEEN SEVERED, LEASED, OR OTHERWISE CONVEYED FROM THE SURFACE ESTATE AND THAT THERE IS A SUBSTANTIAL LIKELIHOOD THAT A THIRD PARTY HOLDS SOME OR ALL INTEREST IN OIL, GAS, OTHER MINERALS, OR GEOTHERMAL ENERGY IN THE PROPERTY; AND
 - THAT SUCH MINERAL ESTATE MAY INCLUDE THE RIGHT TO ENTER AND USE THE PROPERTY WITHOUT THE SURFACE OWNER'S PERMISSION.
 - THIS PLAT AND THE FIELD SURVEY ON WHICH IT IS BASED REFLECT LINEAL UNITS IN U.S. SURVEY FEET.
 - ANY PERSON WHO KNOWINGLY REMOVES, ALTERS OR DEFACES ANY PUBLIC LAND SURVEY MONUMENT OR LAND BOUNDARY MONUMENT OR ACCESSORY COMMITTEE CLASS TWO (2) MDS/EMANOR PURSUANT TO COLORADO STATE STATUTE 18-4-508 C.R.S.
 - CERTIFICATION DEFINED: THE USE OF THE WORDS "CERTIFY" OR "CERTIFICATION" BY A REGISTERED PROFESSIONAL LAND SURVEYOR CONSTITUTES AN EXPRESSION OF PROFESSIONAL OPINION REGARDING THE FACTS AND FINDINGS WHICH ARE THE SUBJECT OF THE CERTIFICATION, AND DOES NOT CONSTITUTE A WARRANTY OR GUARANTEE, EITHER EXPRESSED OR IMPLIED. THE CERTIFICATION SHOWN HEREON EXTENDS ONLY TO THOSE PARTIES SPECIFICALLY MENTIONED THEREIN (PER COLORADO STATE BOARD RULE 6.2.2).
 - IN ACCORDANCE WITH C.R.S. 13-80-105: NOTICE: ACCORDING TO COLORADO LAW YOU MUST COMMENCE ANY LEGAL ACTION BASED UPON ANY DEFECT IN THIS SURVEY WITHIN THREE YEARS AFTER YOU FIRST DISCOVER SUCH DEFECT. IN NO EVENT MAY ANY ACTION BASED UPON ANY DEFECT IN THIS SURVEY BE COMMENCED MORE THAN TEN YEARS FROM THE DATE OF CERTIFICATION SHOWN HEREON.

DEDICATION

KNOW ALL PERSONS BY THESE PRESENTS THAT THE CITY OF BOULDER, A COLORADO HOME RULE CITY, BEING THE OWNER OF THAT REAL PROPERTY DESCRIBED AS FOLLOWS:

LOT 2, AIRPORT SOUTH, A SUBDIVISION LOCATED IN THE NE 1/4 AND SE 1/4 OF SECTION 21 AND THE NW 1/4 OF SECTION 22, ALL IN T1N, R70W OF THE 6TH P.M., CITY OF BOULDER, COUNTY OF BOULDER, STATE OF COLORADO.

HAS CAUSED SAID REAL PROPERTY TO BE LAD OUT, SURVIVED, SUBDIVIDED AND PLATTED UNDER THE NAME OF "AIRPORT SOUTH REPLAT C", A SUBDIVISION IN THE CITY OF BOULDER, COUNTY OF BOULDER, STATE OF COLORADO, AND BY THESE PRESENTS DOES HEREBY GRANT, IN FEE, TO THE CITY OF BOULDER, FOR ITS USE AND THE USE OF THE PUBLIC FOREVER, THAT PORTION OF SAID REAL PROPERTY DESIGNATED AS "PUBLIC RIGHT-OF-WAY" AS PUBLIC RIGHT-OF-WAY, AS SHOWN ON THE ACCOMPANYING PLAT.

FOR THE APPROVAL OF "AIRPORT SOUTH REPLAT C" AND THE DEDICATIONS AND CONDITIONS WHICH APPLY THERETO THIS ____ DAY OF _____, 20____

CITY OF BOULDER,
A COLORADO HOME RULE CITY

BY: _____
JANE S. BRAUTIGAM, CITY MANAGER

ATTEST: _____

CITY CLERK

APPROVED AS TO FORM: _____

CITY ATTORNEY'S OFFICE

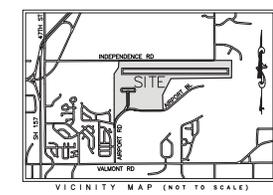
NOTES

- THIS MAP IS BASED ON FIDELITY NATIONAL TITLE INSURANCE COMPANY COMMITMENT NO. 515-FO43245-170-TVO, AMENDMENT NO. 1, DATED JULY 22, 2013, AND PREPARED BY FIDELITY NATIONAL TITLE COMPANY. THIS MAP DOES NOT REPRESENT A TITLE SEARCH PERFORMED BY BOULDER LAND CONSULTANTS, INC.
- BASIS OF BEARINGS SHOWN HEREON IS THE WEST LINE OF THE NORTHWEST 1/4 OF THE SOUTHEAST 1/4 OF SECTION 21, T1N, R70W OF THE 6TH P.M., MONUMENTED AS SHOWN HEREON, AND DETERMINED TO BEAR N 00°17'57" W BY GPS OBSERVATION.

(CONTINUED...)

THIS MAP WAS PREPARED BY
BOULDER LAND CONSULTANTS, INC.

950 LARAMIE BLVD., UNIT D
BOULDER, CO 80304 (303) 443-3616
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FILED: 10/24/2013 10:20 AM

Section 9-2-14(m), Amendments to Approved Site Plans:

(1) No proposal to modify, structurally enlarge, or expand any approved site review, other than a minor modification or minor amendment, will be approved unless the site plan is amended and approved in accordance with the procedures prescribed by this section for approval of a site review, except for the notice and consent provisions of this subsection.

Criteria for Review: No site review application shall be approved unless the approving agency finds that:

(1) Boulder Valley Comprehensive Plan:

✓ (A) The proposed site plan is consistent with the land use map and the service area map and, on balance, the policies of the Boulder Valley Comprehensive Plan.

The proposed plan is consistent with the purposes and policies of the Boulder Valley Comprehensive Plan. Boulder Municipal Airport (BMA) is a general aviation airport owned and operated by the city. On January 16, 2007, council adopted the 2007 Airport Master Plan Update for inclusion in the Boulder Valley Comprehensive Plan. Master plans are developed to be consistent with the policies, plans, and population and employment projections provided in the Boulder Valley Comprehensive Plan.

The proposed Amendment is found to be substantially consistent with the intent of the original approval and subsequent Master Plan updates. The proposed Lot 1C is a portion of the Boulder Municipal Airport that was identified in the 2007 Airport Master Plan for possible sale. It is a small triangle of land with steep slopes located on the southwest corner of the airport and not accessible to the taxiway/apron. The airport intends sell the site to fund other airport improvements.

N/A (B) The proposed development shall not exceed the maximum density associated with the Boulder Valley Comprehensive Plan residential land use designation. Additionally, if the density of existing residential development within a three-hundred-foot area surrounding the site is at or exceeds the density permitted in the Boulder Valley Comprehensive Plan, then the maximum density permitted on the site shall not exceed the lesser of:

Not applicable, as the as the subject lot is located within the P (Public) and IG (Industrial- General) zoning districts and the proposal does not include any new development.

___(i) The density permitted in the Boulder Valley Comprehensive Plan, or,

___(ii) The maximum number of units that could be placed on the site without waiving or varying any of the requirements of [chapter 9-8](#), "Intensity Standards," B.R.C. 1981.

✓ (C) The proposed development's success in meeting the broad range of BVCP policies considers the economic feasibility of implementation techniques required to meet other site review criteria.

The proposed subdivision and site review amendment are required in order to implement the goals of the 2007 Airport Master Plan. The Airport Master Plan assesses the current and anticipated needs of the Airport and plans facility and management improvements for the next 20 years. Major changes to the facility are not proposed; improvements are primarily focused on maintaining the facility and operations, as well as meeting aircraft storage needs if the market demands. The proposed Lot 1C is a portion of the Boulder Municipal Airport that was identified in the 2007 Airport Master Plan for possible sale. It is a small triangle of land with steep slopes located on the

southwest corner of the airport and not accessible to the taxiway/apron. The airport intends sell the site to fund other airport improvements.

(2) Site Design: Projects should preserve and enhance the community's unique sense of place through creative design that respects historic character, relationship to the natural environment, multi-modal transportation connectivity and its physical setting. Projects should utilize site design techniques which are consistent with the purpose of site review in subsection (a) of this section and enhance the quality of the project. In determining whether this subsection is met, the approving agency will consider the following factors:

N/A (A) Open Space: Open space, including, without limitation, parks, recreation areas, and playgrounds:

N/A (i) Useable open space is arranged to be accessible and functional and incorporates quality landscaping, a mixture of sun and shade and places to gather;

Not applicable, as this proposal does not include any new development. The intent of this application is to amend the existing PUD boundary in accordance with the proposed subdivision, thereby removing the proposed Lot 1c from the PUD. Any new development on the subdivided parcel would be required to undergo the city's review process.

N/A (ii) Private open space is provided for each detached residential unit;

Not applicable, as this proposal does not include any new development. The intent of this application is to amend the existing PUD boundary in accordance with the proposed subdivision, thereby removing the proposed Lot 1c from the PUD. Any new development on the subdivided parcel would be required to undergo the city's review process.

N/A(iii) The project provides for the preservation of or mitigation of adverse impacts to natural features, including, without limitation, healthy long-lived trees, significant plant communities, ground and surface water, wetlands, riparian areas, drainage areas and species on the federal Endangered Species List, "Species of Special Concern in Boulder County" designated by Boulder County, or prairie dogs (Cynomys ludovicianus), which is a species of local concern, and their habitat;

Not applicable, as this proposal does not include any new development. The intent of this application is to amend the existing PUD boundary in accordance with the proposed subdivision, thereby removing the proposed Lot 1c from the PUD. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (iv) The open space provides a relief to the density, both within the project and from surrounding development;

Not applicable, as this proposal does not include any new development. The intent of this application is to amend the existing PUD boundary in accordance with the proposed subdivision, thereby removing the proposed Lot 1c from the PUD. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (v) Open space designed for active recreational purposes is of a size that it will be functionally useable and located in a safe and convenient proximity to the uses to which it is meant to serve;

Not applicable, as this proposal does not include any new development. The intent of this application is to amend the existing PUD boundary in accordance with the proposed subdivision, thereby removing the proposed Lot 1c from the PUD. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (vi) The open space provides a buffer to protect sensitive environmental features and natural areas; and

Not applicable, as this proposal does not include any new development. The intent of this application is to amend the existing PUD boundary in accordance with the proposed subdivision, thereby removing the proposed Lot 1c from the PUD. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (vii) If possible, open space is linked to an area- or city-wide system.

Not applicable, as this proposal does not include any new development. The intent of this application is to amend the existing PUD boundary in accordance with the proposed subdivision, thereby removing the proposed Lot 1c from the PUD. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

___(B) Open Space in Mixed Use Developments (Developments that contain a mix of residential and non-residential uses)

N/A (i) The open space provides for a balance of private and shared areas for the residential uses and common open space that is available for use by both the residential and non-residential uses that will meet the needs of the anticipated residents, occupants, tenants, and visitors of the property; and

Not applicable, as this proposal does not include any new development. The intent of this application is to amend the existing PUD boundary in accordance with the proposed subdivision, thereby removing the proposed Lot 1c from the PUD. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (ii) The open space provides active areas and passive areas that will meet the needs of the anticipated residents, occupants, tenants, and visitors of the property and are compatible with the surrounding area or an adopted plan for the area.

Please see response above.

___(C) Landscaping

N/A (i) The project provides for aesthetic enhancement and a variety of plant and hard surface materials, and the selection of materials provides for a variety of colors and contrasts and the preservation or use of local native vegetation where appropriate;

Not applicable, as this proposal does not include any new development. The intent of this application is to amend the existing PUD boundary in accordance with the proposed subdivision, thereby removing the proposed Lot 1c from the PUD. Landscaping standards will apply to any new development that takes place on the new lot in the future.

N/A (ii) Landscape design attempts to avoid, minimize, or mitigate impacts to important native

species, plant communities of special concern, threatened and endangered species and habitat by integrating the existing natural environment into the project;

Not applicable, as the subject site and the surrounding area is also fully developed. There are no species of special concern known in the area, and this proposal does not include any new development. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (iii) The project provides significant amounts of plant material sized in excess of the landscaping requirements of [sections 9-9-12](#), "Landscaping and Screening Standards" and [9-9-13](#), "Streetscape Design Standards," B.R.C. 1981; and

Not applicable, as this proposal does not include any new development. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (iv) The setbacks, yards, and useable open space along public rights-of-way are landscaped to provide attractive streetscapes, to enhance architectural features, and to contribute to the development of an attractive site plan.

Not applicable, as this proposal does not include any new development. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (D) Circulation: Circulation, including, without limitation, the transportation system that serves the property, whether public or private and whether constructed by the developer or not:

Not applicable, as the streets serving the Airport have already been constructed, and no new development is proposed as part of this application. Any new development on the new lot in the future will be subject to the city's site access and transportation design standards.

N/A (i) High speeds are discouraged or a physical separation between streets and the project is provided;

Not applicable, as streets are existing. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (ii) Potential conflicts with vehicles are minimized;

Not applicable, as streets are existing. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (iii) Safe and convenient connections are provided that support multi-modal mobility through and between properties, accessible to the public within the project and between the project and the existing and proposed transportation systems, including, without limitation, streets, bikeways, pedestrianways and trails;

Not applicable, as the existing Airport PUD has been developed in accordance with the adopted Master Plan, and the current proposal does not include any new development. The intent is to remove a portion of the PUD through the subdivision of the lot and concurrent site review amendment. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (iv) Alternatives to the automobile are promoted by incorporating site design techniques, land

use patterns, and supporting infrastructure that supports and encourages walking, biking, and other alternatives to the single-occupant vehicle;

Not applicable, as there is no new development included in this proposal. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (v) Where practical and beneficial, a significant shift away from single-occupant vehicle use to alternate modes is promoted through the use of travel demand management techniques;

Not applicable, as there is no new development included in this proposal. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (vi) On-site facilities for external linkage are provided with other modes of transportation, where applicable;

Not applicable, as there is no new development included in this proposal. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (vii) The amount of land devoted to the street system is minimized; and

Not applicable, as the streets are already existing. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

✓ (viii) The project is designed for the types of traffic expected, including, without limitation, automobiles, bicycles, and pedestrians, and provides safety, separation from living areas, and control of noise and exhaust.

The existing Airport PUD has been developed in accordance with the adopted 2007 Master Plan, which assesses the current and anticipated needs of the Airport and plans facility and management improvements for the next 20 years. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

___(E) Parking

Currently, the Boulder Municipal Airport includes a total of 186 existing on-site parking spaces. The current proposal does not trigger any additional parking requirement, as there is no new development proposed for the new lot. When the new lot is developed in the future, the parking standards for the IG zone district will apply.

N/A (i) The project incorporates into the design of parking areas measures to provide safety, convenience, and separation of pedestrian movements from vehicular movements;

Not applicable, as the parking for the Airport site is already in place and there is no new development included in this proposal. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (ii) The design of parking areas makes efficient use of the land and uses the minimum amount of land necessary to meet the parking needs of the project;

Not applicable, as the parking for the Airport site is already in place and there is no new development included in this proposal. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (iii) Parking areas and lighting are designed to reduce the visual impact on the project, adjacent properties, and adjacent streets; and

Not applicable, as the parking for the Airport site is already in place and there is no new development included in this proposal. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (iv) Parking areas utilize landscaping materials to provide shade in excess of the requirements in Subsection 9-9-6 (d), "Parking Area Design Standards," and Section 9-9-14, "Parking Lot Landscaping Standards," B.R.C. 1981.

Not applicable, as the parking for the Airport site is already in place and there is no new development included in this proposal. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

___(F) Building Design, Livability, and Relationship to the Existing or Proposed Surrounding Area

✓ (i) The building height, mass, scale, orientation, and configuration are compatible with the existing character of the area or the character established by an adopted plan for the area;

This proposal will not affect the existing buildings within the Airport PUD, which have been designed and constructed in accordance with the adopted Airport Master Plan. There is no new development included with this proposal. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

✓ (ii) The height of buildings is in general proportion to the height of existing buildings and the proposed or projected heights of approved buildings or approved plans for the immediate area;

This proposal will not affect the existing buildings within the Airport PUD, which have been designed and constructed in accordance with the adopted Airport Master Plan. There is no new development included with this proposal. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (iii) The orientation of buildings minimizes shadows on and blocking of views from adjacent properties;

Not applicable, as there is no new development included in this proposal. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (iv) If the character of the area is identifiable, the project is made compatible by the appropriate use of color, materials, landscaping, signs, and lighting;

Not applicable, as there is no new development included in this proposal. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (v) Projects are designed to a human scale and promote a safe and vibrant pedestrian experience through the location of building frontages along public streets, plazas, sidewalks and paths, and through the use of building elements, design details and landscape materials that include, without limitation, the location of entrances and windows, and the creation of transparency and activity at the pedestrian level;

Not applicable, as there is no new development included in this proposal. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

✓ (vi) To the extent practical, the project provides public amenities and planned public facilities;

Boulder Municipal Airport (BMA) is a general aviation airport owned and operated by the city. The 2007 Airport Master Plan was adopted by council and is included in the Boulder Valley Comprehensive Plan. The proposed subdivision and site review amendment are required in order to implement the Airport Master Plan, which identifies the subject parcel for subdivision and sale to fund future airport improvements.

N/A (vii) For residential projects, the project assists the community in producing a variety of housing types, such as multifamily, townhouses and detached single family units, as well as mixed lot sizes, number of bedrooms and sizes of units;

Not applicable, as there is no new residential development included in this proposal. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (viii) For residential projects, noise is minimized between units, between buildings, and from either on-site or off-site external sources through spacing, landscaping, and building materials;

Not applicable, as there is no new residential development included in this proposal. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

✓ (ix) A lighting plan is provided which augments security, energy conservation, safety, and aesthetics;

A lighting plan will be required at time of building permit for any new development. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (x) The project incorporates the natural environment into the design and avoids, minimizes, or mitigates impacts to natural systems;

Not applicable, as there is no new development proposed for the site and the surrounding area is fully developed. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (xi) Buildings minimize or mitigate energy use; support on-site renewable energy generation and/or energy management systems; construction wastes are minimized; the project mitigates urban heat island effects; and the project reasonably mitigates or minimizes water use and impacts on water quality.

Not applicable, as there is no new development included in this proposal. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (xii) Exteriors or buildings present a sense of permanence through the use of authentic materials such as stone, brick, wood, metal or similar products and building material detailing;

Not applicable, as there is no new development included in this proposal. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

✓ (xiii) **Cut and fill are minimized on the site, the design of buildings conforms to the natural contours of the land, and the site design minimizes erosion, slope instability, landslide, mudflow or subsidence, and minimizes the potential threat to property caused by geological hazards;**

The existing Airport site is fully graded, and no new development is proposed for the new lot, so this proposal will not result in any new cut or fill. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (xiv) In the urbanizing areas along the Boulder Valley Comprehensive Plan boundaries between Area II and Area III, the building and site design provide for a well-defined urban edge; and

Not applicable, as this site is located in Area I and is not located in an urbanizing area along the Boulder Valley Comprehensive Plan boundary between Area II and Area III. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (xv) In the urbanizing areas located on the major streets shown on the map in Appendix A of this title near the Boulder Valley Comprehensive Plan boundaries between Area II and Area III, the buildings and site design establish a sense of entry and arrival to the City by creating a defined urban edge and a transition between rural and urban areas.

Not applicable, as this site is not a gateway site as anticipated by the BVCP.

N/A (G) Solar Siting and Construction: For the purpose of ensuring the maximum potential for utilization of solar energy in the City, all applicants for residential site reviews shall place streets, lots, open spaces, and buildings so as to maximize the potential for the use of solar energy in accordance with the following solar siting criteria:

Not applicable, as there is no new residential development included in this proposal. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (H) Additional Criteria for Poles Above the Permitted Height: No site review application for a pole above the permitted height will be approved unless the approving agency finds all of the following:

Not applicable, as this proposal does not include a request for a height modification. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (I) Land Use Intensity Modifications:

Not applicable, as this project does not include a request for a land use intensity modification. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (J) Additional Criteria for Floor Area Ratio Increase for Buildings in the BR-1 District:

Not applicable, as the subject lot is located within the P (Public) and IG (Industrial- General) zoning districts and does not include a request for a Land Use Intensity Modification. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

N/A (K) Additional Criteria for Parking Reductions: The off-street parking requirements of [section 9-9-6](#), "Parking Standards," B.R.C. 1981, may be modified as follows:

Not applicable, as this proposal does not include a request for a parking reduction. Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process.

Section 9-12-8, "Final Plat," B.R.C. 1981

In order to obtain city manager review of a final plat, the subdivider shall submit a final plat that conforms to the approved preliminary plat, includes all changes required by the manager or the planning board, and includes the following information:

(1) A map of the plat drawn at a scale of no less than one inch equals one hundred feet (and of a scale sufficient to be clearly legible) with permanent lines in ink and whose outer dimensions are twenty-four inches by thirty-six inches on a reproducible Mylar sheet (maps of two or more sheets shall be referenced to an index placed on the first sheet);

Standard met.

(2) A one inch equals one hundred feet reduction of the plat;

Standard met.

(3) The title under which the subdivision is to be recorded;

Standard met- the title of the proposed subdivision is "Airport South Replat C"

(4) Accurate dimensions for all lines, angles and curves used to describe boundaries, public improvements, ease-ments, areas to be reserved for public use and other important features. (All curves shall be circular arcs and shall be defined by the radius, central angle, tangent, arc and chart distances. All dimensions, both linear and angular, are to be determined by an accurate control survey in the field that must balance and close within a limit of one in ten thousand. No final plat showing plus or minus dimensions will be approved.);

Standard met.

(5) The names of all abutting subdivisions, or, if the abutting land is unplatted, a notation to that effect;

Standard met.

(6) An identification system for all lots and blocks and names for streets;

Standard met. The proposed lots are titled Lot 1C and Lot 2C.

(7) An identification of the public improvements, easements, parks and other public facilities shown on the plat, a dedication thereof to the public use and areas reserved for future public acquisition;

Standard met.

(8) The total acreage and surveyed description of the area;

Standard met.

(9) The number of lots and size of each lot;

Standard met.

(10) Proposed ownership and use of outlots;

Standard met.

(11) A designation of areas subject to the one-hundred-year flood, the estimated flow rate used in determining that designation, and a statement that such designation is subject to change;

Not applicable, as the subject property is not located within a floodplain.

(12) A description of all monuments, both found and set, that mark the boundaries of the property and a description of all control monuments used in conducting the survey;

Standard met.

(13) A statement by the land surveyor that the surveyor performed the survey in accordance with state law;

Standard met.

(14) A statement by the land surveyor explaining how bearings, if used, were determined;

Standard met.

(15) The signature and seal of the Colorado registered land surveyor;

Standard met.

(16) A delineation of the extent of the one hundred year floodplain, the base flood elevation, the source of such delineation and elevation and a statement that they are subject to change;

Not applicable, as the subject property is not located within a floodplain.

(17) The square footage of each lot;

Standard met.

(18) Certification for approval by the following:

(A) Director of planning,

Standard met.

(B) Director of public works and utilities,

Standard met.

(C) Director of parks and recreation, if park land is dedicated on the plat, and

Not applicable.

(D) Director of real estate and open space, if open space land is dedicated on the plat;

Not applicable.

(19) Signature blocks for all owners of an interest in the property; and

The property is city owned, so the signature block is for the city manager's signature. Standard met.

(20) A signature block for the city manager's signature.

Standard met.

(c) The subdivider shall include with the final plat:

(1) Engineering drawings, certified by a professional engineer registered in the State of Colorado, for proposed public and private utility systems meeting the requirements of the City of Boulder Design and Construction Standards;

Standard met.

(2) An update to the preliminary title report or attorney memorandum based upon an abstract of title current as of the date of submitting the plat;

Not applicable, as the property is city-owned.

(3) Covenants for maintenance of private utilities or improvements, as prescribed by subsection [9-12-12\(c\)](#), B.R.C. 1981;

Not applicable, as the property is city-owned.

(4) Copies of documents granting any easements required as part of the plat approval, the county clerk and recorder's recording number and proof of ownership of the property underlying the easement satisfactory to the city attorney;

Standard met.

(5) Evidence that adequate utility services, including electrical, natural gas, telephone and other services, are pro-vided for each lot within the subdivision; and

Standard met.

(6) Agreements with ditch companies, if needed.

Not applicable.

Section 9-12-12, "Standards for Lots and Public Improvements," B.R.C. 1981

Section 9-12-12, "Standards for Lots and Public Improvements," B.R.C. 1981 includes all of the substantive regulatory requirements that need to be met in order to have an approvable final plat. The proposed subdivision meets all of the standards set forth in Section 9-12-12, B.R.C. 1981. Below is a summary of the staff findings on each of the standards.

(a) Conditions Required: Except as provided in subsection (b) of this section, subdivision plats shall comply with section 9-9-17, "Solar Access," B.R.C. 1981, and meet the following conditions:

(1) Standards for Lots: Lots meet the following conditions:

(A) Each lot has access to a public street.

Standard met. Both of the proposed new lots will have frontage on Airport Blvd.

(B) Each lot has at least thirty feet of frontage on a public street.

Standard met.

(C) No portion of a lot is narrower than thirty feet.

Due to the existing shape of the Airport parcel, there is a small portion of the proposed new lot that is narrower than 30 feet; however, this condition is pre-existing and is not a result of the proposed subdivision. [The new subdivision will not result in new portions of the lot less than 30'](#). Therefore, this standard is not applicable.

(D) Lots meet all applicable zoning requirements of this title and section 9-9-17, "Solar Access," B.R.C. 1981.

Standard met. Both lots are located in Solar Access Area III, and therefore do not have solar access protection requirements.

(E) Lots with double frontage are avoided, except where necessary to provide separation from major arterials or incompatible land uses or because of the slope of the lot.

Standard met.

(F) Side lot lines are substantially at right angles or radial to the centerline of streets, whenever feasible.

Not applicable, as the property lot does not have right angles radial to the streets.

(G) Corner lots are larger than other lots to accommodate setback requirements of section 9-7-1, "Schedule of Form and Bulk Standards," B.R.C. 1981.

Not applicable, as neither lot will be a corner lot. Regardless, each lot has more than adequate room to accommodate required setbacks.

(H) Residential lots are shaped so as to accommodate a dwelling unit within the setbacks prescribed by the zoning district.

Not applicable, as the proposed lots are not residential.

(I) Lots shall not be platted on land with a ten percent or greater slope, unstable land, or land with inadequate drainage unless each platted lot has at least one thousand square feet of buildable area, with a minimum dimension of twenty-five feet. The city manager may approve the platting of such land upon finding that acceptable measures, submitted by a registered engineer qualified in the particular field, eliminate or control the problems of instability or inadequate drainage.

Standard met. The proposed new lot has slopes of up to 9 percent; however, each lot also has at least one thousand square feet of buildable area.

(J) Where a subdivision borders an airport, a railroad right-of-way, a freeway, a major street, or any other major source of noise, the subdivision is designed to reduce noise in residential lots to a reasonable level and to retain limited access to such facilities by such measures as a parallel street, a landscaped buffer area, or lots with increased setbacks.

Not applicable, as the lots are not intended for residential use. If the lot is developed for residential use in the future, it will be required to meet the conditional use standards for residential uses in the IG zone, which include a buffering requirement.

(K) Each lot contains at least one deciduous street tree of two-inch caliper in residential subdivisions, and each corner lot contains at least one tree for each street upon which the lot fronts, located so as not to interfere with sight distance at driveways and chosen from the list of acceptable trees established by the city manager, unless the subdivision agreement provides that the subdivider will obtain written commitments from subsequent purchasers to plant the required trees.

Any new development on the subdivided parcel would be required to meet city standards and undergo the city's review process. **(L) The subdivider provides permanent survey monuments, range points, and lot pins placed by a Colorado registered land surveyor.**

Standard met.

(M) Where an irrigation ditch or channel, natural creek, stream, or other drainage way crosses a subdivision, the subdivider provides an easement sufficient for drainage and maintenance.

Not applicable, as the proposed subdivision is not crossed by any irrigation ditch or channel, natural creek, stream, or other drainage way.

(N) Lots are assigned street numbers by the city manager under the city's established house numbering system, and before final building inspection the subdivider installs numbers clearly visible and made of durable material.

Standard met.

(O) For the purpose of ensuring the potential for utilization of solar energy in the city, the subdivider places streets, lots, open spaces, and buildings so as to maximize the potential for the use of solar energy in accordance with the following solar siting criteria:

Not applicable. Please see response to criterion (D) above.

(i) Placement of Open Space and Streets: Open space areas are located wherever practical to protect buildings from shading by other buildings within the development or from buildings on adjacent properties. Topography and other natural features and constraints may justify deviations from this criterion.

Not applicable.

(ii) Lot Layout and Building Siting: Lots are oriented and buildings sited in a way which maximizes the solar potential of each principal building. Lots are designed so that it would be easy to site a structure which is unshaded by other nearby structures and so as to allow for owner control of shading. Lots also are designed so that buildings can be sited so as to maximize the solar potential of adjacent properties by minimizing off-site shading.

Not applicable.

(iii) Building Form: The shapes of buildings are designed to maximize utilization of solar energy. Existing and proposed buildings shall meet the solar access protection and solar siting requirements of section 9-9-17, "Solar Access," B.R.C. 1981.

Not applicable, as there are no buildings included with this proposal.

(iv) Landscaping: The shading impact of proposed landscaping on adjacent buildings is addressed by the applicant. When a landscape plan is required, the applicant shall indicate the plant type and whether the plant is coniferous or deciduous.

A Landscape Plan will be required at time of redevelopment of the new lot.

(2) Transportation Standards for Streets, Alleys, and Sidewalks: Streets, curb and gutters, sidewalks, alleys, and the public rights-of-way therefore, are provided in conformity with the standards in the City of Boulder Design and Construction Standards, and meet the following conditions:

There is an existing sidewalk in front of the subject property, as well as an existing curb cut. No additional transportation improvements are required as part of the proposed subdivision.

(A) Streets are aligned to join with planned or existing streets.

Not applicable, as there are no new streets proposed.

(B) Streets are designed to bear a relationship to the topography, minimizing grade, slope, and fill.

Not applicable, as there are no new streets proposed.

(C) There are no dead-end streets without an adequate turnaround and appropriate barriers.

Not applicable, as there are no new streets proposed.

(D) Access to freeway, arterial, or collector street occurs only at intersections approved by the city manager, if the manager finds that the access provides efficient traffic movement and safety for drivers and pedestrians.

Not applicable, as both lots take access from Airport Blvd., which is a local street.

(E) A street of only one-half width is not dedicated to or accepted by the city.

Standard met.

(F) When the plat dedicates a street that ends on the plat or is on the perimeter of the plat, the subdivider conveys that last foot of the street on the terminal end or outside border of the plat to the city in fee simple, and it is designated by using an outlet.

Not applicable, as the existing lot is owned by the city and the portion of right-of-way being dedicated is intended to accommodate an existing access to the adjacent mobile home park.

(G) Streets are provided as prescribed by the Boulder Valley Comprehensive Plan, adopted subcommunity or area plans, or the Transportation Master Plan.

Standard met.

(H) Alleys are encouraged and should be provided. If they are provided, they are paved or otherwise appropriately surfaced with a material approved by the city manager for the specific application and location.

Standard met. No new alleys are being constructed as part of this subdivision.

(I) Sidewalks are provided in all subdivisions, unless the city manager determines that no public need exists for sidewalks in a certain location.

Standard met. Staff has determined that no public need exists for a sidewalk in that area.

(J) Signs for street names (subject to approval of the city manager), directions, and hazards are provided.

Standard met. Existing street signs are already in place.

(K) Traffic control signs are provided, as required by the city manager for control of traffic.

Standard met. No new traffic control signs are required.

(L) Pedestrian crosswalks are provided, as required by the city manager for traffic control and, at a minimum, between streets where the distance between intersecting streets exceeds one thousand feet.

Standard met. No crosswalks will be required.

(M) Bike paths or lanes are provided in conformity with the City of Boulder Comprehensive Plan for bicycle facilities and are dedicated to the city.

Standard met. No new bicycle lanes are required.

(N) Private streets are not permitted.

Standard met. No private streets are being constructed as part of this subdivision.

(3) Standards for Water and Wastewater Improvements: Water and wastewater utilities are provided in conformity with the construction and design standards in the City of Boulder Design and Construction Standards, and meet the following conditions:

(A) Water and sanitary sewer mains are provided as necessary to serve the subdivision.

Standard met.

(B) Easements are provided for city utilities as prescribed by the City of Boulder Design and Construction Standards.

Standard met.

(C) Easements for utilities other than city utilities are provided as required by the applicable private utility.

Standard met.

(D) Newly installed telephone, electric, and cable television lines and other similar utility service are placed underground. Existing utilities are also placed underground unless the subdivider demonstrates to the manager that the cost substantially outweighs the visual benefit from doing so. But transformers, switching boxes, terminal boxes, meter cabinets, pedestals, ducts, electric transmission and distribution feeder lines, communication long distance trunk and feeder lines, and other facilities necessarily appurtenant to such facilities and to underground utilities may be placed above ground within dedicated easements or public rights-of-way.

Standard met. All new utilities will be underground.

(4) Standards for Flood Control and Storm Drainage: Flood control and storm drainage measures are provided as required by the city's master drainage plan and in conformity with the construction and design standards in the City of Boulder Design and Construction Standards,

and meet the following conditions:

(A) The measures retain existing vegetation and natural features of the drainageway where consistent with the master drainage plan.

Standard met.

(B) Any land subject to flooding by a one hundred-year flood conforms to the requirements of chapter 11-5, "Storm Water and Flood Management Utility," B.R.C. 1981.

Not applicable. The subject property is not located within a floodplain.

(C) Storm drainage improvements and storm sewers are maintained to collect drainage from the subdivision and convey it off-site into a city right of way or drainage system without adversely affecting adjacent property.

Standard met.

(D) Bridges, culverts, or open drainage channels are provided when required by the flood control utility master drainage plan.

Not applicable.

(E) All subdivisions shall be designed to minimize flood damage.

Not applicable.

(F) All subdivisions shall have public utilities and facilities, including, without limitation, sewer, gas, electrical, and water systems, located and constructed to prevent flood damage.

Not applicable.

(G) All subdivisions shall have adequate drainage provided to reduce exposure to flood damage.

Standard met.

(5) Standards for Fire Protection: Fire protection measures meet the following conditions:

(A) Fire hydrants are provided as required by chapter 10-8, "Fire Prevention Code," B.R.C. 1981.

Standard met.

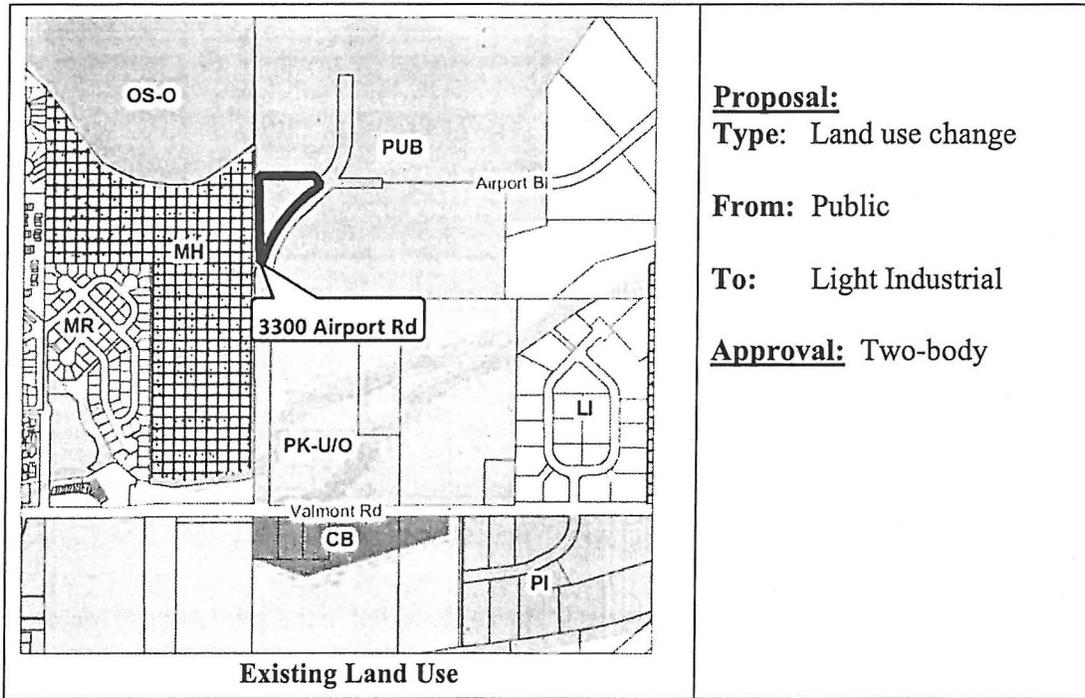
(B) Fire lanes are provided where necessary to protect the area; an easement at least sixteen feet wide for fire lanes is dedicated to the city, remains free of obstructions, and permits emergency access at all times.

Not applicable, as no new fire lanes are required.

BVCP 2010 Major Update

Site #11

3300 Airport Road



Proposal:

Type: Land use change

From: Public

To: Light Industrial

Approval: Two-body

Staff Recommendation:

Staff is recommending approval of the proposed land use change to Light Industrial for the following reasons:

- The site's proximity to the jail,
- Its location along Airport Road, which is comprised of industrial-type uses, and
- Concern about potential airport noise impacts on residential uses.

A more detailed discussion of the rationale for the staff recommendation is provided in the Analysis section.

Current Conditions:

BVCP Designation: Public

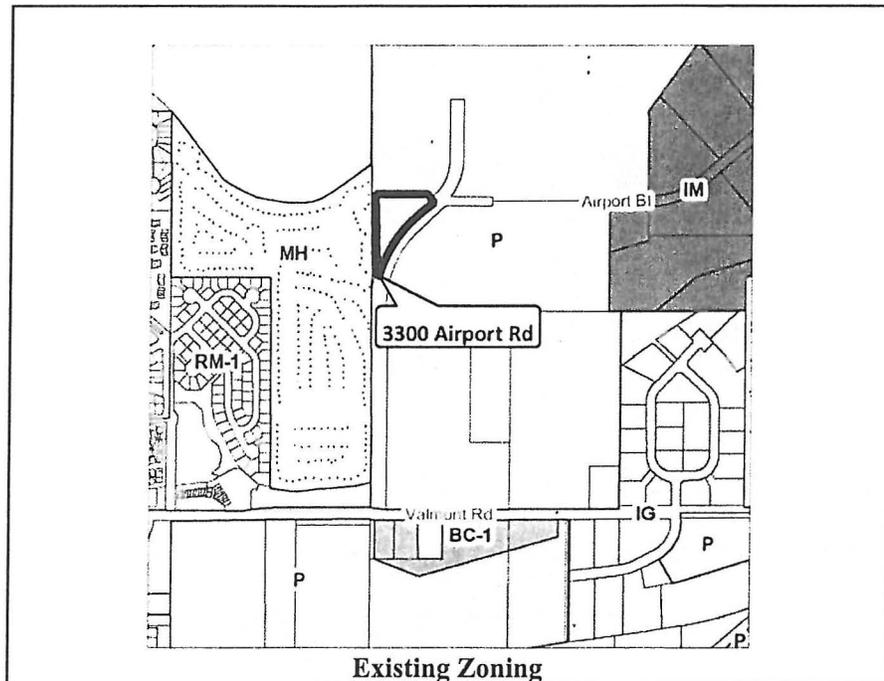
Zoning: Public

Acreage: Approximately 2 acres

Existing Buildings: None

Planning Assumptions:

	<u>Under existing land use:</u>	<u>Under proposed land use:</u>
Future estimated dwelling units	12	0
Future estimated jobs	150	150



Site Description:

This site is a portion of the Boulder Municipal Airport that was identified in the 2007 Airport Master Plan for possible sale. It is a small triangle of land located on the southwest corner of the airport and not accessible to the taxiway/apron. It is bordered by Airport Boulevard on the southeast, with the Boulder County Jail across the street; the airport on the north; and Vista Village mobile home park on the west, with an entrance to the park just south of the site. Hayden Lake (owned by the Boulder and Left Hand Ditch Company) lies northwest of the site; Valmont City Park is southeast of the jail, and Lakecentre Business Park is farther east and north.

Land Use

The current Public land use designation allows “a wide range of public and private nonprofit uses that provide a community service,” including municipal and public utility services, public and private educational facilities, government offices and facilities, and nonprofit facilities, such as cemeteries, churches, hospital and retirement complexes. The corresponding Public zone district is for “public areas in which public and semi-public facilities and uses are located, including, without limitation, governmental and educational uses.” Public zoning also allows residential uses through Use Review. Nearby land use designations include: Manufactured Housing on the Vista Village property; Public for the jail; and Light Industrial for Lakecentre Business Park.

Environmental Concerns and Other Site Issues

The site’s greater than two percent slope is one of the primary reasons that the Airport Master Plan did not identify airport uses for it and recommended considering it for future sale.

A large sculpture by the late Kim Field is located at the southern end of the site. The sculpture was funded in 1973 by the Parks and Recreation Department’s Art in the Park program and moved to this location from the corner of Baseline and Broadway in 1986.

Analysis:

The site is too steep for airplane-related activities, so it was identified in the 2007 Airport Master Plan for potential sale. If the airport were to remain the owner, the FAA would require that any use be aviation-related, for example, airplane part manufacturing. The airport is not interested in pursuing these types of uses and prefers to sell the site to fund other airport improvements. The FAA requires that proceeds from the sale of airport land be retained by the airport.

The existing Public land use could continue if a public or nonprofit entity were to purchase the site. Staff investigated whether any city departments or Boulder County would be interested in the site, but found no interest.

Two alternative land use designations were identified for analysis: Light Industrial or Residential. Light Industrial would allow light manufacturing, or research and development-type of uses and would result in approximately 40,000 square feet of building. This land use would be compatible with the uses located farther along on Airport Boulevard.

A Residential land use would match the residential use adjacent to the west, Vista Village Mobile Home Park. The designation could be either Medium Density Residential (MR) or High Density Residential (HR). MR would result in approximately 12 to 28 units on the site. The units could be in a variety of configurations, ranging from single-family homes or mobile homes to duplexes or townhouses. HR would result in 30 or more apartments, condominiums or townhouses, depending on the zoning.

Criteria for considering land use

Compatibility with surrounding land uses

Site is adjacent to both residential and industrial uses, however, the jail across the street seems less conducive to designating this site residential. Visually and functionally, the site seems more oriented toward and part of Airport Boulevard than Vista Village, because the site wraps around the curve in Airport Boulevard and access to Vista Village is located farther south. If the site is designated Light Industrial, future development will need to address its interface with Vista Village.

Compatibility with airport

BVCP Policy 3.22 Municipal Airport states: "The city shall seek to mitigate noise, safety and other impacts of airport operation, while assuring that new development in proximity will be compatible with existing and planned use of the airport." Noise will be the primary impact of airport operation on the site. Although the landing pattern for most aircraft is north of the airport, tow planes arrive from the south and fly directly over the jail, east of the site. They are descending in preparation to land, so their engines are normally at low throttle. Although airport staff receives only one or two complaints a year from Vista Village about airplane noise, airport noise would probably be more impactful to residential uses on this site than industrial uses. If the site is sold, the buyers will likely be asked to sign an aviation easement stating their acknowledgement that the site is adjacent to the airport and close to a flight path. In terms of

safety, since the site is not near the airport runway or taxiway, its development does not trigger safety concerns and will not affect aircraft taking off or landing. However, the FAA would likely impose a height restriction on the site, based on obstruction clearance, but it would likely be higher than the city-imposed height limit.

Proximity to services and amenities

Housing on this site would benefit from proximity to Valmont City Park and the Wonderland Creek and Goose Creek greenways; however, commercial services and schools would not be nearby. Proximity to services and amenities is less critical for industrial land uses.

Land use needs

Real Estate staff reports that the demand for vacant industrial land is currently greater than the demand for residential land, primarily due to its more limited supply.

Summary of analysis

Criteria:	Residential land use	Light Industrial land use
Compatibility with surrounding land uses	= (Vista Village to west, but concern about proximity to jail)	= (industrial uses to east and north, but would need to address interface with Vista Village)
Compatibility with airport	-	+
Proximity to services and amenities	-	=
Land use needs	=	+

+ Positive = Neutral - Negative

Public Comment:

A neighborhood meeting was held on Oct. 25, 2010, to get feedback on three land use options – Public, Residential and Light Industrial. Approximately five people attended and were asked for input using polling “clickers” during the presentation and written comment forms. The clicker poll revealed that all the participants were Vista Village residents. Three preferred the Residential land use option, and one participant each preferred the Public option and the Light Industrial option. Only one comment form was submitted, asking if the site could be turned into a community garden for Vista Village residents or developed as an expansion of Vista Village.

CITY OF BOULDER
PLANNING BOARD AGENDA ITEM
MEETING DATE: September 4, 2014

AGENDA TITLE: Public hearing and consideration of a **Site and Use Review** application, no. LUR2014-00036, to amend the existing Iris Hollow PUD to allow for a new two-story, 3,131 sq. ft. professional office building at 2619 Iris Hollow Pl. The proposed office building would be an expansion of the existing “Blue Sky Bridge” nonprofit facility located on the adjacent site to the west at 2617 Iris Hollow Pl. The proposal includes a request for a 20% parking reduction to allow for 8 off-street parking spaces where 10 parking spaces are required.

Applicant/ Owner: Blue Sky Bridge

REQUESTING DEPARTMENT:

Community Planning & Sustainability

David Driskell, Executive Director

Susan Richstone, Deputy Director

Charles Ferro, Land Use Review Manager

Chandler Van Schaack, Planner I

OBJECTIVE:

1. Hear Staff and Applicant presentations
2. Hold Public Hearing
3. Planning Board discussion
4. Planning Board action to approve, approve with conditions, or deny

Proposal: **Site and Use Review** application, no. LUR2014-00036, to amend the existing Iris Hollow PUD to allow for a new two-story, 3,131 sq. ft. office building at 2619 Iris Hollow Pl. The proposed office building would be an expansion of the existing “Blue Sky Bridge” facility located on the adjacent site to the west at 2617 Iris Hollow Pl. The proposal includes a request for a 20% parking reduction to allow for 8 off-street parking spaces where 10 parking spaces are required.

Project Name: Blue Sky Bridge Office Expansion

Location: 2619 Iris Hollow Pl

Size of Tract: 2,645 Square feet (0.06 acres)

Zoning: Residential – Medium 3 (RM-3)

Comprehensive Plan: Medium Density Residential

KEY ISSUES:

Staff has identified the following key issues regarding the proposed Site and Use Review application for the board’s consideration:

1. Does the request meet the Site Review Criteria of the Land Use Code section 9-2-14(h), B.R.C. 1981?
2. Does the request meet the Use Review Criteria of the Land Use Code section 9-2-15(e), B.R.C. 1981?
3. Is the request for a parking reduction consistent with the Parking Reduction criteria found in 9-2-14(h)(2)(K)?

PROJECT DESCRIPTION:

This proposal is to develop the currently vacant lot at 2619 Iris Hollow Pl. within the Iris Hollow PUD with a new two-story, 3,131 sq. ft. office building. The proposed office space is intended to serve Blue Sky Bridge, a nonprofit organization that offers consultation services to professionals in Boulder County in regards to concerns about children and families in relation to child abuse allegations, and which currently occupies an office facility located on the adjacent lot to the west. Per the Applicant’s written statement, the physical expansion is not intended to increase the intensity of their existing operations, but instead to give more space to accommodate their existing operations. They do not anticipate an increase in staff, interns, or client traffic. The 2,645 sq. ft. project site is located just southeast of the intersection of Iris Ave. and Folsom St. within the RM-3 zone district. Please refer to **Figure 1** for a vicinity map.



Figure 1: Vicinity Map

The height, mass, scale and orientation of the building are in keeping with the original approved site plan for the Iris Hollow PUD, which included lot regulations calling for a 35’ tall, 3,400 sq. ft. building with minimal setbacks along the south and east sides of the property. The current proposal is slightly lower in height at 30’-3” tall and slightly smaller at 3,131 sq. ft., but honors the original intent by maintaining pedestrian-level interest with a covered entryway and ample fenestration as well as minimal setbacks along the south and east sides of the property. The exterior material palette of shingle siding, cement-board lap and board & batten siding is consistent with the architectural intent of the approved Iris Hollow PUD as well as the existing aesthetic in the area, and the projects includes various elements encouraged in the Iris Hollow PUD approval including a gabled roof, wood-clad windows, and a covered entryway feature.

The applicant is requesting a 20% parking reduction to allow for 8 off-street parking spaces where 10 spaces are required per the parking standards for nonresidential uses in the RM-3 zone district. Per the original Iris Hollow PUD approval, the subject lot is allocated eight reserved parking spaces located in the covered condominium parking garage immediately to the north of the subject site. The original approval also included two on-site parking spaces for the proposed bed and breakfast use; however, because the applicant is not proposing to expand their existing parking demand, they are

proposing to eliminate the two approved on-site spaces called for per the original approval. Given that the parking demand will not increase, the reserved garage spaces in conjunction with ample on-street parking on Iris Hollow Pl. and Iris Walk Ct. will adequately meet the parking needs of the proposed use. Refer to [Attachment A](#) for the applicant's proposed plans and Parking Analysis.

The existing hours of operation are generally from 9:00 am to 5:00 pm, Monday through Friday, with a total of 7 staff positions ranging from 28 to 40 hours per week. There are also one to three volunteer interns on-site at any given time. On average, approximately 7 to 15 clients visit the site each day. There are no changes to the existing staffing or operating characteristics included with this proposal.



Figure 2: View of Existing Site from Iris Hollow Pl.

BACKGROUND:

Existing Use / Existing Site / Area Context.

Then 6.15-acre Iris Hollow PUD was originally approved in 1996 following Annexation, Site and Use Review and Subdivision, as a mixed-use development containing 86 mixed-density residential units and a daycare facility, laundromat, office use and post office, as well as a two-story, 3,400 sq. ft. bed and breakfast use to be located on the subject lot (2619 Iris). Since that time, development has proceeded largely in accordance with the original PUD approval except for the subject lot which has remained vacant due to a lack of market demand for a bed and breakfast use. Please see [Attachment D](#) for additional information on the original Iris Hollow PUD approval.

The existing Blue Sky Bridge office use has been in its current location at 2617 Iris Hollow Pl. since 2000. As mentioned above, Blue Sky Bridge offers consultation services to professionals in Boulder County in regards to concerns about children and families in relation to child abuse allegations. Blue Sky Bridge provides clinical interventions to families who may have experienced trauma through forensic interviews, crisis intervention, treatment and psycho-educational support. The organization also provides educational opportunities through specialized programs, outreach, and formal training. Per the Applicant's written statement, Blue Sky Bridge provides consultation services to nine different law enforcement jurisdictions within Boulder County, and several other organizations, departments, and individuals within the county. These other organizations, departments, and individuals can vary from private organizations with concerns about a child or family and looking for advice, to therapists working with a family, to representatives from the District Attorney's Office, a mental health center, or hospital. In addition to persons described above who visit or contact the center in a professional capacity,

Blue Sky Bridge provides clinical services to approximately 300 families in person per year and 170 individuals in crisis over the phone. Please see [Attachment A](#) for the Applicant’s proposed plans including a written statement.

To the north of the subject lot is a private park area surrounded by attached residential condo units to the north and west, with the existing Blue Sky Bridge office located to the south of the condominium units and immediately to the west of the subject lot. Single family homes lay across Iris Hollow Pl. to the south, and to the east lies a daycare center surrounded by additional single family residential. Parking is provided as a mix of off-street parking for the residential units, on-street parking for visitors and non-residential uses, and a covered garage under the adjacent condominium building that includes 8 reserved spaces for the approved bed and breakfast use.

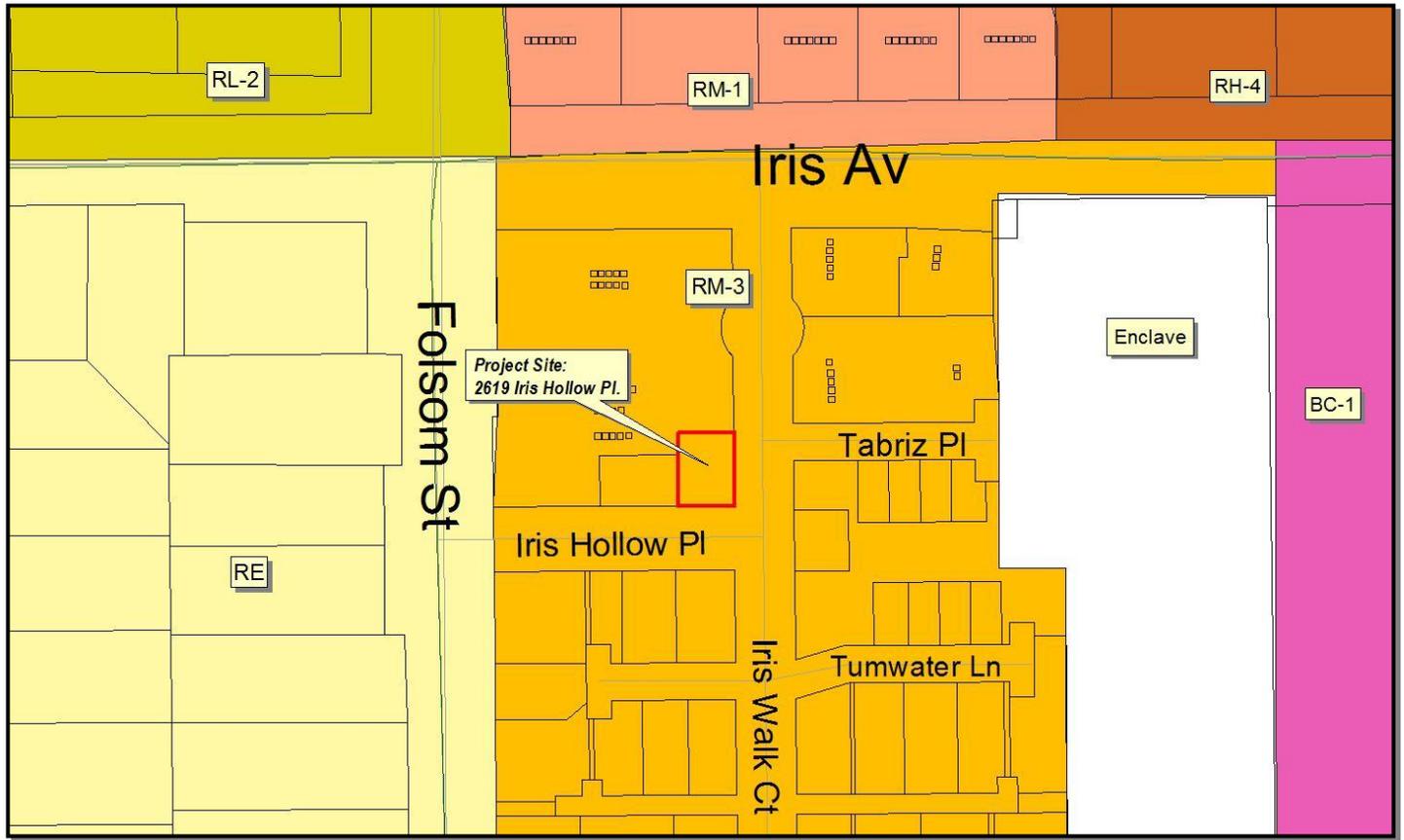


Figure 3: Zoning Map

Zoning. The project site is located just southeast of the intersection of Iris and Folsom within the RM-3 (Residential-Medium 3) zoning district (see [Figure 2](#) for zoning map). Per section 9-5-2(c), B.R.C. 1981, the RM-3 zone district is defined as “Medium density residential areas which have been or are to be primarily used for attached residential development, where each unit generally has direct access to ground level, and where complementary uses may be permitted under certain conditions.” Office uses are allowed in the RM-3 zone district if approved through a Use Review.

PROCESS: As noted above, the site is within the RM-3 zone district and is subject to an existing PUD approval. The existing Blue Sky Bridge use and proposed new facility are considered to be a “Professional Office Use” per section 9-16, B.R.C 1981. Professional office uses are only allowed through Use Review in the RM-3 zone district. In this case, a public hearing is required for the Use Review pursuant to section 9-2-15, B.R.C. 1981, which requires staff to review and submit a recommendation to the Planning Board for *any Use Review of a nonresidential use in a residential zone district*.

Additionally, because the site is subject to the existing Iris Hollow PUD, which was approved in conjunction with the approved Use Review and includes specific parking requirements and design criteria for the approved bed and breakfast

use, the proposal to develop the subject lot with a new use and thereby alter the intent of the original approval requires an amendment to the approved Site Review pursuant to section 9-2-14(m), B.R.C. 1981.

ANALYSIS OF KEY ISSUES

Key Issue #1: Does the request meet the Site Review criteria of the Land Use Code section 9-2-14(h), B.R.C. 1981?

Overall, the application was found to be consistent with the existing Iris Hollow PUD approval in terms of building mass, scale and architecture, as well as the Site Review criteria of section 9-2-14(h), B.R.C. 1981 including the additional criteria for parking reductions found in section 9-2-14(h)(2)(K) , B.R.C. 1981. Please see [Attachment B](#) for staff's complete analysis of the review criteria.

Key Issue #2: Does the request meet the Use Review criteria of section 9-2-15(e), B.R.C. 1981?

The application was found to be consistent with the Use Review criteria of section 9-2-15(e), B.R.C. 1981 (see [Attachment B](#)). Specifically, the proposed operating characteristics of the office use are such that the use will provide a direct service to the surrounding area, and will be compatible with and have a minimal negative impact on the use of nearby properties. In addition, given the variety of uses surrounding the site and the fact that the original PUD approval anticipated a non-residential use of a similar scale in this location, the proposed use will not change the predominant character of the surrounding area.

Key Issue #3: Is the request for a parking reduction consistent with sections 9-2-14(h)(2)(K) and 9-9-6(f), B.R.C 1981?

The proposed 20% parking reduction was found to be consistent with the parking reduction standards found in sections 9-2-14(h)(2)(K) and 9-9-6(f), B.R.C 1981. Specifically, staff has found that with no anticipated change in the existing operating characteristics, the parking needs of the proposed office use will be adequately met through available on-street and off-street parking, and further accommodated by varying time periods of use between residential and non-residential uses. In addition, the site is located in close proximity to a number of transit stops and is well-served bike and pedestrian facilities including the nearby Elmer's Two Mile Multi-Use Path and high frequency transit along Folsom and Iris. The applicant is also proposing to add 8 additional bicycle parking spaces in addition to the existing bike parking on 2617 Iris Hollow Pl. Please refer to [Attachment B](#) for a complete analysis of the parking reduction criteria and [Attachment A](#) for the applicant's proposed plans and Parking Analysis.

PUBLIC COMMENT AND PROCESS

Required public notice was given in the form of written notification mailed to all property owners within 600 feet of the subject property and a sign posted on the property for at least 10 days. All notice requirements of section 9-4-3, "Public Notice Requirements," B.R.C. 1981 have been met. Staff received several questions and comments from nearby property owners, all of whom expressed overall support for the proposal assuming that the impacts to the surrounding uses will not change. Staff also received comments from one neighbor expressing concerns about the loss of the existing views across the vacant lot.

STAFF FINDINGS AND RECOMMENDATION

Staff recommends that the Planning Board approve the Site and Use Review application LUR2014-00036, adopting the staff memorandum as findings of fact and subject to the recommended conditions of approval.

RECOMMENDED CONDITIONS OF SITE REVIEW APPROVAL:

1. The Applicant shall ensure that the **development shall be in compliance with all approved plans** dated

October 6, 2014 on file in the City of Boulder Planning Department, except to the extent that the development may be modified by the conditions of this approval.

2. The Applicant shall **comply with all previous conditions** contained in any previous approvals, except to the extent that any previous conditions may be modified by this approval, including, but not limited to, the following: the Annexation Agreement recorded at Film 2180, Reception No. 01671143 on January 16, 1997; Annexation Ordinance No. 5843; Subdivision Agreement recorded at Reception No. 1741962 on October 24, 1997; and the conditions of the Revised Planning Board Disposition of Approval for Iris Hollow dated July 11, 1996 (SI-96-3 and UR-96-2).

RECOMMENDED CONDITIONS OF USE REVIEW APPROVAL:

3. The Applicant shall ensure that the **development shall be in compliance with all approved plans** dated October 6, 2014 on file in the City of Boulder Planning Department, except to the extent that the development may be modified by the conditions of this approval. Further, the Applicant shall ensure that the approved use is operated in compliance with the following restrictions:
 - a. The Applicant shall operate the business in accordance with the Written Statement dated August 4, 2014, which is attached to this Notice of Disposition, except as modified by these conditions of approval.
4. The Applicant **shall not expand or modify the approved use**, except pursuant to subsection 9-2-15(h), B.R.C. 1981.

ATTACHMENTS:

- A: [Applicant's Proposed Plans and Parking Analysis](#)
- B: [Analysis of Use Review and Site Review Criteria](#)
- C: [Development Review Comments](#)
- D: [Iris Hollow PUD information](#)

BLUE SKY BRIDGE

USE & SITE REVIEW

2619 IRIS HOLLOW PLACE



3020 Carbon Place #203
 Boulder, Colorado
 p: 303-442-3351
 f: 303-447-3933

Blue Sky Bridge

2619 Iris Hollow Pl
 Boulder, CO 80304

PROJECT TEAM

APPLICANT

BLUE SKY BRIDGE
 PO BOX 19122
 BOULDER, CO 80308

ARCHITECT

COBURN DEVELOPMENT
 3020 CARBON PLACE #203
 BOULDER, CO 80301
 P: 303.442.3351
 F: 303.447.3933

PROJECT INFORMATION

LEGAL DESCRIPTION

LOT 39, IRIS HOLLOW, LOCATED IN THE
 SOUTHWEST QUARTER OF SECTION 20,
 TOWNSHIP 1 NORTH, RANGE 70 WEST
 OF THE 6TH P.M., COUNTY OF BOULDER,
 STATE OF COLORADO.

ZONING REQUIREMENTS

ZONING: RM-3
 LOT AREA: 2,645 SF
 PROPOSED BUILDING AREA: 3,131 SF
 PROPOSED USE: OFFICE
 PER IRIS HOLLOW LOT REGULATIONS, NO SETBACKS REQUIRED
 SOUTH BUILD-TO LINE: 5 FT
 EAST BUILD-TO LINE: 0 FT

DRAWING INDEX

SITE PLANS

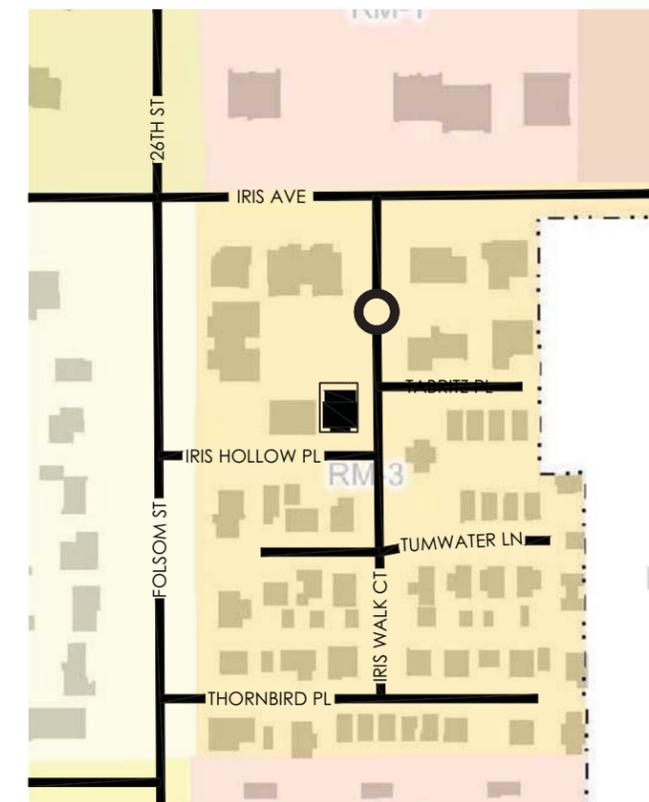
SR-1 TITLE SHEET
 SR-2 SITE PLANS
 SR-3 UTILITIES & DRAINAGE

ARCHITECTURAL PLANS

AR-1 FLOOR PLANS
 AR-2 ELEVATIONS

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 FIELD CONDITIONS OR DIMENSIONS.

DESCRIPTION	ISSUED/REVISION	AUTHOR	CHECKED	SCHEDULE	DATE
USE/SITE REVIEW	FB	PW			05.16.14
SUBMITTAL					
USE/SITE REVIEW	FB	PW			07.01.14
RESUBMITTAL					
USE/SITE REVIEW	FB	PW			07.31.14
CORRECTIONS					



1 Vicinity Map
 1" = 100'-0"

USE/SITE REVIEW
 07.31.2014

NOT FOR
 CONSTRUCTION

SHEET No.

SR-1
 Page 7 of 6 SHEET



COBURN
ARCHITECTURE

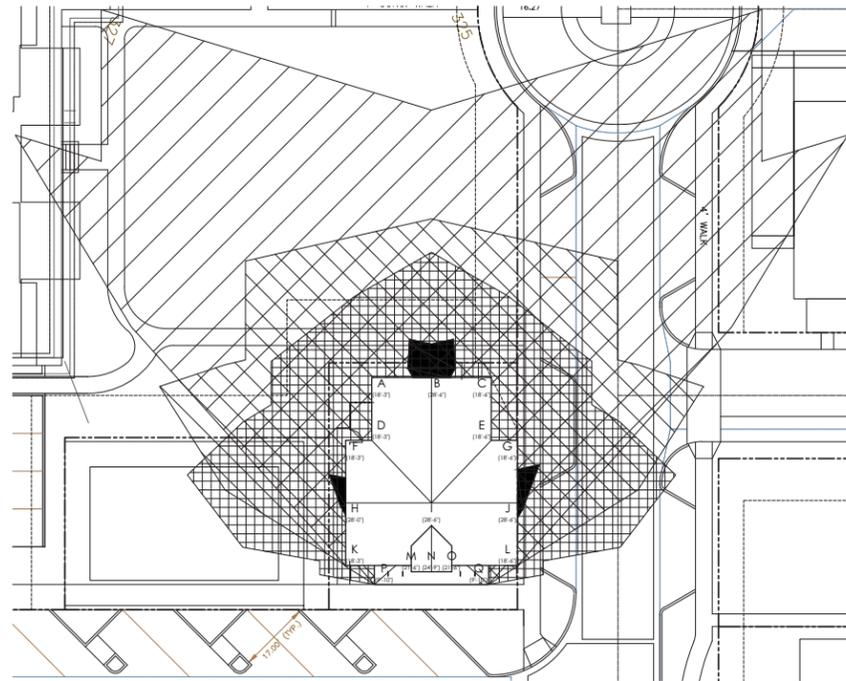
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Boulder, Colorado
p: 303-442-3351
f: 303-447-3933

Blue Sky Bridge

2619 Iris Hollow Pl
Boulder, CO 80304

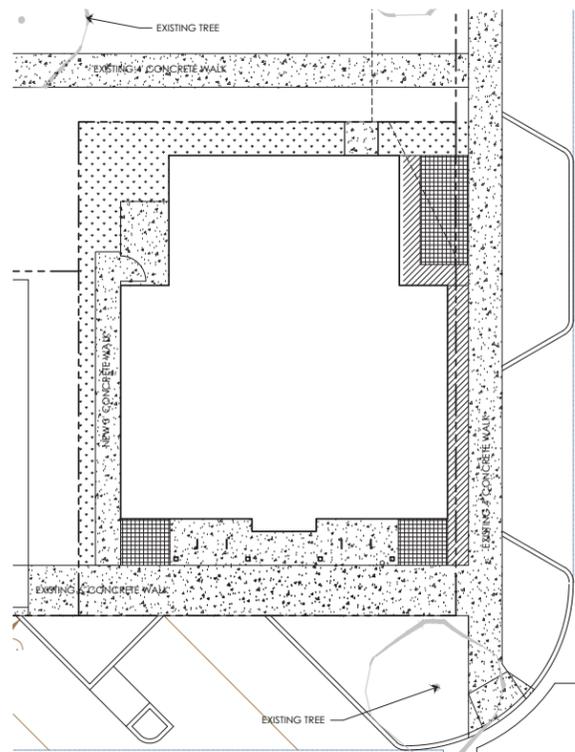
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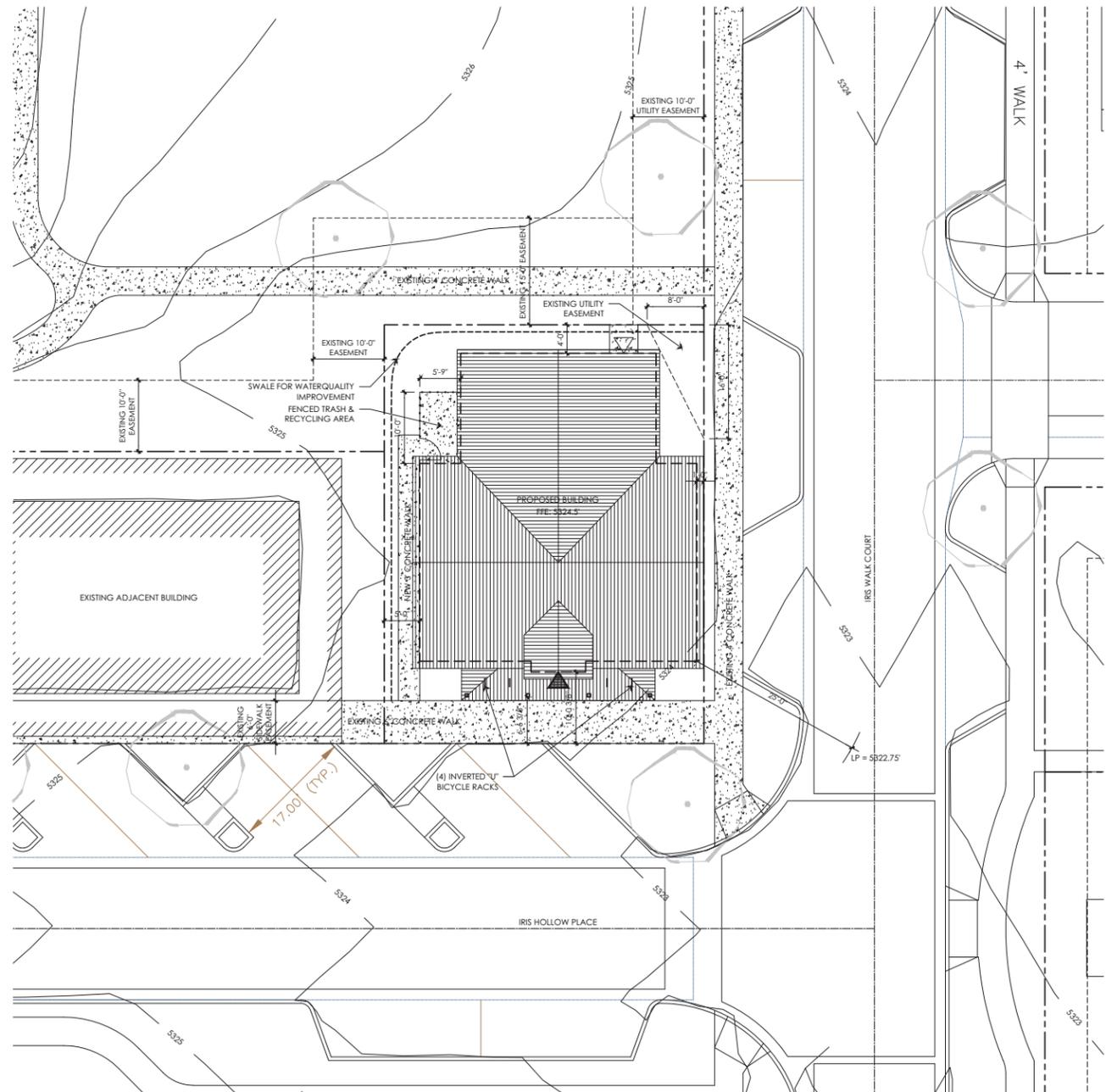
2 Solar Shadow Plan (Solar Access Area II)
1" = 20'-0"

SOLAR SHADOW LEGEND	
	SHADOWS DEC 21 9AM - 3PM
	SHADOWS MAR/SEP 21 9AM - 3PM
	SHADOWS JUN 21 9AM - 3PM
	SHADOWS PER SOLAR ACCESS AREA II - 25 FT SOLAR FENCE



3 Landscape Concept Plan
1" = 10'-0"

LANDSCAPE PLANTING LEGEND	
	NATIVE GRASS
	SPREADING GROUNDCOVER (EVERGREEN; 1'-3' HEIGHT)
	PERENNIAL/ACCENT (0.5'-3' HEIGHT)



1 Site Development Plan
1" = 10'-0"

USE/SITE REVIEW
07.31.2014

NOT FOR
CONSTRUCTION

SHEET No.

SR-2

Page 8 of 11
SITE PLAN



COBURN
ARCHITECTURE

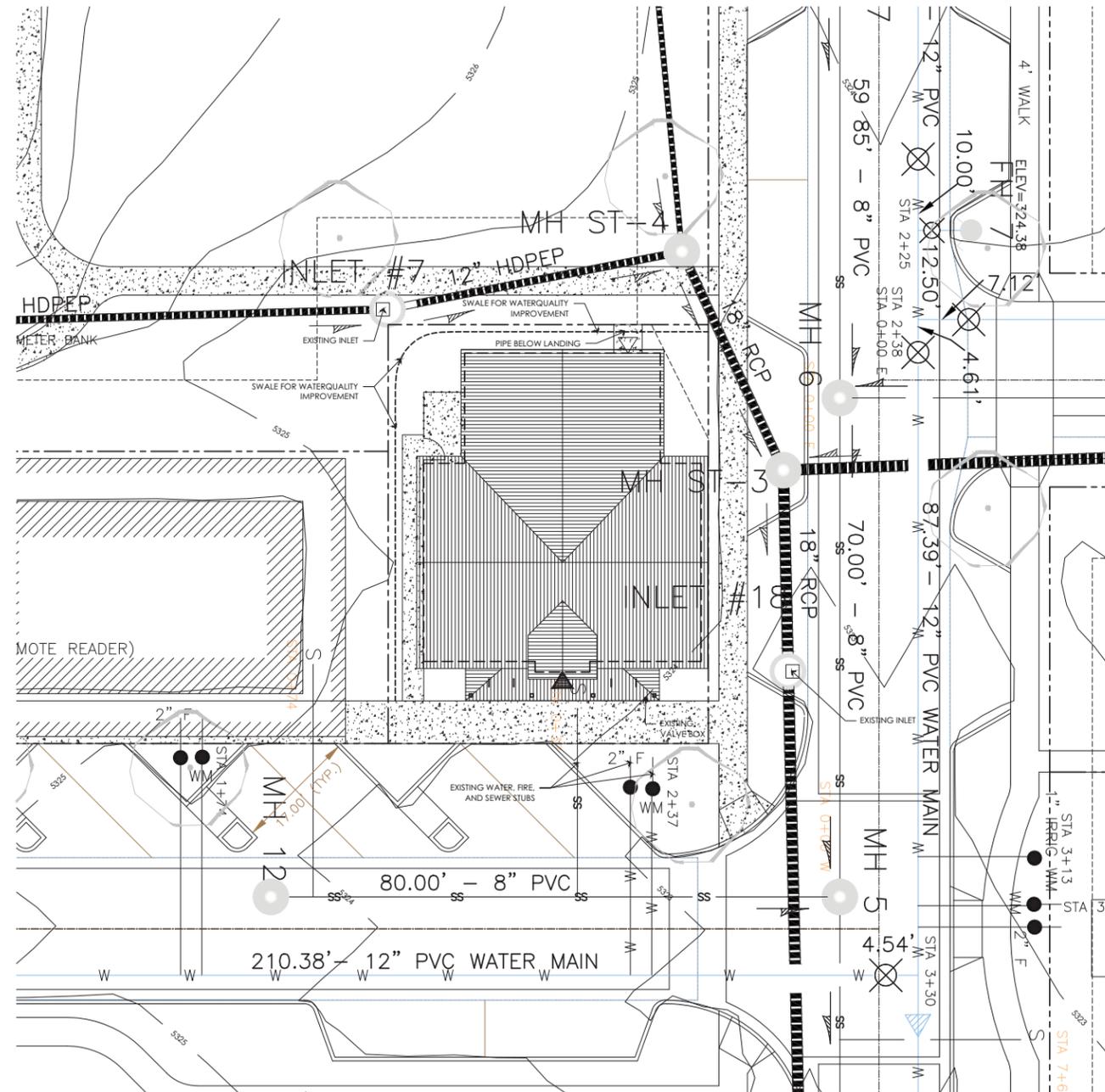
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Blue Sky Bridge

2619 Iris Hollow PI
Boulder, CO 80304

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CORRECTIONS				



NOTE: WATER & SANITARY SEWER STUBS ARE EXISTING. ALL WET UTILITY INFRASTRUCTURE (MAINS, INLETS, MANHOLES, ETC. ARE EXISTING. REFER TO THE ORIGINAL IRIS HOLLOW SITE ENGINEERING PLANS AND AS-BUILTS.

1 As-Built Utility Connection & Drainage Plan
1" = 10'-0"



USE/SITE REVIEW
07.31.2014

NOT FOR
CONSTRUCTION

SHEET No.

SR-3

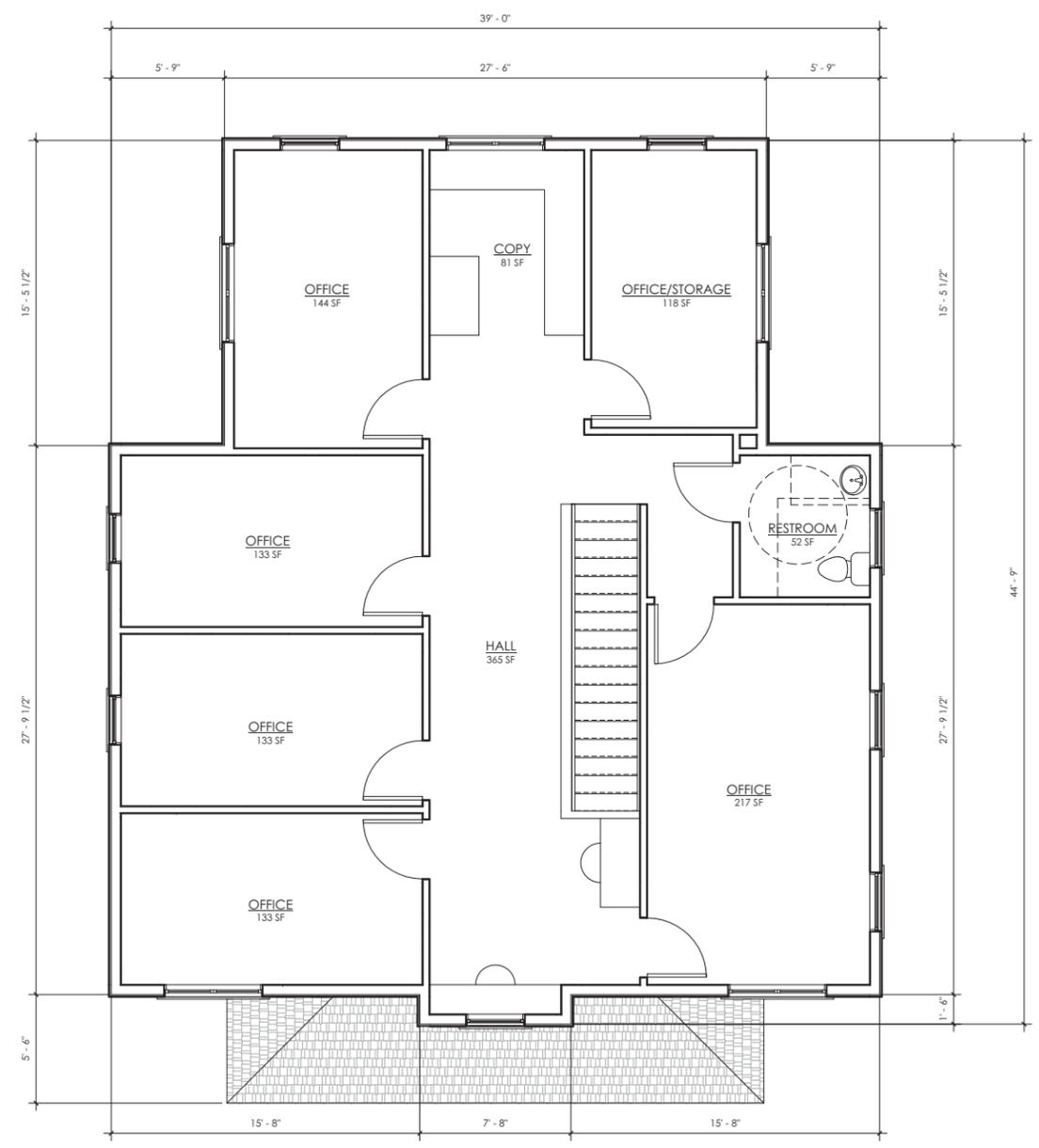
UTILITIES & DRAINAGE

Blue Sky Bridge

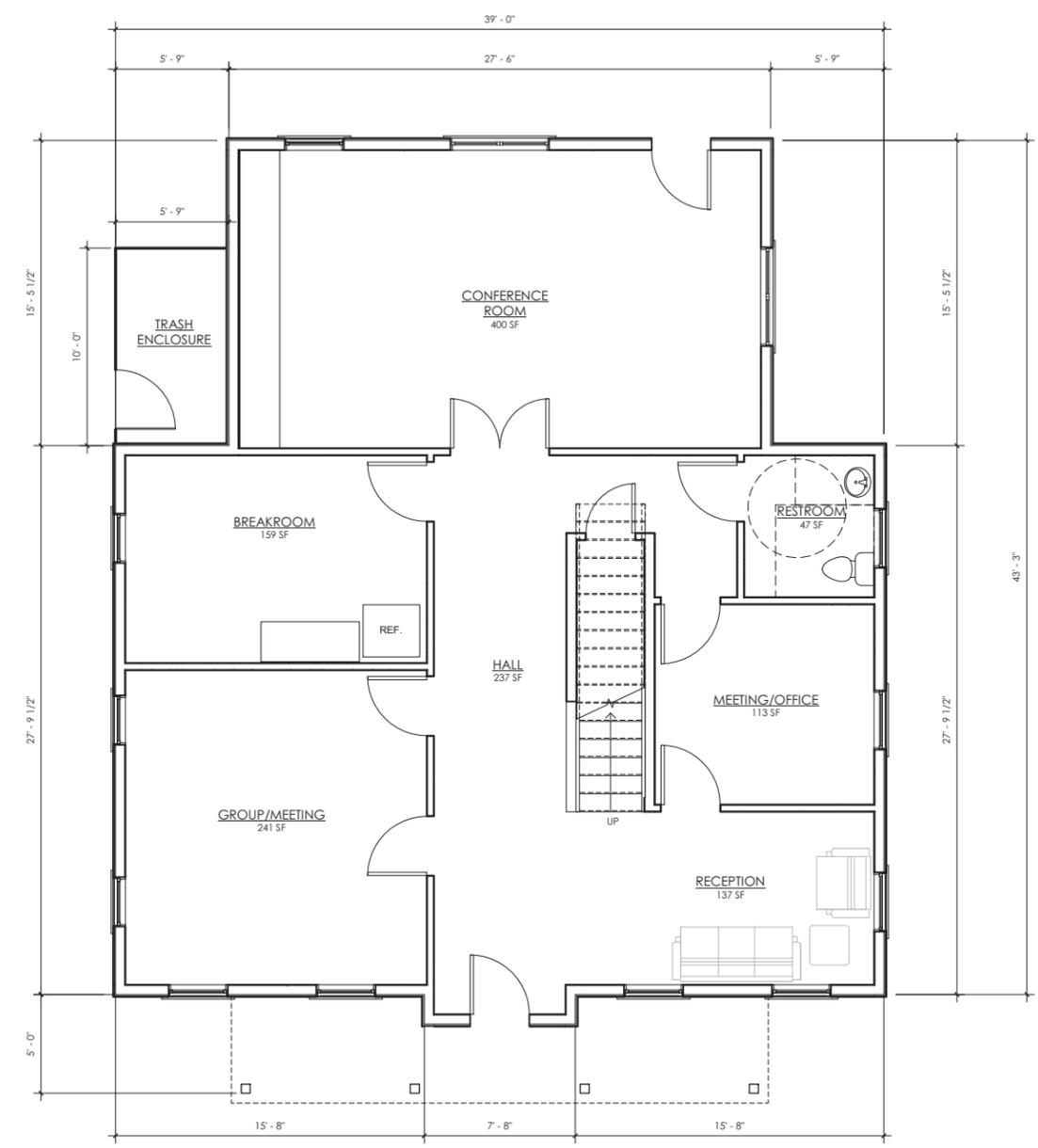
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Boulder, CO 80304

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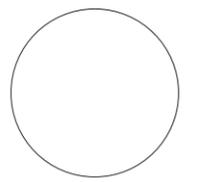
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Resubmittal				
Use/Site Review	FB	PW		07.31.14
Corrections				



2 Second Floor Plan
1/4" = 1'-0"



1 Ground Floor Plan
1/4" = 1'-0"



USE/SITE REVIEW
07.31.2014

NOT FOR
CONSTRUCTION

SHEET No.

AR-1

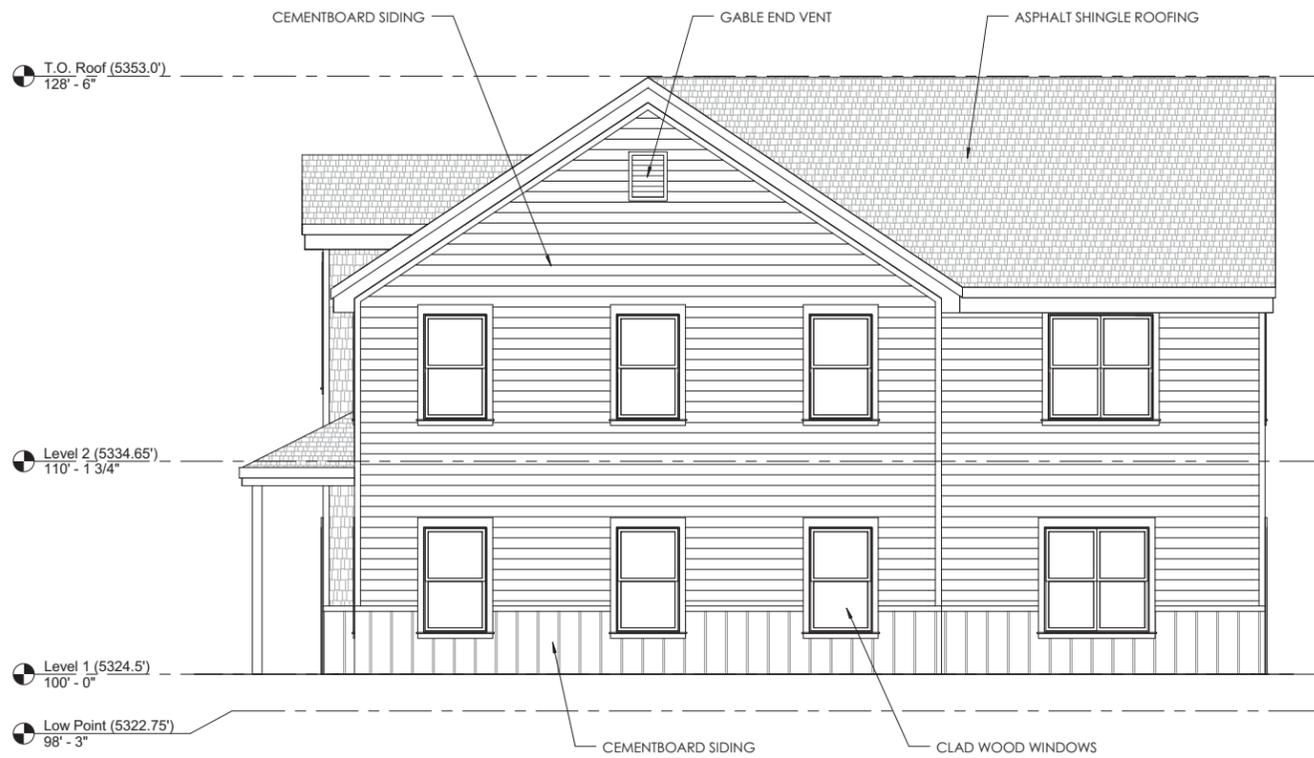


COBURN
ARCHITECTURE

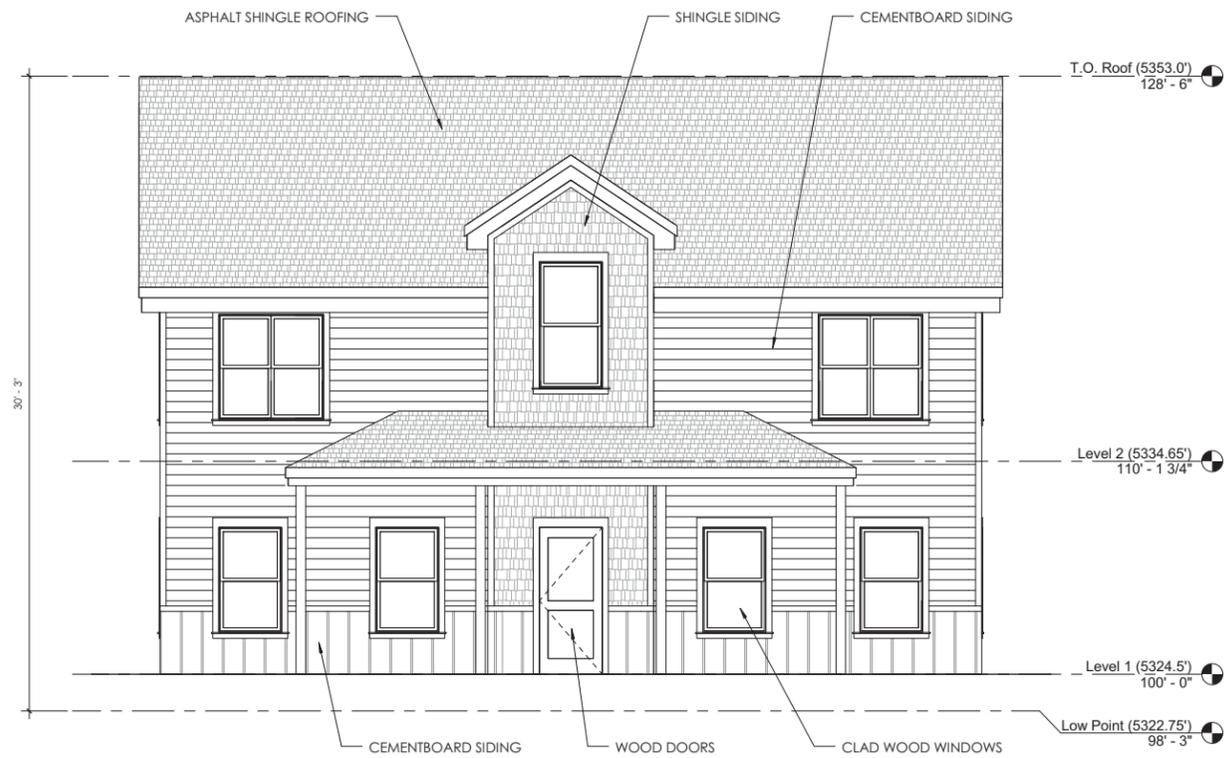
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Blue Sky Bridge

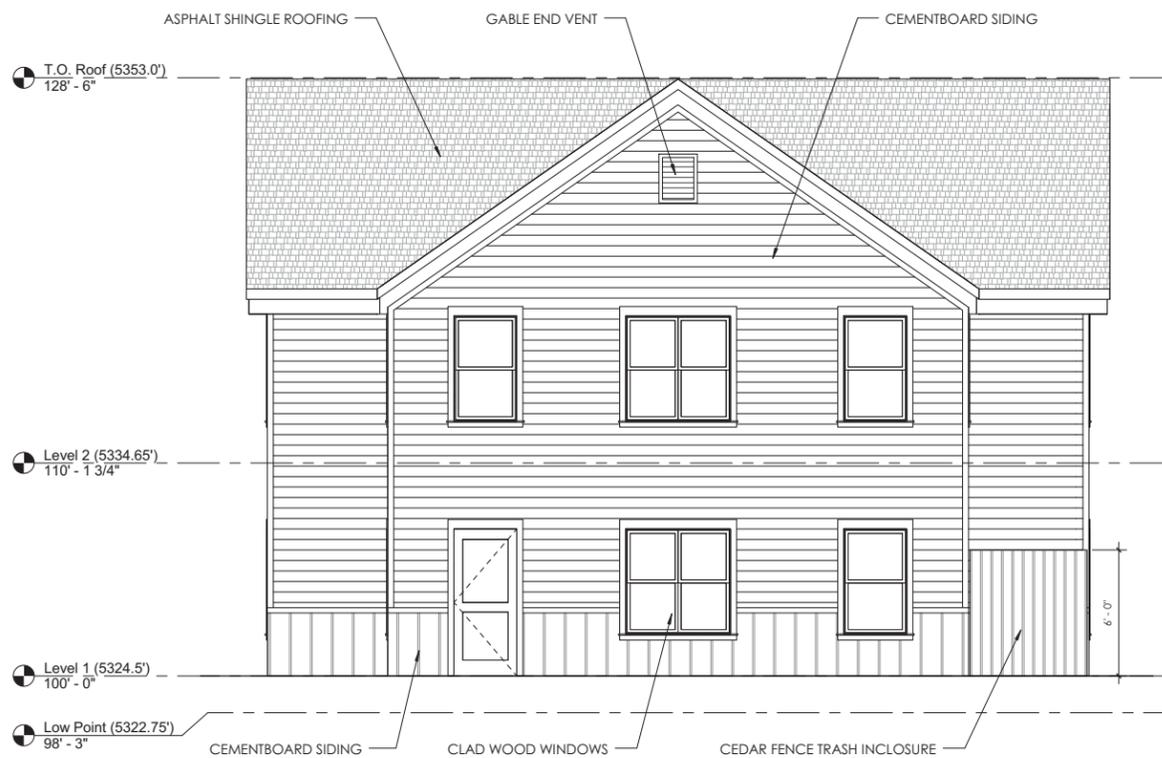
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Boulder, CO 80304



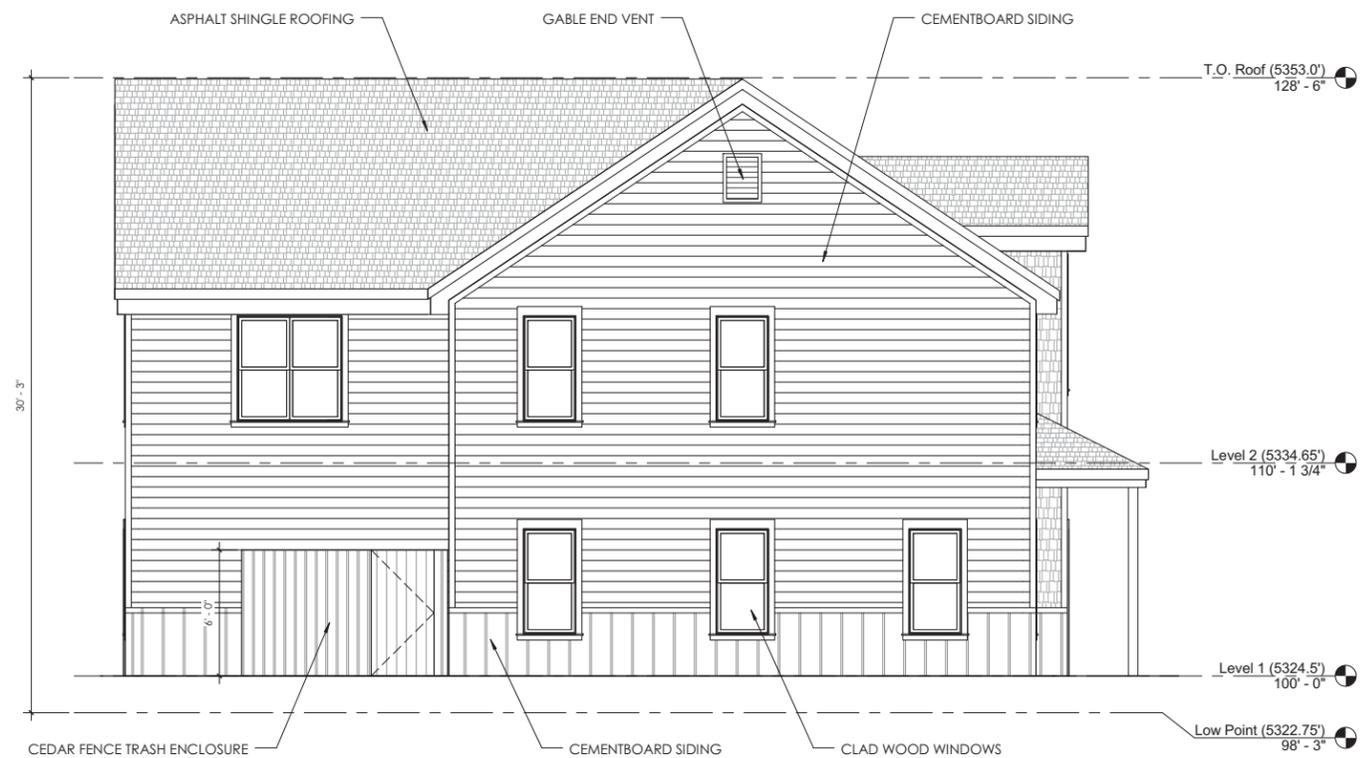
2 East Elevation
1/4" = 1'-0"



1 South Elevation
1/4" = 1'-0"



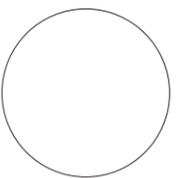
4 North Elevation
1/4" = 1'-0"



3 West Elevation
1/4" = 1'-0"

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Resubmittal					
Use/Site Review	FB	PW			07.31.14
Corrections					



USE/SITE REVIEW
07.31.2014

NOT FOR
CONSTRUCTION

SHEET No.

AR-2

Blue Sky Bridge Written Statement & Parking Analysis

2619 Iris Hollow Place

LUR2014-00036

8/4/2014

Blue Sky Bridge currently has 7 staff positions ranging from 28 to 40 hours per week. In addition, there are generally one to three volunteer interns in the building at any time. Hours are generally 9am to 5pm. Occasionally a staff member or two will be onsite outside of these hours. The staffing plan is not anticipated to change with Blue Sky Bridge's expansion to two buildings.

Most staff members and clients travel to Blue Sky Bridge by car. Two staff members occasionally ride bicycles. One regularly rides a 50cc scooter that does not require a parking space. On average, there are 7 clients at Blue Sky Bridge each day. The most clients in the building at any particular time is generally 5, in 3 cars, and the maximum number of total clients at Blue Sky Bridge throughout any given day is 15.

Each staff member is required to attend off-site meetings on a regular basis. Four staff members have one to two meetings offsite each week lasting 2-3 hours. One staff member is out of the office daily for meetings lasting 1-8 hours. One staff member is out of the office 3-4 days/week, with each off-site lasting 3-4 hours.

Currently there is ample on-street parking near the building to accommodate the needs of Blue Sky Bridge. This additional lot adds 8 off-street parking spaces though it is not anticipated that additional parking is needed, so this will further free up on-street parking.

There are several bicycle parking structures in the area, including on the existing Blue Sky Bridge property, and the neighborhood is accessible by bicycle and pedestrian paths. Public transportation is available in close proximity, with RTD route 208 stops located on Iris Ave and route 205, 208 and BOLT stops located on 28th St.

Blue Sky Bridge currently has sufficient parking that does not impact the parking in the neighborhood. Moving into a second building is not anticipated to cause any increase in traffic or parking requirements. The additional off-street parking available with the new lot will reduce the already minimal demand on on-street parking.

Blue Sky Bridge Written Statement & Parking Analysis

2619 Iris Hollow Place
LUR2014-00036
8/4/2014

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Case #: LUR2014-00036

Project Name: Blue Sky Bridge Office Expansion

Date: Sept. 4, 2014

USE REVIEW CRITERIA

Criteria for Review: No use review application will be approved unless the approving agency finds all of the following:

✓ (1) **Consistency with Zoning and Non-Conformity:** The use is consistent with the purpose of the zoning district as set forth in **Section 9-5-2(c), "Zoning Districts Purposes," B.R.C. 1981**, except in the case of a non-conforming use;

The project site is located just southeast of the intersection of Iris and Folsom within the RM-3 (Residential- Medium 3) zoning district (see **Figure 2** for zoning map). Per section 9-5-2(c), B.R.C. 1981, the RM-3 zone district is defined as "*Medium density residential areas which have been or are to be primarily used for attached residential development, where each unit generally has direct access to ground level, and where complementary uses may be permitted under certain conditions.*" The proposed use is considered a "Professional Office" use per section 9-16, of the Boulder Revised Code. Per section 9-6-1, "Use Standards," B.R.C. 1981, professional office uses are allowed in the RM-3 zone district if approved through a Use Review.

 (2) **Rationale:** The use either:

✓ (A) **Provides direct service or convenience to or reduces adverse impacts to the surrounding uses or neighborhood;**

The proposed use is an expansion of the existing Blue Sky Bridge office facility located adjacent to the subject site at 2617 Iris Hollow Pl. Blue Sky Bridge offers consultation services to professionals in Boulder County in regards to concerns about children and families in relation to child abuse allegations. Blue Sky Bridge provides clinical interventions to families who may have experienced trauma, and provides educational opportunities through specialized programs, outreach, and formal training. In addition to providing clinical services to approximately 300 families and 170 individuals per year, many of whom are Boulder residents, Blue Sky Bridge provides consultation services to nine different law enforcement jurisdictions within Boulder County, and several other organizations, departments, and individuals within the city and county. Overall, Blue Sky Bridge provides a variety of direct services to the community. In addition, the proposed use would represent a reduction in impact from the previously approved bed and breakfast use, as the new building is intended solely to increase the amount of space available for Blue Sky Bridge and does not include any expansion employees, customers or traffic generation. Therefore the impacts to the surrounding area will not change from the existing use, as opposed to a separate entity moving in with different operating characteristics and traffic and parking needs.

N/A (B) **Provides a compatible transition between higher intensity and lower intensity uses;**

N/A (C) **Is necessary to foster a specific city policy, as expressed in the Boulder Valley Comprehensive Plan, including, without limitation, historic preservation, moderate income housing, residential and non-residential mixed uses in appropriate locations, and group living arrangements for special populations; or**

N/A (D) **Is an existing legal non-conforming use or a change thereto that is permitted under subsection (e) of this section;**

✓ (3) Compatibility: The location, size, design, and operating characteristics of the proposed development or change to an existing development are such that the use will be reasonably compatible with and have minimal negative impact on the use of nearby properties or for residential uses in industrial zoning districts, the proposed development reasonably mitigates the potential negative impacts from nearby properties;

The proposal is to develop a vacant infill site within the Iris Hollow PUD with a new office building to serve the existing organization that currently occupies the building on the adjacent lot to the west. The location, size and design of the proposed building are in keeping with the original approved site plan for the Iris Hollow PUD, which included lot regulations calling for a 35' tall, 3,400 sq. ft. building facing south with minimal setbacks along the south and east sides of the property. The current proposal is slightly lower in height at 30'-3" tall and slightly smaller at 3,131 sq. ft., but honors the original intent by maintaining the desired orientation and keeping pedestrian-level interest with extensive fenestration, a covered entryway and minimal setbacks along the south and east sides of the property.

Further, the applicant has indicated that the proposed expansion is to provide additional space but will not entail any changes to the existing operating characteristics. The existing Blue Sky Bridge office has been located at 2617 Iris Hollow Pl. since 2001, and during that time has had minimal impacts on the surrounding uses. There are currently 7 staff positions ranging from 28 to 40 hours per week, as well as one to three volunteer interns in the building at any time. Hours are generally 9am to 5pm, seven days per week. Per the applicant's written statement (see **Attachment A**), there are an average of 7 clients at Blue Sky Bridge each day. The most clients in the building at any particular time is generally 5 and the maximum number of total clients at Blue Sky Bridge throughout any given day is 15. There is ample on-street parking available on both Iris Hollow Pl. and Iris Walk Ct., and the daytime hours of operation coincide with the time of day that many of the residents are at work, so parking has not historically been an issue. Given that the new building will include rights to 8 reserved spaces in the nearby covered condominium garage to the north, the applicant anticipates being able to further free up on-street parking, so the new use may actually reduce the impacts associated with an already low-impact use.

✓ (4) Infrastructure: As compared to development permitted under [Section 9-6-1](#), "Schedule of Permitted Uses of Land," B.R.C. 1981, in the zone, or as compared to the existing level of impact of a non-conforming use, the proposed development will not significantly adversely affect the infrastructure of the surrounding area, including, without limitation, water, wastewater, and storm drainage utilities and streets;

All of the existing infrastructure required to serve the proposed use is existing, as the subject lot is a vacant infill site within an otherwise fully developed mixed-use neighborhood, and has been anticipated for commercial development since the time of the original Iris Hollow PUD approval.

✓ (5) Character of Area: The use will not change the predominant character of the surrounding area or the character established by adopted design guidelines or plans for the area; and

The project site is within the Iris Hollow PUD, which was originally approved in 1996 as a mixed-use development containing 86 mixed-density residential units and a daycare facility, laundromat, office use and post office. In addition, the subject lot was intended to hold a two-story, 3,400 sq. ft. bed and breakfast use; however, since that time there has not been a market demand for a bed and breakfast use, so the site has remained vacant. At 30'-3" tall, the current proposal is slightly lower in height than the previously approved use and slightly smaller at 3,131 sq. ft., but honors the original intent by maintaining pedestrian-level interest with a covered entryway and ample fenestration as well as minimal setbacks along the south and east sides of the property. The exterior material palette of shingle siding, cement-board lap and board & batten siding is consistent with the architectural intent of the approved Iris Hollow PUD as well as the existing aesthetic in the area, and the projects includes various elements encouraged in the Iris Hollow PUD approval including a gabled roof, wood-clad windows, and a covered entryway feature. Overall, staff has found that the proposal is in keeping with the intent of the Iris Hollow PUD, and will maintain the existing small-scale neo-traditional mixed use character of the surrounding neighborhood.

N/A (6) **Conversion of Dwelling Units to Non-Residential Uses:** There shall be a presumption against approving the conversion of dwelling units in the residential zoning districts set forth in [Subsection 9-5-2\(c\)\(1\)\(a\)](#), B.R.C. 1981, to non-residential uses that are allowed pursuant to a use review, or through the change of one non-conforming use to another non-conforming use. The presumption against such a conversion may be overcome by a finding that the use to be approved serves another compelling social, human services, governmental, or recreational need in the community including, without limitation, a use for a day care center, park, religious assembly, social service use, benevolent organization use, art or craft studio space, museum, or an educational use.

Not applicable, as the proposal does not include the conversion of any dwelling units to non-residential use.

SITE REVIEW CRITERIA

Criteria for Review: No site review application shall be approved unless the approving agency finds that:

(1) Boulder Valley Comprehensive Plan:

✓ (A) The proposed site plan is consistent with the land use map and the service area map and, on balance, the policies of the Boulder Valley Comprehensive Plan.

The proposed plan is consistent with the purposes and policies of the Boulder Valley Comprehensive Plan. Specifically, the proposal to construct a high-quality building within an existing mixed-use neighborhood to expand an existing local agency providing critical social services meets the following BVCP goals:

- 2.01 Unique Community Identity
- 2.03 Compact Development Pattern
- 2.14 Mix of Complementary Land Uses
- 2.30 Sensitive Infill and Redevelopment
- 2.32 Physical Design for People
- 8.10 Support for Community Facilities

N/A (B) The proposed development shall not exceed the maximum density associated with the Boulder Valley Comprehensive Plan residential land use designation. Additionally, if the density of existing residential development within a three-hundred-foot area surrounding the site is at or exceeds the density permitted in the Boulder Valley Comprehensive Plan, then the maximum density permitted on the site shall not exceed the lesser of:

Not applicable, as the proposed use is not residential but an office use.

___(i) **The density permitted in the Boulder Valley Comprehensive Plan, or,**

___(ii) **The maximum number of units that could be placed on the site without waiving or varying any of the requirements of [chapter 9-8](#), "Intensity Standards," B.R.C. 1981.**

✓ (C) The proposed development's success in meeting the broad range of BVCP policies considers the economic feasibility of implementation techniques required to meet other site review criteria.

The proposed project sensitively utilizes an infill site in providing an appropriate use for the existing mixed-use neighborhood context. This is achieved by maintaining consistency with the existing Iris Hollow PUD standards in terms of the scale and massing of the building design, and by maintaining the existing operating characteristics of the adjacent use that the new

building will serve so as to ensure that no additional impacts will be generated. The use utilizes an infill site where utilities, roads, and other infrastructure exist.

(2) Site Design: Projects should preserve and enhance the community's unique sense of place through creative design that respects historic character, relationship to the natural environment, multi-modal transportation connectivity and its physical setting. Projects should utilize site design techniques which are consistent with the purpose of site review in subsection (a) of this section and enhance the quality of the project. In determining whether this subsection is met, the approving agency will consider the following factors:

✓(A) Open Space: Open space, including, without limitation, parks, recreation areas, and playgrounds:

While the small lot doesn't allow large amounts of open space, the property is located directly adjacent to park space and in close proximity to Boulder's multi-use path network.

✓(i) Useable open space is arranged to be accessible and functional and incorporates quality landscaping, a mixture of sun and shade and places to gather;

The subject lot is a part of the Iris Hollow PUD, which has a variety of accessible and functional open space areas that provide a mixture of sun and shade and places to gather.

✓(ii) Private open space is provided for each detached residential unit;

The subject lot is a part of the Iris Hollow PUD, which has a variety of accessible and functional open space areas that provide a mixture of sun and shade and places to gather. Each of the existing single family detached units has access to private open space in some capacity.

N/A(iii) The project provides for the preservation of or mitigation of adverse impacts to natural features, including, without limitation, healthy long-lived trees, significant plant communities, ground and surface water, wetlands, riparian areas, drainage areas and species on the federal Endangered Species List, "Species of Special Concern in Boulder County" designated by Boulder County, or prairie dogs (*Cynomys ludovicianus*), which is a species of local concern, and their habitat;

Not applicable, as the subject site is already graded and the surrounding area is also fully developed.

✓(iv) The open space provides a relief to the density, both within the project and from surrounding development;

The subject lot is a part of the Iris Hollow PUD, which has a variety of accessible and functional open space areas that provide a relief to the density of the development and places for both active and passive recreation. Each of the existing single family detached units has access to private open space in some capacity.

✓(v) Open space designed for active recreational purposes is of a size that it will be functionally useable and located in a safe and convenient proximity to the uses to which it is meant to serve;

The subject lot is a part of the Iris Hollow PUD, which has a variety of accessible and functional open space areas that provide a mixture of sun and shade and places for both active and passive recreation.

✓(vi) The open space provides a buffer to protect sensitive environmental features and natural areas; and

The subject lot is a part of the Iris Hollow PUD, which has a variety of accessible and functional open space areas that provide a buffer to protect sensitive environmental features and natural areas.

✓ (vii) If possible, open space is linked to an area- or city-wide system.

The subject lot is a part of the Iris Hollow PUD, which includes several linkages to bike paths along Iris and Folsom.

___ (B) *Open Space in Mixed Use Developments (Developments that contain a mix of residential and non-residential uses)*

✓ (i) The open space provides for a balance of private and shared areas for the residential uses and common open space that is available for use by both the residential and non-residential uses that will meet the needs of the anticipated residents, occupants, tenants, and visitors of the property; and

The subject lot is a part of the Iris Hollow PUD, which has a variety of private and shared open space areas, including a shared park space immediately to the north of the subject site, that provide a mixture of sun and shade and places for both residents and visitors to gather.

✓ (ii) The open space provides active areas and passive areas that will meet the needs of the anticipated residents, occupants, tenants, and visitors of the property and are compatible with the surrounding area or an adopted plan for the area.

Please see response above.

___ (C) *Landscaping*

✓ (i) The project provides for aesthetic enhancement and a variety of plant and hard surface materials, and the selection of materials provides for a variety of colors and contrasts and the preservation or use of local native vegetation where appropriate;

The proposed landscaping will be compatible with the plant materials existing throughout the Iris Hollow neighborhood. To provide an attractive streetscape, the building setbacks along the public rights of way will be landscaped with perennial beds and spreading groundcover. The native grass in the back yard will blend in with the surrounding areas.

N/A (ii) Landscape design attempts to avoid, minimize, or mitigate impacts to important native species, plant communities of special concern, threatened and endangered species and habitat by integrating the existing natural environment into the project;

Not applicable, as the subject site is already graded and the surrounding area is also fully developed. There are no species of special concern known in the area.

✓ (iii) The project provides significant amounts of plant material sized in excess of the landscaping requirements of [sections 9-9-12](#), "Landscaping and Screening Standards" and [9-9-13](#), "Streetscape Design Standards," B.R.C. 1981; and

A detailed landscape and irrigation plan meeting the requirements of section 9-9-12(d)(1) B.R.C. 1981 will be required at the time of building permit.

✓ (iv) The setbacks, yards, and useable open space along public rights-of-way are landscaped to provide attractive streetscapes, to enhance architectural features, and to contribute to the development of an attractive site plan.

The proposed landscaping will be compatible with the plant materials existing throughout the Iris Hollow neighborhood. To provide an attractive streetscape, the building setbacks along the public rights of way will be landscaped with perennial beds and spreading groundcover. The native grass in the back yard will blend in with the surrounding areas.

✓(D) Circulation: Circulation, including, without limitation, the transportation system that serves the property, whether public or private and whether constructed by the developer or not:

As this is an urban infill project, the streets have already been built and this project supports its design with pedestrian scale and architectural interest. As a part of the Iris Hollow neighborhood, pedestrians are supported with existing sidewalks and nearby access to public transportation. The multi-use path network is in close proximity, supporting pedestrians and bikes.

✓(i) High speeds are discouraged or a physical separation between streets and the project is provided;

Streets and sidewalks are existing.

✓(ii) Potential conflicts with vehicles are minimized;

The proposed building is in keeping with the existing street system in the Iris Hollow PUD. 8 new bike racks will be provided off the existing sidewalk in excess of the code requirement.

✓(iii) Safe and convenient connections are provided that support multi-modal mobility through and between properties, accessible to the public within the project and between the project and the existing and proposed transportation systems, including, without limitation, streets, bikeways, pedestrianways and trails;

There are existing sidewalks across the south and east sides of the subject lot, which will remain in place following construction of the proposed office building.

✓(iv) Alternatives to the automobile are promoted by incorporating site design techniques, land use patterns, and supporting infrastructure that supports and encourages walking, biking, and other alternatives to the single-occupant vehicle;

The applicant has requested a 20% parking reduction to allow for 8 spaces where 10 are required. 8 reserved spaces are provided for the site in the covered condominium garage to the north. The applicant is proposing to provide 8 bicycle parking spaces on-site where no bike spaces are required by the parking standards for the RM-3 zone. As a part of the Iris Hollow neighborhood, pedestrians are supported with existing sidewalks and nearby access to public transportation along Iris. The multi-use path network is in close proximity, supporting pedestrians and bikes.

✓(v) Where practical and beneficial, a significant shift away from single-occupant vehicle use to alternate modes is promoted through the use of travel demand management techniques;

Please see Attachment A, "Applicant's Proposed Plans and Written Statement," for additional information. Because the proposal is to maintain the current operating characteristics of the existing office use at 2617 Iris and no additional traffic or parking demand will be generated, staff has determined that requiring additional TDM strategies would not be practical or beneficial.

✓(vi) On-site facilities for external linkage are provided with other modes of transportation, where applicable;

As a part of the Iris Hollow neighborhood, pedestrians are supported with existing sidewalks and nearby access to public transportation. The multi-use path network is in close proximity, supporting pedestrians and bikes.

N/A (vii) The amount of land devoted to the street system is minimized; and

Not applicable, as the streets are already existing.

✓ (viii) The project is designed for the types of traffic expected, including, without limitation, automobiles, bicycles, and pedestrians, and provides safety, separation from living areas, and control of noise and exhaust.

All of the transportation infrastructure is existing, and this project will be designed to fit into the existing context. Ample on-street parking is available for visitors, and the new building will also include 8 bike parking spaces in front of the building in excess of the parking requirements for the zone.

(E) Parking

Eight parking spaces located in the covered condominium parking garage are allocated to this project. The project proposes to eliminate the two on-site spaces called for per the original Iris Hollow Site Review and requests a parking reduction from 10 required spaces to 8. There is ample street parking existing along Iris Hollow PI and Iris Walk Ct, and the new building will not result in expansion of operations or increases in staff and traffic demand.

N/A (i) The project incorporates into the design of parking areas measures to provide safety, convenience, and separation of pedestrian movements from vehicular movements;

Not applicable, as the applicant is not proposing to add any additional parking to the subject lot. There are currently off-street 8 parking spaces allocated for use of this lot.

N/A (ii) The design of parking areas makes efficient use of the land and uses the minimum amount of land necessary to meet the parking needs of the project;

Not applicable, as the applicant is not proposing to add any additional parking to the subject lot. There are currently off-street 8 parking spaces allocated for use of this lot.

N/A (iii) Parking areas and lighting are designed to reduce the visual impact on the project, adjacent properties, and adjacent streets; and

Not applicable, as the applicant is not proposing to add any additional parking to the subject lot. There are currently off-street 8 parking spaces allocated for use of this lot.

N/A (iv) Parking areas utilize landscaping materials to provide shade in excess of the requirements in Subsection [9-9-6](#) (d), "Parking Area Design Standards," and Section [9-9-14](#), "Parking Lot Landscaping Standards," B.R.C. 1981.

Not applicable, as the applicant is not proposing to add any additional parking to the subject lot. There are currently off-street 8 parking spaces allocated for use of this lot.

(F) Building Design, Livability, and Relationship to the Existing or Proposed Surrounding Area

✓ (i) The building height, mass, scale, orientation, and configuration are compatible with the existing character of the area or the character established by an adopted plan for the area;

The height, mass, scale and orientation of the building are in keeping with the original approved site plan for the Iris Hollow PUD, which included lot regulations calling for a 35' tall, 3,400 sq. ft. building facing south with minimal setbacks along the south and east sides of the property. The current proposal is slightly lower in height at 30'-3" tall and slightly smaller at 3,131 sq. ft., but honors the original intent by maintaining the desired orientation and keeping pedestrian-level interest with extensive fenestration, a covered entryway and minimal setbacks along the south and east sides of the property.

✓ (ii) The height of buildings is in general proportion to the height of existing buildings and the proposed or projected heights of approved buildings or approved plans for the immediate area;

The height of the proposed building is 30'-3", which is within the 35' height initially approved for the site in the Iris Hollow PUD documents and is also consistent with the existing buildings in the area, which range from 25' to 32' in height.

✓ (iii) The orientation of buildings minimizes shadows on and blocking of views from adjacent properties;

The building orientation is in keeping with the building orientation approved as part of the original Iris Hollow PUD. Further, the site immediately to the north of the subject lot is a park, and as such will not be affected by shading from the proposed building.

✓ (iv) If the character of the area is identifiable, the project is made compatible by the appropriate use of color, materials, landscaping, signs, and lighting;

The exterior material palette of shingle, cement-board lap and board & batten siding is consistent with the architectural intent of the approved Iris Hollow PUD as well as the existing aesthetic in the area. The roof pitch, window style and configuration, and covered entryway feature are also specifically encouraged in the Iris Hollow PUD documents.

✓ (v) Projects are designed to a human scale and promote a safe and vibrant pedestrian experience through the location of building frontages along public streets, plazas, sidewalks and paths, and through the use of building elements, design details and landscape materials that include, without limitation, the location of entrances and windows, and the creation of transparency and activity at the pedestrian level;

The proposed building successfully creates activity and transparency at the pedestrian level by incorporating a variety of siding materials including vertical and horizontal cementboard and shingles, as well as several first-story windows and a covered entryway facing the sidewalk. The proposed building is in keeping with the pedestrian-scaled design of the existing buildings in the area.

N/A (vi) To the extent practical, the project provides public amenities and planned public facilities;

Not applicable, as the subject lot is part of the Iris Hollow PUD, which included numerous public amenities and public facilities that have already been developed.

N/A (vii) For residential projects, the project assists the community in producing a variety of housing types, such as multifamily, townhouses and detached single family units, as well as mixed lot sizes, number of bedrooms and sizes of units;

Not applicable, as this is a non-residential project.

N/A (viii) For residential projects, noise is minimized between units, between buildings, and from either on-site or off-site external sources through spacing, landscaping, and building materials;

Not applicable, as this is a non-residential project.

✓ (ix) A lighting plan is provided which augments security, energy conservation, safety, and aesthetics;

A lighting plan will be required at time of building permit.

N/A (x) The project incorporates the natural environment into the design and avoids, minimizes, or mitigates impacts to natural systems;

Not applicable, as the site is currently graded and the surrounding area is fully developed.

✓ (xi) Buildings minimize or mitigate energy use; support on-site renewable energy generation and/or energy management systems; construction wastes are minimized; the project mitigates urban heat island effects; and the project reasonably mitigates or minimizes water use and impacts on water quality.

The building will be designed to comply with the 2012 IECC as adopted by the City of Boulder and its location and roof plan are ideal for future installation of solar panels. The majority of construction waste will be recycled during construction. The open space features of the Holiday neighborhood, including a large park adjacent to the subject property, help mitigate urban heat island effects.

✓ (xii) Exteriors or buildings present a sense of permanence through the use of authentic materials such as stone, brick, wood, metal or similar products and building material detailing;

The exterior material palette of cement-board lap and board & batten siding and asphalt shingles is consistent with the architectural intent of the approved Iris Hollow PUD as well as the existing aesthetic in the area. The roof pitch, window style and configuration, and covered entryway feature are also specifically encouraged in the Iris Hollow PUD documents. Additional high-quality materials included in the project are wood shingle siding in the entry way and wood-clad windows.

✓ (xiii) Cut and fill are minimized on the site, the design of buildings conforms to the natural contours of the land, and the site design minimizes erosion, slope instability, landslide, mudflow or subsidence, and minimizes the potential threat to property caused by geological hazards;

There will be no cut and fill on site, as the existing site is already graded.

N/A (xiv) In the urbanizing areas along the Boulder Valley Comprehensive Plan boundaries between Area II and Area III, the building and site design provide for a well-defined urban edge; and

Not applicable, as this is not located in an urbanizing area along the Boulder Valley Comprehensive Plan boundary between Area II and Area III.

N/A (xv) In the urbanizing areas located on the major streets shown on the map in Appendix A of this title near the Boulder Valley Comprehensive Plan boundaries between Area II and Area III, the buildings and site design establish a sense of entry and arrival to the City by creating a defined urban edge and a transition between rural and urban areas.

Not applicable, as this site is not a gateway site as anticipated by the BVCP

N/A (G) Solar Siting and Construction: For the purpose of ensuring the maximum potential for utilization of solar energy in the City, all applicants for residential site reviews shall place streets, lots, open spaces, and buildings so as to maximize the potential for the use of solar energy in accordance with the following solar siting criteria:

Not applicable, as this project is non-residential.

N/A (H) Additional Criteria for Poles Above the Permitted Height: No site review application for a pole above the permitted height will be approved unless the approving agency finds all of the following:

Not applicable, as this proposal does not include a request for a height modification.

N/A (I) Land Use Intensity Modifications:

Not applicable, as this project does not include a request for a land use intensity modification.

N/A (J) Additional Criteria for Floor Area Ratio Increase for Buildings in the BR-1 District:

Not applicable.

(K) Additional Criteria for Parking Reductions: The off-street parking requirements of [section 9-9-6](#), "Parking Standards," B.R.C. 1981, may be modified as follows:

✓ (i) Process: The city manager may grant a parking reduction not to exceed fifty percent of the required parking. The planning board or city council may grant a reduction exceeding fifty percent.

The applicant is requesting a parking reduction of 20% to allow for 8 off-street parking spaces where 10 spaces are required per the RM-3 zone district parking standards for non-residential uses.

(ii) Criteria: Upon submission of documentation by the applicant of how the project meets the following criteria, the approving agency may approve proposed modifications to the parking requirements of [section 9-9-6](#), "Parking Standards," B.R.C. 1981 (see tables 9-1, 9-2, 9-3 and 9-4), if it finds that:

- (a) For residential uses, the probable number of motor vehicles to be owned by occupants of and visitors to dwellings in the project will be adequately accommodated;**

Not applicable, as the proposed use is a professional office.

- (b) The parking needs of any non-residential uses will be adequately accommodated through on-street parking or off-street parking;**

Per the original Iris Hollow PUD approval, the subject lot is allocated eight reserved parking spaces located in the covered condominium parking garage immediately to the north of the subject site. The original approval also included two on-site parking spaces for the proposed bed and breakfast use; however, because the applicant is not proposing to expand their existing parking demand, they are proposing to eliminate the two approved on-site spaces called for per the original approval. Given that the parking demand will not increase, the reserved garage spaces in conjunction with ample on-street parking on Iris Hollow Pl. and Iris Walk Ct. will adequately meet the parking needs of the proposed use. Refer to **Attachment A** for the applicant's proposed plans and Parking Analysis.

- (c) A mix of residential with either office or retail uses is proposed, and the parking needs of all uses will be accommodated through shared parking;**

The proposal is to construct an office building within an existing mixed-use neighborhood. While no formal shared parking agreement is required, the applicant has indicated that the on-street parking is more than adequate for their existing and proposed parking demand due in part to the fact that many of the residents leave the development during the day to go to work, which corresponds with the office's business hours. In addition, all of the existing residential units have designated off-street parking.

- (d) If joint use of common parking areas is proposed, varying time periods of use will accommodate proposed parking needs; and**

As mentioned above, the applicant has indicated that the majority of the available on-street parking along Iris Hollow Pl. and Iris Walk Ct. is free during daytime hours due to the fact that many residents are at work during that timeframe. In addition, there are 8 designated parking spaces reserved for the proposed use in the covered condominium garage adjacent to the site on the north.

- (e) If the number of off-street parking spaces is reduced because of the nature of the occupancy, the applicant provides assurances that the nature of the occupancy will not change.**

If approved, the Use Review for the proposed use will include conditions indicating that the existing operating characteristics are not to be expanded.

(L) Additional Criteria for Off-Site Parking: The parking required under [section 9-9-6](#), "Parking Standards," B.R.C. 1981, may be located on a separate lot if the following conditions are met:

- ✓ (i) The lots are held in common ownership;**

The reserved parking spaces are located within the condominium parking garage adjacent to the site on the north. All of the lots within Iris Hollow are subject to the HOA, which manages parking.

- ✓ (ii) The separate lot is in the same zoning district and located within three hundred feet of the lot that it serves; and**

The lot on which the off-site reserved parking is located is subject to the Iris Hollow PUD regulations, is within 300 feet of the subject property and is within the RM-3 zone district.

- ✓ (iii) The property used for off-site parking under this Subsection continues under common ownership or control.**

The reserved parking spaces are located within the condominium parking garage adjacent to the site on the north. All of the lots within Iris Hollow are subject to the HOA, which manages parking.



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CITY OF BOULDER
LAND USE REVIEW RESULTS AND COMMENTS

DATE OF COMMENTS: **June 6, 2014**
 CASE MANAGER: **Chandler Van Schaack**
 PROJECT NAME: **Blue Sky Bridge Office Expansion**
 LOCATION: **2619 IRIS HOLLOW PL**
 COORDINATES: **N05W04**
 REVIEW TYPE: **Site and Use Review**
 REVIEW NUMBER: **LUR2014-00036**
 APPLICANT: **CATRINA WEIGEL**
 DESCRIPTION: **Simple site review and use review to create a new two-story professional office building within the Iris Hollow PUD to allow for the expansion of the existing Blue Sky Bridge office use.**

REQUESTED VARIATIONS FROM THE LAND USE REGULATIONS: **None**

I. REVIEW FINDINGS

Overall, staff is in support of the proposal and finds that it appears to be generally consistent with the intent of the Site and Use Review criteria; however, staff has identified several issues that will require additional information as well as changes to the site and building plans. Please see staff's comments below for additional detail. The comments below will require a revision-level resubmittal; therefore, please revise the plan set as noted in the comments below and submit **five (5) copies** of the revised plans as well as a digital copy of the revised plans in pdf form to the front counter of the P&DS Service Center prior to the start of a three-week review track. Review tracks begin on the first and third Mondays of each month.

Please contact the Case Manager, Chandler Van Schaack, at vanschaack@bouldercolorado.gov or 303-441-3137 with any questions or to set up a meeting.

II. CITY REQUIREMENTS

Access/Circulation David Thompson, 303-441-4417

1. The proposed building design encroaches into the public access easement for the six-foot sidewalk along Iris Hollow Place, which is not permitted pursuant to Section 9-9-10(b) of the BRC. Please revise the design to remove the proposed encroachments.
2. Per Section 9-9-18 of the Boulder Revised Code, 1981, (BRC) please revise the site plan to show the location of the trash storage and recycling area.
3. Per Section 9-9-7 of the BRC, all driveways must have an unobstructed sight triangle measured as 15 feet along the edge of the driveway and 15 feet along the alley right of way line, with a line connecting these two lines. Exceptions are made for trunks of trees whose branches are higher than 8 feet above the roadway, objects less than 30" tall, or objects that are no less than 75% visually permeable. The site plans shows the building encroaching within the sight triangle area. If 100 percent visibility is not provided, a diagram clearly showing how the 75 percent visibility requirement is being met must be shown or the plans must be revised to comply with this regulation.
4. Consistent with technical drawing 2.21 of the City of Boulder Design and Construction Standards (DCS) a skewed driveway ramp is not permitted. Please revise plans to meet technical drawing 2.21.
5. Pursuant to Table 2-2 of the DCS for driveways serving commercial uses, please revise the site plan to show a minimum driveway width of fifteen-feet wide.

Address: 2619 IRIS HOLLOW PL

Building Design Chandler Van Schaack, Case Manager

1. Overall, staff finds the proposed building design to be consistent with the Site Review criteria found in [section 9-2-14](#) of the Boulder Revised Code as well as the established design guidelines for the Iris Hollow PUD; however, additional information should be provided to more clearly demonstrate how the project will comply with the Site Review criteria. Specifically, please revise the written statement included with the application to specifically address the following criteria:

9-2-14(h)(2)(C)(iv): The setbacks, yards and useable open space along public rights of way are landscaped to provide attractive streetscapes, to enhance architectural features and to contribute to the development of an attractive site plan.

9-2-14(h)(2)(F)(xi): Buildings minimize or mitigate energy use; support on-site renewable energy generation and/or energy management systems; construction wastes are minimized; the project mitigates urban heat island effects; and the project reasonably mitigates or minimizes water use and impacts on water quality.

2. As noted in the review comments under “Engineering” and “Access/Circulation,” it will be necessary to remove the proposed covered entry from the public access easement along the south side of the property. In terms of building design, the proposed entryway is a significant architectural feature that helps to create visual interest at the pedestrian level, and as such should be re-incorporated into the revised building design in a way that meets all applicable engineering requirements.

Drainage Jessica Stevens, 303-441-3121

The applicant will be required to comply with Sections 7.12 - Detention and 7.13 - Storm Water Quality of the City of Boulder Design and Construction Standards. Please revise the Utilities & Drainage Plan, Sheet SR-3 to demonstrate how the proposal will address these requirements.

Engineering Jessica Stevens, 303-441-3121

As indicated in Access/Circulation comments, Section 9-9-10 of the Boulder Revised Code, 1981 prohibits the construction of any portion of a structure, including footings and eaves within a public easement. The design also proposes an eave encroachment within the utility easement in the northeastern corner of the property. Please revise the design accordingly to remove these encroachments.

Fees

Please note that 2014 development review fees include a \$131 hourly rate for reviewer services following the initial city response (these written comments). Please see the P&DS Questions and Answers brochure for more information about the hourly billing system.

Landscaping Elizabeth Lokocz, 303-441-3138

Please update the site plan to show all of the existing elements in the landscape areas adjacent to the proposed driveway such as the street tree and manhole cover. Per the access comment above, the ramp and driveway alignment will require curb and gutter replacement and parallel parking changes. If adequate parking is provided in the garage, consider eliminating the onsite parking to reduce the overall impact and allow for a bit more flexibility in building location and setbacks. Please note that if any landscaping has been removed or damaged immediately adjacent to the project, it will need to be replaced at the time of final building permit inspections.

Legal Documents Julia Chase, City Attorney’s Office, 303-441-3020

1. Prior to signing the Development Agreement, if approved, the Applicant shall provide the following:
 - a) an updated title commitment current within 30 days; and
 - b) Proof of authorization to bind on behalf of the owners.

Neighborhood Comments Chandler Van Schaack, Case Manager

Staff has received several questions and comments from nearby property owners, all of whom expressed overall support for the proposal assuming that the impacts to the surrounding uses will not change.

Parking Chandler Van Schaack, Case Manager

The proposed 3,131 square foot building generates a parking requirement of 10 parking spaces per the parking standards for nonresidential uses in the RM-3 zoning district. The original approved “Iris Hollow Lot Regulations” indicate that there would be 2 on-site parking spaces as well as 8 parking spaces in the covered condominium parking garage reserved for use by the previously approved “guest house” use on Lot 39. Please provide confirmation that these parking spaces will

still be available for use by the proposed office use. If the 8 parking spaces are no longer available, it will be necessary to document all parking that will be available to the proposed use, and any reduction from the 10 required parking spaces will require approval of a parking reduction through this review process. A request for a parking reduction will be reviewed for consistency with the parking reduction standards found in sections 9-2-14(h)(2)(K) and [9-9-6\(f\)](#), B.R.C 1981.

Review Process Chandler Van Schaack, Case Manager

Pursuant to section 9-2-15, "Use Review," B.R.C. 1981, the city manager shall review and submit a recommendation to the Planning Board for any application for a Use Review of a nonresidential use in a residential zone. A hearing date for this application has not yet been scheduled. Once the issues in these comments have been addressed, please contact the case manager to discuss planning board scheduling options.

Utilities Jessica Stevens, 303-441-3121

During a site inspection City staff observed what appears to be a water valve box in the southeastern corner of the property covered by an orange cone. The applicant must identify the improvement on the plans and determine if any conflicts exists with the proposed design.

III. INFORMATIONAL COMMENTS

Access/Circulation

Pursuant of Section 8-2-17(b) of the BRC, at time of building permit application the building plans must show the replacement of two sections of sidewalk along Iris Hollow Place between the angled parking and the street intersection.

Building and Housing Codes Matt English, 303-441-3206

Building Codes require the west wall to be constructed in accordance with section 602 and table 602. The west wall and eave to be constructed in compliance with section 705 and tables 705.2 and 705.8. Submittal materials do not have sufficient detail to determine compliance with IBC Chapter 11 "Accessibility." It is unclear how the proposal provides accommodation for an accessible route (1104) (one that also *connects* 1st & 2nd floors)(1104.4 #1 item 1.2), accessible entrance (1105) and accessible parking (1106) as required. Per Boulder Revised Code 10-8, a fire sprinkler is required. "(28) A new subsection is added to Section 903.2.1 to read: **903.2.1.6 Group B occupancies.** An automatic sprinkler system shall be provided throughout all new Group B occupancies greater than 2,000 gross square feet (185.8 m²)."

Utilities, Jessica Stevens, 303-441-3121

1. The proposed project includes work within the public right-of-way or public easements. A right-of-way permit must be obtained by a right-of-way licensed contractor prior to initiating this construction.
2. The applicant is notified that any groundwater discharge to the storm sewer system will require both a state permit and a city agreement. The steps for obtaining the proper approvals are as follows:

Step 1 -- Identify applicable Colorado Discharge Permit System requirements for the site.

Step 2 -- Determine the history of site contamination (underground storage tanks, groundwater contamination, industrial activities, landfills, etc.) If there is contamination on the site or in the groundwater, water quality monitoring is required.

Step 3 -- Submit a written request to the city to use the municipal storm sewer system (MS4). This submittal should include a copy of the Colorado Department of Public Health and Environment (CDPHE) permit application. The written request should include the location, description of the discharge, and brief discussion of all discharge options (e.g., discharge to MS4, groundwater infiltration, off-site disposal, etc.) The request should be addressed to: City of Boulder, Stormwater Quality, 4049 75th St, Boulder, CO 80301 Fax: 303-413-7364

Step 4 -- The city's Stormwater Quality Office will respond with a DRAFT agreement, which will need to be submitted with the CDPHE permit application. CDPHE will not finalize the discharge permit without permission from the city to use the MS4.

Step 5 -- Submit a copy of the final discharge permit issued by CDPHE back to the City's Stormwater Quality Office so that the MS4 agreement can be finalized.

For further information regarding stormwater quality within the City of Boulder contact the City's Stormwater Quality Office at 303-413-7350. All applicable permits must be in place prior to building permit application.

IV. NEXT STEPS

Please revise the plan set as noted herein and submit **five (5) copies** of the revised plans as well as a digital copy of the revised plans in pdf form to the front counter of the P&DS Service Center prior to the start of a three-week review track. Review tracks begin on the first and third Mondays of each month.

Please contact the Case Manager, Chandler Van Schaack, at vanschaackc@bouldercolorado.gov or 303-441-3137 with any questions or to set up a meeting.

V. CITY CODE CRITERIA CHECKLIST

A completed checklist will be provided following review of the revised plan set.



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CITY OF BOULDER
LAND USE REVIEW RESULTS AND COMMENTS

DATE OF COMMENTS: **July 25, 2014**
 CASE MANAGER: **Chandler Van Schaack**
 PROJECT NAME: **BLUE SKY BRIDGE OFFICE EXPANSION**
 LOCATION: **2619 IRIS HOLLOW PL**
 COORDINATES: **N05W04**
 REVIEW TYPE: **Site and Use Review**
 REVIEW NUMBER: **LUR2014-00036**
 APPLICANT: **CATRINA WEIGEL**
 DESCRIPTION: **Simple site review and use review to create a new two-story professional office building in the Iris Hollow P.U.D.**

REQUESTED VARIATIONS FROM THE LAND USE REGULATIONS:

- Section 9-9-6, "Parking Standards," – Request for a 20% parking reduction to allow for 8 off-street parking spaces where 10 spaces are required.

I. REVIEW FINDINGS

Staff finds the revised proposal to be in keeping with the intent of the Site Review Amendment criteria found in section 9-2-14, B.R.C. 1981 as well as the Use Review criteria found in section 9-2-15, B.R.C. 1981. A few minor corrections to the plan set and some additional information required, as outlined in the review comments below. Once the final corrections have been received, staff will move forward with a recommendation of approval to the Planning Board. The hearing is currently tentatively scheduled for **September 4, 2014**. Please contact the case manager, Chandler Van Schaack, at 303-441-3137 or vanschaack@bouldercolorado.gov with any questions.

Once the issues below have been addressed, please email pdf copies of the corrected plan set directly to the case manager, Chandler Van Schaack, at vanschaack@bouldercolorado.gov, by no later than **August 20, 2014**. Please contact the case manager with any questions or concerns. Staff is happy to meet to discuss the comments found herein at your convenience.

II. CITY REQUIREMENTS

Access/Circulation David Thompson, 303-441-4417

In support of the parking reduction and pursuant to section 9-9-6(f)(3)(D) B.R.C. 1981, please revise the site plan to include four (4) inverted "u" racks in order to accommodate short-term bicycle parking for eight bicycles. The location of the short-term bicycle parking must be pursuant to section 9-9-6(g)(3) B.R.C. 1981.

Drainage, Jessica Stevens, 303-441-3121

Portions of the proposed water quality swale have been called out as rock mulch on the Landscape Concept Plan on Sheet SR-2. A rock mulch swale is not considered to provide water quality treatment; alternate materials should be selected for this location. Please review the Urban Storm Drainage Criteria Manual – Volume 3 for water quality treatment design alternatives.

Fees

Please note that 2014 development review fees include a \$131 hourly rate for reviewer services following the initial city response (these written comments). Please see the P&DS Questions and Answers brochure for more information about the hourly billing system.

Landscaping Elizabeth Lokocz, 303-441-3138

Staff appreciates the site redesign. Please note a detailed landscape and irrigation plan is required at the time of Technical Document Review if required, or at building permit if not. See section 9-9-12(d)(1) B.R.C. 1981 ([9-9-12 link](#)) for a list of what is typically included. For this small site, the two plans may be combined if legible.

Legal Documents Julia Chase, City Attorney's Office, 303-441-3020

1. Prior to signing the Development Agreement, if approved, the Applicant shall provide the following:
 - a) an updated title commitment current within 30 days; and
 - b) Proof of authorization to bind on behalf of the owners.

Plan Documents Chandler Van Schaack, Case Manager

1. Please provide additional information on the existing operating characteristics in support of the requested parking reduction. Specifically, please provide a written statement that addresses each of the applicable criteria found in sections [9-2-14\(h\)\(2\)\(K\)\(ii\)\(b\)](#) through (e), and [9-9-6\(f\)\(3\)](#), B.R.C. 1981. Please title the document: "Blue Sky Bridge Parking Analysis," and include the project address, case number and date. The following information should be included in the analysis:
 - Existing hours of operation and employee shift schedules;
 - Current typical employee and client travel modes to and from the site and average number of clients per day;
 - Relevant use-specific travel/ visitation requirements (i.e., number of off-site visits per day, typical length of visits on and off-site, etc.)
 - A description of the existing on-street parking usage and the extent to which varying time periods of use will accommodate proposed parking needs;
 - A description of existing bicycle/pedestrian facilities serving the site as well as existing transit stop locations in relation to the site (please see comments under "Access/Circulation" above); and
 - Any other relevant information that supports the Applicant's request for a parking reduction.
2. Please revise Sheet AR-2 to include scaled elevations of the proposed trash and recycling enclosure, including height and materials for the proposed fence.
3. If possible, please provide additional information on the history of the subject site. Specifically, any information on why the previously approved bed and breakfast use was determined not to be feasible would help to provide some context in the discussion of the proposal with planning board.

Site Design Chandler Van Schaack, Case Manager

Currently, the proposed location for the trash and recycling enclosure is surrounded by native grass and rock mulch, with no clear path for transporting bins to a curb for pick-up. The applicant should provide a hard surface connection between the trash area and the sidewalk to improve the route for taking trash and recyclables to curb pick-up.

III. INFORMATIONAL COMMENTS

Drainage, Jessica Stevens, 303-441-3121

The applicant will be required to provide design details for the water quality swale at the time of building permit.

Engineering, Jessica Stevens, 303-441-3121

1. Section 9-9-10 of the Boulder Revised Code, 1981 prohibits the construction of any portion of a structure, including footings and eaves within a public easement. The applicant will be required to demonstrate that no portion of the structure encroaches within an easement at the time of building permit.
2. The proposed project includes work within the public right-of-way or public easements. A right-of-way permit must be obtained by a right-of-way licensed contractor prior to initiating this construction.
3. The applicant is notified that any groundwater discharge to the storm sewer system will require both a state permit and a city agreement. The steps for obtaining the proper approvals are as follows:

Step 1 -- Identify applicable Colorado Discharge Permit System requirements for the site.

Step 2 -- Determine the history of site contamination (underground storage tanks, groundwater contamination, industrial activities, landfills, etc.) If there is contamination on the site or in the groundwater, water quality monitoring is required.

Step 3 -- Submit a written request to the city to use the municipal storm sewer system (MS4). This submittal should include a copy of the Colorado Department of Public Health and Environment (CDPHE) permit application. The written request should include the location, description of the discharge, and brief discussion of all discharge options (e.g., discharge to MS4, groundwater infiltration, off-site disposal, etc.) The request should be addressed to: City of Boulder, Stormwater Quality, 4049 75th St, Boulder, CO 80301 Fax: 303-413-7364

Step 4 -- The city's Stormwater Quality Office will respond with a DRAFT agreement, which will need to be submitted with the CDPHE permit application. CDPHE will not finalize the discharge permit without permission from the city to use the MS4.

Step 5 -- Submit a copy of the final discharge permit issued by CDPHE back to the City's Stormwater Quality Office so that the MS4 agreement can be finalized.

Review Process Chandler Van Schaack, Case Manager

Please note that pursuant to section 9-2-7(b)(2), B.R.C. 1981, following the date of the final decision by the Planning Board there will be a 30-day call-up period during which time city council may call up the board's decision for a public hearing. A planning board decision not called up by the city council is final thirty days after the date of the decision.

IV. NEXT STEPS

Once the final corrections have been received, staff will move forward with a recommendation of approval to the Planning Board. The hearing is currently tentatively scheduled for **September 4, 2014**. Please contact the case manager, Chandler Van Schaack, at 303-441-3137 or vanschaackc@bouldercolorado.gov with any questions or to reschedule the hearing date.

Once the issues above have been addressed, please email pdf copies of the corrected plan set directly to the case manager, Chandler Van Schaack, at vanschaackc@bouldercolorado.gov, by no later than **August 20, 2014**. Please contact the case manager with any questions or concerns.

V. CITY CODE CRITERIA CHECKLIST

A completed criteria checklist will be provided once the additional information requested above has been received. The final criteria analysis will also be included with the staff memorandum to the Planning Board.

VI. CONDITIONS ON CASE

Draft conditions of approval will be included in the staff memorandum to the Planning Board. Please contact the case manager to discuss these conditions prior to **August 20, 2014**.

IRIS HOLLOW ARCHITECTURAL CODE

WALLS	ELEMENTS	ROOFS	OPENINGS	MISCELLANEOUS
<p>Building Materials</p> <p>Ext. wall finish: a) Painted wood, or masonite, clapboard, drop siding, or board & batten, with a 6" max. reveal. No fake grain siding. b) Clay brick selected from the approved list. c) Stucco with smooth sand finish. d) Indigenous stone. e) Metal. f) Cedar shingles (no decorative shingles).</p> <p>Garden walls shall be of masonry or stucco or match the principal building.</p> <p>Material Configuration</p> <p>Wall materials shall be combined horizontally with the heavier material below, i.e., wood above stucco and stucco above brick or stone.</p> <p>Walls may be finished with no more than two siding materials.</p> <p>Exterior chimneys shall be galvanized metal or finished in brick, stucco or indigenous stone.</p> <p>Garden walls shall be no less than 6" inches thick.</p> <p>Windows and doors must have double brick mold or a 3 1/2" width trim minimum.</p> <p>Foundations: minimize exposed concrete, where exposed by more than 6", the concrete shall be screened or covered with stucco.</p> <p>Main level finished floor shall be a maximum of 36" above grade.</p>	<p>Configuration</p> <p>Brick and stucco arches shall be no less than 12 inches in thickness. Piers shall be no less than 12 x 12 inches.</p> <p>Posts shall be no less than 5 x 5 inches.</p> <p>Cantilevers shall be permitted only as open balconies or porches supported by visible brackets.</p> <p>Balconies shall not exceed 4 feet in depth.</p> <p>Porches shall be no less than 6' or as shown on lot regulations, without special permission from the Iris Hollow ACC.</p> <p>Spindles and balusters on railings shall not exceed 4 inches on center.</p> <p>Bay or bow windows shall come to the ground and shall be habitable.</p> <p>Porches and arcades shall be one or two stories superimposed but not two stories high.</p> <p>Undercroft of porches shall be open between pier supports or infilled with a wood lattice running vertical-horizontal or wood boards running horizontally or vertically one inch spacing between boards.</p> <p>Columns must be supporting a visible beam.</p> <p>Decks should be located in rear yards. An Attempt should be made to screen the deck from the street.</p>	<p>Cladding</p> <p>Roofs shall be finished in asphalt shingles, or metal.</p> <p>Configuration</p> <p>The principal roof if sloped shall be a symmetrical gable or hip.</p> <p>Gable roof slopes shall be between 8:12 and 12:12.</p> <p>Hip roof slopes shall be no less than 5:12 with a 18" minimum overhang.</p> <p>Penetrations of the principal roof such as monitors, towers, and cupolas which are less than 250 S.F. are exempt from roof slope requirements.</p> <p>No clipped gable ends.</p> <p>Ancillary roofs may be sheds sloped no less than 2:12.</p> <p>Overhanging eaves may have exposed rafters.</p> <p>Porch soffit to be exposed rafters, tongue and groove boards or grooved plywood.</p> <p>All penetrations from the roof shall match or be painted to match the color of the roof.</p> <p>Skylights shall be flat panels. Solar collectors shall be fixed parallel to the roof slope.</p> <p>An Attempt shall be made to minimize roof penetrations. Where penetrations occur they shall be located towards the rear of the house.</p> <p>Roof vents where provided shall be continuous ridge type.</p>	<p>Materials</p> <p>Exterior doors shall be made of wood. Glass shall be clear or frosted.</p> <p>Windows in detached housing shall be wood.</p> <p>Configuration</p> <p>The total glazing area on each facade shall not exceed 40% of its surface. The glazed area of the south and west facades may be 60% of the surface.</p> <p>Windows shall be square or rectangular with a vertical proportion of no less than 1:1.5.</p> <p>There may be no more than one semicircular or circular window on each building.</p> <p>Hexagonal, trapezoidal, and triangular windows are outlawed.</p> <p>Window muntins shall be true divided lights, creating panels vertical in proportion (no snap in grills).</p> <p>Porch and arcade openings shall be vertical in proportion.</p> <p>Garage doors shall be a maximum of 9 feet in width and made of wood or masonite.</p> <p>Windows may be equipped with operable wood shutters sized to match the opening.</p> <p>No external glass block.</p> <p>Awnings if provided shall be of rectangular geometry.</p>	<p>Each lot must provide a bird house.</p> <p>Variations to the Architectural Code may be granted on the basis of architectural merit by the Iris Hollow Architectural Control Committee.</p> <p>Exterior surfaces shall be painted or semitransparent stain.</p> <p>No prefabricated sheds allowed.</p> <p>Each lot must provide 1 Iris for every 2 feet of lot frontage on a Public R.O.W. (see landscape plan)</p> <p>Exterior lights shall be of 40 watts or less.</p> <p>An attempt shall be made to screen the following from streets, alleys, & paths: HVAC equipment, satellite dishes, permanent play equipment, hot tubs, solar collectors and antennas. Where possible these items shall be located in the rear of the yard.</p> <p>Each structure on Cannonball & Red Zinger alley shall have at least one light fixture facing the alley activated by a photo voltaic cell.</p> <p>Each house shall have at least one exterior light located on the front elevation that is activated by a photo voltaic cell.</p> <p style="text-align: center;">FENCES</p> <p>Fences shall be made of painted wood pickets, woven wire (not chain link) or open lattice. Fences adjacent to streets and walks shall be no taller than 36" and open in nature. Fences on an alley may be 48" and may be solid. Fence posts may be spaced no greater than 6 feet on center.</p>

Coburn Development Inc.

* Format used with permission of the Kiki Wallace Co.

Prepared: January 16, 1996
 Revised: April 16, 1996
 Revised: May 7, 1996



Iris Hollow

Approved Site Review (Including All Disposition Requirements)



DESIGN • RENOVATION • CONSTRUCTION
 1711 PEASE ST. THIRD FLOOR, BOULDER, CO 80502
 (303)442-2211 FAX (303)442-0811

Iris South Neighborhood

Lot#	Type	Lot Size
1	Existing	5009 SF
2	Large	3758 SF
3	Cottage	1540 SF
4	Cottage	1375 SF
5	Shotgun	1375 SF
6	Cottage	1375 SF
7	Cottage	1375 SF
8	4-Square	3000 SF
9	4-Square	3132 SF
9a	Small	3360 SF
10	Small	2989 SF
11	4-Square	2622 SF
12	4-Square	2577 SF
13	Small	2098 SF
14	Small	2451 SF
15	Large	3507 SF
16	Small	2868 SF

Iris West Neighborhood

Lot#	Type	Lot Size
17	Existing	4677 SF
18	Small	2598 SF
19	Small	2940 SF
20	Small	2985 SF
21	Small	2781 SF
22	Existing	5560 SF
23	Shotgun	1797 SF
24	Small	3244 SF
25	Large	3517 SF

Iris East Neighborhood

Lot#	Type	Lot Size
26	Large	3641 SF
27	Large	3557 SF
28	Large	3849 SF
29	Large	3317 SF
30	Large	4580 SF
31	Cottage	1274 SF
32	Cottage	1274 SF
33	Cottage	1274 SF
34	Cottage	1427 SF
35	Cottage	1296 SF
36	Cottage	1250 SF
37	Cottage	1250 SF
38	Cottage	1250 SF
41	School	2055 SF
42	Treehouse	15352SF

Iris North Neighborhood

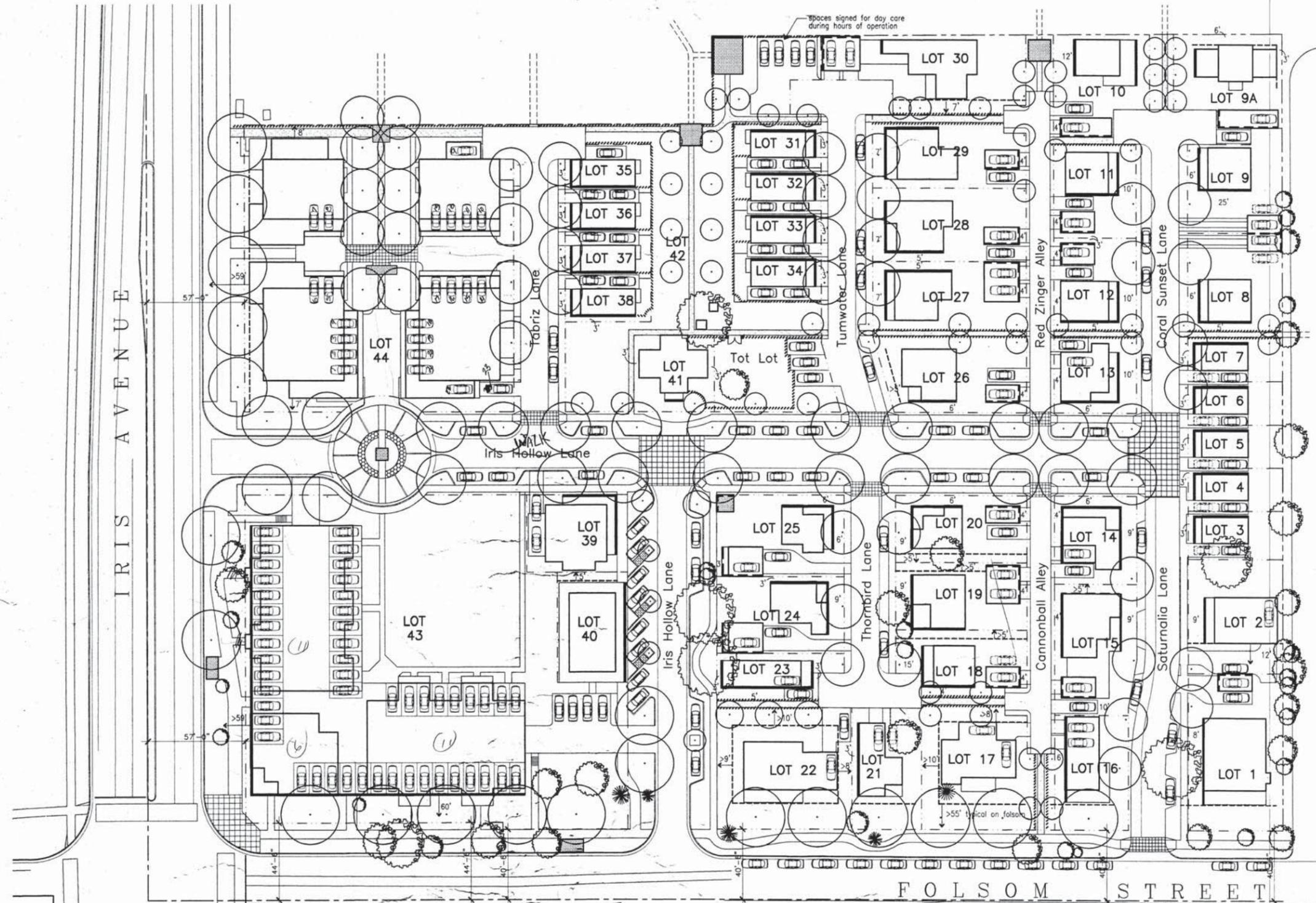
Lot#	Type	Lot Size
39	Guest House	2645 SF
40	Business Studio	2582 SF
43	Condominiums	36901 SF
44	Rowhouses	25496 SF

Lot Information/Requirements

scale 1:30

Legend

-  deferred parking space
-  primary build to line
-  accessory build to line
-  set back line
-  required structures



Iris Hollow

Approved Site Review (Including All Disposition Requirements)



IRIS HOLLOW LOT REGULATIONS

	Single Family								Multi-Family			Public				
	Carriage Studio	Carriage Apartment	Cottage	Shotgun	Four Square	Small Lot	Large Lot	Existing	Row House	Row Loft	Condo	Guest House	Business Studio	School	Pavilions	Barn
Location Plan																
Lot # & Lot Size	Lot 24 Lot 25	Lot 10 3357 SF Lot 19 3075 SF Lot 27 3557 SF Lot 29 3222 SF Lot 30 4744 SF	Lot 3 1540 SF Lot 4 1375 SF Lot 6 1375 SF Lot 7 1375 SF Lot 31 1400 SF Lot 32 1250 SF Lot 33 1250 SF Lot 34 1400 SF Lot 35 1316 SF Lot 36 1250 SF Lot 37 1250 SF Lot 38 1250 SF	Lot 5 1375 SF Lot 23 1797 SF	Lot 8 3132 SF Lot 9 3029 SF Lot 11 2822 SF Lot 12 2577 SF	Lot 9a 3380 SF Lot 10 2969 SF Lot 13 2295 SF Lot 14 2451 SF Lot 16 2933 SF Lot 18 2598 SF Lot 19 2340 SF Lot 20 2985 SF Lot 21 2831 SF Lot 24 3244 SF	Lot 2 3758 SF Lot 15 3507 SF Lot 25 3517 SF Lot 26 3841 SF Lot 27 3557 SF Lot 28 3849 SF Lot 29 3317 SF Lot 30 4580 SF	Lot 1 5096 SF Lot 17 4767 SF Lot 22 5646 SF	Lot 44 26570 SF	Lot 44 10050 SF	Lot 43 38216 SF	Lot 39 2645 SF	Lot 40 3023 SF	Lot 41 4890 SF		Lot 42 12616 SF
Build to Line <small>Note: Build to lines are measured from the building face or column face, eaves may project into build to line.</small>	Lot 24 N3 W0 Lot 25 N3 W0	Lot 10 N4 W0 Lot 19 N4 W0 Lot 27 N4 E0 Lot 29 N4 E0 Lot 30 N0 E0	Lot 3,4,6,7 N3 E3 Lots 31,32,33,34 S3 E3 Lots 35,36,37,38 N3 W3	Lot 5 E3 Lot 23 N3 W5 S3	Lot 8 Princ. N6 W5 Garage S0 E3 Lot 9 Princ. N6 W2S Garage S0 W0 Lot 11 Princ. S10 W2S Garage N4 W3 Lot 12 Princ. S10 W5 Garage N4 E3	Lot 9a Princ. S3 E3 Garage S0 W0 Lot 10 Princ. N12 S8 E-3 Garage N4 W0 Lot 13 S10 W6 N-4 Lot 14 N4 S9 E5 Lot 16 N6 W5 W-20 Lot 18 Princ. N15 W4 E-5 Garage S4 W0 Lot 19 N9 W0 E-5 Lot 20 Princ. N9 W-5 S5 Garage S4 E6 Lot 21 N3 W-20 S9 E3	Lot 2 N6 W-12 Lot 15 N4 S-9 E-5 Lot 25 Princ. S6 E8 Outbdg N0 E0 (100sf) Lot 26 Princ. N5 W6 Garage S4 W8 Lot 27 N7 E5 Lot 28 Princ. N7 W5 Garage S4 W10 Lot 29 N7 E0 Lot 30 E-3 W-7	Lot 1 Princ. N8 W-5S Garage N18 E0 Lot 17 N-9 S-8 E-10 W-5S Lot 22 N-9 S-8 E-10 W-5S	N-57.5 W-7 (Exclude Roundabout R.O.W.) E-8 S0 S0	N-60 E0 S-3-3 N-58.5 W60	S5 E0	S0 E-5	W3			NO E0
Max. Square Feet	750 SF Total Main Floor 50% (Parking) Upper Floor 50% (Living Area)	650 SF Main Floor 50% Upper Floor 50%	350-500 SF	1145 SF	1300 SF Principal				18000 SF Excludes Garages	Main building foot print 41' x 145' Porches & balconies may exceed build to line.	22300 SF	3400 SF	2900 SF	1200 SF		
Floor Area Ratio						.45 FAR <small>May increase by 50 SF after certificate of occupancy.</small>	.45 FAR <small>May increase by 100 SF after certificate of occupancy.</small>	.45 FAR								
Maximum Height <small>- Max height of main level, finished floor = 3'-0" above grade.</small>	26'	24'	Lots 31 - 38 22' Lots 3,4,6,7 28'	28'	28' Principal Lots 11 & 12 14' Garage - 1 Car Lots 8 & 9 18' Garage - 2 Car Shared	14' Garage - 1 Car 18' Garage - 2 Car Side by Side 14' Garage - 2 Car Tandem	32' Principal 14' Garage	26' 33' Roof Top Access	35'	25' Parapet Wall 32' Pitched Roof 35' Tower	35'	25'	25' Ridge 30' Tower			
Tower <small>- Max 35' high - Max 20 SF cupolas allowed on access bldgs not counted in max. height.</small>		Cupolas & spires allowed				Maximum 100 SF	Maximum 200 SF		80 SF per Roof Top Access	1500 SF Total 40'	35' 500 SF Max.			Maximum 200 SF		
Porch <small>- Minimum Requirement</small>	Lot 24 N 100% Lot 25 N 100% Interior Stair	-Nose Required -Eyebrow Allowed in alley setback Exterior Stair	All Cottages 5' Front 100% Lots 31-38 3' Rear 30%	Lot 5 25% Lot 23 100%	Front 100%	Lot 9a N80% W20% Lot 13 S60% W30% Lot 14 S30% E60% Lot 15 S60% Lot 16 S30% Lot 18 N30% Lot 19 N60% Lot 20 N60% E30% Lot 21 W30% E20% Lot 24 S60%	Lot 2 N60% Lot 13 S60% W30% Lot 25 S50% E30% Lot 26 N30% W10% Lot 27 N100% Lot 28 N60% Lot 29 N100% Lot 30 W50%	West 60%	South 30%	South 40% North 3' overhang reqd. above garages.	West 40%	South 60%	East 40%	6' South 100% (access easement)	West 20%	
Parking <small>O = Open Off Street A = Attached Garage D = Detached Garage T = Tandem Off Street DEF = Deferred Maximum Footprint 2 Car Garage 440 SF 1 Car Garage 240 SF</small>			2 Spaces 10 IT Exception: Lot 35 1 space off street.	2 Spaces Lot 5 10 IT Lot 23 1A	2 Spaces Lot 8 1D 1T 1DEF Lot 9 1D 1T 1DEF Lot 11 1D 10 Lot 12 1D 10	Lot 9a 1D 10 Lot 13 1A 10 Lot 14 1A 10 Lot 15 1A 10 Lot 16 1A 10 Lot 18 1D 1DEF Lot 19 2D Lot 20 1D 10 Lot 21 1A 1DEF Lot 24 1D 10	Lot 2 1A 10 Lot 10 1D 10 Lot 25 1D 10 Lot 26 - 1D 10 or 2A Lot 27 2D Lot 28 1D 10 Lot 29 1D 10 Lot 30 2D or 2A	Lot 1 1D 10 Lot 17 2A Lot 22 1A 10	22 Covered 1 Tandem	14 Single garages 49 Covered spaces	8 Covered in condo garage 1 Porte-cochere 1 Tandem	4 Open spaces	4 spaces (Barn) (Time Restricted) 3 Spaces		4 spaces (Barn) (Time Restricted) Guest Parking	
Out Buildings <small>- Max of 3 structures per lot - An out bldg. under 60 SF & 12 FT tall may be placed anywhere behind the front build to line.</small>			Lots 3,4,6,7 1 Out Building Allowed Otherwise Not Allowed.	Lot 5 1 Out Building Allowed	Allowed	Allowed	Allowed							Allowed		

To Be Determined by Iris Hollow Architectural Committee



Iris Hollow

Approved Site Review (Including All Disposition Requirements)



Iris Hollow Open Space Calculation

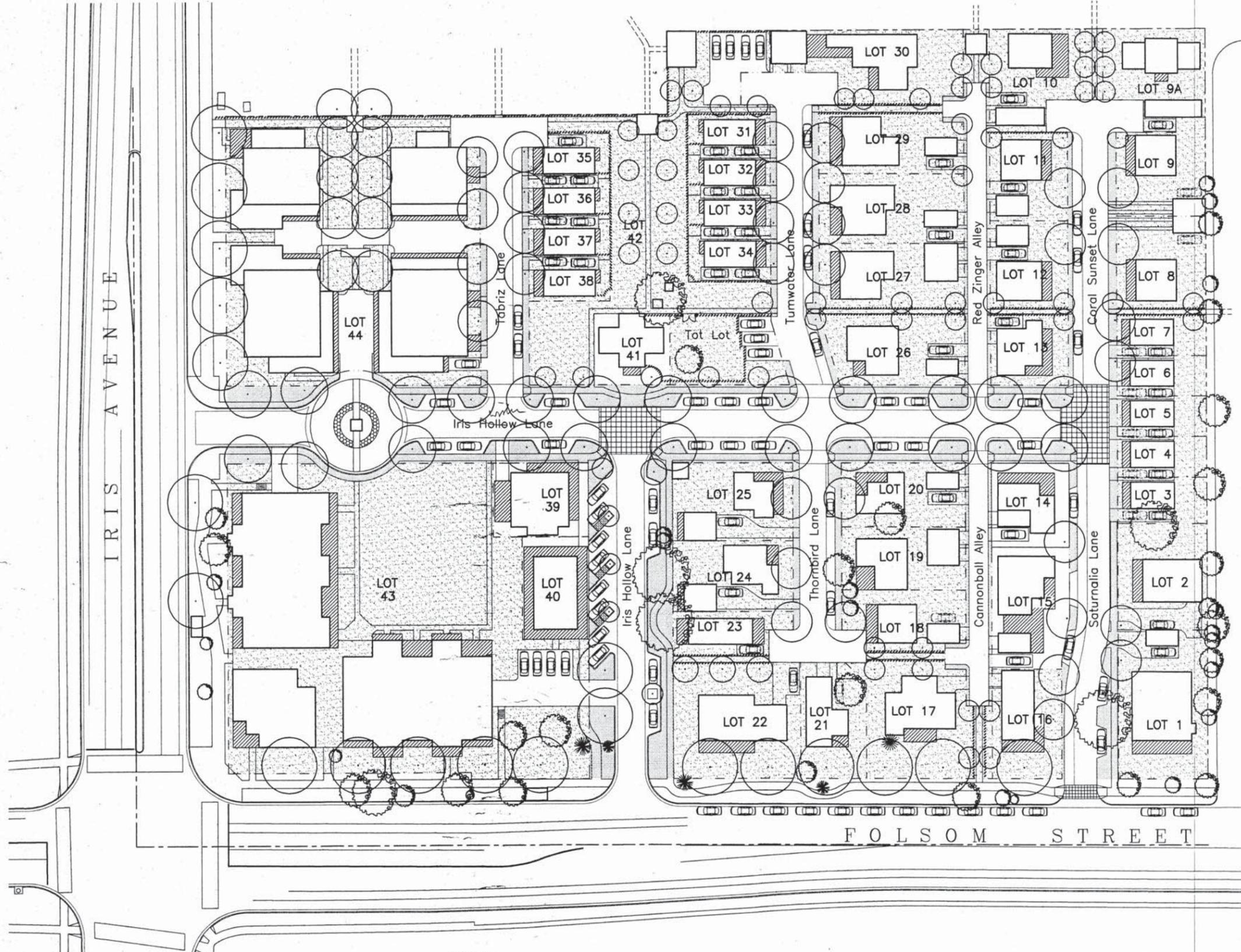
LOT NUMBER	LOT AREA	MINIMUM OPEN SPACE	MAXIMUM OPEN SPACE	MINIMUM R.O.W. OPEN SPACE	MINIMUM OPEN SPACE	MINIMUM OPEN SPACE	TOTAL MINIMUM OPEN SPACE
1	3758	1118	0	0	100	296	1514
2	1540	513	0	0	0	164	677
3	1375	513	0	0	0	80	593
4	1375	513	0	0	0	80	593
5	1375	513	0	0	0	80	593
6	1375	513	0	0	0	80	593
7	1375	513	0	0	0	80	593
8	3132	775	200	0	0	150	2307
9	3020	775	200	0	0	150	2304
10	3300	982	383	0	0	40	2109
11	2999	982	383	0	0	40	1972
12	2622	775	240	0	0	150	1737
13	2577	775	240	0	0	150	1712
14	2098	980	0	0	50	250	1418
15	2451	1202	0	0	0	292	1541
16	3907	1500	220	0	0	548	2626
17	2888	1133	0	0	0	280	2013
18	4677	1384	0	0	100	160	3553
19	2598	745	240	0	0	170	1783
20	2940	982	440	0	0	162	1720
21	2985	740	200	0	0	170	2215
22	2781	910	0	0	0	87	1938
23	5560	1450	0	0	100	252	4253
24	1707	863	0	0	0	228	1162
25	3244	1018	340	320	0	151	1708
26	3317	887	340	420	50	287	2198
27	3451	923	200	0	50	67	2413
28	3557	1035	440	0	0	168	2250
29	3849	1124	200	0	50	120	2695
30	3117	1144	200	0	0	226	2198
31	4580	1530	440	0	100	348	3058
32	1274	632	0	0	0	120	762
33	1274	632	0	0	0	120	762
34	1427	632	0	0	0	120	915
35	1274	632	0	0	0	120	784
36	1274	632	0	0	0	120	738
37	1274	632	0	0	0	120	738
38	1274	632	0	0	0	120	738
39	1535	390	0	0	0	140	1375
40	3690	1488	0	1878	2015	2208	24415
41	25496	12976	0	2441	3854	544	14477
A	165	0	0	0	0	0	165
B	402	0	0	0	0	0	402
C	140	0	0	0	0	0	140
D	144	0	0	0	0	0	144
E	499	0	0	0	0	0	499
F	192	0	0	0	0	0	192
G	267	0	0	0	0	0	267
H	384	0	0	0	0	0	384
I	528	156	0	0	156	528	528
J	236	0	0	0	0	0	236
K	416	49	0	0	0	49	416
L	0	0	0	0	0	0	0
M	0	0	0	0	0	0	0
N	0	0	0	0	0	0	0
O	0	0	0	0	0	0	0
P	0	0	0	0	0	0	0
PROVIDED LOT TOTALS	192482	68648	5186	5477	6866	11397	131522
RECOGNIZED LOT TOTALS (Sum of all lots except 39, 40 and 41)	185200	63810	5186	5877	6619	9970	127796
LANDSCAPED R.O.W. PROVIDED							19373
LANDSCAPED R.O.W. RECOGNIZED (max. 10% of required)							13680
OPEN SPACE REQUIRED FOR 85 UNITS							136,000
RECOGNIZED SITE TOTALS (Sum of the Recognized Landscaped R.O.W. and all lots except Lots 39, 40 and 41)	185200	68648	5186	5477	6866	11397	141396

NOTES:
 1. Building footprint includes porch area.
 2. Non-usable open space includes accessory buildings, and parking and drive areas for non-single family lots.

Open Space
scale 1:30

Legend

-  picket fence 3' high
-  porch
-  open space
-  r.o.w. open space



Iris Hollow

Approved Site Review (Including All Disposition Requirements)



CITY OF BOULDER
AGENDA ITEM PLANNING BOARD
MEETING DATE: September 4, 2014

AGENDA TITLE:

CONCEPT PLAN REVIEW AND COMMENT: Request for citizen, staff and Planning Board comment on a proposal to redevelop the existing properties located at 3085, 3155 and 3195 Bluff Street totalling approximately 4.25 acres into 77 dwelling units consisting of: 24 three-bedroom, for-sale townhomes; 45 two and three-bedroom permanently affordable rental townhomes; and eight two-bedroom townhomes, which may be market rate or affordable. Total of 84,534 square feet of habitable area on three lots: 3085, 3155 and 3195 Bluff Street. Review case number LUR2014-00050.

Applicant: Adrian Sopher
Property Owner: 1240 Cedar, LLC

REQUESTING DEPARTMENT:

Community Planning and Sustainability:

David Driskell, Executive Director
Susan Richstone, Deputy Director
Charles Ferro, Development Review Manager
Elaine McLaughlin, Senior Planner

OBJECTIVE:

1. Planning Board hears applicant and staff presentations
2. Hold Public Hearing
3. Planning Board discussion and comment on Concept Plan. No action is required by Planning Board

PROPOSAL AND SITE SUMMARY:

Proposal: Redevelop an existing 4.25 acre site into a 77 residential units including 45 permanently affordable rental units. The proposal includes 24 three-bedroom, for-sale townhomes; 45 two and three-bedroom rental townhomes; and and eight two-bedroom townhomes, (rental or for-sale and market or affordable to be determined). Also includes a proposal to amend the TVAP Connections Plan.

Project Name: S'PARK_west

Location: 3085, 3155 and 3195 Bluff Street

Zoning: Residential High - 6 (RH-6)

Comprehensive Plan: High Density Residential

Key Issues for Discussion:

In addition to an analysis of the criteria for Concept Plan review, staff has identified two keys issues for the board's consideration. Staff's analysis of the criteria and the key issues can be found in Section III of this memo.

- 1) Is the proposed project consistent with the intent of the TVAP – HDR1 (high density residential – 1) land use and the TVAP Design Guidelines?
- 2) Is the proposed amendment to the TVAP Transportation Connections Plan consistent with plan objectives?

I. BACKGROUND

A Concept Plan for a portion of the subject site was previously reviewed as a Concept Plan at a January 2012, Planning Board hearing, a web link to the memo and minutes provided [here](#). At the time, only the property at 3085 Bluff St. was under consideration. Since that time, the property at 3155 Bluff Street was incorporated into the Concept Plan that is now under consideration. On Mar. 6, 2014, the Planning Board also reviewed the former Sutherlands property as a Concept Plan, a weblink to the memo and minutes provided [here](#), referred to as S'PARK that interfaces with the Concept Plan area on the east. Figure 1 illustrates the relationship of the Concept Plan for S'PARK with the Concept Plan for S'PARK_west currently under consideration. It is anticipated that redevelopment of the two properties will create a new northern neighborhood for Boulder Junction. At the time of Site Review, the two project sites will be combined into one overall Site Review.

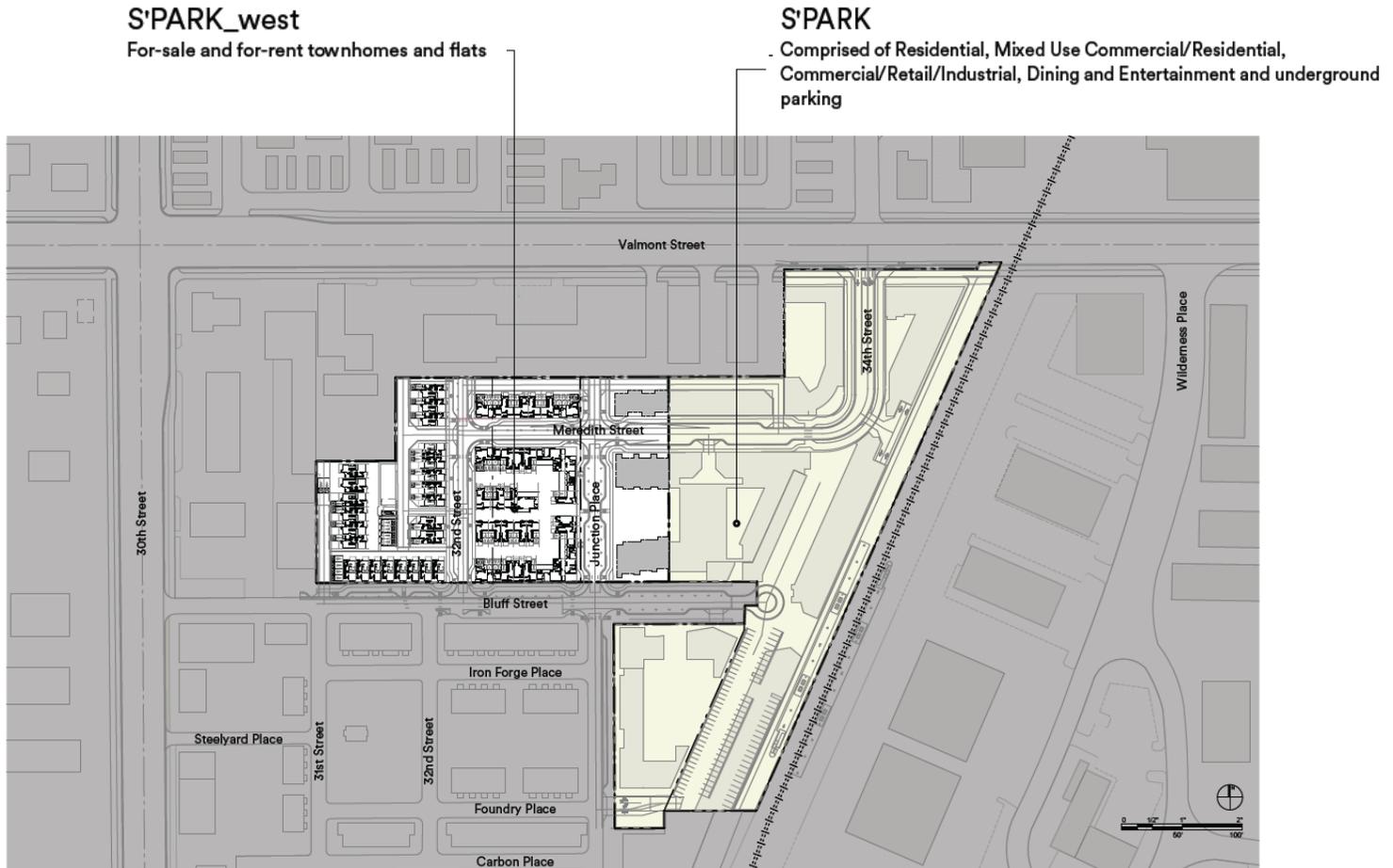


Figure 1: Proposed Concept Plan (currently under consideration) with relationship to S'PARK Concept Plan

II. PROPOSED CONCEPT PLAN SUMMARY

Figure 2 is the conceptual site plan followed by a birds-eye perspective of the plan in Figure 3. The Concept Plan is proposed to include the following:

- On the west side of the site are 24 for-sale, three-bedroom, 2½ bath townhome units of two and three stories and a single car garage; with 13 units at approximately 1,600 square feet and 11 units at approximately 1,900 square feet to provide for pricing variety to the market.
- Proposed within the center of the site are 45 deed-restricted permanently affordable rental townhomes: with 36 offered to residents with incomes up to 50 percent of the Area Median Income (AMI) and nine for residents with incomes up to 60

percent of AMI. Proposed are 24, two-bedroom units with an average size of 909 square feet; and 21 three-bedroom units each with an average size of 1,096 square feet. Each unit is proposed with one off-street parking space. The affordable housing would be provided through a combination of Inclusionary Housing and city affordable housing funding.

- On the east side of the property are eight townhomes, planned to be developed as a part of S'PARK. At this time, the applicant did not identify the planned market for the units as either for-sale or rental, market-rate or permanently affordable. These townhomes are proposed to be developed as a part of the S'PARK property to the east, and details on the units are forthcoming at a later date.
- At the center of the site is a community center planned to serve the residents.

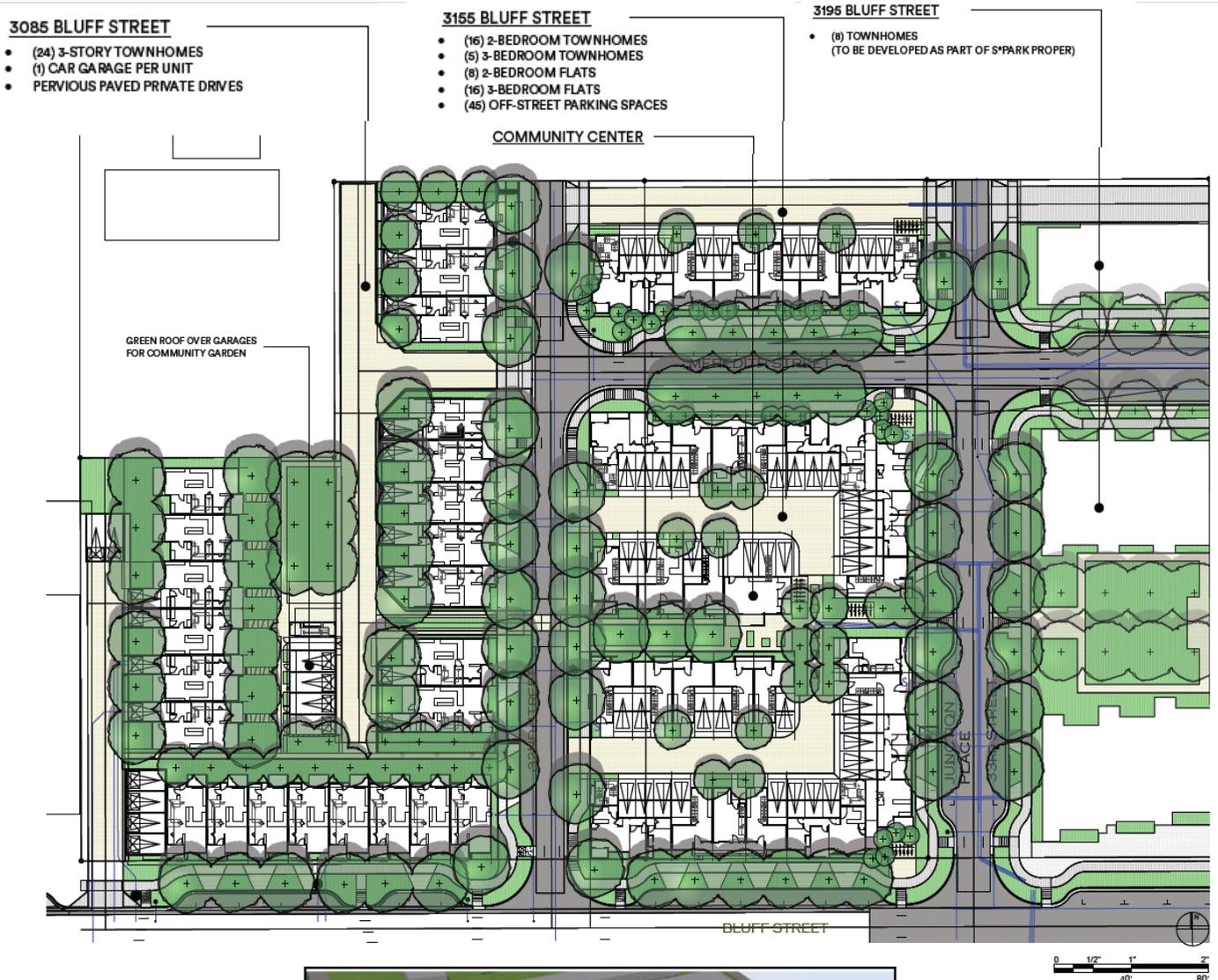


Figure 3: Birds Eye Perspective looking Northwest

III. ANALYSIS

Concept Plan Review Criteria for Planning Section 9-2-13(e)

The following guidelines will be used to guide the Planning Board's discussion regarding the proposal. It is anticipated that issues other than those listed in this section will be identified as part of the concept plan review and comment process. The Planning Board may consider the following guidelines when providing comments on a concept plan.

- (1) **Characteristics of the site and surrounding areas, including, without limitation, its location, surrounding neighborhoods, development and architecture, any known natural features of the site including, without limitation, mature trees, watercourses, hills, depressions, steep slopes and prominent views to and from the site;**

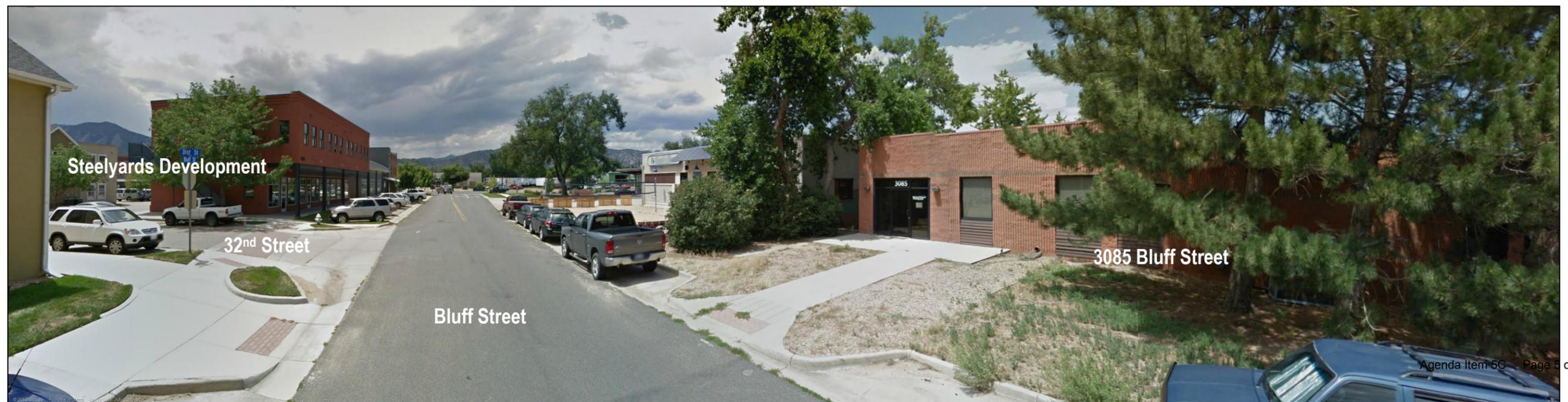
The 4.25 acre site is located on north side of Bluff Street, just east of 30th Street and south of Valmont Road within Boulder Junction and the Transit Village Area Plan and guidelines. The site as shown in Figure 4, is comprised of three lots and has served as an industrial manufacturing and service use for several decades. It is essentially flat, with a two percent cross slope from northwest to southeast. There are a number of manmade berms and dirt trails on the site, used in recent years as a motocross course on the westernmost lot. The site backs up to service commercial uses that front on Valmont Road, to the east and west are other service commercial uses. Directly east is the S'PARK redevelopment site, reviewed as a Concept Plan in February 2014. South across Bluff Street is the Steelyards development consisting of vertical mixed use buildings along 30th Street, townhomes directly across the street, and live-work units on the south side of the development. The rear of the townhomes at Steelyards face Bluff Street as shown on the following page. There are several existing trees on the site, and as noted in the Landscape comments above, a tree survey will be required with any healthy mature trees incorporated into plan refinements.



Figure 4: Aerial of Site and Surroundings



Figures 5a and 5b: Views of 3085 Bluff from the streetscape along Bluff Street.
 a) Above: Looking East
 b) Below: Looking West





Bluff Street looking east

Figures 6a and 6b Views of 3155 Bluff (above) and 3195 Bluff (below) from street



Bluff Street looking east

- (2) Community policy considerations including, without limitation, the review process and likely conformity of the proposed development with the Boulder Valley Comprehensive Plan and other ordinances, goals, policies, and plans, including, without limitation, subcommunity and subarea plans;

TVAP Land Use Designation. The site is within the Transit Village Area Plan, shown in Figure 7, that is intended to be a more focused plan that provides guidance to implement the goals and policies within the BVCP. As such, consistency with the goals, objectives, and guidelines of TVAP is recommended with some opportunities for modifications delineated herein prior to application for Site Review. As noted on page 9 of TVAP, “at the outset of the Transit Village Area planning process, the City Council and Planning Board adopted the following vision to provide direction for the development of the plan.” Among the vision statements applicable to the Concept Plan are those that follow:

- A place that is not overly planned, with a “charming chaos” that exhibits a variety of building sizes, styles, and densities where not everything looks the same.
- A place that emphasizes and provide for alternative energy, sustainability, walking, biking and possible car-free areas, e.g. “eco-village.” As shown below, the site is designated as HDR1 (High Density Residential 1) under TVAP.

On page 16 of TVAP, High Density Residential – 1 is defined as follows: “Urban townhomes and garden apartments with individual garages, surface parking lots, or underground parking. Mainly two to three stories.” Within the HDR-1, the intended density is 15-24 dwelling units per acre.

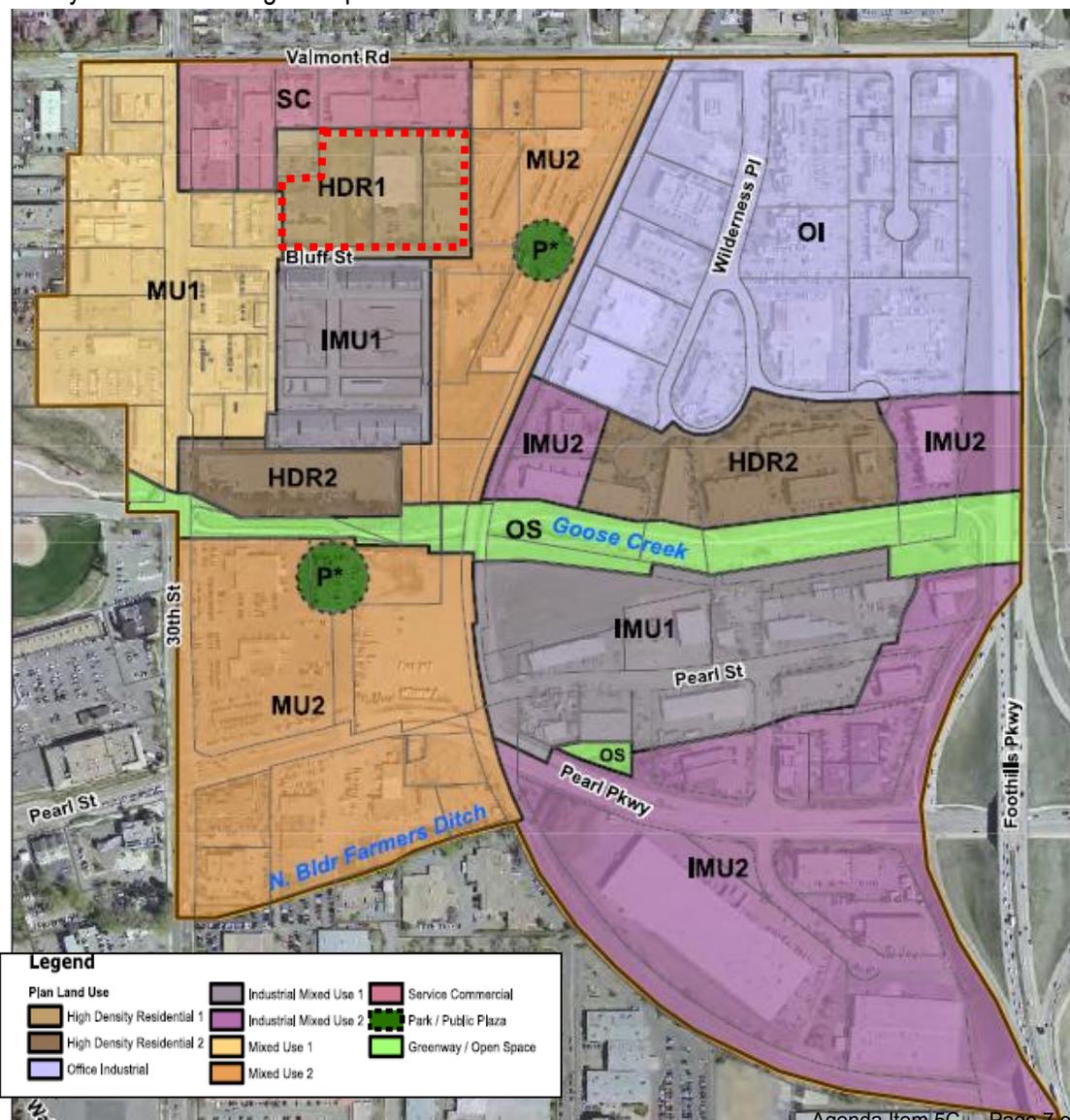


Figure 7: Site Context within Transit Village Area Plan Land Use

Within TVAP, prototypes for the H-DR-1 Land Use were also illustrated as shown in Figure 8 below. As can be seen by the massing diagrams provided by the applicant (Figures 9a through 9e), the proposed residential units address the prototypes illustrated for the H-DR-1 Land Use with the plan for market rate townhomes as well as the permanently affordable units configured as townhomes. The planned heights range from two to three stories. The combination of these different types of units is important for the overall S’PARK redevelopment as it would help to address some of the concerns articulated by the Planning Board and staff during the S’PARK Concept Plan discussion about the need for a diversity of housing types and ensuring that the massing is not consistently 55 feet throughout the area.

Land Use Prototypes		Residential
High-Density Residential -1-	15-24 Dwelling Units per Acre	Who would live here?
		Middle and upper incomes. <ul style="list-style-type: none"> • Garden apartments tend to attract moderate income singles, couples, families and some seniors. Often have facilities for small children. • With their vertical arrangement, urban townhomes may not be suited for people with disabilities or seniors.
Urban townhomes and garden apartments with individual garages, surface parking lots, or underground parking. Mainly two to three stories.		

Figures 8: HDR1 Land Use Prototypes from TVAP



Figures 9a through 9d of massing models from proposed Concept Plan

The TVAP area was divided into eight character districts, primarily based on future land use, and to provide more specific guidelines to promote plan goals related to urban design, public spaces and livability. As noted in TVAP, “the guidelines will be considered in the Site Review process to ensure that new development will be compatible with the character established by this plan.” The Concept Plan site is predominantly located within the district identified in TVAP as the “Steelyards District” as shown below in Figure 10, the Character Districts plan from page 19 of TVAP.

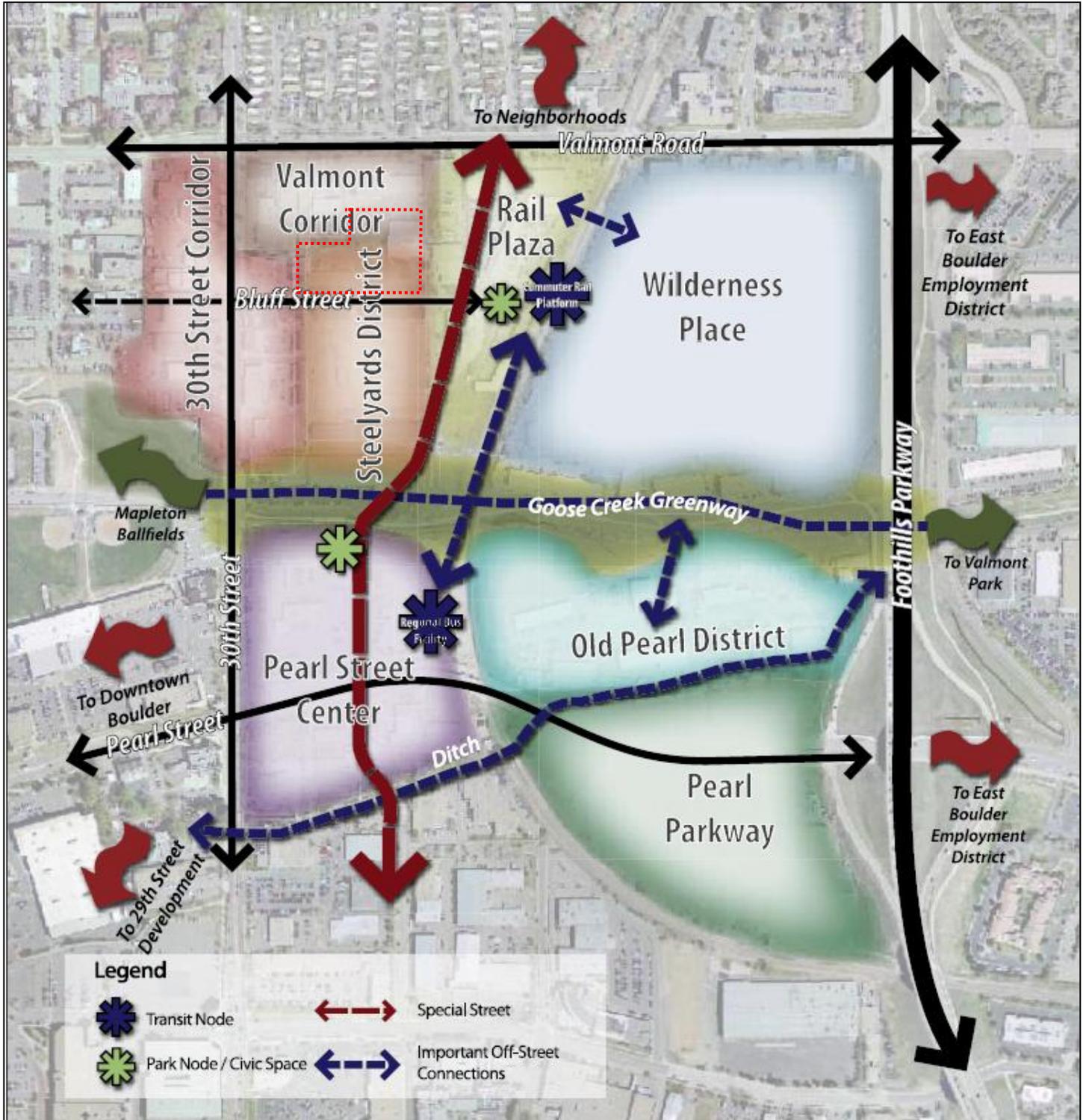


Figure 10: Site Context within Transit Village Area Plan Character Districts

The Steelyards District is described in TVAP as follows,

“most of the Steelyards District was recently developed by the Steelyards project, a mixture of housing, shops and small-scale service businesses. The industrial uses on the north side of Bluff Street will transition to high-density residential, such as urban townhouses. The southern part of the district is mixed-use industrial, one- to two-story live/work units.”

Plan Goals and Objectives. The following objective within TVAP is related directly to the desire to provide affordable housing in the plan area:

“Support Diversity: A variety of housing types at a range of market rate to affordable (including housing for very low, low, moderate and middle income households) to meet diverse needs (workforce housing, senior housing, family housing and housing for special populations such as those with disabilities.)

TVAP Connections Plan. The site must also provide connections consistent with the intent of the TVAP connections plan, shown in Figure 11 below for the area surrounding the site. In keeping with the Boulder Valley Comprehensive Plan (BVCP) and Transportation Master Plan (TMP), the Transportation Connections Plan was developed in conjunction with the proposed area land uses and to support the city’s sustainability goals. The Connections Plan is also designed to: support the area’s new regional rail and bus facilities; facilitate the Transportation Demand Management (TDM) program; and support the plan’s urban design goals. The objectives are to:

- *Establish a fine-grained, multimodal network of transportation connections that will:*
 - *establish a pedestrian-friendly environment;*
 - *create safe and convenient access to transit;*
 - *establish a rich variety of safe and convenient connections to all modes within the area and to major activity centers and the rest of the community, including Twenty Ninth Street, CU, Downtown, nearby neighborhoods, and the employment and industrial area to the east; and*
 - *support the changes in land use, increases in density, and urban character proposed by the area plan.*
- *Provide new roads on an approximate 400-foot grid.*
- *Provide pedestrian connections approximately every 200 feet to provide mid-block access for bicycle and pedestrian access while providing flexibility for property owners.*
- *Provide key alley connections that are shown on the Connections Plan, where they are required for access or to separate different land uses. Encourage additional alleys, particularly in locations with higher intensity land uses that anticipate buildings located up to the street.*
- *Locate connections to straddle property lines when possible to reduce the burden on individual property owners.*
- *While providing significant flexibility through the amendment process, show some connections on the map as flexible in order to emphasize that the intent is a complete and appropriately spaced connection, rather than a precise alignment.*
- *Provide new traffic signals on Pearl Parkway, 30th Street, and Valmont Road at 34th Street to facilitate transit and traffic movement and provide safe pedestrian crossings and connections to the surrounding neighborhoods.*

As shown below in the connections plan there are several planned connections through the site that are numbered to reference descriptions of each connections, presented on page 55 of TVAP, and described below.

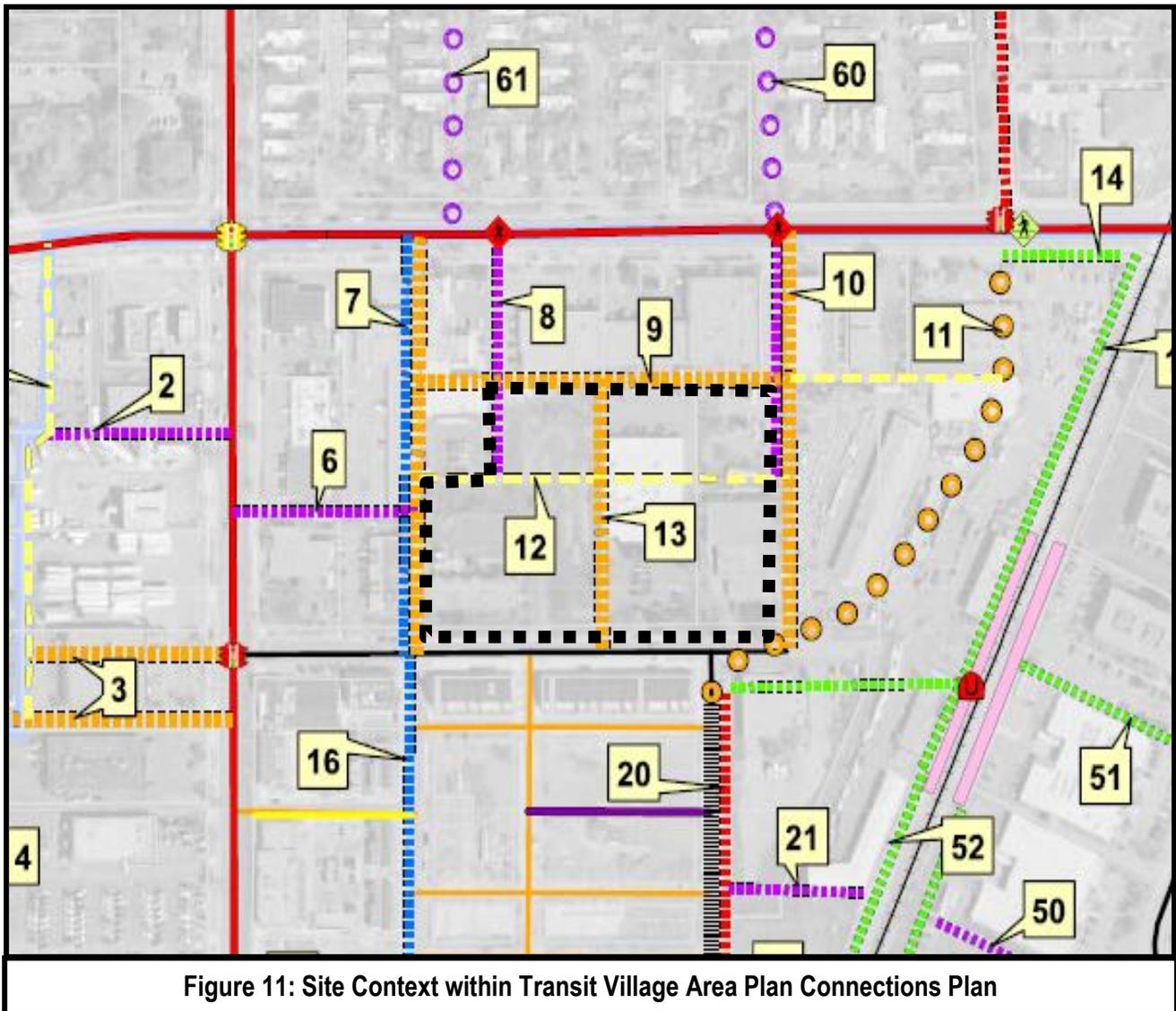


Figure 11: Site Context within Transit Village Area Plan Connections Plan

7) Local Connection: “this road will extend the existing 31st Street in Steelyards and is located on existing property boundaries. This connection is located about 250 feet east of 30th Street and will create the fine-grained street network that is friendly to pedestrians. While this is a desirable long-term connection, existing uses and the Service Commercial land use designation retained on the parcels along Valmont will make this connection dependent on property consolidation and redevelopment. Retaining the Service Commercial land use designation limits the incentive for redevelopment and will make it more difficult to achieve this connection. Without significant redevelopment opportunities, it is likely that the city would need to pay for right-of-way (ROW), if it can be acquired at all. Given the likely long-term nature of this connection through the properties along Valmont Road, a near-term pedestrian connection (#8) is shown on the east side of the eastern property to provide pedestrian access through the currently impermeable block face along Valmont Road.”

8) Pedestrian Connection: “this pedestrian connection will penetrate the current barrier of development along Valmont Road and allow pedestrian access from Valmont Road into the redevelopment of the area. While it is mapped on the property boundary, its location is flexible and would be more desirable closer to 30th Street.”

9) Local Connection: “This connection is shown as a local road, providing a break and buffer between the service commercial land uses on Valmont Road and the high-Density Residential-1 land to the south. The road is located on the property boundaries and had previously been identified as an alley, but was requested as a road by the major property owner to the south.

10) Local Connection and Pedestrian Connection. “Local road and pedestrian connection: This connection will provide access by penetrating the current barrier of development along Valmont Road and allow movement from Valmont Road into the redeveloped portion of the area. The connection also provides a break and buffer between the land uses of Service Commercial and High-Density Residential-2. Both types of connections are shown, as the Service Commercial designation of properties along Valmont Road limits their redevelopment potential. So it is hoped that a pedestrian connection could be achieved in the near term without significant impact on the properties, and the road connection achieved if redevelopment does occur. The connection is about 450 feet from the first pedestrian connection east of 30th Street and therefore is slightly past the upper limit of the desired length between pedestrian connections. While it is mapped on the property boundary, its location is flexible and would be more desirable closer to 30th Street.”

12) Local Connection: “This connection is shown as an alley, providing parking and service access to the adjacent parcels. It is located on the property boundaries of two parcels and divides three parcels, but will provide the needed access to the very deep and oddly shaped properties along Bluff Street.”

13) Local Connection: “This local road breaks up the long block along Bluff Street and provides access to both the east-west alley and local road to the north. It is located on property boundaries to minimize impacts on each property.”

Refer to Key Issue 2 on page 17 for an analysis of the proposed connections.

Zoning. As shown in Figure 12 below, consistent with the HDR-1 land use designation under TVAP the site is zoned Residential High – 6 (RH-6). The RH-6 zone district was developed to implement the HDR-1 land use designation in TVAP. The intent of the RH-6 is defined in the land use code section 9-5-2 (c)(1)(H) B.R.C. 1981 as follow,

“(H) Residential - High 6: High density residential urban areas that are predominately townhouses in close proximity to either a primary destination or a transit center and where complementary uses may be allowed.”

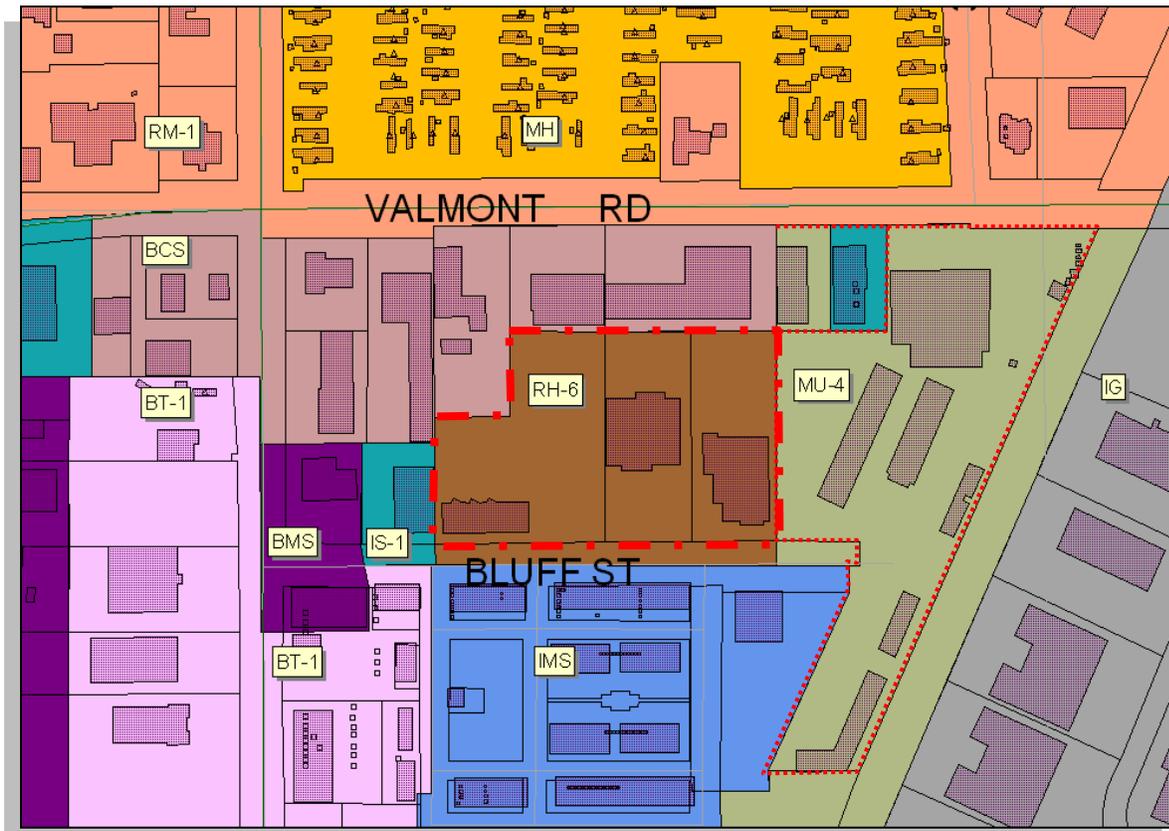


Figure 12: Site within Zoning Context

(3) Applicable criteria, review procedures, and submission requirements for a site review;

Following Concept Plan review, the applicant will be required to submit for a Site Review given the request for a modification to setback standards. For a project to be found consistent with the Site Review criteria, consistency with the TVAP guidelines is required. A preliminary analysis of this consistency is provided below.

(4) Permits that may need to be obtained and processes that may need to be completed prior to, concurrent with, or subsequent to site review approval;

The applicant will be required to submit for a Preliminary Plat concurrent with Site Review, to dedicate the roadways as shown on the Concept Plan as well as lot line eliminations between properties. Similarly, all required Inclusionary Housing documents including the Determination of Inclusionary Housing Compliance form, covenants to secure permanent affordability of the units, and an Off-site Agreement must be signed and recorded prior to application for any residential building permit. Any applicable cash-in-lieu contribution must be made prior to receipt of a residential building permit. On or off-site permanently affordable units must be marketed and constructed concurrently with the market-rate units.

(5) Opportunities and constraints in relation to the transportation system, including, without limitation, access, linkage, signalization, signage, and circulation, existing transportation system capacity problems serving the requirements of the transportation master plan, possible trail links, and the possible need for a traffic or transportation study;

The applicant illustrates roadway connections that are not yet consistent with the intent of the TVAP connections plan. Refer to Access and Circulation comments above.

(6) Environmental opportunities and constraints including, without limitation, the identification of wetlands, important view corridors, floodplains and other natural hazards, wildlife corridors, endangered and protected species and habitats, the need for further biological inventories of the site and at what point in the process the information will be necessary;

There are no known special status plant or animal species located on the site. The site has been developed with the office building and parking lot for approximately 30 years and is denuded of most vegetation except for some landscaping in the front of the building, and several mature trees in the front and side yard setbacks. Otherwise, the site primarily contains weedy plant species.

(7) Appropriate ranges of land uses;

While the site is designated within TVAP for High Density Residential – 1 (HDR-1) and predominately townhome units, the proposed project meets other objectives for TVAP by the provision of affordable housing.

(8) The appropriateness of or necessity for housing.

As noted, TVAP anticipated this site for residential, predominately townhome configuration. The proposed 45 units of permanent affordable townhomes supports the overall Boulder Valley Comprehensive Plan policies for provision of affordable housing within the city. Those policies include:

7.01 Local Solutions to Affordable Housing

The city and county will employ local regulations, policies, and programs to meet the housing needs of their low and moderate income households and workforce. Appropriate federal, state and local programs and resources will be used locally and in collaboration with other jurisdictions.

The city recognizes that affordable housing provides a significant community benefit and will continually monitor and evaluate its policies, programs and regulations to further the city's affordable housing goals.

7.02 Permanently Affordable Housing

The city will increase the proportion of permanently affordable housing units to an overall goal of at least ten percent of the total existing housing stock through regulations, financial subsidies and other means. City resources will also be directed toward maintaining existing permanently affordable housing units and securing replacements for lost low and very low income units.

The TVAP- HDR1 (high density residential – 1) land use, as defined on page 16 of TVAP is as follows,

“Urban townhomes and garden apartments with individual garages, surface parking lots, or underground parking. Mainly two to three stories.”

Similarly, the intent of the RH-6 zoning designation is stated in the Land Use Code section 9-5-2, B.R.C. 1981 as,

“High density residential urban areas that are predominately townhouses in close proximity to either a primary destination or a transit center and where complementary uses may be allowed.”

Townhouse is further defined in section 9-16-1, B.R.C. 1981 as follows:

“Townhouse means an attached single family dwelling unit located or capable of being located on its own lot, and is separated from adjoining dwelling units by a wall extending from the foundation through the roof which is structurally independent of the corresponding wall of the adjoining unit.”

The applicant's written statement indicates that all of the 77 planned units will be either for-sale, rental, or permanently affordable rental townhomes.

Key Issue 1: Does the Concept Plan preliminarily meet the Vision of TVAP along with the General Design Guidelines?

In an initial consistency review of the plan with TVAP, the project appears consistent with the TVAP vision and guidelines. As noted on page 9 of TVAP, *“at the outset of the Transit Village Area planning process, the City Council and Planning Board adopted the following vision to provide direction for the development of the plan. Among the vision statements applicable to the Concept Plan are those that follow:*

A place that is not overly planned, with a “charming chaos” that exhibits a variety of building sizes, styles, and densities where not everything looks the same.” A place that emphasizes and provide for alternative energy, sustainability, walking, biking and possible car-free areas, e.g. “eco-village.”

Below is an assembly of images from the surroundings and the approved projects is presented below, in Figure 13, to illustrate the variation throughout Boulder Junction. The Concept Plan sketches illustrate buildings that appear to be in keeping with the variety expected.



Figure 13: Variation in character anticipated in TVAP

General Guidelines. The TVAP also contains General Guidelines for all character districts. Following is a brief consistency analysis of the Concept Plan to the guidelines.

Building Placement and Design

- ***Orient the main facade to the street and provide an entrance on the street side of the building.***

The Concept Plan (Figure 14 below) illustrates street-facing entrances on all of the public streets: Bluff Street, the future 32nd Street, and the extension of Junction Place. The applicant also provided massing diagrams and precedent images that indicate the intent for “walkable design” with street facing entrances.

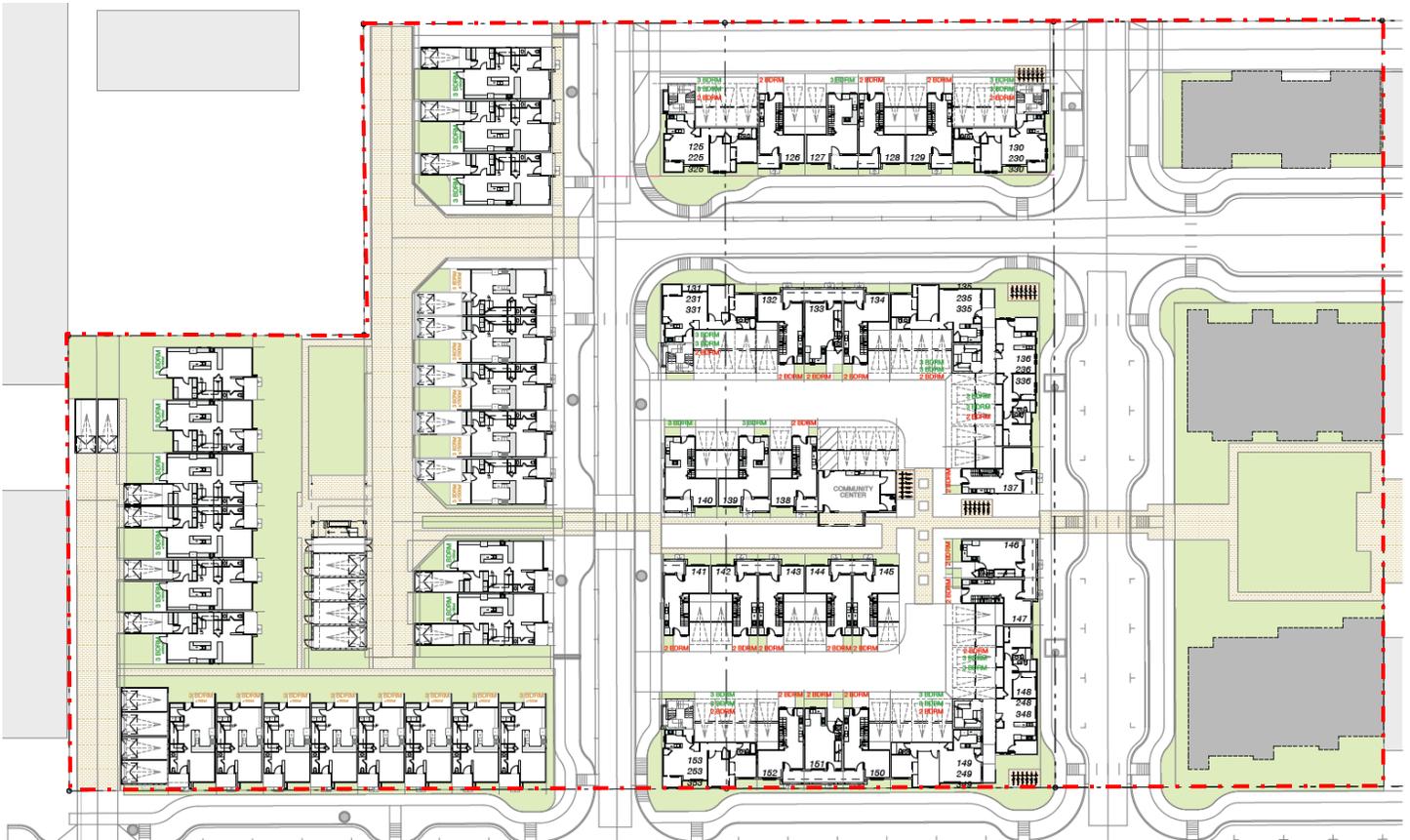


Figure 14: Concept Plan layout with street facing entrances

- ***Design buildings with pedestrian-scale materials and architectural articulation, particularly on the first floor. Avoid large blank walls. Along streets and sidewalks provide pedestrian interest, including transparent windows and well-defined building entrances.***

While the exhibits provided don't illustrate this level of detail,, precedent images illustrate an intent to meet this guideline. There are well-defined building entrances and there are no large blank walls that are evident in the exhibits, albeit massing diagrams at this stage of review. The success of how well this guideline is ultimately met in Site Review will depend upon the final design of the entrances as well as the types of finish materials. As noted above, the Site Review criteria requires that projects utilize “authentic materials and detailing” that will augment the pedestrian experience of the buildings.

- **Consider opportunities to frame or preserve views of the Flatirons to the southwest.**

The Concept Plan utilizes an interior open space corridor from east to west. The configuration of open area to building may provide an opportunity for units toward the back of the site to access views. Also, as is shown in Figure 15, below, there are existing “keyhole” views from the subject site along Bluff Street of the Flatirons because of the roadway to building and parking configurations on the Steelyards development. Inherent in any developed environment in Boulder is the loss of views as well as the framing of view of the Flatirons, as can be seen with even two story development. However, with the planned open spaces in the development, opportunities for view are possible. As project plans progress, potential viewshed capture should be explored further using modeling and placement of windows.



Figure 15: “Keyhole” View through Steelyards Development from 3085 Bluff St. Site

Useable Open Space

- ***Incorporate well-designed, functional open spaces with tree, quality landscaping and art, access to sunlight and places to sit comfortably. Where public parks or open spaces are not within close proximity, provide shared open spaces for a variety of activities. Where close to parks, open spaces provided by development may be smaller.***

The RH-6 zoning requires 600 square feet of open space per dwelling unit. There is approximately 51,892 square feet of open space illustrated preliminarily with approximately 77 units illustrated for a total of 674 square feet of open space per unit. While the open space appears to tally greater than the requirement, as project plans progress, the plans will need to detail elements described in this guideline as well as within the requirements of 9-9-11, B.R.C. 1981, “Useable Open Space.”



Figure 16: Open Space Diagram

Permeability

- ***While the improved street network will provide more frequent pedestrian connections, also provide multiple opportunities to walk from the street into projects, thus presenting a street face that is permeable. Also provide opportunities to walk within the interior between abutting properties. This is especially important where street blocks are large, for example in the Wilderness Place District.***

With the need to establish the connections consistent with the intent of the connections plan, there appears to be notable permeability throughout the concept plan layout.

Energy Efficiency Goals within TVAP. The intent statement in TVAP is the following: *A place that emphasizes and provides for alternative energy, sustainability, walking, biking and possible car-free areas, e.g. “eco-village.”* Among the plan goals and objectives are those that emphasize use of energy efficiency in architecture, site planning and urban design:

4. Connect to the natural and built environment:

Create a place that reflects Boulder’s commitment to environmental sustainability and “green” development is integrated with the natural features in the area and connects to the larger city fabric, including:

- a. Innovative “green” energy efficient site planning, architecture and urban design.*

The applicant submitted a preliminary “Sustainability Report” on page 20 of the submittal that describes conceptually, the proposed sustainable design techniques. As project plans progress, the expectation will be that greater information on specific techniques to meet this TVAP goal as well as the Site Review criteria for energy efficiency will be provided. The overall goal, as expressed in the Sustainability report is, “the incorporation of green building with smart growth principals mixed land uses, walkability and access to transit.” Because specific details on how to achieve green building techniques are often associated with a more detailed design stage, conceptually, the application would meet the intent of the TVAP energy efficiency goals.

Key Issue 2: Do the proposed modifications to the TVAP Connections Plan meet the intent of TVAP?

The TVAP connections plan was established with rationale for each connection to create connectivity, and there is an assumed level of interpretation within the connections plan. As noted on page 59 of TVAP, Appendix 3, Connections Explanation and Rationale,

“The purpose of this appendix is to provide a detailed explanation and rationale for each connection on the Transportation Connections Plan. It will be used to help interpret the Connections Plan for capital improvement planning and review of individual development review applications.”

Staff finds the proposed modifications to meet the intent of TVAP. Given the goals of connectivity and specific intent for each connection, Staff reviewed an earlier iteration of the proposed connections with the applicant and recommended revisions to be more in keeping with the original intent of each connection. The applicant revised their plan accordingly and staff finds the proposed requests to change the connections plan to meet the intent of the connections plan.

The proposed modifications to the TVAP connections plan are illustrated in a comparison in Figures 17a and 17b with the connections plan and the proposed connections respectively. For the proposed project, and the streets intended to be dedicated per the Connections Plan (numbers 7, 9, 10, 12, and 13), the applicant is proposing the following:

Proposed Changes to the Connections Plan:

- Consolidate 7 and 13 local connections
- Relocate 9 slightly south with a new addition of an alley to serve Valmont Properties
- Junction Place and 10 are partially consolidated
- Keep 12 as a connection through the site

Staff notes that the elimination of 7 appears acceptable given the goal to have two points of vehicular access to Valmont Road. Connection 13 and 10 will achieve the two points of access as proposed. On page 59 of TVAP, with regard to connection 7 it states,

“While this (#7 connection) is a desirable long-term connection, existing uses and the Service Commercial land use designation retained on the parcels along Valmont will make this connection dependent on property consolidation and redevelopment. Retaining the Service Commercial land use designation limits the incentive for redevelopment and will make it more difficult to achieve this connection. Without significant redevelopment opportunities, it is likely that the city would need to pay for right-of-way (ROW), if it can be acquired at all. Given the likely long-term nature of this connection through the properties along Valmont Road, a near-term pedestrian connection (#8) is shown on the east side of the eastern property to provide pedestrian access through the currently impermeable block face along Valmont Road.”

As shown in the comparison of existing Connections Plan to proposed in Figures 17a and 17b, the applicant is intending to combine the roadway access (yellow dashed line), bike route (blue dashed line) and the pedestrian connection (purple dashed line) to achieve the intent of connections 7, 8 and 13 and understanding that there are adjacent properties to the north not yet likely to redevelop in the near future. The partial consolidation of 10 and Junction Place is really just the alignment of Junction Place orthogonally through the site, until the intersection with local connection 9. At that point, Junction Place is intended to move through the S’PARK development to the east and ultimately turn north to connect to Valmont Avenue at 34th Street. Figures 18a and 18b illustrate a broader context of the proposed changes.

Modifications to TVAP require Planning Board approval and, if connections are consolidated or eliminated, City Council approval as well. As project plans progress, a traffic study will be required to help confirm that the modifications plan need to also meet the land use code standards, the Site Review criteria and Design and Construction Standard

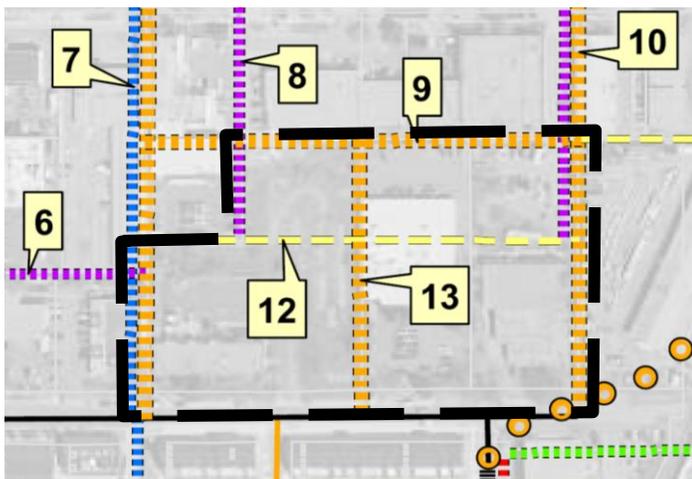


Figure 17a: Connections Plan through Site

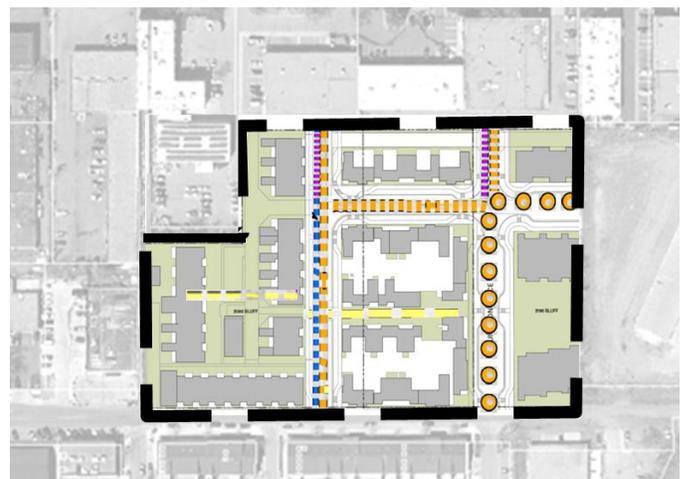


Figure 17b: Proposed Connections through Site

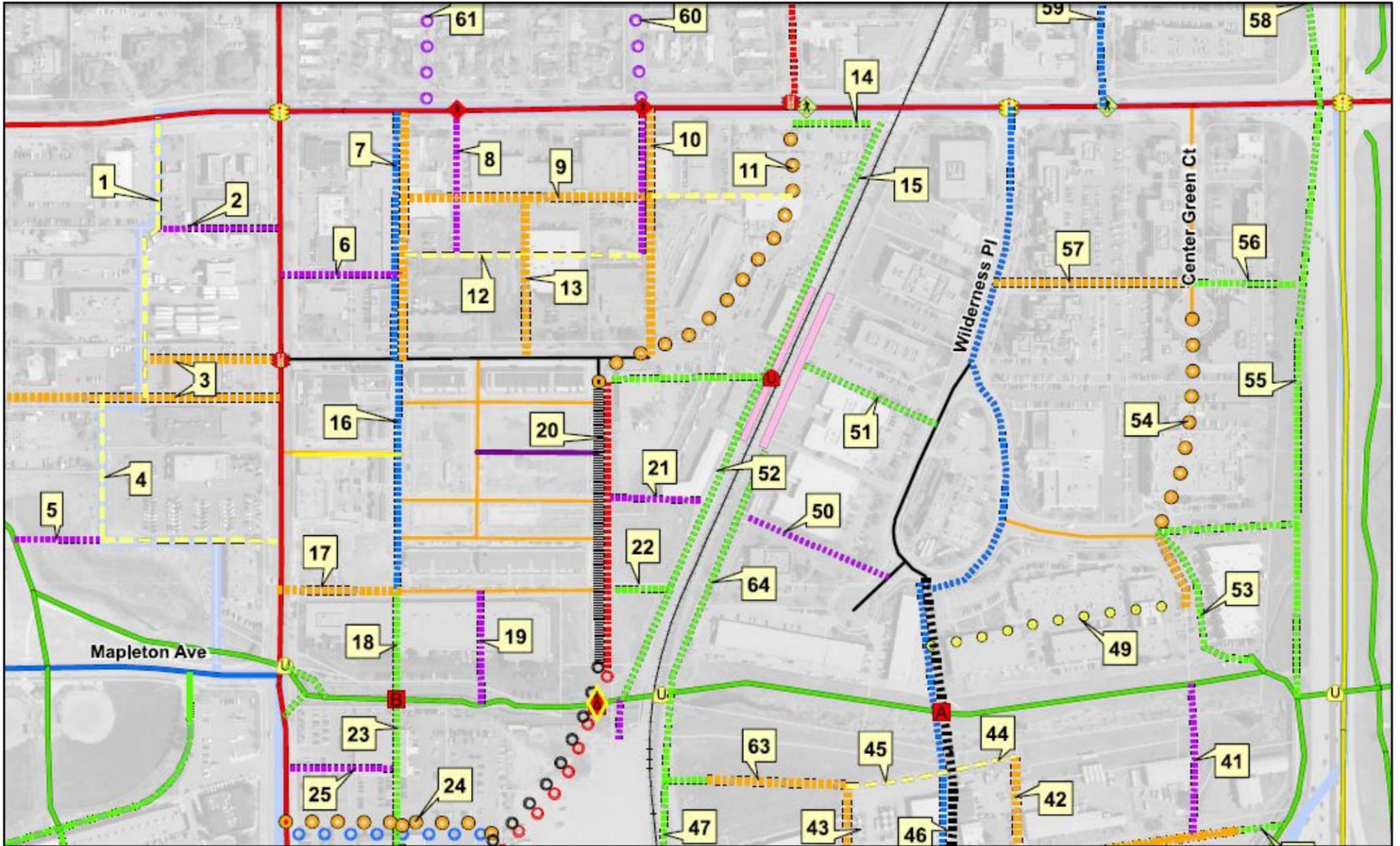


Figure 18a: Existing TVAP Connections Plan: Northern Half

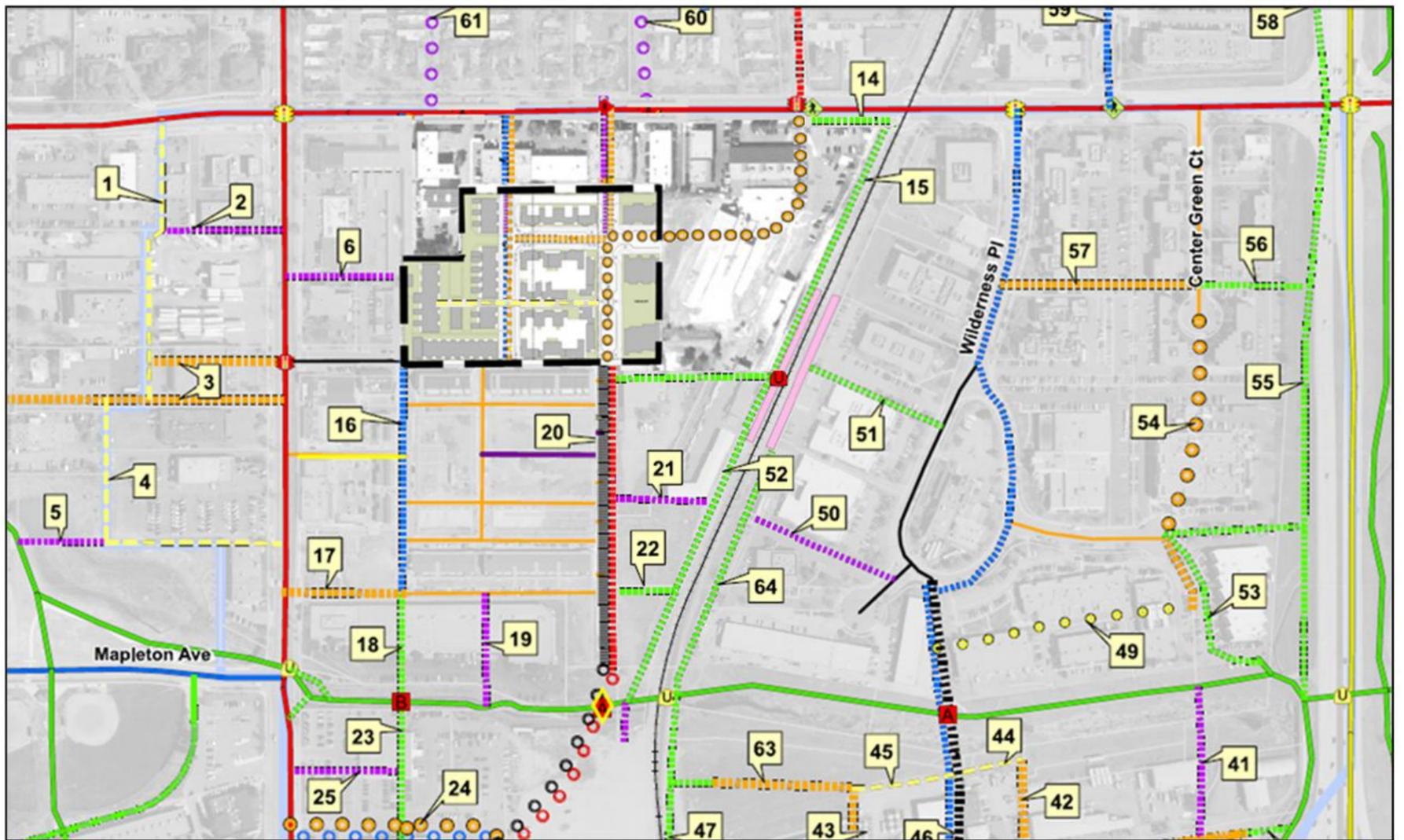


Figure 18b: Proposed TVAP Connections Plan: Northern Half

Legend

Existing Street Connections	Proposed Street Connections	Existing Bike and Ped Connections	Proposed Bike and Ped Connections	Existing Bike/Ped Crossing
— Collector Street	— Collector Street	— Multi Use Path	— Multi Use Path	— Enhanced Crossing
— Local Street	○ Collector Street - Flexible Alignment*	— On Street Bike Lane	— Multi Use Path Existing Upgrade	— Underpass
— Railroad	— Collector Street - Upgrade Existing	— Designated Bike Route	— On Street Bike Lane	— Traffic Signal
— Plan Area Boundary	— Local Street	— Sidewalk Connection	— On Street Bike Lane - Flexible Alignment*	
— Proposed Rail Platform	○ Local Street - Flexible Alignment*	— Paved Shoulder	— Designated Bike Route	
	— Local Street - Upgrade Existing		— Designated Bike Route - Flexible Alignment*	
	— Alley		— Sidewalk Connection	
	○ Alley - Flexible Alignment*		— Sidewalk Connection - Flexible Alignment*	

V. PUBLIC NOTIFICATION AND COMMENT

Required public notice was given in the form of written notification mailed to all property owners within 600 feet of the subject site and a sign posted on the property for at least 10 days. All notice requirements of Section 9-4-10(g), B.R.C. 1981 have been met. Staff received two phone calls from individuals inquiring about the project, but who didn't express particular concerns but asked to be notified for the public hearing.

VI. PLANNING BOARD ACTION:

No action is required by Planning Board. Planning Board, Public and staff comments will be documented for use by the applicant. Concept Plan review and comment is intended to give the applicant preliminary feedback on the development concepts, and direction for site review applications.

Approved By:



David Driskell, Executive Director
Department of Community Planning and Sustainability

Attachments

[Attachment A: Applicant's Submittal Materials](#)



PLANNING • ARCHITECTURE • DESIGN
 www.sopherarchitects.com
 O: 303 444-6902 asopher@sopherarchitects.com
 1919 14th STREET, SUITE 610 BOULDER, CO 80302

MEMORANDUM

To: Charles Ferro – CITY OF BOULDER
 Sam Assefa – CITY OF BOULDER
 Elaine McLaughlin – CITY OF BOULDER
 Edward Stafford – CITY OF BOULDER
 David Thompson – CITY OF BOULDER
 Elizabeth Lokocz – CITY OF BOULDER

From: Adrian Sopher - SOPHER ARCHITECTS

Project: **SPARK_west** – 3085 & 3195 Bluff Street

Date: 7 July 2014

Re: SUPPLEMENTAL INFORMATION FOR CONCEPT PLAN REVIEW SUBMITTAL – Description of Planning and Design Considerations in development of submittal.

As background for review of the Concept Plan submittal package and as follow-up from the meeting we had in our office in early May, I thought it might generally be useful for staff to have a description of the process we went through in the development of the package submitted. Below is an outlined attempt to do so...

GENERAL PLANNING CONSIDERATIONS

General Design Intent

To design an integrated, village-like set of neighborhoods that linked together the varying communities that might share the overall SPARK landscape, all oriented towards the shared streets and commonly linked open spaces, while maintaining a high-level design quality and character for each neighborhood.

Planning Criteria And Organization Of Overall Site

Generally, we had three sites to work with in the RH-6 zone; from west to east, 3085, 3155, & 3195 Bluff Streets. Our goal was to organize a circulation and land use pattern for these parcels, based on the following considerations:

- STREET LAYOUT
 - Develop an alignment of streets and private drives that incorporates the intent of TVAP for a fine-grained and walkable network, while adjusting it to configure more appropriately to the changes arising out of the SPARK Concept Review.
 - Accept the alignment of Junction Place in the already established north/south location as discussed and supported by Planning Board at the SPARK Concept Review hearing.
 - Extend Meredith and the East/West alley westward along the north property line, accepting the the alignments established thru SPARK at CPR.
 - Develop an alignment for 32nd Street to work reasonably well to connect with Steelyards – especially for pedestrians and bikes, but without implying a thru connection for autos.

- Develop a method of traffic calming along Meredith and 32nd Streets that does not encourage auto thru-traffic in the townhouse zone, except via Junction Place and Bluff Streets.
- OPEN SPACE
 - Establish a set of open spaces that serve each of the three sites appropriately in terms of the populations that are being served.
 - Link them to one another so that the various housing community sites become connected as a single integrated neighborhood by way of green spaces, as well as streets.
- BUILDING CONFIGURATION & PARKING –
 - Orient all buildings either directly to the public street layout or towards a shared open space.
 - Orient as many front porches as possible along the public street faces and where not possible, along the shared common open space.
 - Bring the porches close to the street themselves, but provide a grade separated landscape buffer that still allows for human interaction while maintaining a degree of privacy and ownership for the residents.
 - Have no cars parked on-site visible from the public streets (other than on-street short-term parking).
 - Maximize the amount of family oriented 2 & 3-bedroom affordable housing units as a stand-alone project west of Junction Place, with a minimum of 45 units (3155 Bluff Street).
 - Leave as much space as possible for a market rate townhouse project to the west, with the intent of developing 24 units at ±1500 gsf each (3085 Bluff Street).
 - Incorporate whatever number of remaining units allowable – per the **Net Site Area** method of calculation – into the SPARK market rate rental residential project that sits east of the MU-4 boundary. These units would be designed as rental townhouses and flats, and would be located east of Junction Place and west of the zone boundary line (3195 Bluff Street).

A WORD ON THE NET VS. GROSS ORDINANCE

...And Its Effect On This Project

The ordinance reviewed by Planning Board and about to go to Council, significantly affects the overall unit count in the RH-6 zone. We have worked closely with staff to determine ways to minimize the impact of the adopted Connections Plan on this and the larger SPARK property, however the net vs gross site area method has the biggest impact on this portion of the project, while it takes no consideration for the amounts of open space provided in the larger SPARK context.

According to the method for determining allowable numbers of units, there are two calculations that limit the amount of units buildable on site. Both are affected by the net vs gross issue:

- **Site Area:** total site area in the RH-6 zone (185,000 sf), dedicated rights-of-way per the connections shown, remove over 41,200 sf from the three combined sites overall (22%). At 1800 sf required per unit...
 - **Net area yield: 79 units**
 - **Gross area yield: 102 units**
- **Open Space:** currently shown as ±51,000 sf across the zone, incorporating all the areas needed for water quality. We may be able to increase this by adding all of the private open space (±8000 sf), but have not included this, as we may still lose some open space to miscellaneous hardscape areas. At 600 sf required per unit...
 - **Net area yield:** we are limiting our unit count to **77 units** on this basis, until we have a more defined project.
 - Gross area yield (including 70% of row towards OS calculation): **additional 49 units** allowed.

...And How It Affected Our Approach To Planning The Site

We have approached the planning of **SPARK_west** by establishing our priorities as follows...

- **First, do a viable Affordable Housing Project** – This is not the first time that this team has put forward an affordable housing project on this site...
 - Over a year ago, Planning Board reviewed and favorably responded to a Concept Plan submittal for 3085 Bluff. The project at that time was planned as 41 permanently affordable units, of which over 90% were flats, with a few townhouses. Colorado Housing and Finance Authority (our then hoped-for funding source) reviewed the project favorably, however they expressed extreme displeasure over the amount of infrastructure required by the city, for which all of the costs were to be born by the project itself.
 - Subsequent to that, the city found funds to support the project infrastructure cost, and we redesigned the project with a smaller building footprint to allow for a future phase, whereby additional density may later be added should the governing planning ordinance change. Before we could re-submit the Concept Plan, we

began working on SPARK and our approach to the larger site made us reconsider how we might configure the affordable component in that context.

- In the process, we have learned that without sufficient density to defray the costs associated with building in Boulder, whether due to land cost, infrastructure requirements, or direct land use code limitations on density, the viability of affordable housing projects – from the perspective of potential funding sources – is in jeopardy.
- Consequently, we have taken the approach that if we want to do an affordable housing component for SPARK, given the density limitations we have in the RH-6 zone, we first have to allocate an appropriately viable number of units to the affordable component, even before the market rate unit numbers are met.
- To that end, we targeted a minimum number of units for the affordable component, and then tried to fit – from an allowable unit perspective – everything else around it. With the approval of the net vs gross ordinance in the future, it would then support the viability of the market rate RH-6 sites.
- Second, leave room for a viable Market Rate Townhouse project to the west of it, separated by 32nd Street – This meant that the location of the street separating the two projects had to be located in such a manner that allowed for a viable site plan for both building types.
- Third, apply whatever units remaining (per the Land Use Code Net Area calculation method) to 3195 Bluff – This meant that the area east of Junction Place west of the MU-4 zone line, would take the remainder number of units. However if we look at it as a stand-alone site...
 - 50,400 gsf – area prior to ROW dedications.
 - 23,302 gsf – 46% of the individual site area goes to ROW dedication (does not include the alley private drive at north side of site).
 - 27,008 gsf – net site area for 3195 Bluff.
 - 15 units net or 28 units gross – number of units allowable as a stand-alone site.
 - However on the basis of the Net Area method of calculation across the entire RH-6 zone, this left us a total of 8 units, to be spread across the north and south sides of Meredith Street and facing southward on Bluff Street.
 - **This leaves us with a high density zone at a density of 7 units per acre on a 1.1 acre site in the Transit Village, clearly not in keeping with the intent of the zone.**
 - However even under the residual calculation for the RH-6 as a whole, a total of 102 units would be allowed in the zone, where 77 units are currently shown.

Please note that if the net vs gross ordinance is approved by council, it would be our intent to pursue a total of approximately 24 units which meet the requirements of the RH-6 zone, on the 3195 site at the time of Site Plan Review, bringing our total to ±93 units in the RH-6 zone.

PLANNING CONSIDERATIONS FOR THE INDIVIDUAL SITES

3195 Bluff – Market Rate Rental Townhouses & Flats (At Corners)

- Revise the RH-6 portion of SPARK to meet the requirements of the RH-6 zone, without requesting rezoning to allow for flats alone (as was requested of the board previously). In so doing, no rezoning of that site would be required.
- **Note:** Units on this portion of the RH-6 zone will be incorporated into the SPARK Site Plan Review submittal subsequent to the SPARK_ *west* Concept Plan Review.
- Establish a Common Green which fronts onto the private indoor and outdoor recreation area for SPARK, but orients westward towards and links directly to the open space for adjacent affordable housing project.
- Establish a footprint for the units in this zone, which would nominally remain the same whether or not the net vs gross ordinance is approved by Council.

3155 Bluff – Permanently Affordable Rental Townhouses & Flats (At Corners)

- Develop a method for calming the street traffic thru the site...
 - Maintaining the street pattern generally established by Meredith and the northern alley.
 - Establish a location for 32nd Street north of Bluff that can work as an offset alignment from the sector south of Bluff in Steelyards.
 - Note: One-way orientation and narrow ROW of these streets help to accomplish this. And because of one-way alignment with a right-out from 32nd, the minimum offset from the southern leg of 32nd Street is supportable.
 - In the central block, develop an internal loop road for vehicular access to the rear of the units, and leave sufficient space in the middle of the site for a shared Common Open Space that links to the Green east of Junction Place on 3195 Bluff.

- As much possible, group the locations where autos are parked on the internal loop road. This would create zones that do not require the full 24' backup for cars, and can therefore allow for intensified planting areas in key spots along the roadway.
- Along the main block fronts...
 - Orient street front townhouse units towards the streets, raise 1.5' above the sidewalks to maintain separation.
 - Associate wherever possible, a single tuck-under parking space to the rear of the structure.
 - Orient all main Living Room areas towards the front porch and street, and all Kitchens towards the internal drive, thereby keeping as many eyes as possible on the back-side of the block.
- In the Central Greenspace area...
 - Provide a 900-1000 gsf Common House fronting onto that Green, with facilities for laundry and small child play area.
 - Orient porch front townhouse units towards the green.
 - Orient all main Living Room areas towards the front porch and green, and all Kitchens towards the internal drive, thereby keeping as many eyes as possible on the back-side of the block, as well as on the front.
 - Provide other common facilities, including guest and overflow bike parking, additional long-term storage, and handicap van parking space.
 - Associate wherever possible, a single tuck-under parking space to the rear of the structure, and shared parking at the internal corner.
 - Locate all bedrooms on the 2nd floor.
- Site Corners...Use the corners of the block for flats (as allowed by the LUC), to turn the corner and maintain a sense of neighborhood for the block as whole.
 - Corner buildings would have their stair towers placed at the inside corner of structure, thereby allocating the dead corner mass for a use that has no impact on habitable space.
 - Place handicap accessible Type A & B units at the ground level flats, and associate the handicap parking spaces with the entry aisle for access into the structure.
 - Provide tuck-under parking at either side of the corner towers, effectively using the internal drive side of the ground level.
 - Wrap the street side of the corner structure with a single unit in each direction. Smaller units on the ground level, and larger units above, where the autos are no longer limiting the footprint available.
 - Orient all main Living Room areas and all Kitchens towards the street, thereby keeping as many eyes as possible on the public realm.
- **3085 Bluff – Market Rate Townhouses**
- Develop an internal site drive pattern which would...
 - Align auto entry to the site to correspond with the extension of Meredith in the east/west direction, and 31st Street in the north/south direction.
 - Connect the pedestrian east/west link to 3155 and 3195, terminating the greenspace at a shared open space for this site.
 - Minimize the amount of dead-end length of drives to what would be acceptable for fire protection service. Provide the ability for truck turnaround at the location of the east/west greenspace link to 3155 & 3195.
 - Provide attached parking garages wherever possible (limited to the larger footprint units), and shared detached parking garages in locations where attached parking would undermine the viability for private open space (for the smaller units).
 - In the central zone, provide rooftop gardens above the shared garage structures.
- For the units...
 - Orient all main Living Room areas towards the front porch and street and publicly accessible greenspace, thereby keeping as many eyes as possible on the public areas.
 - Orient all Dining areas towards the rear semi-private garden areas and back patios, thereby connecting these areas with outdoor dining and more private ground floor outdoor space.
 - Locate all bedrooms on the 2nd floor.
 - Provide a flexible use 3rd Floor space (possible family space, home office, studio), in lieu of basements.
 - Orient all 3rd Floor outdoor deck areas towards the southern or western mountain views, as orientation allows.

cc Karl Guiler – CITY OF BOULDER
 Scott Holton – ELEMENT PROPERTIES







SPARK: a modern urban village with active greenspaces and cutting-edge sustainability for people to live, work, eat, and play — a true mixed use and transit-oriented place for Boulder Junction. A place for the crafters, the makers, and the innovators.

SPARK_west: SPARK's residential neighborhood designed to connect people with a sense of place as much as each other.

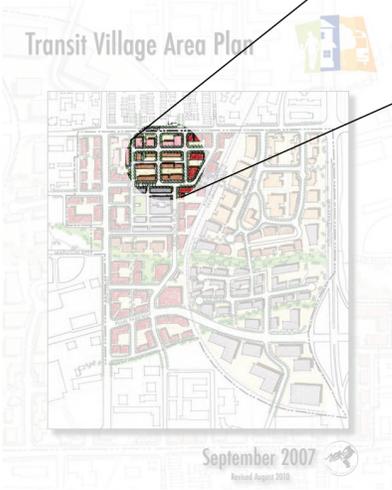
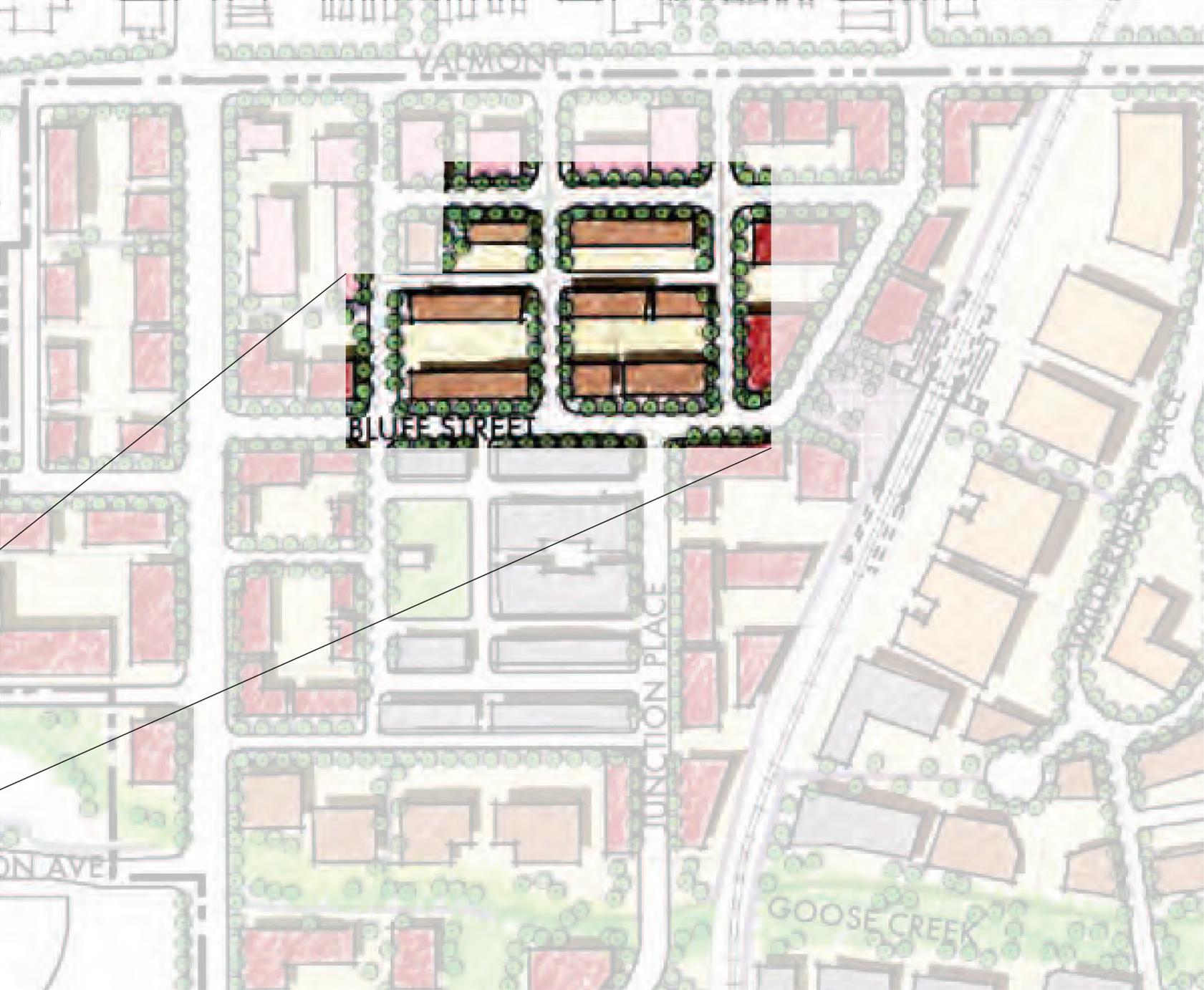
- Innovative Mixed Income
- Inclusive Open Spaces
- Inspiring Walkable Design

5	Boulder Junction		
6	Site Area	A1	Appendix Site, Ground
7	Existing Conditions	A2	Site, Second
8	Context Maps	A3	Site, Third
9	Adopted Zoning and Connections	A4	Water Quality
10	Proposed Zoning and Connections	A5	Utilities
11	Open Space Distribution	A6-A7	Site Survey
12	Proposed Re-platting		
13	Site Plan		
14	Building Areas		
15	Vision Imagery		
16	Spark & Spark_west Plan		
17	Conceptual Views		
18	Conceptual Views		
19	Written Statement		
20	Sustainability Report		
21	Team		

Boulder Junction

The Boulder Junction Plan (formerly known as the Transit Village Area Plan, or TVAP) was the result of a robust community process that was adopted by the City Council in September 2007 and subsequently updated in August 2010. The plan sought to encourage the creation of a new, transit-oriented place in Boulder to support diverse uses and a new regional transit center to better serve the greater community.

1. Create a well-used and well loved pedestrian oriented place.
2. Support diversity
3. Enhance economic vitality
4. Connect to the natural and built environment
5. Maximize the community benefit of the transit investment
6. Create a plan that will adapt to be resilient for Boulder's long term future



Boulder Junction - SPARK_{west} Townhomes and Affordable Housing

3085 Bluff Street
This Site will be developed to include (24) 3-Bedroom Condominium units with (1) garage per unit

3155 Bluff Street
This site will be developed to include (45) affordable housing units comprised of 2 and 3-bedroom condominium units and flats with (1) covered parking space per unit

3195 Bluff Street
This site will be developed with for-rent Condominium units with (1) parking space per unit. Quantity of units is to be determined, based upon the City's determination of net-to-gross density.



3085 BLUFF STREET
PARCEL #0510153
COUNTY APN: 146329119004
FULL LEGAL DESCRIPTION: LOT 1 KBOL MINOR SUBDIVISION PER REC 2727584 BCR
AREA: 83,600 SF
ZONING: RH-6
BUILT: 1981

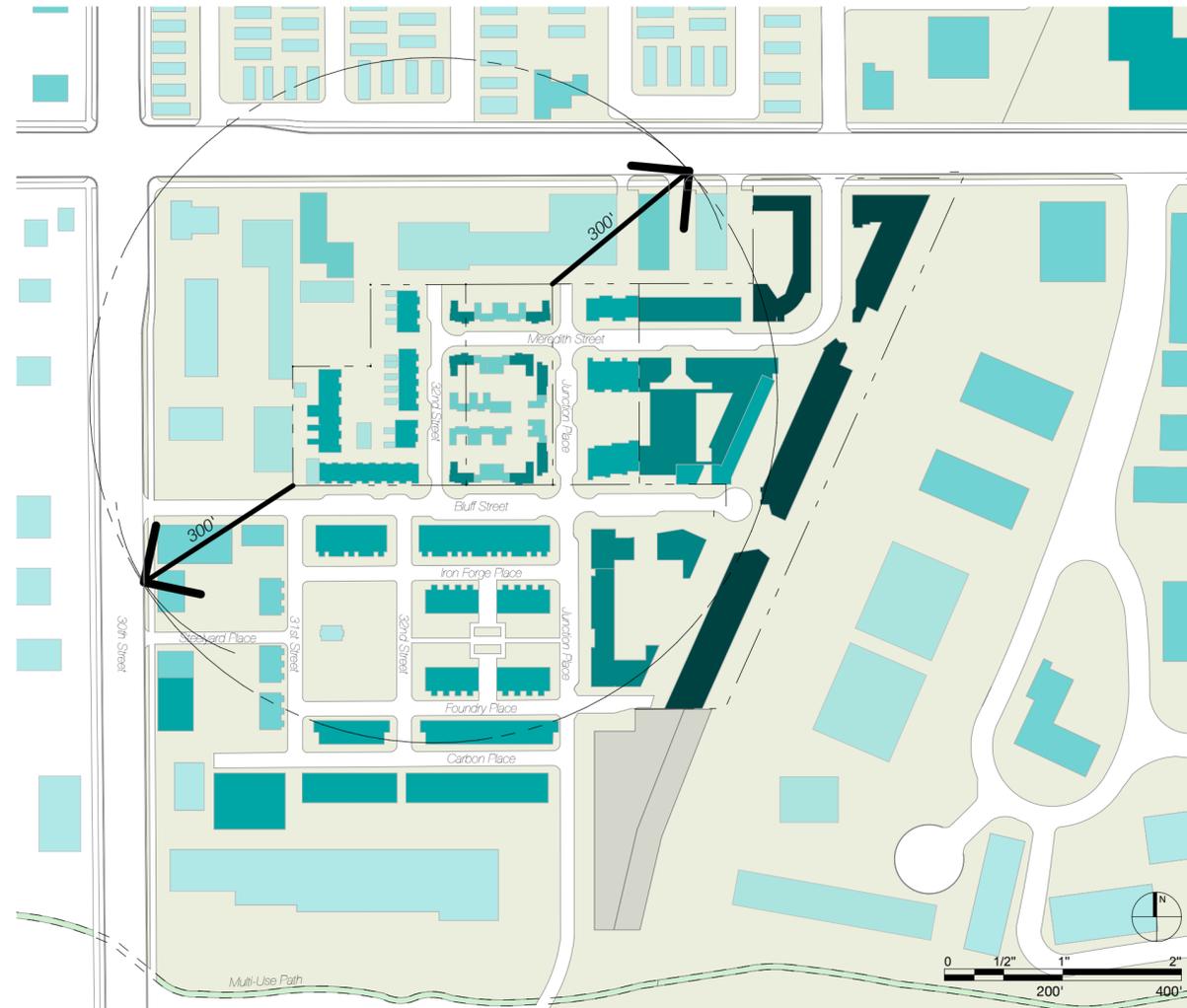
3155 BLUFF STREET
PARCEL #0015793
COUNTY APN: 146329101010
FULL LEGAL DESCRIPTION: LOT 10 WALKERS & S 1/2 VACATED STREET
AREA: 51,000 SF
ZONING: RH-6
BUILT: 1978

3195 BLUFF STREET
PARCEL #0015821
COUNTY APN: 146329101009
FULL LEGAL DESCRIPTION: LOT 9 & VAC ORTION OF MEREDITH STREET ABUTTING LOT VAC
IS RECORDED BOOK 87 1 PAGE 390 WALKERS
AREA: 50,400 SF
ZONING: RH-6
BUILT: 1964

Existing Conditions



Context: Proposed Height



LEGEND

- 10'-15' HEIGHT
- 15'-25' HEIGHT
- 25'-35' HEIGHT
- 35'-45' HEIGHT
- 45'-55' HEIGHT

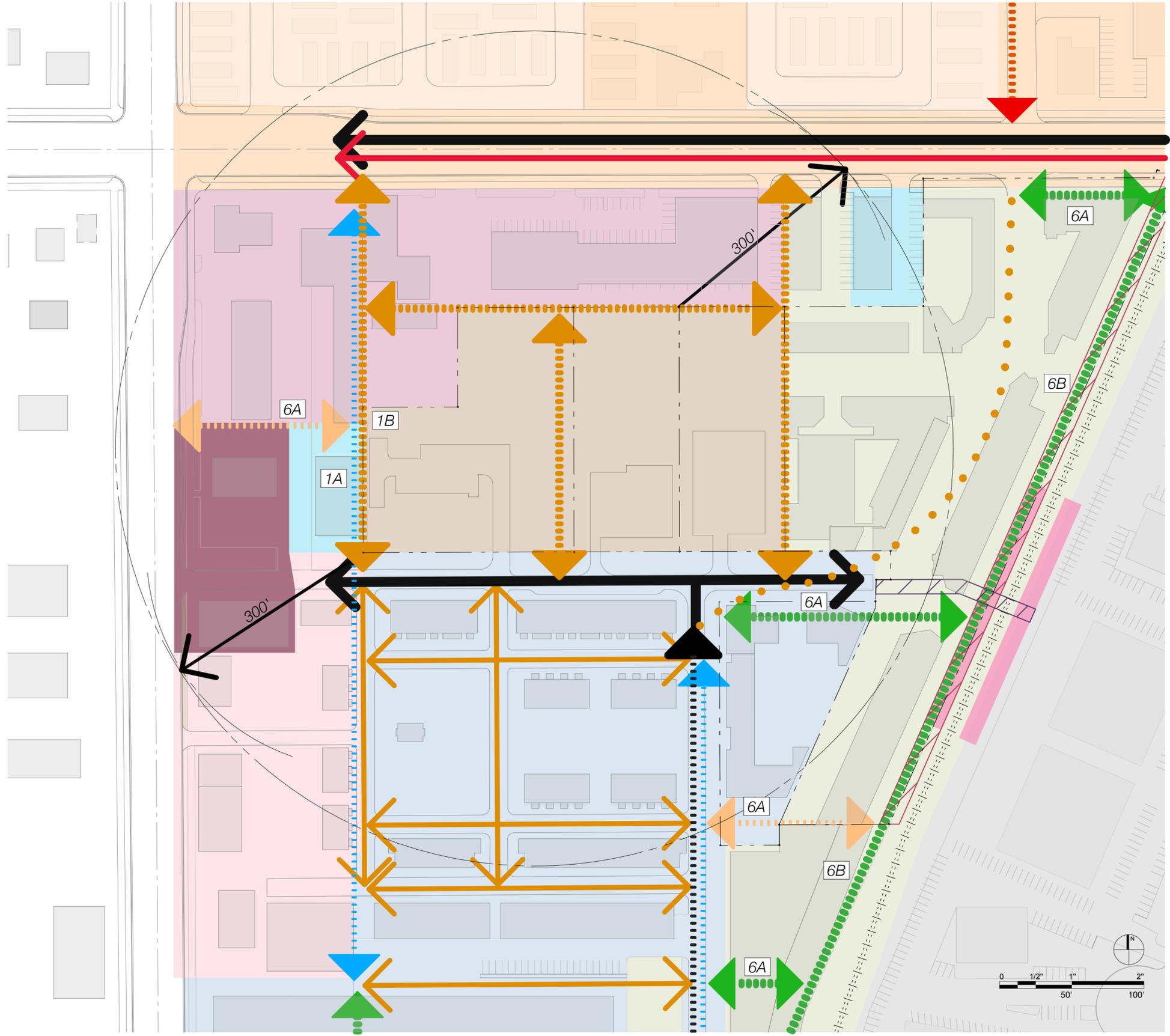
Context: Proposed Use



LEGEND

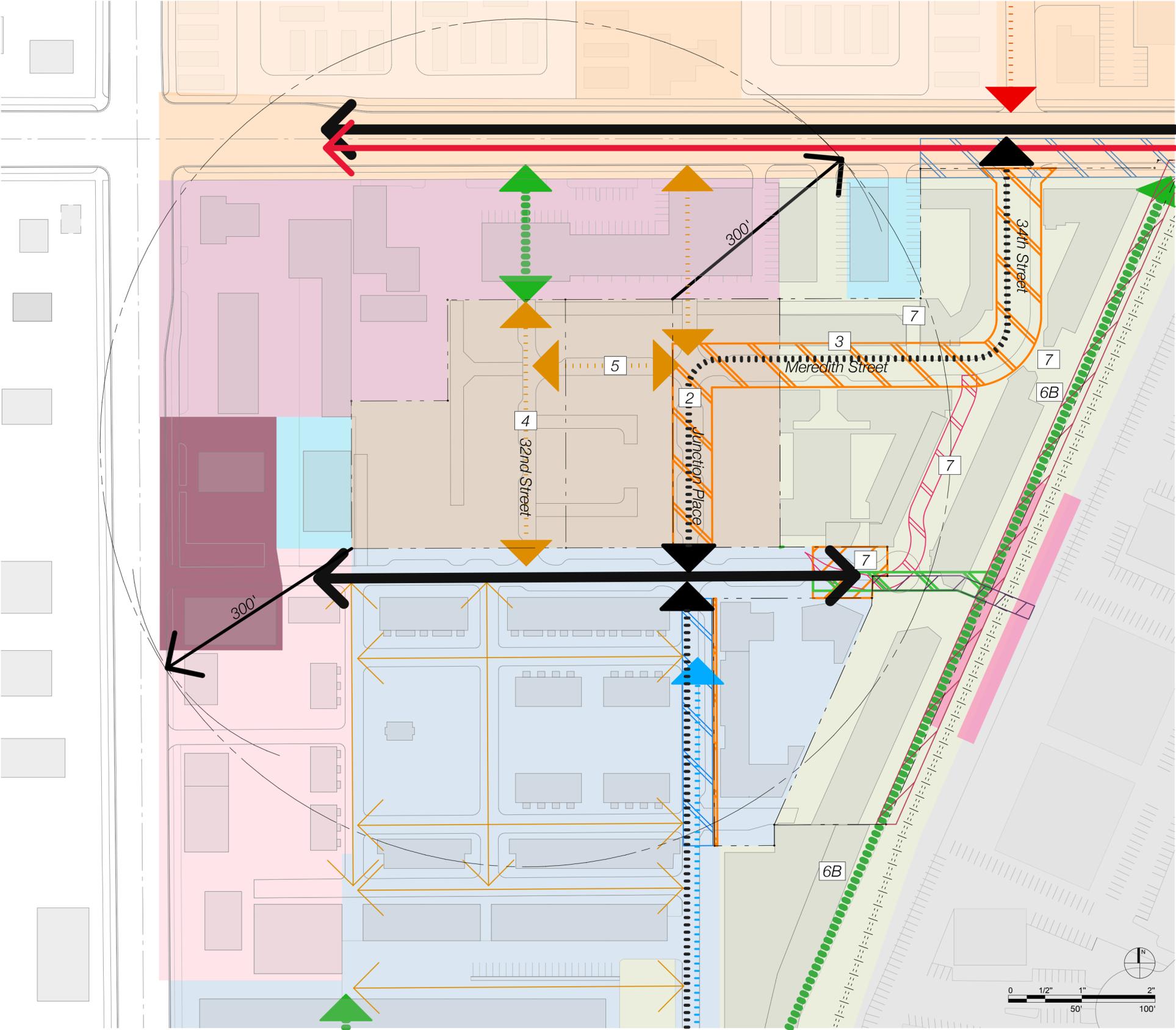
- RESIDENTIAL
- COMMERCIAL, RETAIL, INDUSTRIAL
- MIXED USE (RESIDENTIAL AND COMMERCIAL)
- OFFICE, MEDICAL, FINANCIAL
- DINING AND ENTERTAINMENT
- PUBLIC, INSTITUTIONAL
- PARKING

Adopted Zoning & Connections



- 1A Adopted Proposed On Street Bike Route to be deleted
 - 1B Adopted Proposed Local Street to be deleted
 - 2 Proposed routing of Junction Place
 - 3 Proposed Meredith Street relocated from adopted Connections Plan
 - 4 Proposed 32nd Street relocated from adopted Connections Plan
 - 5 Proposed relocation of future local streets from adopted Connections Plan
 - 6A Adopted Proposed Path to be deleted
 - 6B Adopted Proposed Multi-Use Path to be Proposed Bike Path
 - 7 Proposed Private Drive
- | | | |
|--------------------|-------------------------------------|---|
| MU-4 | ROW Dedication | Existing Collector Street |
| IS-1 | ROW Vacation | Proposed Collector Street |
| IMS | Sanitary Easement | Existing Local Street |
| RH-6 | Existing Drainage Easement | Proposed 2-way Local Street |
| BCS | Proposed Drainage Easement | Proposed 1-way Local Street |
| BMS | Railroad Easement | Proposed Local Street -flexible alignment |
| RM-1 | Proposed Railroad Platform | Existing on street bike lane |
| MH | Proposed Woonerf (Private Drive) | Proposed On Street Bike Lane |
| BT-1 | Proposed Pedestrian Access Easement | Proposed On Street Bike Route |
| IG | | Existing Multi-Use Path |
| PROPOSED RH-3 | | Proposed Bike Path |
| PROPOSED FLEX ZONE | | Proposed Soft Surface Pedestrian Path |
| | | Existing Railroad |

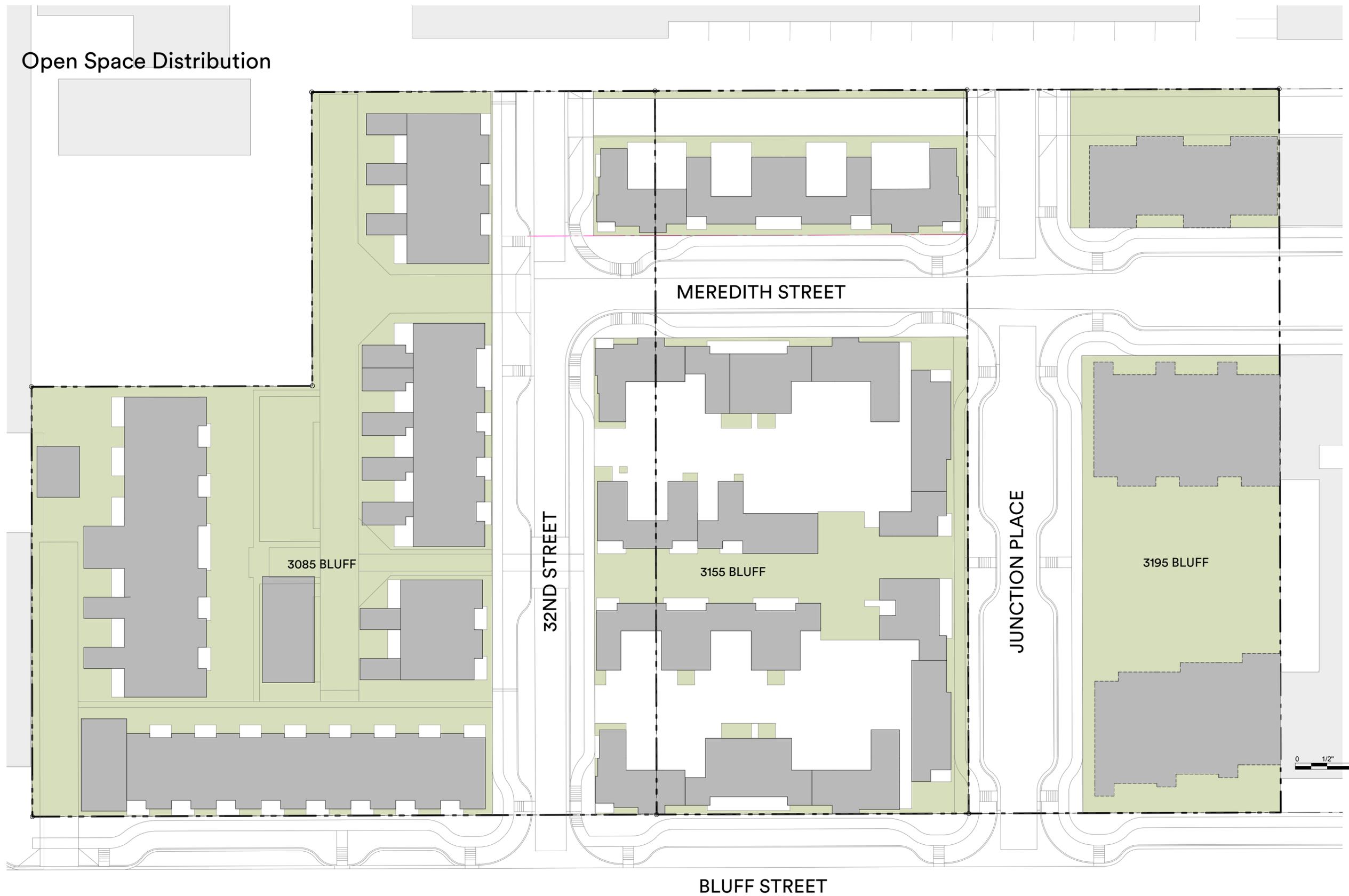
Proposed Zoning & Connections



- 1A Adopted Proposed On Street Bike Route to be deleted
- 1B Adopted Proposed Local Street to be deleted
- 2 Proposed routing of Junction Place
- 3 Proposed Meredith Street relocated from adopted Connections Plan
- 4 Proposed 32nd Street relocated slightly west of adopted Connections Plan
- 5 Proposed continuation of Meredith Street
- 6A Adopted Proposed Path to be deleted
- 6B Adopted Proposed Multi-Use Path to be Proposed Bike Path
- 7 Proposed Private Drive

MU-4	ROW Dedication	Existing Collector Street
IS-1	ROW Vacation	Proposed Collector Street
IMS	Sanitary Easement	Existing Local Street
RH-6	Existing Drainage Easement	Proposed 2-way Local Street
BCS	Proposed Drainage Easement	Proposed Local Street -flexible alignment
BMS	Railroad Easement	Existing on street bike lane
RM-1	Proposed Railroad Platform	Proposed On Street Bike Lane
MH	Proposed Woonerf (Private Drive)	Proposed On Street Bike Route
BT-1	Proposed Pedestrian Access Easement	Existing Multi-Use Path
IG		Proposed Bike Path
PROPOSED RH-3		Proposed Soft Surface Pedestrian Path
PROPOSED FLEX ZONE		Existing Railroad

Open Space Distribution



REQUIRED OPEN SPACE AREA

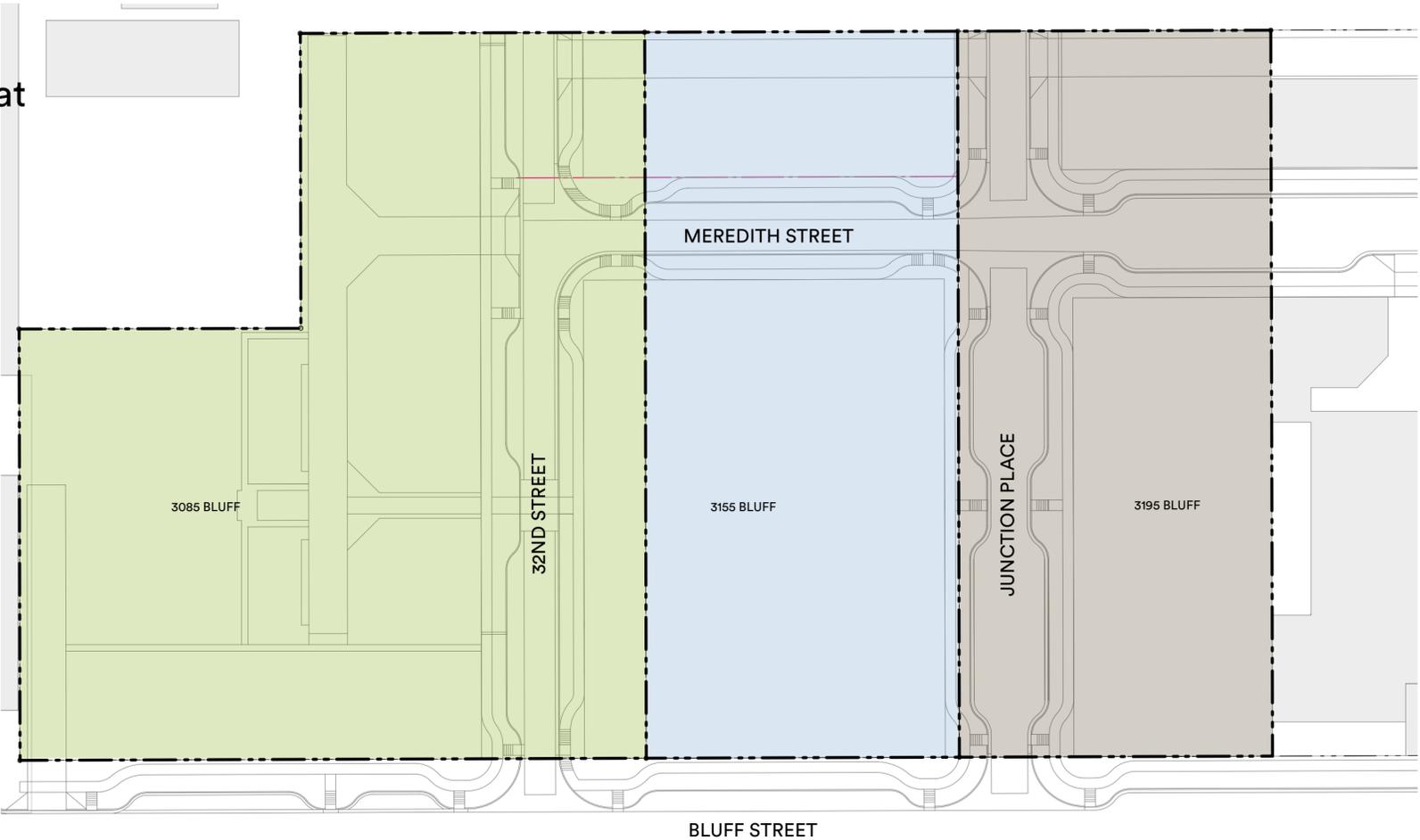
SITE 3085:	24 UNITS x 600 SF =	14,400 SF
SITE 3155:	45 UNITS x 600 SF =	27,000 SF
SITE 3195:	4 UNITS x 600 SF =	2,400 SF
TOTAL REQUIRED =		43,800 SF

PROVIDED OPEN SPACE AREAS

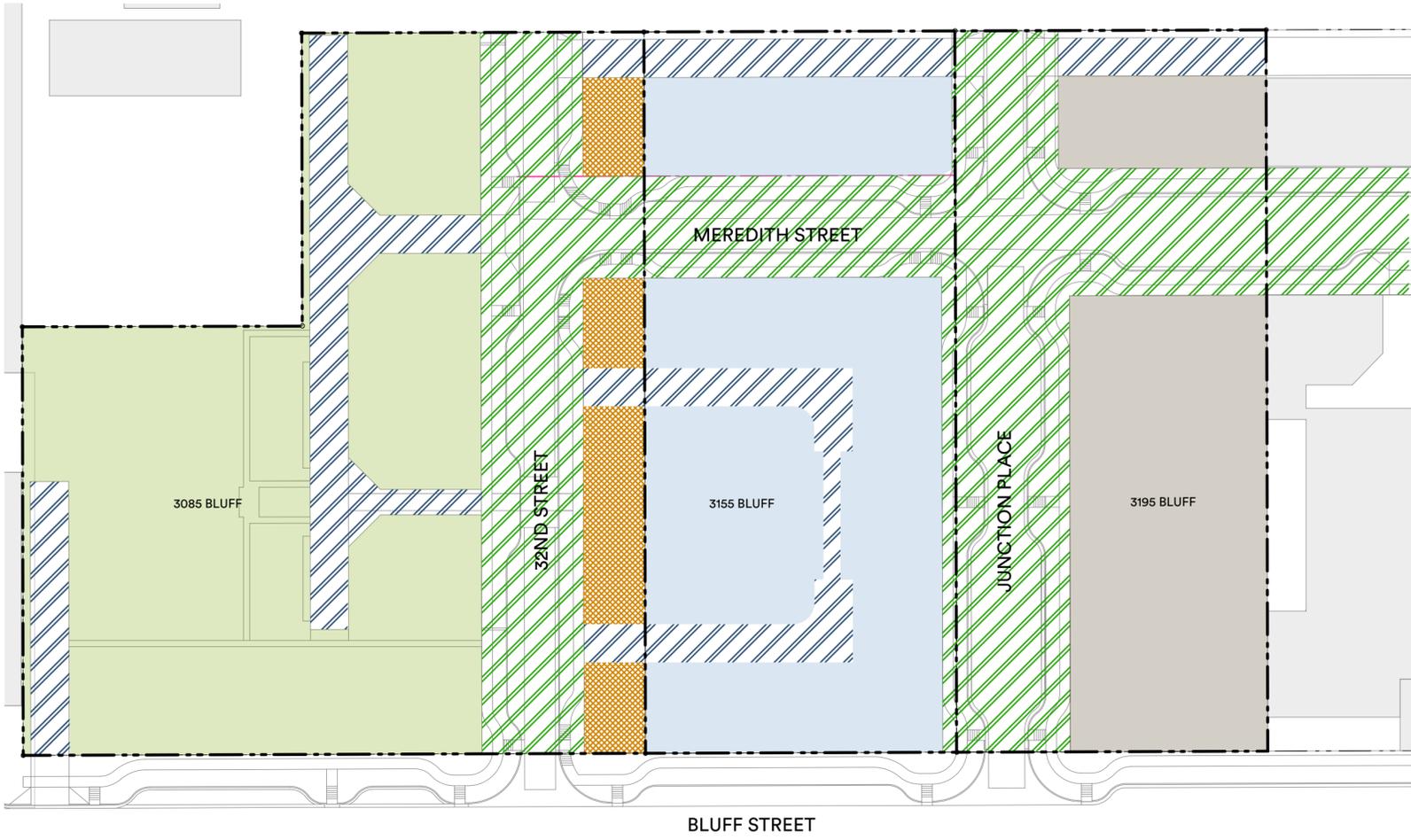
AREA BREAKDOWN	
SITE 3085:	28,128 SF
SITE 3155:	10,990 SF
SITE 3195:	13,874 SF
TOTAL:	52,992 SF

- BUILDING FOOTPRINT
- OPEN SPACE
- PERVIOUS PAVED PRIVATE DRIVE

Proposed Re-Plat



EXISTING



PROPOSED

-  PUBLIC STREET RIGHT OF WAY
-  PRIVATE DRIVE RIGHT OF WAY
-  3195 BLUFF STREET
-  3155 BLUFF STREET
-  3085 BLUFF STREET
-  3085 PROPERTY TO BE RE-ALLOCATED TO 3155

Site Plan



- 3085 BLUFF STREET**
- (24) 3-STORY TOWNHOMES
 - (1) CAR GARAGE PER UNIT
 - PERVIOUS PAVED PRIVATE DRIVES

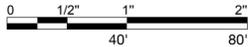
- 3155 BLUFF STREET**
- (16) 2-BEDROOM TOWNHOMES
 - (5) 3-BEDROOM TOWNHOMES
 - (9) 2-BEDROOM FLATS
 - (16) 3-BEDROOM FLATS
 - (45) OFF-STREET PARKING SPACES

- 3195 BLUFF STREET**
- (8) TOWNHOMES (TO BE DEVELOPED AS PART OF S*PARK PROPER)

GREEN ROOF OVER GARAGES FOR COMMUNITY GARDEN

COMMUNITY CENTER

BLUFF STREET



Permeable Pavers & Rain Gardens



Lush Streetside Plantings with Elevated Porches



Native, Sustainable Plantings & Lawns



Community Center Gathering Space



Urban Park for Play

Building Areas

S*PARK west – PROPERTY BUILDING AREAS

SITE PARCELS

	PARCEL GROSS AREA (S.F.)	GROSS AREA IN ACRES	PUBLIC ROW AREA (S.F.)	NET AREA (S.F.)	ALLOWABLE UNIT DENSITY (@1800 S.F./UNIT)	PROPOSED UNITS	PROPOSED AREA/ DWELLING UNIT
3085 BLUFF STREET	81,196	1.864	17,701	63,495	35	24	1,758
3155 BLUFF STREET	49,681	1.141	8,575	41,106	23	45	999
3195 BLUFF STREET	49,671	1.140	23,412	26,259	15	tbd	
TOTALS	180,548	4.145	49,688	130,860	73	69+ *	1,378

*TOTAL NUMBER OF UNITS TO BE PROPOSED FOR 3195 WILL BE DEPENDENT ON RULING FOR NET-TO-GROSS ISSUE

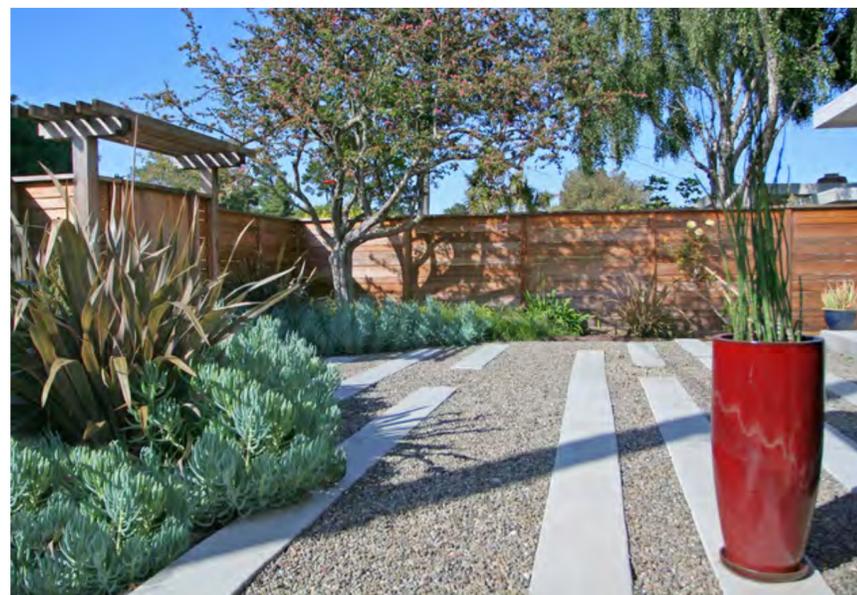
Innovative Mixed Income



Inclusive Open Spaces



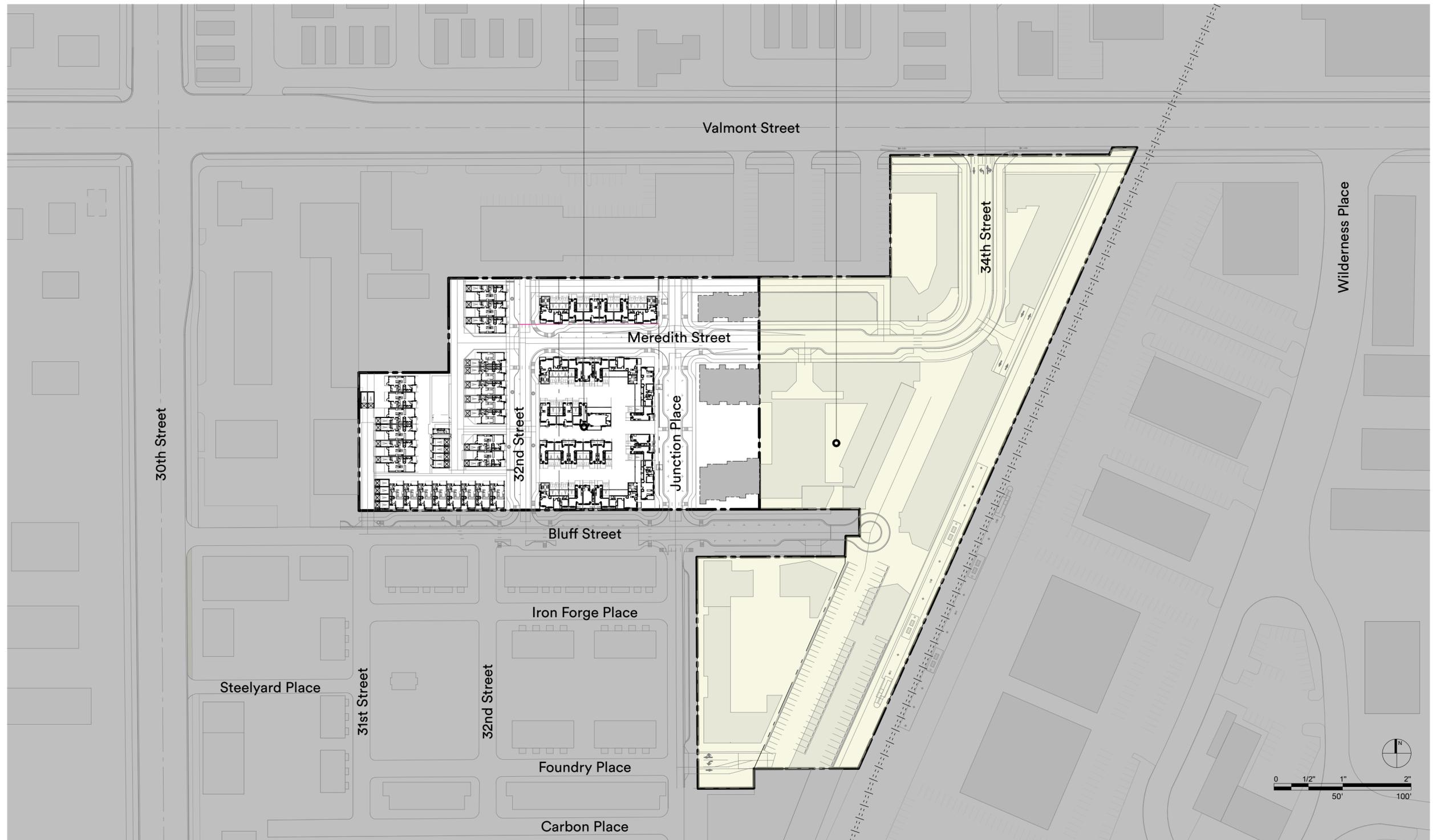
Inspiring Walkable Design



S'PARK - Overall Plan

S'PARK_{west}
For-sale and for-rent townhomes and flats

S'PARK
Comprised of Residential, Mixed Use Commercial/Residential, Commercial/Retail/Industrial, Dining and Entertainment and underground parking



Conceptual Views



Birdseye view of site



View of 3155 site, southwest along Junction Place



View of 3155 site at northeast corner of Bluff and 32nd



View of 3155 site, looking west along Meredith

Conceptual Views



View of 3085 site along Bluff St



View of 3085 Site along 32nd

Written Statement

SPARK: a modern urban village with active greenspaces and cutting-edge sustainability for people to live, work, eat, and play — a true mixed use and transit-oriented place for Boulder Junction. A place for the crafters, the makers, and the innovators.

SPARK_west: SPARK's residential neighborhood designed to connect people with a sense of place and each other.

Innovative Mixed Income Inclusive Open Spaces Inspiring Walkable Design

SPARK_west will ensure that SPARK is a more complete and inclusive place and fulfills our community's vision for Boulder Junction and the Transit Village Area Plan (TVAP). SPARK_west resides within SPARK but in a smaller neighborhood area along Bluff Street that is within an area reserved for townhome style units which will serve to promote economic diversity, establish neighborhood stakeholders, and leverage the virtues of a compact development pattern.

SPARK_west will play an important role in building an inclusive and enduring community in SPARK and Boulder Junction, enhancing and contributing to the overall SPARK goals of:

Vibrant & Fun

A curated eclectic mix of uses, tenants, and residents; featuring local artisan providers and partners; intense-enough for serendipity, collaborations, and new connections; a focus on local culinary and craft.

Modern & Sustainable

Inspiring design and a mix of public and private spaces and places; repurposing and recycling of legacy materials from the former lumberyard and past industry; LEED-ND Platinum and world-class holistic sustainability.

Urban & Connected

Immediate access to the RTD Bus Rapid Transit (BRT) station for regional and local connectivity, the Goose Creek Multi-Use path, and local bike and pedestrian destinations; possible future location of RTD commuter rail; access to shopping, services, employment, and education; a venue for exchange of interests and ideas.

PROJECT COMPONENTS

45 Affordable 'For-Rent' Residences

An integral part of SPARK_west, located in the heart of SPARK, will be a new, 100% permanently affordable residential community comprised of 45 for-rent townhomes and flats in partnership and with the support of funding from the City of Boulder Housing Division and tentatively from the Colorado Division of Housing (CDOH) under a Community Development Block Grant (CDBG) funding allocation from HUD — a laudable example of successful public private partnership.

Notably, the property is located in a Severely Distressed Census Tract — an indication of this neighborhood's 23.5% poverty rate. SPARK_west will be an incredible opportunity for Boulder to provide affordable — and market rate — residents with access to our community's existing and planned infrastructure investments. Residents of the income-restricted housing will also benefit from a partnership with the SPARK development for employment opportunities in the planned commercial uses. This opportunity to provide mixed-income housing with resident services is a fundamental portion of the triple bottom line performance metrics that will determine the project's success.

Of these 45 residences, 36 will be offered to residents with incomes up to 50% of the Area Median Income (AMI) with the remaining 9 residences being limited to 60% of AMI. This includes 24 two-bedroom residences with an average size of 909 square feet. The remaining 21 residences will have three bedrooms each with an average size of 1,096 square feet. Each residence will have a corresponding reserved off-street parking space. These residences will be situated around Junction Place, Meredith Street, 32nd Street, and Bluff Street. With all car parking accessed off of alleys and private drives, the design of the residences is tailored to the pedestrian experience along the street connections encouraging community interaction and alt modes of transportation.

The center of this portion of SPARK_west will be anchored by a community center, which will provide for resident meeting space as well as programming and services. Adjacent to the community center and in the center of the site will be a large landscaped common area as a central amenity for the residents, including children's play equipment and outdoor seating. This 'central green' will provide a walkable pedestrian connectivity from the west side of SPARK_west all the way to the proposed commercial and retail amenities proposed at SPARK.

This affordable housing project is not a receiving site for the inclusionary housing obligations from a market-rate project located elsewhere in Boulder. It is entirely at the voluntary election of the development team to fulfill the vision of the TVAP and provide a complete project that has social returns that are as great as the environmental and economic objectives at SPARK.

24 Market-Rate 'For-Sale' Residences

Bookending the entire SPARK project and SPARK_west on the west end of Bluff Street, are the 24 market rate 'for sale' townhome residences. At the bedrock of any established community are the long-term residents who normally own their homes — the stakeholders. The developers are experienced in designing and selling compact development homes that will conform to national lending standards and broaden the appeal and ability of future buyers to obtain financing. While the project will be designed and marketed to a broad audience, it will be particularly attractive to families or retirees looking to downsize or just live a less auto-dependent lifestyle. The future owners of these residences will join dozens of other families, professionals, and denizens at the Steelyards project next door as the pioneering permanent residents of a new and exciting place, who the development team has affectionately nicknamed as the YIMBYs — that is, Yes-In-My-Back-Yard.

These townhomes will be two and three stories, with a combination of private and shared outdoor spaces, a single garage with extra storage for each, and all with three bedroom and two-and-a-half baths. As currently configured, there will be 13 units at a size of around 1600sf and 11 units at a size of around 1900sf to provide for pricing variety to the market.

The townhomes will provide the front-stoop-style know-your-neighbor kind of living that Boulderites crave and only a walkable, pedestrian focused place can provide. Strong communities are built on connections, whether each morning out the door on the way to work, through a neighborhood childcare cooperative, sharing a ride, or at the community barbecue on the weekends. All of these virtues will help make SPARK a more complete place for residents, office workers, visitors, commuters, and the Boulder community.

Mixed income placemaking is as much about connecting people with a place as just connecting people. SPARK responds to our community's call to create a new neighborhood center in Boulder and SPARK_west seizes the opportunity to preserve family and affordable housing options.

TRANSIT VILLAGE AREA PLAN (TVAP)

TVAP Vision for Housing

"A lively and engaging place with...housing that serves a diversity of ages, incomes, and ethnicities...To meet diverse needs and incomes the plan provides for a variety of housing types from urban townhomes to stacked flats to live/work units. Prices will range from affordable to high-end market rate...improve the balance of jobs and housing."

Response: SPARK_west is comprised of the city's only three properties that are a designated land use of HDR1 (High Density Residential) and zoned RH-6 which is reserved for primarily townhome style units and corner flats. While workforce housing is important (and will play an important role in SPARK), an often-overlooked component of the in-commuting population are employees representing families (approximately 40% of all the in-commuters). SPARK_west's mixed income offering serves to help Boulder Junction address these important community goals with 45 affordable 'for rent' residences and 24 market-rate 'for sale' residences.

TVAP Vision for a Pedestrian-Oriented Place

"Engaging, convenient and safe pedestrian and bike connections to surrounding neighborhoods...active, walkable streets in a fine-grain grid pattern in the hub area...and connections from the larger area to adjacent areas..."

Response: The project components, particularly the interpretations of the Transportation Master Plan (TMP), respond to the vision statement of TVAP to create a safe place for inhabitants with traffic calming interventions such as raised pavement at crossings, color differentiation, a one-way patterned access drive, and strategic road narrowing — many aspects borrowed from the adjacent Steelyards project and all at this time viewed favorably by the City of Boulder Fire Marshall. Boulder Junction is a logical location for families with alt modes of transit connectivity for safe active transit — walking, biking, and public transportation.

TVAP Vision for Sustainability

"Create a place that reflects Boulder's commitment to environmental sustainability and "green" development that is integrated with natural features...innovative energy efficient site planning...overall stormwater management...connections to existing to natural amenities such as Goose Creek."

Response: SPARK_west responds to these important criteria using the US Green Building Council LEED for Neighborhood Development (ND) program as a template and expects to achieve a Platinum status, which would make SPARK the first to achieve such in Colorado and the second in the U.S. Indeed, SPARK's approach to sustainability is not limited to building energy envelope performance, but community-wide accessibility, diversity, and healthy living. Further, the SPARK development team has been working closely with City of Boulder planning staff to explore SPARK as a pilot program for an "Eco-District" — a model for neighborhood-scale sustainability efforts that requires a collaborative private-public-civic partnership.

TVAP Vision for Urban Character

"Buildings will range in height from two to four stories...variety in building styles and sizes is preferred...much of the new parking will be in structures, underground or tucked-under..."

Response: The project responds to all of these aspects — townhomes and flats in buildings ranging from two to just three stories and not a single exposed surface parking space visible from a public street for SPARK_west or SPARK, at all. The result will be an appropriately non-automobile emphasis, promoting walkability and biking, as well as a lower intensity land use gradually increasing in height and intensity towards the train tracks, activity, and heart of SPARK.

Sustainability Report

SPARK West will be included in the anticipated LEED ND certification for the SPARK development. This designation represents the incorporation of green building with smart growth principles such as mixed land uses, walkability and access to transit that all contribute to well designed and well-loved places.

Complete, Deep Green

3085 Bluff and 3155 Bluff look to achieve similar sustainability goals with their respected design and construction. In an effort to help achieve LEED ND Platinum status for the overall SPARK development, these two adjacent parcels will capitalize on a variety of sustainable design techniques and social and economical efforts.

Both projects are designed to be multi-generational, diverse, and complex, and by doing so each becomes interesting, active, and ultimately sustainable. Each will take advantage of their prime location in the heart of the new Boulder Junction neighborhood where future transit options will be offered in the form of a new RTD regional bus terminal, easy access to the existing Goose Creek greenway and bike path, and minutes away from the platform stop for the future RTD Northwest Rail Line. Both transit-connected infill redevelopments will create a pedestrian-friendly environment that welcomes public interaction and permeability, encourages multimodal transportation, and creates active street level uses. Each will also have composting, recycling and zero waste systems built in as operational imperatives. In order to minimize the negative impacts of each development, the buildings and infrastructure will be built with low embodied energy materials and designed to the highest standards of efficiency. Finally, efforts will be made to ensure water falling on both sites percolates and is cleansed on its way back to the water table.

For most developments, the townhome product proposed for both project sites may not be considered viable in such a transit rich and connected environment. A standalone project would likely need more density to work, allowing residents to truly reduce their carbon footprints on an individual basis. However, a townhome product option allows people and families who out-grow their smaller apartments a place they can expand into without having to move out of the neighborhood. The affordable portion offered on 3155 Bluff allows income-restricted housing for families and anyone needing extra space as well to fully participate in this community for years to come.



LEED and Enterprise Green Community Strategy for 3155 Bluff

3155 Bluff plays a vital role in the overall goal of reaching LEED ND Platinum for the SPARK development. Most importantly, mixed-income housing is significantly weighted in the rating system. The rating system looks not only at total quantity of affordable housing but also at the mix of incomes targeted and the mix of for-sale and rental units. In this case, of the 45 permanently affordable rental units provided in the project, 36 units are set at an income limit of 50% AMI and the remaining nine units are set at 60% AMI. An additional factor that LEED ND considers is the contribution to the overall diversity of housing types from a design perspective. Townhomes under 1250 SF area is a distinct category that will help drive up the overall housing diversity of the project. Within that designation, the design team is providing even more diversity with 7 different sizes and designs of units within the development. This will ensure visual interest and allow for the creation of a more dynamic street front.

In keeping with the diversity and accessibility built into the overall neighborhood, the project will also include a community center. This space is intended to be shared free of rent with non-profit partner for community programming for all ages. The community center will also provide a common laundry facility and a computer lab for resident use.

Another area of impact that LEED ND recognizes is the creation of public spaces for residents to use. The central green space will count towards the overall required civic space as well as provide a gathering space for residents to enjoy the community's overall primary tenant, the public realm. The green space will also serve as a primary means of creating a connection to the townhomes at 3085 Bluff to the west as well as the rest of the SPARK development to the east. This type of internal pedestrian corridor is vital to the overall connectivity of the project as well achieving safe walkability throughout the entire neighborhood.

Using Enterprise Green Communities (EGC) in addition to LEED ND for this block will help ensure that the unit specific details such as healthy materials and ventilation design are incorporated. Aside from the measures that will help reach LEED ND certification such as installing Energy Star appliances, reducing the urban heat island effect and using innovative stormwater techniques, the project will include a number of EGC specific measures as well. Those measures consist of ongoing measurement of resource consumption, an integrated pest management plan, and a resident orientation on how to effectively use the high performance features of the buildings.

SPARK

LEED for Neighborhood Development
Project Facts

LEED Platinum 81

Anticipated out of a possible 100 points.

Smart Location & Linkage 21/27

Taking an infill site to its maximum potential will allow users to take advantage of existing amenities and transit.

Neighborhood Pattern & Design 36/44

Designing world-class connectivity with an intense focus on the public realm will differentiate the project and make it a sought-after destination.

Green Infrastructure & Buildings 14/29

Focusing on the entire carbon footprint of the project, from curbs to windows, will help Boulder meet sustainability goals and showcase best practices in green building.

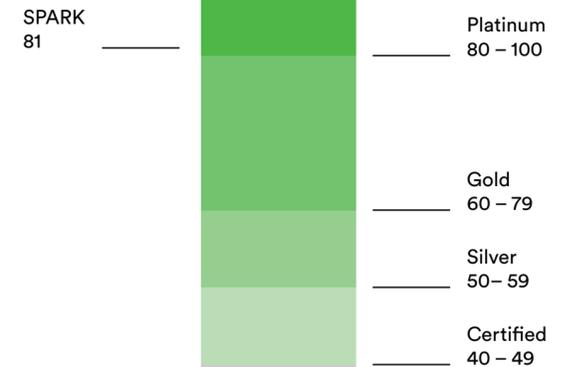
Innovation & Design Process 6/6

This project will seize the opportunity to innovate beyond what is anticipated by LEED ND and obtain credit for doing so.

Regional Priority Credits 4/4

As currently positioned, the project will capture the full set of credits deemed to be most regionally important.

LEED Strategy



Team



Development



Architecture



Sustainability



Civil Engineering

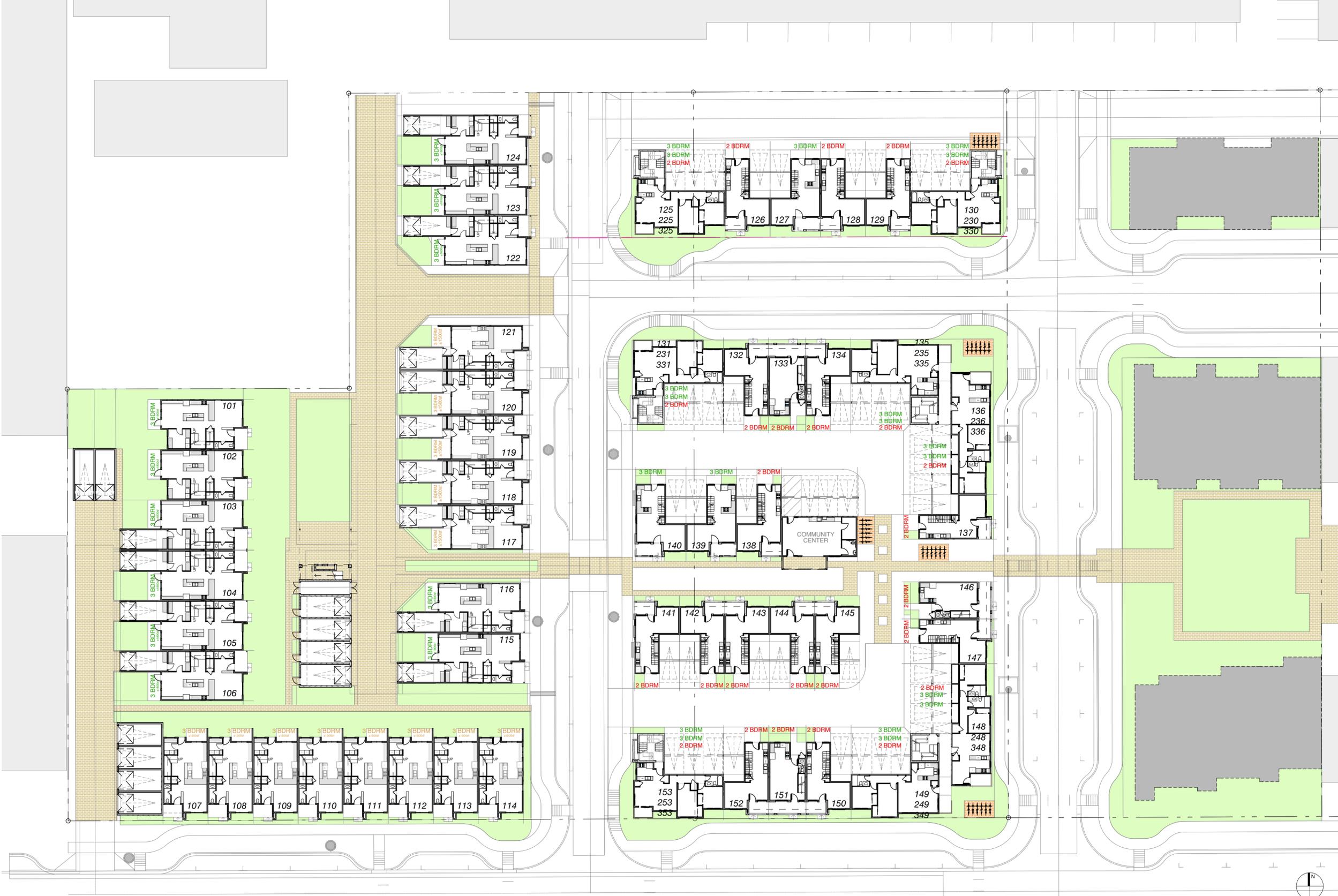


Landscape

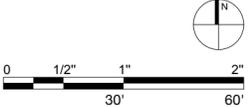


Traffic

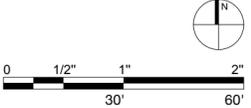
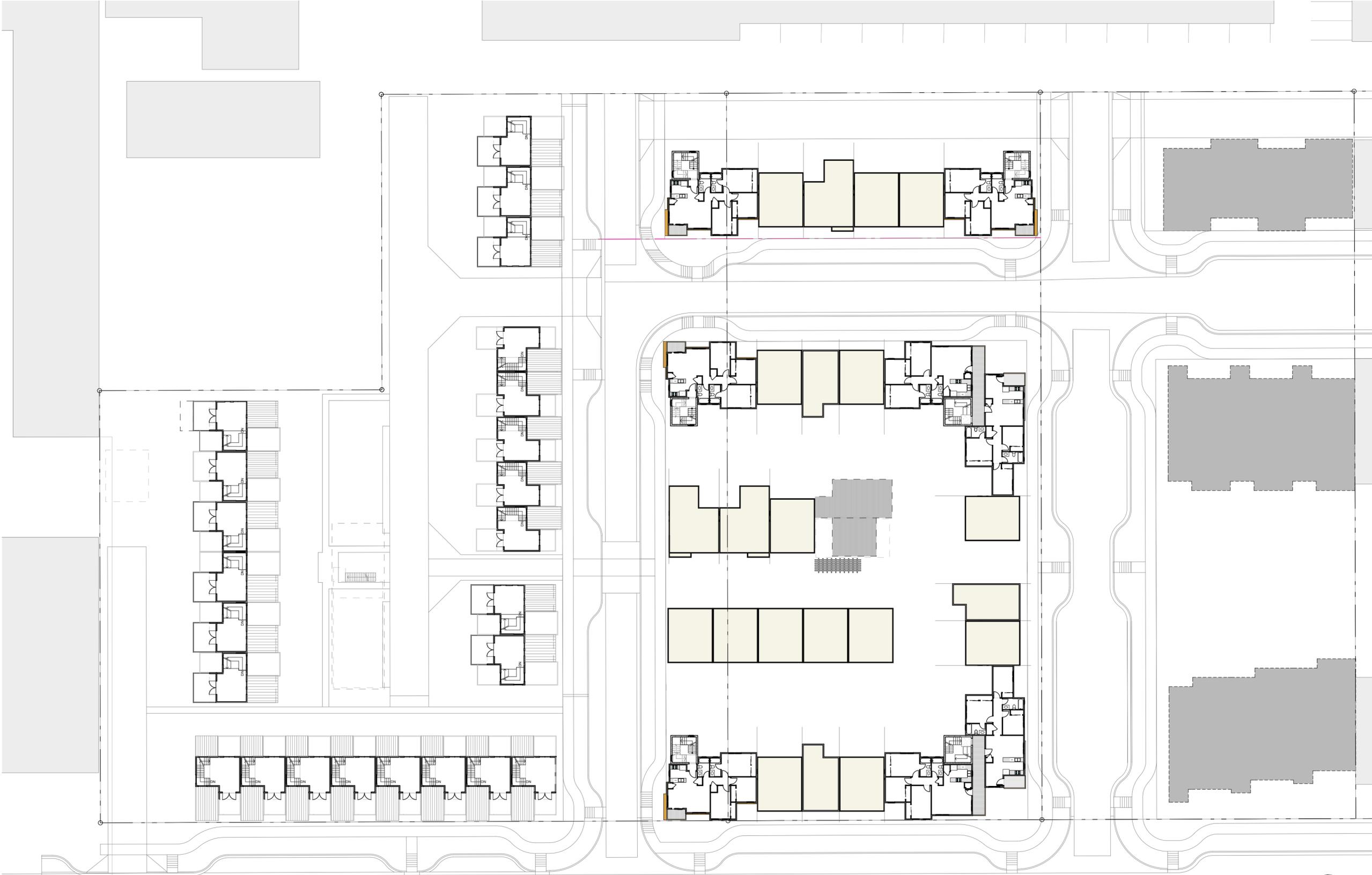
Appendix: Ground Floor Plan



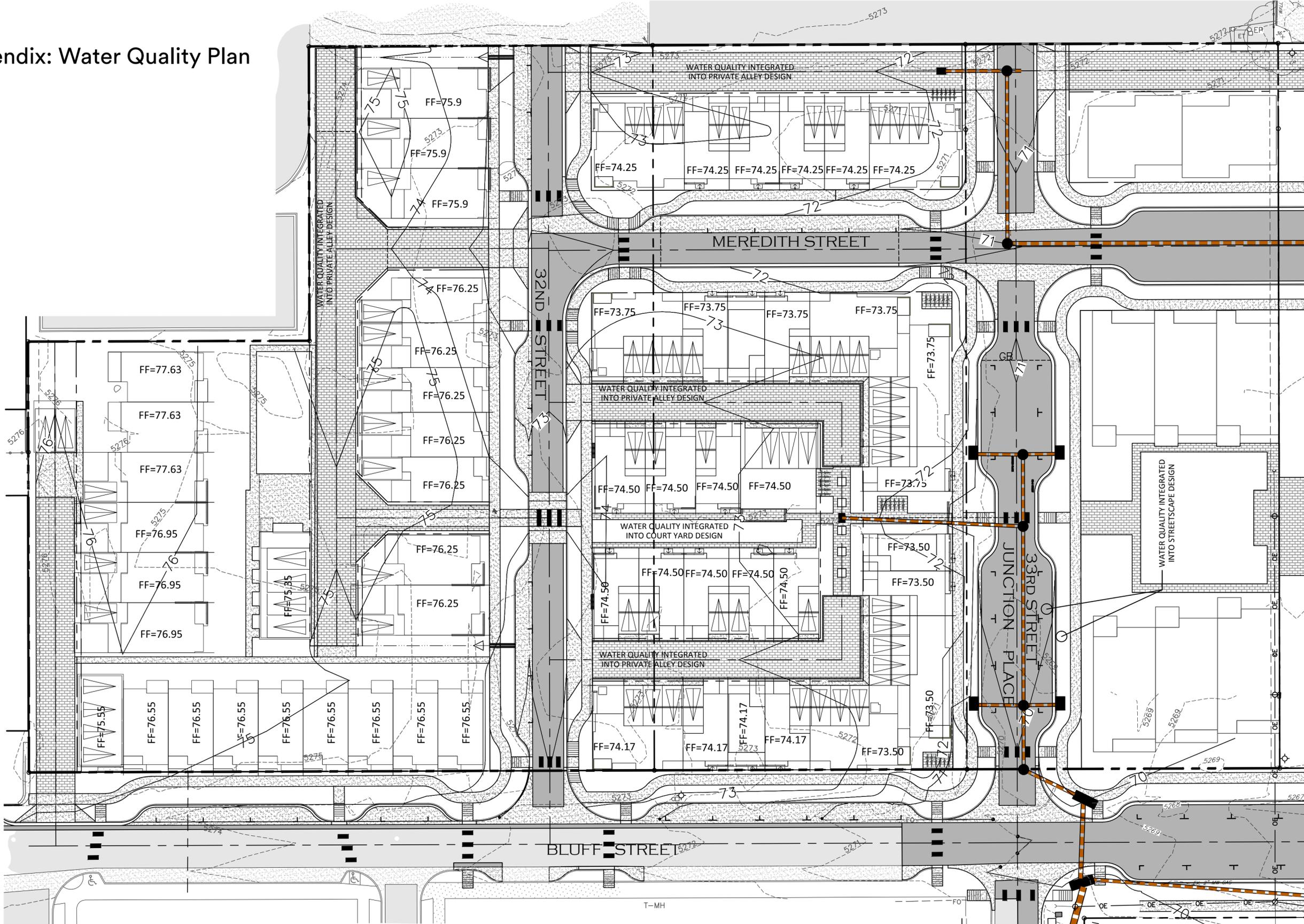
Appendix: Second Floor Plan



Appendix: Third Floor Plan



Appendix: Water Quality Plan



GRADING, DRAINAGE AND STORM WATER QUALITY CONCEPTS

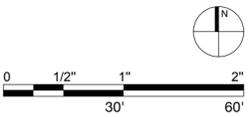
THE EXISTING PROPERTY IS DEVELOPED AND GRADED TO DRAIN THROUGH SPARK TOWARDS TWO DISTINCT DRAINAGE OUTFALL POINTS ON THE SPARK DEVELOPMENT: A 24" CMP CULVERT UNDER THE BNSF RAILROAD TRACKS AND TO AN OPEN DITCH THAT OUTFALLS ONTO THE STEEL YARDS DEVELOPMENT AT THE SOUTH PROPERTY LINE.

THE PROPOSED DEVELOPMENT WILL MAINTAIN THE HISTORIC DRAINAGE OUTFALL LOCATIONS AND CONTINUE TO ACCEPT HISTORIC OFF-SITE FLOWS. STORM WATER QUALITY TREATMENT SUCH AS POROUS PAVERS AND RAIN GARDENS WILL BE INTEGRATED THROUGHOUT THE DEVELOPMENT.

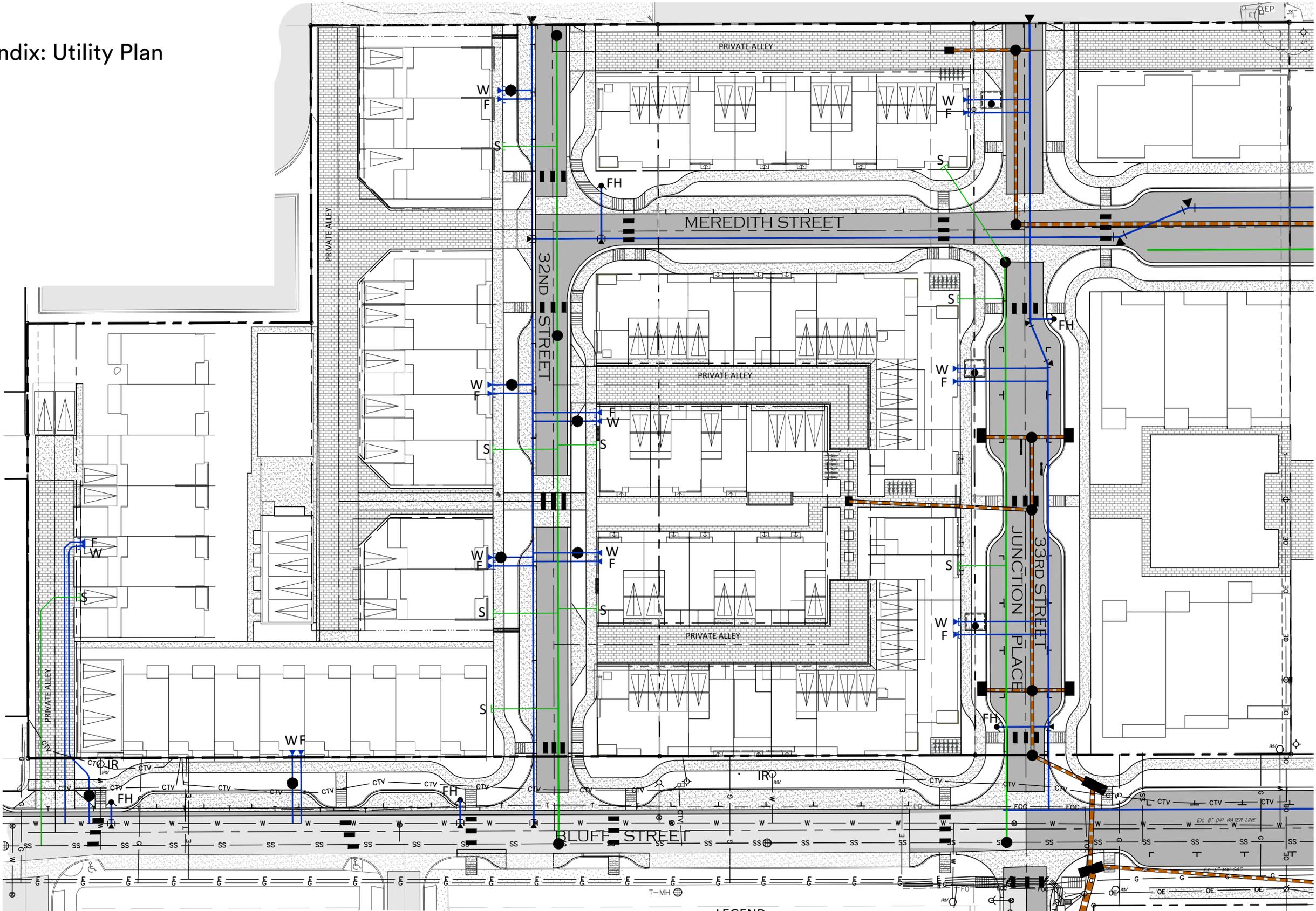
THE MAJORITY OF THE EXISTING SITE IS PRIMARILY IMPERVIOUS (REFER TO IMPROVEMENT SURVEYS). WITH DEVELOPMENT, DIRECTLY CONNECTED IMPERVIOUS AREAS WILL BE REDUCED TO FURTHER ENHANCE STORM WATER QUALITY AND ATTENUATE DEVELOPED STORM FLOWS. A SMALL DETENTION AREA IS ANTICIPATED AT THE SOUTHWEST CORNER OF THE PROJECT AND WILL BE INTEGRATED INTO THE COURT YARD DESIGNS.

LEGEND

- PROPOSED STORM SEWER
- EX. MAJOR CONTOUR
- EX. MINOR CONTOUR
- EX. MAJOR CONTOUR
- EX. MINOR CONTOUR



Appendix: Utility Plan



UTILITY CONCEPTS

THE PROJECT DEVELOPMENT AREA IS SERVED BY EXISTING CITY OF BOULDER WATER AND SANITARY SEWER SYSTEMS THAT ARE LOCATED IN THE VALMONT ROAD, BLUFF STREET AND 33RD STREET RIGHTS-OF-WAY. THE SANITARY SEWER MAIN LOCATED IN BLUFF STREET EXTENDS EAST THROUGH THE SPARK DEVELOPMENT AREA AND UNDER THE BNSF RAILROAD TRACK.

WITH DEVELOPMENT, WATER MAINS WILL BE EXTENDED WITHIN THE PROPOSED PUBLIC RIGHTS-OF-WAY TO PROVIDE WATER SERVICE AND FIRE PROTECTION FOR THE NEW BUILDINGS. THE WATERLINE IN 33RD STREET WILL BE STUBBED-OUT TO THE NORTH FOR FUTURE EXTENSION AS THE PROPERTIES TO THE NORTH REDEVELOP.

SANITARY SEWER MAINS WILL BE EXTENDED WITHIN THE PROPOSED PUBLIC RIGHTS-OF-WAY TO PROVIDE SEWER SERVICE FOR THE NEW BUILDINGS.

LEGEND

- EX. SANITARY SEWER SS SS
- EX. WATER LINE W W
- EX. NATURAL GAS LINE G G
- EX. UNDERGROUND TELECOMM LINE T T
- EX. UNDERGROUND ELECTRICAL LINE E E
- EX. OVERHEAD ELECTRICAL LINE OE OE
- EX. UNDERGROUND CABLE TV CTV
- PROPOSED STORM SEWER (orange dashed line)
- PROPOSED SANITARY SEWER (green line)
- PROPOSED WATER LINE (blue line)



Notes:

- 1) LAND TITLE GUARANTEE COMPANY ORDER NUMBER ABB70383002 DATED JUNE 17, 2013 AT 5:00 P.M. WAS ENTIRELY RELIED UPON FOR RECORDED INFORMATION REGARDING RIGHTS-OF-WAY, EASEMENTS AND ENCUMBRANCES IN THE PREPARATION OF THIS SURVEY FOR PARCELS I AND II.

EXCEPTION NO. 2 - THE PROPERTY IS SUBJECT TO THE RIGHT OF WAY, WHETHER IN FEE OR EASEMENT ONLY, TO CONSTRUCT, OPERATE AND MAINTAIN ITS LINES OF TELEPHONE AND TELEGRAPH AS GRANTED TO MOUNTAIN STATES TELEPHONE AND TELEGRAPH CO., BY INSTRUMENT RECORDED APRIL 15, 1916 IN BOOK 395 AT PAGE 124 (BLANKET EASEMENT OVER NW 1/4 NE 1/4 OF SECTION 29).

EXCEPTION NO. 3 - THIS PROPERTY IS SUBJECT TO THE RIGHT OF WAY, WHETHER IN FEE OR EASEMENT ONLY, TO CONSTRUCT, OPERATE AND MAINTAIN ITS LINES OF TELEPHONE AND TELEGRAPH AS GRANTED TO THE MOUNTAIN STATES TELEPHONE AND TELEGRAPH CO., BY INSTRUMENT RECORDED APRIL 15, 1916 IN BOOK 395 AT PAGE 125 (BLANKET EASEMENT OVER NW 1/4 NE 1/4 OF SECTION 29).

EXCEPTION NO. 4 - THIS PROPERTY IS SUBJECT TO THE TERMS, CONDITIONS, PROVISIONS, AGREEMENTS AND OBLIGATIONS CONTAINED IN THE INSTRUMENT RECORDED MARCH 10, 1971 ON FILM 724 AT RECEPTION NUMBER 969533.

EXCEPTION NO. 5 - THIS PROPERTY IS SUBJECT TO THE EASEMENT AND RIGHT OF WAY FOR A RAILROAD EASEMENT, DRAINAGE FACILITIES AND APPURTENANCES AS RESERVED IN QUITCLAIM DEED RECORDED OCTOBER 12, 1989 IN FILM 1598 AT RECEPTION NUMBER 01007999.

EXCEPTION NO. 6 - THIS PROPERTY IS SUBJECT TO THE EXCLUSIVE EASEMENT FOR A TWENTY-FOUR (24) FOOT WIDE ACCESS / UTILITY EASEMENT AS RESERVED IN DEED RECORDED NOVEMBER 29, 1989 IN FILM 1604, AT RECEPTION NUMBER 01015669.

EXCEPTION NO. 10 - THIS PROPERTY IS SUBJECT TO THE TERMS, CONDITIONS, PROVISIONS, BURDENS, OBLIGATIONS AND EASEMENTS AS SET FORTH AND GRANTED IN EASEMENT DEED BY COURT ORDER RECORDED MARCH 14, 2013 UNDER RECEPTION NUMBER 03297035, IN WHICH A BLANKET EASEMENT WAS GRANTED FOR FACILITIES OPERATED BY SPRINT COMMUNICATIONS REGARDING RIGHTS-OF-WAY, EASEMENTS AND ENCUMBRANCES IN THE PREPARATION OF THIS SURVEY FOR PARCEL IV.

EXCEPTION NO. 9 - THIS PROPERTY IS SUBJECT TO A PUBLIC SERVICE COMPANY EASEMENT AS RESERVED IN INSTRUMENT RECORDED AUGUST 4, 2005 AT RECEPTION NUMBER 2710439. SAID EASEMENT IS SHOWN HEREON.

LAND TITLE GUARANTEE COMPANY ORDER NUMBER ABB70408006 DATED MARCH 28, 2014 AT 5:00 P.M. WAS ENTIRELY RELIED UPON FOR RECORDED INFORMATION REGARDING RIGHTS-OF-WAY, EASEMENTS AND ENCUMBRANCES IN THE PREPARATION OF THIS SURVEY FOR PARCEL V.

EXCEPTION NO. 11 - THE PROPERTY IS SUBJECT TO AN ACCESS EASEMENT FOR THE BENEFIT OF LOT 9, WALKER'S SUBDIVISION OVER THE EAST TWENTY FEET AS RESERVED IN INSTRUMENT RECORDED OCTOBER 31, 1978 AT RECEPTION NUMBER 307153. SAID EASEMENT IS SHOWN HEREON.

LAND TITLE GUARANTEE COMPANY ORDER NUMBER 70409638 DATED APRIL 15, 2013 AT 5:00 P.M. WAS ENTIRELY RELIED UPON FOR RECORDED INFORMATION REGARDING RIGHTS-OF-WAY, EASEMENTS AND ENCUMBRANCES IN THE PREPARATION OF THIS SURVEY FOR PARCEL VI.

EXCEPTION NO. 10 - THE PROPERTY IS SUBJECT TO A FUTURE SIDEWALK EASEMENT AS RESERVED IN INSTRUMENT RECORDED AUGUST 12, 1970 AT RECEPTION NUMBER 951131. NO SIZE OR LOCATION IS SPECIFIED.

EXCEPTION NO. 11 - THE PROPERTY IS SUBJECT TO A UTILITY EASEMENT GRANTED TO PUBLIC SERVICE COMPANY OF COLORADO AS RESERVED IN INSTRUMENT RECORDED MARCH 10, 1977 AT RECEPTION NUMBER 213797. SAID EASEMENT IS SHOWN HEREON.

EXCEPTION NO. 15 - THE PROPERTY IS SUBJECT TO AN EASEMENT FOR INGRESS AND EASEMENT AS RESERVED IN INSTRUMENT RECORDED OCTOBER 28, 1980 AT RECEPTION NUMBER 419704. SAID EASEMENT IS SHOWN HEREON. SAID EASEMENT IS FOR THE MUTUAL BENEFIT OF PARCELS "A" AND "B", WHICH NOW COMPRISE ALL OF LOT 1, KBOL MINOR SUBDIVISION.
- 2) ACCORDING TO COLORADO LAW YOU MUST COMMENCE ANY LEGAL ACTION BASED UPON ANY DEFECT IN THIS SURVEY WITHIN THREE YEARS AFTER YOU FIRST DISCOVER SUCH DEFECT. IN NO EVENT MAY ANY ACTION BASED UPON ANY DEFECT IN THIS SURVEY BE COMMENCED MORE THAN TEN YEARS FROM THE DATE OF THE CERTIFICATION SHOWN HEREON.
- 3) UNDERGROUND WATER, STORM AND SANITARY SEWER LINES HAVE BEEN SHOWN HEREON BASED UPON VISIBLE SURFACE EVIDENCE AND MAPS PROVIDED BY THE CITY OF BOULDER.

UNDERGROUND GAS AND ELECTRIC LINES HAVE BEEN SHOWN HEREON BASED UPON VISIBLE SURFACE EVIDENCE AND MAPS PROVIDED BY XCEL ENERGY.

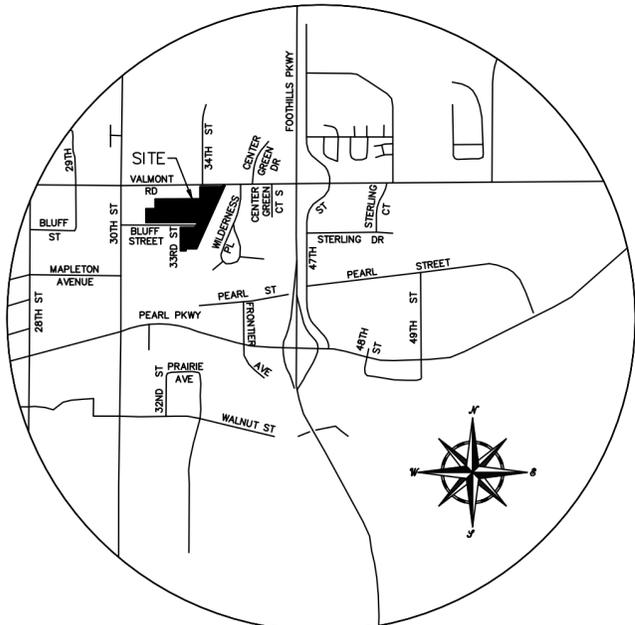
ADDITIONALLY, UNDERGROUND UTILITIES HAVE BEEN SHOWN PER MARKINGS PROVIDED BY DIVERSIFIED UNDERGROUND, INC. DIVERSIFIED UNDERGROUND, INC. HAS PROVIDED MARKINGS ALONG VALMONT ROAD AND BLUFF STREET ONLY.

OTHER UNDERGROUND UTILITIES MAY EXIST, FOR WHICH THERE ARE NO PUBLIC RECORDS READILY AVAILABLE. ALL UNDERGROUND UTILITIES SHOULD BE FIELD LOCATED BY THE APPROPRIATE AGENCY PRIOR TO ANY CONSTRUCTION OR DIGGING ON OR ADJACENT TO THE SUBJECT PROPERTY.

LACKING EXCAVATION, THE EXACT LOCATION OF UNDERGROUND FEATURES CANNOT BE ACCURATELY, COMPLETELY AND RELIABLY DEPICTED. WHERE ADDITIONAL OR MORE DETAILED INFORMATION IS REQUIRED, THE CLIENT IS ADVISED THAT EXCAVATION MAY BE NECESSARY.
- 4) BASIS OF BEARINGS: ASSUMED NORTH 89°49'00" EAST ALONG THE NORTH LINE OF THE SUBJECT PROPERTY AS SHOWN HEREON. BEARINGS IN THE LEGAL DESCRIPTION OF PARCEL II WERE ROTATED 00°07'33" CLOCKWISE.
- 5) THE SUBJECT PROPERTY CONTAINS 477,515 SQUARE FEET (10.962 ACRES). THERE IS A WRITTEN GAP CONTAINING 116 SQUARE FEET WITHIN THE SUBJECT PROPERTY. IF THE GAP AREA WERE TO BE INCLUDED THE TOTAL AREA OF THE PROJECT SITE WOULD THEN CONTAIN 477,631 SQUARE FEET (10.965 ACRES).
- 6) THE SUBJECT PROPERTY IS LOCATED IN ZONE X (UNSHADED), THE AREA DETERMINED TO BE OUTSIDE OF THE 500 YEAR FLOOD PLAIN, ACCORDING TO THE CURRENT FEMA FLOOD INSURANCE RATE MAP, MAP NUMBERS 080130394E AND 080130413J, EFFECTIVE DATE DECEMBER 18, 2012. FLOOD INFORMATION IS SUBJECT TO CHANGE.
- 7) BENCHMARK: STAR FASTENER IN WALK 11 FEET EAST AND 3.6 FEET NORTH OF THE NORTHWEST CORNER OF THE BUILDING AT THE SOUTHWEST CORNER OF 30TH STREET AND VALMONT ROAD. ELEVATION = 5285.68 FEET (CITY OF BOULDER AND USGS 1960 DATUM).
- 8) THIS SURVEY IS BASED UPON PREVIOUS SURVEYS WHICH ARE FILED WITH THE COUNTY OF BOULDER LAND USE DEPARTMENT AS LS-08-0070 AND LS-92-0091. THE PREVIOUS SURVEYS WERE PREPARED BY DREXEL, BARRELL & CO. THESE PREVIOUS SURVEYS WERE FURTHER BASED UPON A PROTRATION MAP OF WALKER'S SUB-DIVISION PREPARED BY DREXEL, BARRELL & CO. IN JUNE 1976, JOB NO. 2487-7.

ALTA/ACSM Land Title Survey

Lot 1, KBOL MINOR SUBDIVISION, and a portion of Lot 4, vacated Meredith Street, vacated Hill Street and vacated Bluff Street, and all of Lots 5, 6, 7, 8, 9, 10, 20 and 21, all in WALKER'S SUB-DIVISION and a tract of land located in the Northeast Quarter of Section 29, Township 1 North, Range 70 West of the 6th P.M., County of Boulder, State of Colorado
 Sheet 1 of 4



Surveyor's Certificate

TO ELEMENT PROPERTIES, SUTHERLAND BLDG. MATERIAL SHOPPING CENTERS, INC., A DELAWARE CORPORATION AND LAND TITLE GUARANTEE COMPANY:

THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2011 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/ACSM LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS 1, 2, 3, 4, 5, 7(g), 8, AND 11(g) OF TABLE A THEREOF. THE FIELD WORK WAS COMPLETED ON AUGUST 6, 2013 FOR PARCELS I, II AND III. THE FIELD WORK FOR PARCEL IV WAS COMPLETED ON JANUARY 22, 2014. THE FIELD WORK FOR PARCEL V WAS COMPLETED ON APRIL 9, 2014. THE FIELD WORK FOR PARCEL VI WAS COMPLETED ON MAY 6, 2014.

THIS SURVEY WAS MADE UNDER MY SUPERVISION, RESPONSIBILITY AND CHECKING IN ACCORDANCE WITH THE REQUIREMENTS SET FORTH IN SECTION 38-51-106 OF THE COLORADO REVISED STATUTES.

STEVEN J. SELLARS DATE
 COLORADO L.S. #27815
 FLAGSTAFF SURVEYING JOB NO. 14-16860

Flagstaff Surveying, Inc.
 637 SOUTH BROADWAY, SUITE C
 TABLE MESA SHOPPING CENTER
 BOULDER, COLORADO 80305
 303-499-9737
 Steven.Sellers@FlagstaffSurveying.com

Property Description

PARCELS I AND II AS PROVIDED BY LAND TITLE GUARANTEE COMPANY

PARCEL I:
 THE EAST 1/3 OF LOT 4, AND THE WEST 49.4 FEET OF LOT 5, WALKER'S SUB-DIVISION, TOGETHER WITH THE NORTH 1/2 OF THAT CERTAIN TRACT OF LAND FORMERLY DESIGNATED AS MEREDITH STREET, ADJACENT TO LOTS 4 AND 5, WALKER'S SUB-DIVISION OF THE NW 1/4 OF THE NE 1/4 OF SECTION 29, TOWNSHIP 1 NORTH, RANGE 70 WEST OF THE 6TH P.M., VACATED BY RESOLUTION OF BOARD OF COUNTY COMMISSIONERS OF BOULDER COUNTY, COLORADO, WHICH RESOLUTION IS RECORDED AUGUST 9, 1950 IN BOOK 871 AT PAGE 390 IN THE OFFICE OF THE CLERK AND RECORDER, BOULDER COUNTY, COLORADO;

AND
 LOT 8, WALKER'S SUB-DIVISION, IN THE CITY OF BOULDER, TOGETHER WITH THAT PORTION OF THE SOUTH 1/2 OF VACATED MEREDITH STREET ATTACHED THERETO ADJOINING SAID LOT 8 ON THE NORTH, AS VACATED BY RESOLUTION OF THE BOARD OF COUNTY COMMISSIONERS OF BOULDER COUNTY, COLORADO, WHICH RESOLUTION IS RECORDED AUGUST 9, 1950 IN BOOK 871 AT PAGE 390 IN THE OFFICE OF THE CLERK AND RECORDER ON OCTOBER 29, 1981 AS RECEPTION NO. 470382.

PARCEL II:
 A PARCEL OF LAND LOCATED IN THE NORTHEAST QUARTER OF SECTION 29, TOWNSHIP 1 NORTH, RANGE 70 WEST OF THE 6TH P.M., BOULDER COUNTY, COLORADO, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCING AT THE NORTHWEST CORNER OF THE NORTHEAST QUARTER OF THE NORTHEAST QUARTER OF SECTION 29; THENCE SOUTH 00°09'55" EAST ALONG THE WESTERLY LINE OF SAID NORTHEAST QUARTER OF THE NORTHEAST QUARTER A DISTANCE OF 40.50 FEET TO THE POINT OF BEGINNING;

THENCE FOLLOWING TWO (2) COURSES ALONG THE SOUTHERLY AND EASTERLY LINE OF A PARCEL OF LAND DESCRIBED IN FILM 1196, RECEPTION NO. 482601, BOULDER COUNTY CLERK AND RECORDER'S OFFICE:

1) NORTH 89°41'28" EAST ALONG A LINE 40.50 FEET SOUTHERLY OF AND PARALLEL WITH THE NORTHERLY LINE OF THE NORTHEAST QUARTER OF SAID SECTION 29 A DISTANCE OF 60.66 FEET;

2) THENCE NORTH 24°33'16" EAST A DISTANCE OF 11.57 FEET; THENCE NORTH 89°41'28" EAST ALONG A LINE 30.00 FEET SOUTHERLY OF AND PARALLEL WITH SAID NORTHERLY LINE OF THE NORTHEAST QUARTER OF THE NORTHEAST QUARTER A DISTANCE OF 34.88 FEET;

THENCE SOUTH 23°58'53" WEST ALONG A LINE 25 FEET WESTERLY OF AND PARALLEL WITH THE CENTERLINE OF THE BURLINGTON NORTHERN RAILROAD TRACK A DISTANCE OF 995.61 FEET;

THENCE SOUTH 89°39'35" WEST ALONG THE NORTHERLY DEED LINE OF VACATED HILL STREET, RECORDED IN BOOK 1071 AT PAGE 320 AND 335, BOULDER COUNTY CLERK AND RECORDER'S OFFICE A DISTANCE OF 152.49 FEET;

THENCE NORTH 23°55'31" EAST A DISTANCE OF 328.76 FEET TO THE NORTHERLY LINE OF LOT 21, WALKER'S SUB-DIVISION, WHENCE THE NORTHWEST CORNER OF SAID LOT 21 BEARS SOUTH 89°40'35" WEST A DISTANCE OF 125.17 FEET;

THENCE NORTH 00°19'25" WEST ALONG A LINE PERPENDICULAR TO BLUFF STREET A DISTANCE OF 40.00 FEET; THENCE NORTH 89°40'35" EAST ALONG THE CENTERLINE OF SAID BLUFF STREET A DISTANCE OF 20.99 FEET;

THENCE NORTH 00°19'25" WEST ALONG A LINE PERPENDICULAR TO SAID BLUFF STREET A DISTANCE OF 40.00 FEET;

THENCE NORTH 00°47'17" WEST ALONG THE WESTERLY LINE OF LOT 7, WALKER'S SUB-DIVISION A DISTANCE OF 299.91 FEET TO THE NORTHWEST CORNER OF SAID LOT 7;

THENCE NORTH 00°19'03" WEST ALONG A LINE PERPENDICULAR TO VACATED MEREDITH STREET A DISTANCE OF 40.00 FEET; THENCE NORTH 89°40'57" EAST ALONG THE CENTERLINE OF SAID VACATED MEREDITH STREET A DISTANCE OF 195.90 FEET;

THENCE NORTH 00°19'03" WEST ALONG A LINE PERPENDICULAR TO VACATED MEREDITH STREET A DISTANCE OF 40.00 FEET TO THE SOUTHERLY LINE OF LOT 5, WALKER'S SUB-DIVISION;

THENCE NORTH 00°09'55" WEST ALONG A LINE 112.00 FEET WESTERLY OF AND PARALLEL WITH THE EASTERLY LINE OF SAID LOT 5 A DISTANCE OF 137.33 FEET; THENCE NORTH 89°41'27" EAST ALONG A LINE 40.50 FEET SOUTHERLY OF AND PARALLEL WITH THE NORTHERLY LINE OF THE NORTHWEST QUARTER OF THE NORTHEAST QUARTER OF SAID SECTION 29, ALSO BEING THE SOUTHERLY LINE OF A PARCEL OF LAND DESCRIBED IN FILM 1196, RECEPTION NO. 482601, BOULDER COUNTY CLERK AND RECORDER'S OFFICE A DISTANCE OF 112.00 FEET TO THE POINT OF BEGINNING.

PARCEL III PER SPECIAL WARRANTY DEED RECORDED APRIL 30, 2004 AT RECEPTION NUMBER 2582300.

PARCEL III:
 LOT 20, EXCEPT THE WEST 55 FEET THEREOF, AND LOT 21, WALKER'S SUB-DIVISION; EXCEPT A PORTION THEREOF DECREED TO THE COLORADO AND SOUTHERN RAILWAY COMPANY, A CORPORATION, BY VIRTUE OF CONDEMNATION PROCEEDINGS RECORDED MARCH 11, 1958 IN BOOK 1069, AT PAGE 378

TOGETHER WITH THE SOUTH 10 FEET OF VACATED BLUFF STREET ABUTTING THE NORTH LOT LINES OF SAID LOT 20, EXCEPT THE WEST 55 FEET THEREOF, AND LOT 21, AS VACATED BY VACATION RESOLUTION RECORDED JUNE 28, 1962 IN BOOK 1235, PAGE 324; AND

TOGETHER WITH THAT PORTION OF THE NORTH ONE-HALF OF VACATED HILL STREET ABUTTING THE SOUTH LOT LINE OF SAID PORTION OF LOT 20, EXCEPT THE WEST 55 FEET THEREOF, AS VACATED BY ORDINANCE NO. 4151 OF THE CITY OF BOULDER RECORDED DECEMBER 9, 1976 ON FILM 947 AS RECEPTION NO. 202633, COUNTY OF BOULDER, STATE OF COLORADO.

PARCEL IV PROVIDED BY COMMONWEALTH LAND TITLE INSURANCE COMPANY

LOT 9, TOGETHER WITH VACATED PORTION OF MEREDITH STREET ABUTTING SAID LOT, SAID VACATION IS RECORDED IN BOOK 871, PAGE 390, WALKER'S SUBDIVISION, COUNTY OF BOULDER, STATE OF COLORADO, THE PLAT OF WHICH IS RECORDED IN PLAT BOOK 2, PAGE 55.

PARCEL V PROVIDED BY LAND TITLE GUARANTEE COMPANY

LOT 10 AND THE SOUTH 1/2 OF VACATED MEREDITH STREET ADJACENT THERETO, WALKER'S SUBDIVISION, COUNTY OF BOULDER, STATE OF COLORADO.

PARCEL VI PROVIDED BY LAND TITLE GUARANTEE COMPANY

LOT 1, "KBOL MINOR SUBDIVISION", AS SET FORTH AND DESCRIBED ON KBOL MINOR SUBDIVISION LOT LINE ELIMINATION MAP RECORDED OCTOBER 5, 2005 AT RECEPTION NO. 2727584, COUNTY OF BOULDER, STATE OF COLORADO, BEING MORE PARTICULARLY DESCRIBED ON SAID LOT LINE ELIMINATION MAP AS FOLLOWS:

BEGINNING AT THE SOUTHEAST CORNER PARCEL "A", KBOL MINOR SUBDIVISION, CITY OF BOULDER, COUNTY OF BOULDER, STATE OF COLORADO; THENCE SOUTH 89°48'20" WEST, ALONG THE SOUTH LINE OF SAID PARCELS "A" AND "B", KBOL MINOR SUBDIVISION, A DISTANCE OF 292.23 FEET TO THE SOUTHWEST CORNER OF SAID PARCEL "B"; THENCE NORTH 00°07'15" WEST, ALONG THE WEST LINE OF SAID PARCEL "B", A DISTANCE OF 201.98 FEET; THENCE NORTH 89°48'45" EAST, A DISTANCE OF 131.51 FEET; THENCE NORTH 00°07'15" WEST, A DISTANCE OF 138.00 FEET TO A POINT ON THE CENTERLINE OF VACATED MEREDITH STREET (VACATION RESOLUTION RECORDED AUGUST 9, 1950 IN BOOK 871 AT PAGE 390); THENCE NORTH 89°48'45" EAST, ALONG CENTERLINE OF VACATED MEREDITH STREET A DISTANCE OF 160.70 FEET TO THE NORTHEAST CORNER OF PARCEL "B"; THENCE SOUTH 00°07'30" EAST, ALONG THE EAST LINE OF SAID PARCELS "A" AND "B", A DISTANCE OF 338.95 FEET, TO THE POINT OF BEGINNING.

Appendix: Site Survey

