

MEMORANDUM
November 5, 2014

TO: Landmarks Board

FROM: Lesli Ellis, Comprehensive Planning Manager
James Hewat, Senior Historic Preservation Planner
Marcy Cameron, Historic Preservation Planner
Angela Smelker, Historic Preservation Intern

SUBJECT: Public hearing and consideration of a Landmark Alteration Certificate to construct a 308 sq. ft. detached garage at 2250 6th St. in the Mapleton Hill Historic District, per section 9-11-18 of the Boulder Revised Code (HIS2014-00309).

STATISTICS:

- | | | |
|----|------------------|--------------------------|
| 1. | Site: | 2250 6th St. |
| 2. | Zoning: | RL-1 (Residential Low-1) |
| 3. | Owner: | Bud and Chris Willis |
| 4. | Applicant: | David Waugh |
| 5. | Site Area: | 9,620 sq. ft. |
| 6. | Proposed Garage: | 308 sq. ft. |

STAFF RECOMMENDATION:

It is staff's opinion that if the applicant complies with the conditions below, the proposed new construction will be generally consistent with the conditions specified in Section 9-11-18, B.R.C. 1981, the *General Design Guidelines*, and the *Mapleton Hill Historic District Design Guidelines*. Therefore, staff recommends that the Landmarks Board adopt the following motion:

I move that the Landmarks Board adopt the staff memorandum dated November 5, 2014, as the findings of the board and approve a Landmark Alteration Certificate for the proposed construction of a garage as shown on plans dated July 11, 2014, finding that they generally meet the standards for issuance of a Landmark Alteration Certificate in Chapter 9-11-18, B.R.C. 1981, subject to the following conditions:

CONDITIONS OF APPROVAL:

1. The applicant shall be responsible for ensuring that the development is constructed in compliance with approved plans dated July 11, 2014 on file in the City of Boulder Community Planning and Sustainability Department, except as modified by these conditions of approval.
2. Prior to submitting a building permit application and final issuance of the Landmark Alteration Certificate, the applicant shall submit the following, which shall be subject to the final review and approval of the Landmarks design review committee: final details regarding roofing, windows and pedestrian and garage door details. These design details shall be reviewed and approved by the Landmarks design review committee prior to the issuance of a building permit. The applicant shall demonstrate that the design details are in compliance with the intent of this approval and the *General Design Guidelines* and the *Mapleton Hill Historic District Design Guidelines*.

SUMMARY

- On August 27th, 2014, the Landmarks design review committee (Ldrc) reviewed and approved a Landmark Alteration Certificate for an addition and deck remodeling at the rear of the house, pending final review of details by staff.
- The proposal for a new, free-standing garage was referred to the full Landmarks Board for review by the Ldrc.
- Staff recommends that, provided the state conditions are met, the Landmarks Board approve the request to construct a new garage in that the proposal generally meets the standards of Section 9-11-18 of the Boulder Revised Code 1981 for issuance of a Landmark Alteration Certificate.



Figure 1. Location Map, 2250 6th St.

PROPERTY HISTORY:

The property at 2250 6th St. is part of the Mapleton Addition to the city, which was platted in 1888. The one-and-a-half story, Colonial Revival residence on the property was constructed in 1922 and features a steeply-pitched side gable roof, long shed dormer, multi-over-single light windows and a symmetrical façade. The entry features sidelights with a fanlight above. When the property was surveyed in 1994, it was identified as a “well-preserved example of the Colonial Revival style” and is considered to be contributing to the character of the Mapleton Hill Historic District. *See Attachment A: Historic Building Inventory Form.*



Figure 2. Photo of Harry B. McClure, c. 1900.

The house was constructed for Harry B. and Ella McClure. Mr. McClure was the manager of the Federal Gas Company, later known as the Public Service Co., and also served as a Boulder County treasurer. Harry was a son of the well-known Boulder pioneer, George McClure, who brought his family to Boulder in the 1870s when Harry was a young child. Before moving to 2250 6th St., Harry McClure built the house at 637 Pine St., which was individually landmarked in 1989 as the McClure House. Harry, Ella, and their son, George, lived at 2250 6th St. from 1922 to 1942.

From 1942 to 1977, a retired couple, Walter and Faith Luhnnow, resided in the house and during this time, Walter became well-known in Boulder based on his outspoken weekly columns featured in the Boulder Daily Camera concerning local and national politics and economics.



Figure 3. Tax Assessor Photograph, 2250 6th St., c.1949.

PROPERTY DESCRIPTION:

The 9,620 sq. ft. lot is located on the southeast corner of 6th St. and Highland Ave., and the house fronts 6th St. The property abuts 620 Highland Ave. to the east and 2230 6th St. to the south with the lot sloping from west to east. A garage was originally located beneath the house and accessed from 6th St. but has since been enclosed as living space.



Figure 4. Close up of façade, 2250 6th St., 2014

PROPOSED NEW GARAGE:

The applicant proposes to construct a one story, 308 sq. ft. garage.

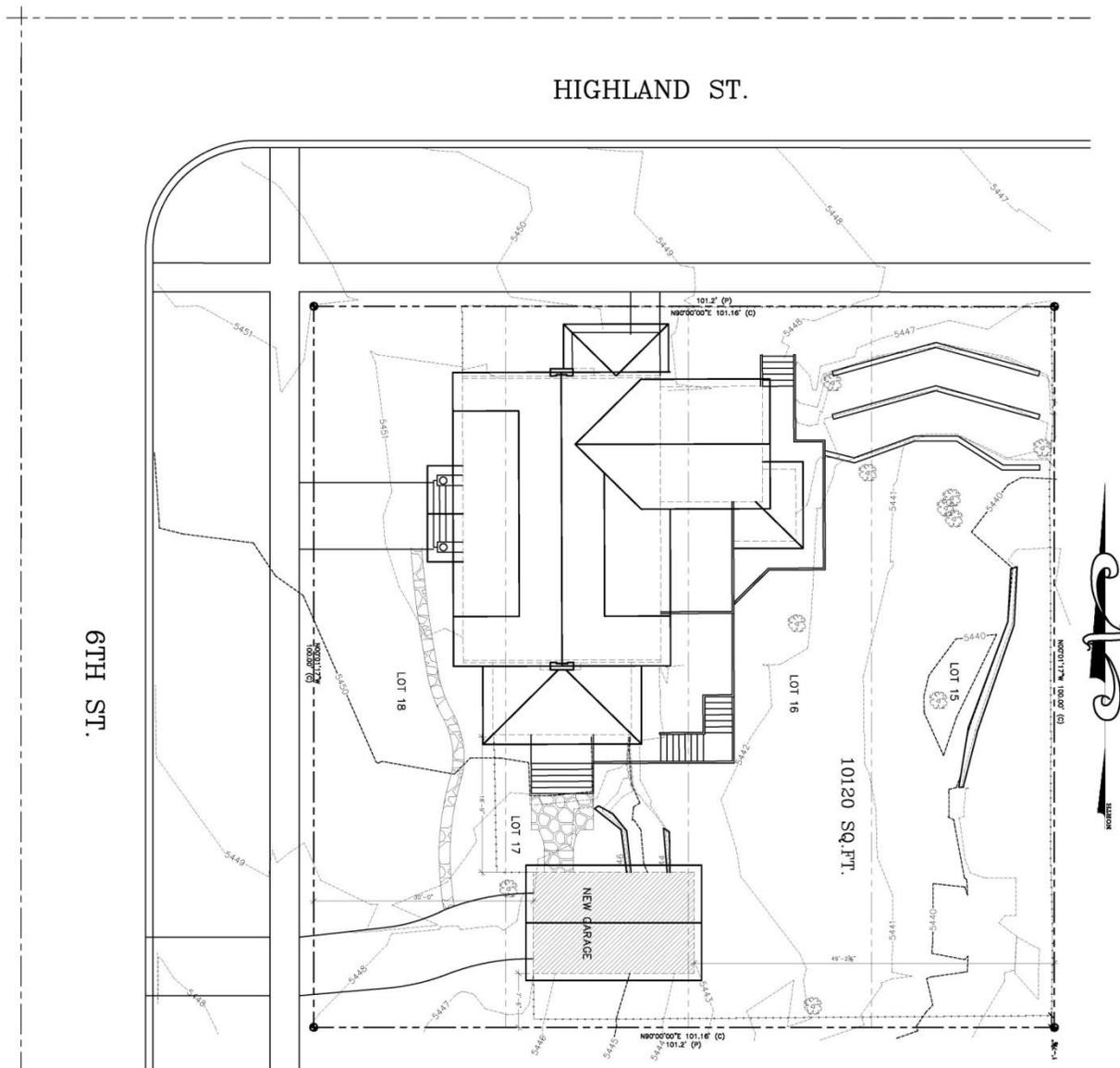


Figure 5. Proposed site plan, 2250 6th St.

In plan, the proposed garage is shown to be located south of the house, and accessed from 6th St., utilizing an existing curb cut, as the property is not bounded by an alley. The façade of the proposed garage is shown to be set back 30 ft. from the west property line, behind the edge of the façade of the house. The proposed garage has a 14 ft. by 22 ft. footprint, and the north wall of the proposed garage is shown to be located 18'9 ft. from the south wall of the house and 7'6" from the south property line.



Figure 6. Driveway and fenced in yard at south of property, 2250 6th St., 2014.

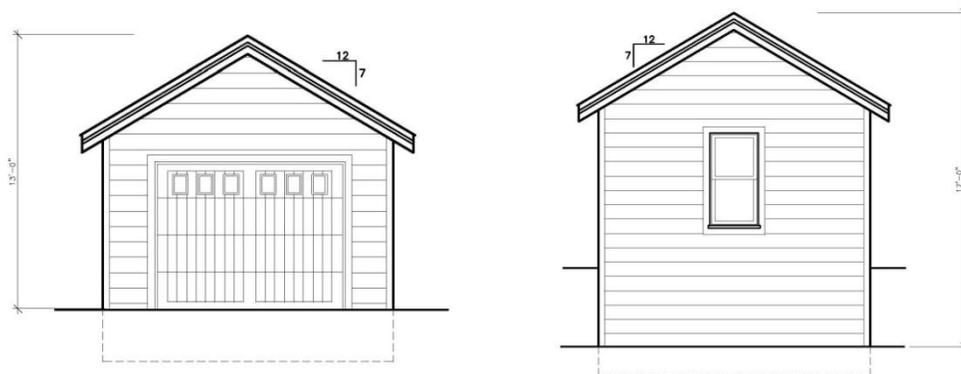


Figure 7. Proposed west elevation (left, facing 6th St.) and east elevation (right, interior lot)

The simple, gable roof garage is shown to measure 13' in height from the west (street-facing) elevation and 17' in height on the east elevation, as the grade drops down. Elevations show the building to have a traditional 7:12 pitch roof, complimentary to the steeper slope of the roof of the house. The west elevation is shown to feature a wooden garage door with 6 small, rectangular windows while the entire building is shown to be sheathed in wooden lap siding.



Figure 8. Proposed north (left, facing house) and south (right) elevations.

Drawings show the north elevation of the garage is shown to have a pedestrian door located at the west end, and a double-hung window located at the east end while the south elevation does not feature any fenestration.



Figure 9. Proposed garage (right) in context with the primary house (left), west facades.

CRITERIA FOR THE BOARD'S DECISION

Subsection 9-11-18(b), B.R.C. 1981, sets forth the standards the Landmarks Board must apply when reviewing a request for a Landmark Alteration Certificate.

(b) Neither the Landmarks Board nor the City Council shall approve a Landmark Alteration Certificate unless it meets the following conditions:

(1) The proposed work preserves, enhances, or restores and does not damage

- or destroy the exterior architectural features of the landmark or the subject property within an historic district;
- (2) The proposed work does not adversely affect the special character or special historic, architectural, or aesthetic interest or value of the landmark and its site or the district;
 - (3) The architectural style, arrangement, texture, color, arrangement of color, and materials used on existing and proposed constructions are compatible with the character of the existing landmark and its site or the historic district;
 - (4) With respect to a proposal to demolish a building in an historic district, the proposed new construction to replace the building meets the requirements of paragraphs (b)(2) and (3) above.
- (c) In determining whether to approve a landmark alteration certificate, the Landmarks Board shall consider the economic feasibility of alternatives, incorporation of energy-efficient design, and enhanced access for the disabled.

ANALYSIS

1. Does the proposed application preserve, enhance, or restore, and not damage or destroy the exterior architectural features of the landmark or the subject property within a historic district?

Staff finds that, provided the listed conditions are met, the construction of the proposed garage will be generally compatible and consistent with the *General Design Guidelines* and the *Mapleton Hill Historic District Guidelines* (see Design Guidelines Analysis section).

2. Does the proposed application adversely affect the special character or special historical, architectural, or aesthetic interest or value of the district?

Staff finds that the proposed application will not adversely affect the special character or special historic, architectural, or aesthetic interest or value of the district because the proposed new garage will be generally compatible with the *General Design Guidelines* and the *Mapleton Hill Historic District Guidelines* in terms of mass, scale, height, design and color (see Design Guidelines Analysis section).

3. Is the architectural style, arrangement, texture, color, arrangement of color, and materials used on existing and proposed structures compatible with the character of the historic district?

Staff considers the architectural style, arrangement, texture, color, arrangement of color, and materials of the proposed garage to be compatible with the contributing house on the property and it will be generally compatible with the character of the historic district in terms of mass, scale, height, setback, and design (see Design Guidelines

Analysis section).

4. Does the proposal to demolish the building within the Mapleton Hill Historic District and the proposed new construction to replace the proposed demolished building meet the requirements of paragraphs 9-11-18(b)(2), 9-11-18(b)(3) and 9-11-18(b)(4) of this section?

Not applicable.

DESIGN GUIDELINES

The Historic Preservation Ordinance sets forth the standards the Landmarks Board must apply when reviewing a request for a Landmark Alteration Certificate and the board has adopted the *General Design Guidelines* to help interpret the ordinance. The following is an analysis of the submitted proposal with respect to relevant guidelines. It is important to emphasize that design guidelines are intended to be used as an aid to appropriate design, and not as a checklist of items for compliance.

The following is an analysis of the proposal’s compliance with the applicable design guidelines:

GENERAL DESIGN GUIDELINES FOR GARAGES & OTHER ACCESSORY BUILDINGS

2.0	Site Design		
	<i>Site design includes a variety of character-defining elements of our historic districts and buildings. Individual structures are located within a framework of streets and public spaces that set the context for the neighborhood. How structures occupy their site, in terms of alignment, orientation, and spacing, creates much of the context of the neighborhood. In combination with public and private walks, fences, tree lawns, landscaping, and retaining walls, the site design features help to define the individual sites and the relationship between public and private space in a neighborhood.</i>		
2.1	Building Alignment, Orientation, and Spacing		
	The pattern of setbacks is an important element in defining neighborhood character. A front yard setback serves as a transitional space between the public sidewalk and private building entry. When repeated along the street, these yards enhance the character of the area. The relatively uniform alignment of building fronts, as well as similar spacing between primary buildings, contributes to a sense of visual continuity.		
	GUIDELINES:	ANALYSIS:	CONFORMS
.1	<i>Locate structures within the range of alignments seen traditionally in the aream matintaining traditional setbacks at the front, side, and rear of the property.</i>	Proposed garage is located approximately 30’ from the west property line, behind the front plane of the house and consistent	Yes

		with traditional setbacks in the area. While the original garage beneath the house was accessed by a driveway from the existing curb cut, there is historic precedent for the location of garages facing the street on north-south running streets on Mapleton Hill.	
.6	<i>In neighborhoods with alleys, garages should be located at the rear of the lot and accessed from the alley.</i>	Property is not bounded by an alley; there is historic precedent for the location of garages facing the street on north-south running streets on Mapleton Hill.	Yes
.7	<i>Preserve a backyard area between the house and the accessory buildings, maintaining the general proportion of built mass to open space found within the area.</i>	As proposed, the backyard area will be maintained. Construction of the proposed garage will not significantly affect the amount of built mass on the property.	Yes

7.0	Garages & Other Accessory Structures		
	<p><i>Accessory structures include barns, sheds, garages and outbuildings. Originally accessory structures were used for storage of equipment, animals, or carriages. Generally, these structures have been adapted for the storage of cars. In most cases, accessory building were located to the rear of the lot and accessed by alleys. They were subordinate in size and detailing to the primary house. Over time they have emerged as important elements of many lots and alleys in the district. Efforts should be made to protect the eclectic character of alleys.</i></p> <p><i>Both additions to existing accessory buildings and new accessory building will be evaluated in terms of how they affect the historic character of the individual site and the district as a whole. In the past, larger accessory structures have been allowed than may be appropriate today.</i></p>		
7.2	New Accessory Buildings		
	<p><i>New accessory buildings should follow the character and pattern of historic accessory buildings. While they should take design cues from the primary buildings, they must be subordinate in size, massing, and detailing. Alley buildings should maintain a scale that is pleasant to walk along and comfortable for pedestrians.</i></p>		
.1	<i>It is inappropriate to introduce a new garage or accessory building if doing so will detract from the overall historic character of the principal building, and the site, or if it will require removal of a</i>	Proposed garage to be located behind the front plane of the house, utilizing an existing curb cut. In terms of design, the proposed garage is simple and generally	Yes

	<i>significant historic building element or site feature, such as a mature tree.</i>	compatible in terms of detailing and proportion to historic buildings in the district. Its construction will not require the removal of significant site features.	
.2	<i>New garages and accessory buildings should generally be located at the rear of the lot, respecting the traditional relationship of such buildings to the primary structure and the site.</i>	The property is not bounded by an alley; when the house was constructed in 1922, the garage (incorporated into the house) was accessed from 6 th St. There is historic precedent for the location of garages facing the street on north-south running streets on Mapleton Hill.	Yes
.3	<i>Maintain adequate spacing between accessory buildings so alleys do not evolve into tunnel-like passageways.</i>	N/A	
.4	<i>Preserve a backyard area between the house and the accessory buildings, maintaining the general proportion of built mass to open space found within the area.</i>	There is historic precedent for the location of garages facing the street on north-south running streets on Mapleton Hill. As proposed, the backyard area will be maintained;	Yes
Mass and Scale			
.5	<i>New accessory structures should take design cues from the primary building on the property, but be subordinate to it in terms of size and massing.</i>	Proposed design is subordinate in size to the one-and-a-half story house on the property, and is simply detailed, taking cues from traditional accessory buildings in the historic district.	Yes
.6	<i>New garages for single-family residences should generally be one story tall and shelter no more than two cars. In some cases, a two-car garage may be inappropriate.</i>	Proposed one-car garage is one-story tall.	Yes
.7	<i>Roof form and pitch should be complementary to the primary structure.</i>	Roof form is complementary to the main house; it is less steep, but traditionally sloped to minimize the height of the proposed garage.	Yes
Materials and Detailing			

.8	<i>Accessory structures should be simpler in design and detail than the primary building.</i>	As shown, garage is simpler than main house in design, material, and detailing.	Yes
.9	<i>Materials for new garages and accessory structures should be compatible with those found on the primary structure and in the district. Vinyl siding and prefabricated structures are inappropriate.</i>	Proposed materials (wood siding, windows, and doors) are compatible with character of historic district. Submit details to staff for final review.	Yes
.10	<i>Windows, like all elements of accessory structures, should be simpler in detailing and smaller in scale than similar elements on primary structures.</i>	Proposed design of double-hung window on north elevation appears to be compatible in terms of type, size and detailing with similar elements on the primary building.	Yes
.11	<i>If consistent with the architectural style and appropriately sized and located, dormers may be an appropriate way to increase storage space in garages.</i>	Dormers are not proposed.	N/A
.12	<i>Garage doors should be consistent with the historic scale and materials of traditional accessory structures. Wood is the most appropriate material and two smaller doors may be more appropriate than one large door.</i>	Garage doors appear to be consistent in terms of scale and materials, submit details to staff for final review.	Maybe
.13	<i>It is inappropriate to introduce features or details to a garage or an accessory building in an attempt to create a false historical appearance.</i>	Building is simple and of its time.	Yes
.14	<i>Carports are inappropriate in districts where their form has no historic precedent.</i>	Carport not proposed.	N/A

Mapleton Hill Historic District Guidelines

The following section is an analysis of the proposal relative to Section VI of the *Mapleton Hill Historic District Design Guidelines*. Only those guidelines that further the analysis of the proposed project are included and those that reflect what has been evaluated in the previous section are not repeated.

B SITE			
	<i>Traditional settlement patterns generally placed houses in the center of a site, with garages, carriage houses, etc. and parking at the rear...</i>		
	Guideline	Analysis	Conforms?
.1	<i>Accessory buildings such as sheds and garages, and driveways should be located at the rear of the lot as is traditional. Adding them between existing buildings interrupts the rhythm and spacing.</i>	Garage proposed south of the primary house, fronting onto 6 th St. and utilizing an existing curb cut. There is historic precedent for the location of garages facing the street on north-south running streets on Mapleton Hill.	Yes
2.	<i>Accessory buildings should generally be small in scale and mass and simply detailed. They are clearly secondary in importance to the primary house.</i>	Garage shown to be small in scale and mass and simply detailed, with wood clapboard siding that references traditional accessory buildings in the historic district. Garage will be secondary to primary house.	Yes

P GARAGES, CARPORTS AND ACCESSORY STRUCTURES			
	<i>A variety of accessory buildings has been adapted for use as garages in the Mapleton Hill Historic District. Whether carriage houses or sheds, these structures have certain similarities. They are plain and utilitarian and are located at the rear of the property on the alley. Materials and building elements are varied.</i>		
	Guideline	Analysis	Conforms?
3.	<i>If a new structure is to be constructed, design ideas might be found in existing historic accessory building located nearby</i>	Proposed design reflects traditional design of accessory buildings found in the district in terms of form, materiality and detailing.	Yes

4.	<i>The new building should be secondary in nature to the main house and smaller in scale.</i>	Proposed design is secondary to main house through massing, scale and simplicity.	Yes
5.	<i>Accessory buildings should be small in scale and small, and constructed in a manner which is complimentary to the character of the house and alley. They are clearly secondary in importance to the primary structure. Typically, prefabricated sheds are discouraged.</i>	Proposed garage is small in scale, and of wood frame construction, compatible with the character of property and historic district. Building clearly secondary to primary house.	Yes

Staff considers that the design for the new garage is generally appropriate in terms of site planning, mass, scale, material, and detailing. The building has a traditional, gable-end roof form and is simply detailed. It will be clearly secondary to the house in terms of massing, scale and simplicity. The corner lot is not bounded by an alley, and there is historic precedent for the location of garages facing the street on north-south running streets on Mapleton Hill. Staff considers that the modest, simple garage will not detract from the historic character of the contributing property, streetscape along 6th St., or the historic district as a whole and that the proposal meets the standards set out in Section 9-11-18 B.R.C. 1981. The design is consistent with the *General Design Guidelines* and the *Mapleton Hill Historic District Design Guidelines*. Details including roofing material, windows, doors, trim detail should be reviewed by the staff to ensure that the garage will be compatible with the historic character of the Mapleton Hill Historic District.

FINDINGS

Staff recommends that the Landmarks Board approve the application and adopt the following findings:

1. The proposed new garage will not have an adverse effect on the value of the district, as it will be generally compatible in terms of mass, scale, or orientation with other buildings in the district.
2. In terms of mass, scale, and orientation the proposed new garage will be generally consistent with Section 9-11-18, B.R.C. 1981, the *General Design Guidelines*, and the *Mapleton Hill Historic District Guidelines*.

ATTACHMENTS:

- A: Historic Building Inventory Forms
- B: Assessor Card
- C: Photographs
- D: Plans and Elevations
- E: Applicant's Submittal

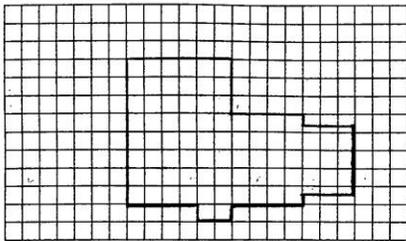
Attachment A: Historic Building Inventory Forms

COLORADO HISTORICAL SOCIETY
Office of Archaeology and Historic Preservation
1300 Broadway, Denver, Colorado 80203

HISTORIC BUILDING INVENTORY RECORD

NOT FOR FIELD USE	
<input type="checkbox"/> Eligible	<input type="checkbox"/> Nominated
<input type="checkbox"/> Det. Not Eligible	<input type="checkbox"/> Certified Rehab.
Date _____	

PROJECT NAME: Boulder Survey of Historic Places, 1994		COUNTY: Boulder	CITY: Boulder	STATE ID NO.: 5BL4632
		TEMPORARY NO.: 1461-25-4-11-004		
CURRENT BUILDING NAME:		OWNER: KOWALSKI WILLIAM J & ELIZABETH L GAEDDERT 2250 6TH ST BOULDER CO 80302		
ADDRESS: 2250 6TH ST BOULDER, CO 80302		TOWNSHIP 1N RANGE 71W SECTION 25 SE 1/4 SW 1/4		
HISTORIC NAME:		U.S.G.S. QUAD NAME: Boulder, Colo. YEAR: 1966 (PR1979) X 7.5' 15'		
DISTRICT NAME: Mapleton Hill		BLOCK: 7 LOT(S): 15-18p ADDITION: Mapleton YR. OF ADDITION: 1888		
FILM ROLL NO.: 94-11 BY: R. Whitacre	NEGATIVE NO.: 11A	LOCATION OF NEGATIVES: Boulder City Plng.	DATE OF CONSTRUCTION: ESTIMATE: ACTUAL: 1922 SOURCE: Boulder County Assessor	
ATTACH PHOTOGRAPH HERE		USE: PRESENT: Residential HISTORIC: Residential		
		CONDITION: X EXCELLENT GOOD FAIR DETERIORATING		
		EXTENT OF ALTERATIONS: X MINOR MODERATE MAJOR DESCRIBE: Frame addition under construction on east, rear; skylight.		
		CONTINUED YES X NO		
STYLE: Colonial Revival		STORIES: 1 1/2	ORIGINAL SITE X MOVED DATE(S) OF MOVE:	
MATERIALS: Brick, Wood, Concrete		SQ. FOOTAGE: 2202	NATIONAL REGISTER ELIGIBILITY	
ARCHITECTURAL DESCRIPTION: Steeply pitched side gable roofed brick dwelling with large shed roofed dormer on facade with lap siding and three 6/1 light windows with shutters. Central entrance with paneled door with fanlight and sidelights. Small porch with gabled roof with arched truss is supported by classical columns and has brick deck. Flanking porch are paired, 8/1 light windows. Full-height corbelled chimneys extend through gable ends. One-story, hipped roof sunroom on south with bands of tall, narrow, 1/1 light windows. Concrete foundation.		INDIVIDUAL: YES X NO		
		CONTRIBUTING TO DISTRICT: X YES NO		
		LOCAL LANDMARK DESIGNATION: No NAME: DATE:		
		ASSOCIATED BUILDINGS? YES X NO TYPE: IF INVENTORIED, LIST ID NOS.:		
ADDITIONAL PAGES: YES X NO		CONTINUED? YES X NO		

PLAN SHAPE: 	ARCHITECT: Unknown	STATE ID NO.: 58L4632		
	SOURCE:	ORIGINAL OWNER: Harry B. and Ella M. McClure		
	BUILDER/CONTRACTOR: Unknown	SOURCE: City Directory, 1923		
SOURCE:		THEME(S): Urban Residential Neighborhoods, 1858-present		
CONSTRUCTION HISTORY (DESCRIPTION, NAMES, DATES, ETC., RELATING TO MAJOR ALTERATIONS TO ORIGINAL STRUCTURE): <p style="text-align: right;">CONTINUED YES X NO</p>				
HISTORICAL BACKGROUND (DISCUSS IMPORTANT PERSONS AND EVENTS ASSOCIATED WITH THIS STRUCTURE): In 1923, this was the home of Harry B. and Ella M. McClure. Harry McClure was the manager of the Federal Gas Company.				
<p style="text-align: right;">CONTINUED YES X NO</p>				
SIGNIFICANCE (CHECK APPROPRIATE CATEGORIES AND BRIEFLY JUSTIFY BELOW): <table style="width: 100%; border: none;"> <tr> <td style="width: 50%; vertical-align: top;"> ARCHITECTURAL SIGNIFICANCE: REPRESENTS THE WORK OF A MASTER POSSESSES HIGH ARTISTIC VALUES <input checked="" type="checkbox"/> REPRESENTS A TYPE, PERIOD, OR METHOD OF CONSTRUCTION </td> <td style="width: 50%; vertical-align: top;"> HISTORICAL SIGNIFICANCE: ASSOCIATED WITH SIGNIFICANT PERSONS ASSOCIATED WITH SIGNIFICANT EVENTS OR PATTERNS <input checked="" type="checkbox"/> CONTRIBUTES TO AN HISTORIC DISTRICT </td> </tr> </table>			ARCHITECTURAL SIGNIFICANCE: REPRESENTS THE WORK OF A MASTER POSSESSES HIGH ARTISTIC VALUES <input checked="" type="checkbox"/> REPRESENTS A TYPE, PERIOD, OR METHOD OF CONSTRUCTION	HISTORICAL SIGNIFICANCE: ASSOCIATED WITH SIGNIFICANT PERSONS ASSOCIATED WITH SIGNIFICANT EVENTS OR PATTERNS <input checked="" type="checkbox"/> CONTRIBUTES TO AN HISTORIC DISTRICT
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TIER EVALUATION: Contributing Building				
STATEMENT OF SIGNIFICANCE: This house is a well-preserved example of the Colonial Revival style as reflected in the side gabled roof, the front dormer, the prominent entrance, multi-over-single-light windows, one-story sunroom, and symmetrical facade.				
<p style="text-align: right;">CONTINUED YES X NO</p>				
REFERENCES (BE SPECIFIC): Boulder County Assessor, real estate information; Boulder Daily Camera biographical files; Boulder Carnegie Library, Boulder County Assessor collection; Boulder City Directories; Boulder Genealogical Society, Census Indexes, 1900 and 1910				
<p style="text-align: right;">CONTINUED YES X NO</p>				
SURVEYED BY: R.L. Simmons/T.H. Simmons	AFFILIATION: Front Range Research Associates, Inc.	DATE: August 1994		





Attachment C: Photographs



View of façade and side yard, 2250 6th St., 2014.



Façade, 2250 6th St., 2014.



South Elevation, 2250 6th St., 2014.



View looking east from driveway, 2250 6th St., 2014.



Additional view looking east from driveway, 2250 6th St., 2014.



View of front yard looking south, 2250 6th St., 2014.



North Elevation and backyard, 2250 6th St., 2014.



East (rear) Elevation, 2250 6th St., 2014.

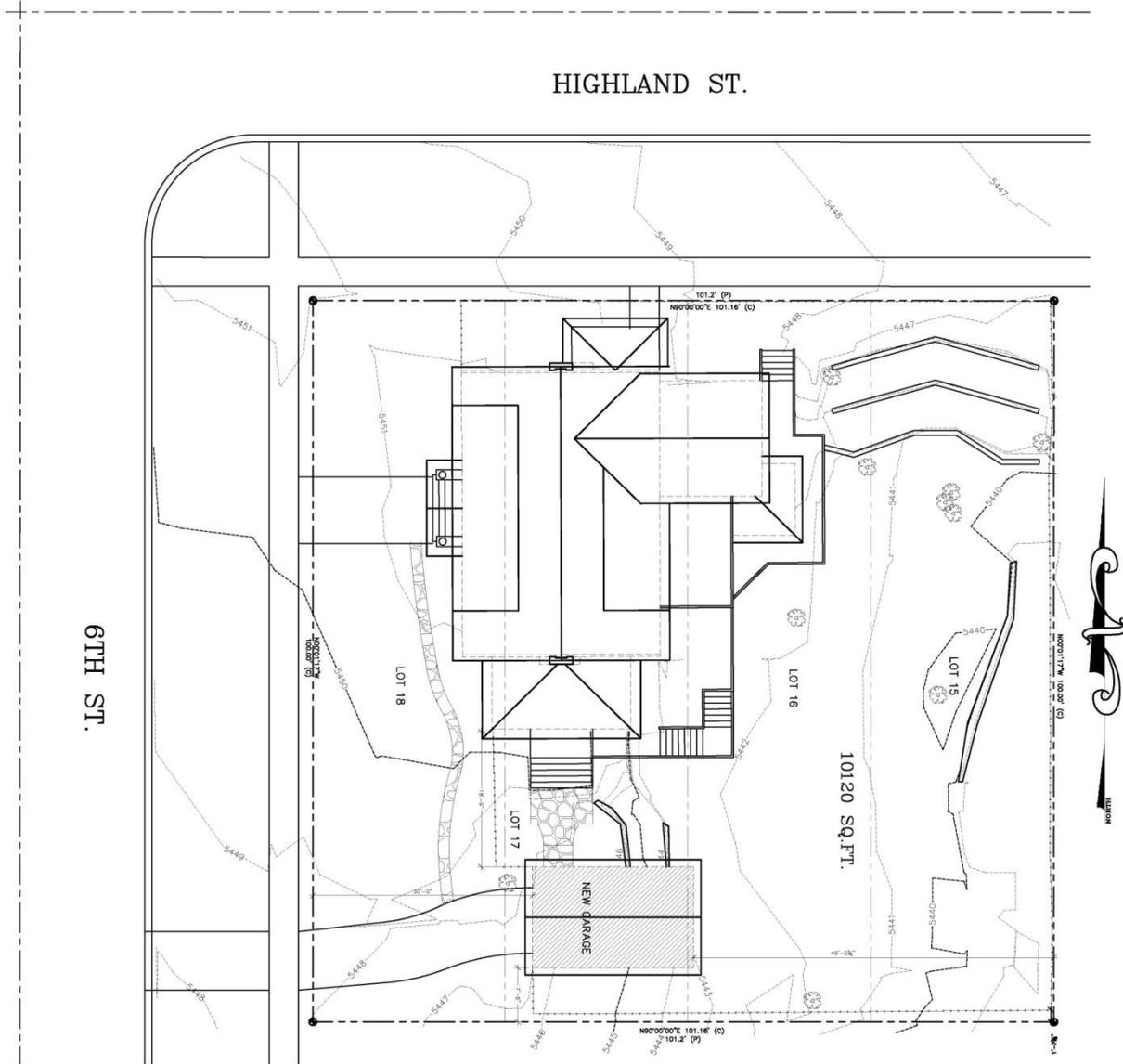


View into backyard looking south, 2250 6th St., 2014.



View from street looking northeast, 2250 6th St., 2014.

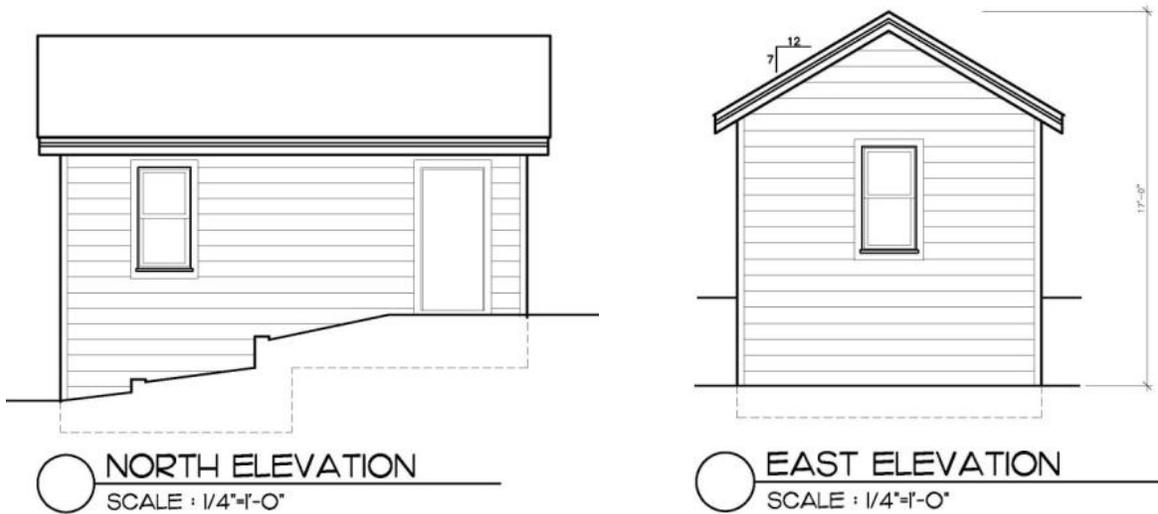
Attachment D: Plans and Elevations



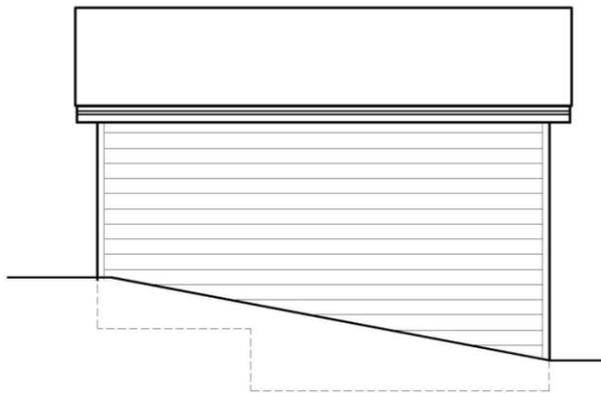
Proposed Site Plan.



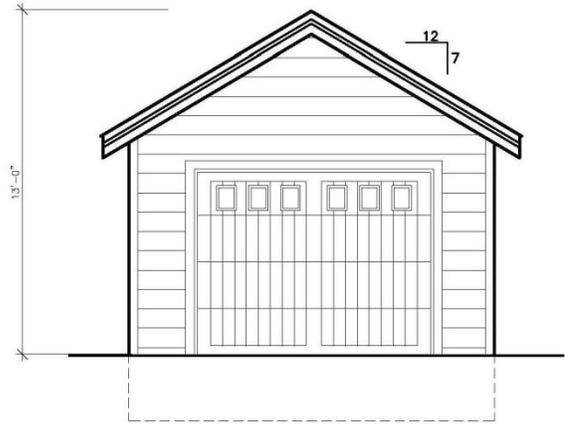
Proposed West Elevation showing house and garage.



Proposed North and East Elevations of garage.



○ SOUTH ELEVATION
SCALE : 1/4"=1'-0"



○ WEST ELEVATION
SCALE : 1/4"=1'-0"

Proposed South and West Elevations for garage.

Attachment E: Applicant's Submittal

WILLIS RESIDENCE
2250 6th STREET
LANDMARKS BOARD

November 5, 2014

PREPARED BY:

WAUGH AND ASSOCIATES
ARCHITECTS/PLANNERS

71 BOWEN
LONGMONT, COLORADO

720-494-7602

GENERAL DESIGN GUIDELINES - ADDITIONS TO HISTORIC

BUILDINGS

4.1 Protection of Historic Structures and Sites

The primary concern of the Landmarks Board in reviewing additions to historic structures is the protection of the existing structure and the character of the site and district.

- The proposed new one car detached garage totally preserves the existing historic fabric by not removing any of the existing facades. The location is based on the historic existing drive location, which used to go to a garage in the basement of the residence. That portion of the basement has been refinished as useable space. The addition of a detached garage will not affect the existing historic structure.

- The mass and scale of the detached garage is significantly smaller compared to the existing residence, and considerable effort has been made to articulate both walls and roof forms to match the existing non historic addition.

4.2 Distinction from Historic Structures

All additions should be discernible from the historic structure. When the original design is duplicated the historic evolution becomes unclear. Instead, additions should be compatible with the historic architecture but clearly recognizable as new construction.

- The proposed detached garage is clearly distinguished from the existing residence by matching the non historic addition. Siding would be a horizontal lap siding as opposed to the brick main level and would match the horizontal siding in the gables and dormers. The change in materials, helps to distinguish the new from the old.

- The use of a shallower roof pitch than the existing roof further helps to distinguish the new from the old.

- Exterior detailing has been kept to a minimum to simplify the new garage. The exterior style of the existing residence is very simple with the use of brick and its small covered front porch. The garage is to be as simplistic as possible, yet it remains distinctly different by matching the non historic addition.

4.3 Compatibility with Historic Buildings

Introducing new construction that contrasts sharply with an existing historic structure or site detracts from the visual continuity that marks our historic districts. While additions should be distinguishable from the historic structure, they must not contrast so sharply as to distract from the original building and/or site. Additions should never overwhelm historic structures or the site, in mass, scale or detailing.

We have tried to minimize the size of the garage through the use of a very compact, simplified design.

The relationship of solids and voids is handled with wall articulation and fenestration of a more modest style which will be both simple in design and consistent with the simple style of the existing addition.

4.4 Compatibility with Historic Site and Setting

Additions should be designed and located so that significant site features, including mature trees, are not lost or obscured. The size of the addition should not overpower the site or dramatically alter its historic character.

This lot is large at 9,619 sq. ft. and will accommodate the added garage without overpowering the site. The new garage needs to be relatively close to 6th Street due to the severe grade drop off in the Southeast corner of the lot. With the detached garage at the side of the lot, and close to the street, landscape improvements can replace the open parking area off of 6th Street, providing more usable open space and less paved driveway.

4.5 Key Building Elements

Roofs, porches, dormers, windows and doors are some of the most important character-defining elements of any building. As such, they require extra attention to assure that they compliment the historic architecture. In addition to the guidelines below, refer also to Section 3.0 Alterations for related suggestions.

The new gabled roof will be the predominant roof seen from 6th Street, as the new garage will not exceed one story in height and is mostly tucked below

the historic residence.

Windows and door are simple rectangular shapes to further the simplicity of the existing home.

MAPLETON HILL DESIGN GUIDELINES - MAJOR EXTERIOR RENOVATION - ADDITIONS AND SECOND STORIES

Massing

While the specific details of the historic architectural styles of Mapleton Hill vary considerably, the most significant and identifiable feature is its massing. Buildings of Italianate styling are square and vertical. Bungalows are low and rectangular, while Queen Anne styling is asymmetrical with many projections and details. Replication of stylistic detailing is not encouraged or necessary, however, the form which defines the building, should be respected.

The new garage will not change the symmetry of the existing residence. The existing residence is extremely wide and tall. While the new garage is a one story, it is stepped back and lowered in height as it is viewed from Highland Ave.

Garages, Carports and Accessory Structures

A variety of accessory buildings have been adapted for use as garages in the Mapleton Hill Historic District. Whether carriage houses or sheds, these structures have certain similarities. They are plain and utilitarian and are located at the rear of the property or alley. Materials and building elements are varied.

No historic accessory buildings are on the site.

GENERAL DESIGN GUIDELINES

Garages and other Accessory Structures

A primary concern of the Landmarks Board in reviewing proposed changes in the historic districts is the protection of existing historic accessory structures and the character of the site and district.

No historic accessory buildings are on the site.

New Accessory Buildings

New accessory buildings should follow the character and pattern of historic accessory buildings, they must be subordinate in size, massing, and detailing.

The proposed new 1 car garage will be similar in design to the new addition and will not impact the character of the original building. As the historic drive access has always been from the 6th Street, this access will be preserved. The mass and scale will be appropriate with the new addition and existing residence. Materials will match the new addition and will not attempt to create a historic look. The new garage will provide the only indoor parking available on the site.