

**CITY OF BOULDER**  
**PLANNING BOARD AGENDA ITEM**  
**MEETING DATE: August 20, 2015**

**AGENDA TITLE:** Public hearing and consideration of Site and Use Review applications for redevelopment of an approximately 1.34-acre site located at 2440 and 2490 Junction Place within Boulder Junction. Referred to as “The Commons,” the proposed commercial development would consist of two, 3-4 story, 55’ tall buildings totalling roughly 100,000 sq. ft. that would include professional office space, restaurant space and “flex” space intended as community gathering space. The proposal also includes 56 underground parking spaces, a proposed “mobility hub” that includes a car share program and B-Cycle Station, a central public plaza area and multiple multi-modal connections through the site. The project is reviewed under two separate case no. LUR2015-00048 (Site Review) and LUR2015-00065 (Use Review, required for proposed restaurant over 1,500 sq. ft. in size).

Applicant: Bill Hollicky/Coburn Architecture  
Property Owner: 2440 Junction Pl., LLC & 2490 Junction Pl., LLC

**REQUESTING DEPARTMENT:**

Community Planning & Sustainability  
David Driskell, Executive Director  
Susan Richstone, Deputy Director  
Charles Ferro, Land Use Review Manager  
Chandler Van Schaack, Planner I

**OBJECTIVE:**

Define the steps for Planning Board consideration of this request:

1. Hear Applicant and Staff presentations
2. Hold Public Hearing
3. Planning Board discussion
4. Planning Board action to approve, approve with conditions, or deny the Site and Use Review applications.

**SUMMARY:**

Proposal: The proposed commercial development would consist of two 3-4 story, 55’ tall buildings totalling roughly 100,000 sq. ft. that would include professional office space, restaurant space and “flex” space intended as community gathering space. The proposal also includes 56 underground parking spaces, a proposed “mobility hub” that includes a car share program and B-Cycle Station, a central public plaza area and multiple multi-modal connections through the site.

Project Name: The Commons  
Location: 2440 & 2490 Junction Pl.  
Size of Tract: 1.34 acres (58,272 sq. ft.)  
Zoning: Mixed Use – 4  
Comprehensive Plan: Mixed Use Business

**KEY ISSUES:**

Staff has identified the following key issues regarding the proposed project:

1. Is the proposed project consistent with the Transit Village Area Plan?
2. Does the proposal meet Site Review Criteria, including Boulder Valley Comprehensive Plan (BVCP) policies?
3. Is the proposed restaurant use consistent with the Use Review Criteria?

## I. INTRODUCTION AND BACKGROUND

A Concept Plan for the proposal was heard by the Planning Board on April 2, 2015. Overall, the board found the proposal to be consistent with the vision and goals of the Transit Village Area Plan (TVAP), and also expressed support for the project's proposed sustainability and building efficiency features. The board made some recommendations for site and building improvements and parking management strategies and also suggested potentially adding residential units to the project based on an analysis of the overall balance of residential and commercial space within the Transit Village as a whole. Additional considerations discussed by the board included increasing the amount of landscaping within the project, enlivening the eastern façade along the multi-use path, and creating more of an open space amenity on the north side of the northern building. The staff memorandum to Planning Board, minutes, meeting audio, and other related background materials are on the city website for Planning Board, available [here](#) (Follow the links: 2015 → 04 APR → 04.02.2015 PB Packet).

**Project Proposal.** In keeping with the Concept Plan, the current proposal consists of two commercial buildings on the remaining two vacant lots in the Steel Yards development to the east of Junction Place. This application is seeking an amendment to Site Review approval #SI-98-5. The northern building is 59,998 sq. ft. in size and consists of a roughly "H" shaped design that steps down the massing from four stories along the rail road (east) side of the site to three stories along the Junction Pl. frontage to the west. The southern building totals 40,230 sq. ft. in size, and incorporates a similar stepped design that places a four-story building element along the railroad tracks which steps down to three stories along Junction Pl. Both buildings incorporate a 10-foot setback along Junction Pl., with the ground level frontages consisting primarily of brick storefronts with metal and wood elements. The north and south elevations of each building consist primarily of metal paneling with high -pressure laminate and wood siding. As will be discussed in further detail below, the eastern facades along the railroad tracks will consist almost entirely of integrated solar panels. Please refer to **Attachment A** for Applicant's Proposed Plans.

The project provides a total of nearly 20,000 sq. ft. of open space (nearly 33% of the site area where 20% is the minimum required per the MU-4 zone standards) through a variety of site design features. The main open space feature is a large public plaza in the center of the site, which provides access to the main building entries for the office tenants and community room/ flex space, and includes a multi-use path connection between Junction Pl. and multi-use path along the railroad to the east. The scale of the plaza is broken down through the incorporation of sculptural artwork, landscape elements, patterned paving, trellised seating areas and architectural lighting, and recessed building corners on either side of the plaza entry help to create more intimate pocket plazas around the coffee shop patio and pedestrian garage entry. **Figure 1** illustrates the proposed site plan, and **Figures 2a – 2c** below show architectural renderings of the proposed buildings.

In terms of land uses, the majority of the floor area is to be programmed as office space; however, the applicant also intends to incorporate a variety of community based elements including a "mobility hub," a gallery that can also function as a community room, a planned coffee shop on the south side of the central plaza and a restaurant with an outdoor patio area facing the Goose Creek path to the south of the site.



**Figure 1: Proposed Site Plan**

**Figure 2a (right):** Rendering of the proposed project looking northeast across the central plaza from Junction Pl. toward the north building.



**Figure 2b (left):** Rendering of the proposed project looking southeast across the central plaza from Junction Pl. toward the south building.



**Figure 2c (right):** Rendering of the proposed project looking north from Goose Creek toward the south building and proposed restaurant seating area.

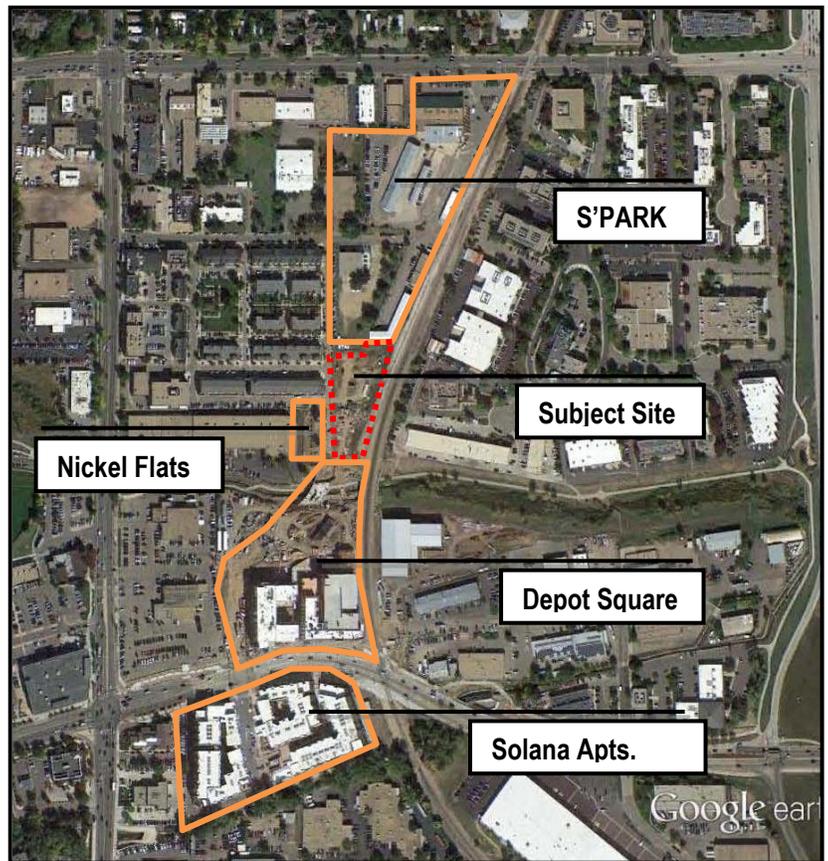


The intent of the proposed project is to include a variety of sustainability features intended to achieve a LEED platinum standard at a minimum. The design and orientation of the buildings allows for optimal daylighting and uninterrupted solar access on the roof and east facade of the buildings and the project integrates solar panels into the architectural design of these elements. The fenestration patterning and materiality are intended to create a building envelope consisting of thermally broken windows (A thermal break is an element of low thermal conductivity placed in an assembly to reduce or prevent the flow of thermal energy between conductive materials), tuned window-to-glass ratios per facade, and architectural designed shades to reduce heat gain in summer and heat loss in winter. Per the applicant's written statement, the project will also seek to reduce its demand loads on an annualized basis. The project design also includes venting to provide air-flow through the building for natural cooling at night, and thermal mass in the floors to help carry that cooling throughout the day. The applicant is also exploring responsive HVAC strategies such as a variable refrigerant system to allow the building to respond to the actual unit needs rather than conditioning on a full-building scale, and a variety of tenant-based ideas are being considered to reduce plug loads.

**Existing Site.** As shown in **Figure 3**, the 1.33-acre project site is located on the east side of the Steel Yards development, abutting the Burlington Northern and Santa Fe Rail Line just south of the intersection of Junction Pl. and Bluff St. The project site lies within Boulder Junction, and is overseen by the vision, goals and guidelines of the Transit Village Area Plan (TVAP).

The site is comprised of of the two remaining undeveloped lots in the Steel Yards Subdivision. Surrounding uses include the new Nickel Flats residential building immediately across Junction Pl. and the mixed residential and live-work Steelyards development further to the west; the former Sutherlands site (currently under review for the S'Park development) immediately to the north, and mixed office and light industrial uses to the east across the railroad tracks. The Goose Creek Greenway runs just south of the site, with the mixed use Depot Square development and residential Solana development abutting the west side of the railroad tracks further to the south.

**Figure 4** includes images of the site surroundings. The site is located in the northern portion of Boulder Junction. The Boulder Junction area is guided by the [Transit Village Area Plan](#) and is anticipated to redevelop as several new urban, mixed-use, transit-oriented neighborhoods. Consistency of the project with TVAP is discussed in Key Issue #1 below. Redevelopment within Boulder Junction began in 2012 with the Solana Apartments, which include 319 units nearing completion. Currently under construction across Pearl Parkway from 3100 Pearl is Depot Square, a mixed use development that includes a below grade bus transit facility, a 150-room Hyatt Hotel, an above grade, wrapped parking structure and 71 permanently affordable, attached residential units. Also nearing completion is the restoration of the historic depot building on the site that will include a public plaza space along with roadway improvements such as the construction of Junction Place, a new north-south roadway along with a new bridge over Goose Creek and implementation of a multi-way boulevard on Pearl Parkway.



**Figure 3: Subject Property in relation to nearby projects under construction**

Also under construction within Boulder Junction is a 17-unit attached apartment building known as Nickel Flats, just north of Goose Creek on Junction Place. To the west of the site is the Steelyards, a mixed use development, with residential and live-work units along with retail and offices. Directly north of the project site is the former Sutherland Lumber Supply Co. site, which includes large vacant warehouse buildings and a branch of Air Gas Co., a supplier of industrial, medical and specialty gas and accessories, safety products and welding supplies. The former Sutherland site is currently under Site and Use Review for a mixed use development known as "S'PARK," which is proposed as a mixed use and mixed-income neighborhood comprised of seven individual areas: Markt: an approximately 52,000 square foot, four story retail and office building featuring a brew pub and micro restaurants; Ciclo: a four story mixed use building with the ground floor housing Community Cycles and with 32 permanently affordable apartments above; Railyards: an approximately 69,000 square foot, four story building with ground floor retail and upper story office; Timber: an approximately 115,000 square foot four-story apartment building with 121 apartments along with 8 townhomes and ground floor office and retail; Meredith House: a four story apartment building of 20,690 square feet; and S'PARK\_west with 45 units of permanently affordable attached residential and 24 market rate townhomes. The proposed project includes a new pocket park, a woonerf shared street, and a public plaza in anticipation of the future rail stop. **Figure 4** below illustrates the existing and proposed context surrounding the project site.



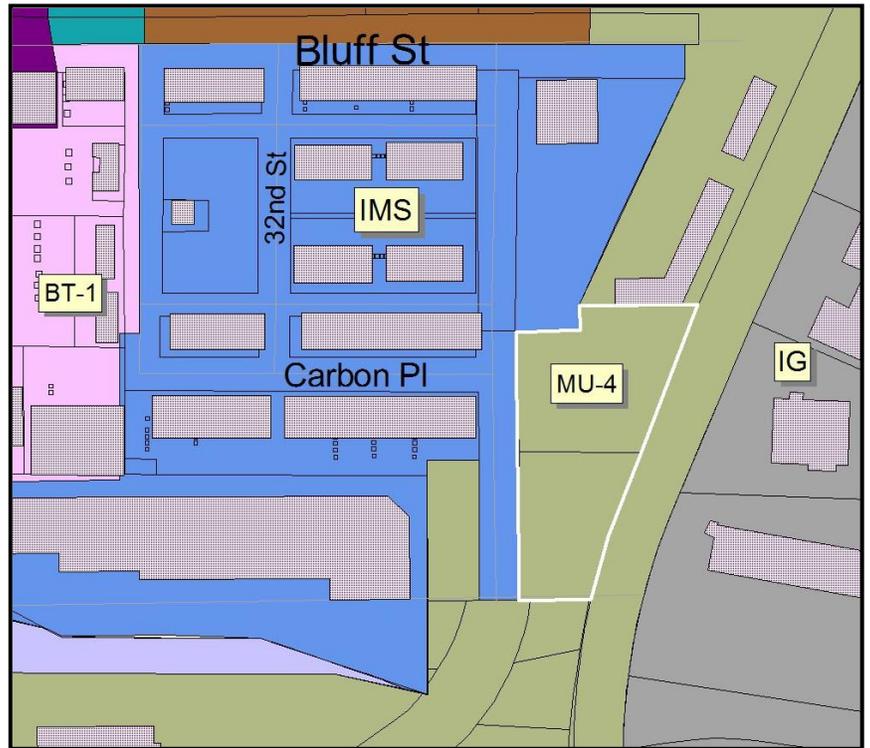
Figure 4:  
Photos of  
Surrounding Context

**Site Zoning.** The project site is zoned MU-4 (Mixed Use – 4) as shown in **Figure 5**. The following is an excerpt from the Land Use Code Section 9-5-2, B.R.C. 1981 for the zoning district definition:

***MU-4: Mixed use – 4**, residential areas generally intended for residential uses with neighborhood-serving retail and office uses; and where complementary uses may be allowed. It is anticipated that development will occur in a pedestrian-oriented pattern, with buildings built up to the street.*

The MU-4 zone is a relatively new district established to help implement the vision of TVAP. The zoning district permits up to a 2.0 FAR by-right. Portions of the zoning district were amended in 2013 to allow additional uses to allow greater variety in the zoning district. Added were Commercial Kitchen and Catering; Small Manufacturing uses less than 15,000 square feet; and Wholesale businesses. Also added were Live-Work units as an allowed use. Reference the staff memo from the second reading of the ordinance to amend the code [here](#). A zero lot line front yard setback is permitted by-right in MU-4 with a maximum by-right height of three stories or 38 feet, which can only be modified through Site Review.

It should also be noted that on March 31, 2015, City Council approved a height ordinance that establishes a two-year period during which modifications to the by-right height for new buildings will only be considered through the Site Review process in specific parts of the city or in particular circumstances. The areas where height modifications may be considered are specific parts of the city where adopted area plans provide guidance for the consideration of proposed modifications, including Boulder Junction, so the subject property is still be able to request a height modification through the Site Review process.



**Figure 5: Zoning Map**

The proposed project includes several modifications to the form and bulk standards for the MU-4 zone district. These modifications would be considered through the Site Review process, and are listed below:

- Minimum front yard setback from a street for 3rd story & above:

Request to allow for a 10-foot setback for the first three stories of both buildings where 20 feet is the minimum required setback.

- Maximum number of stories:

Request to allow for buildings up to four stories in height where three stories is the maximum number of stories permitted. Site Review is required in MU-4 for projects over 3 stories in height.

- Maximum principal building height:

Request to allow the two buildings to reach a height of 55 feet where 38 feet is the maximum permitted height. As mentioned above, the adopted height ordinance includes an exemption provision for properties within Boulder Junction, so the subject property is able to request a height modification through the Site Review process.

- Maximum floor area of any principal building:

Request to allow for two buildings with a total combined floor area of approximately 100,000 square feet (The northern building would be approximately 60,000 square feet and the southern building would be approximately 40,000 square feet) where 15,000 square feet is the maximum floor area for a principal building.

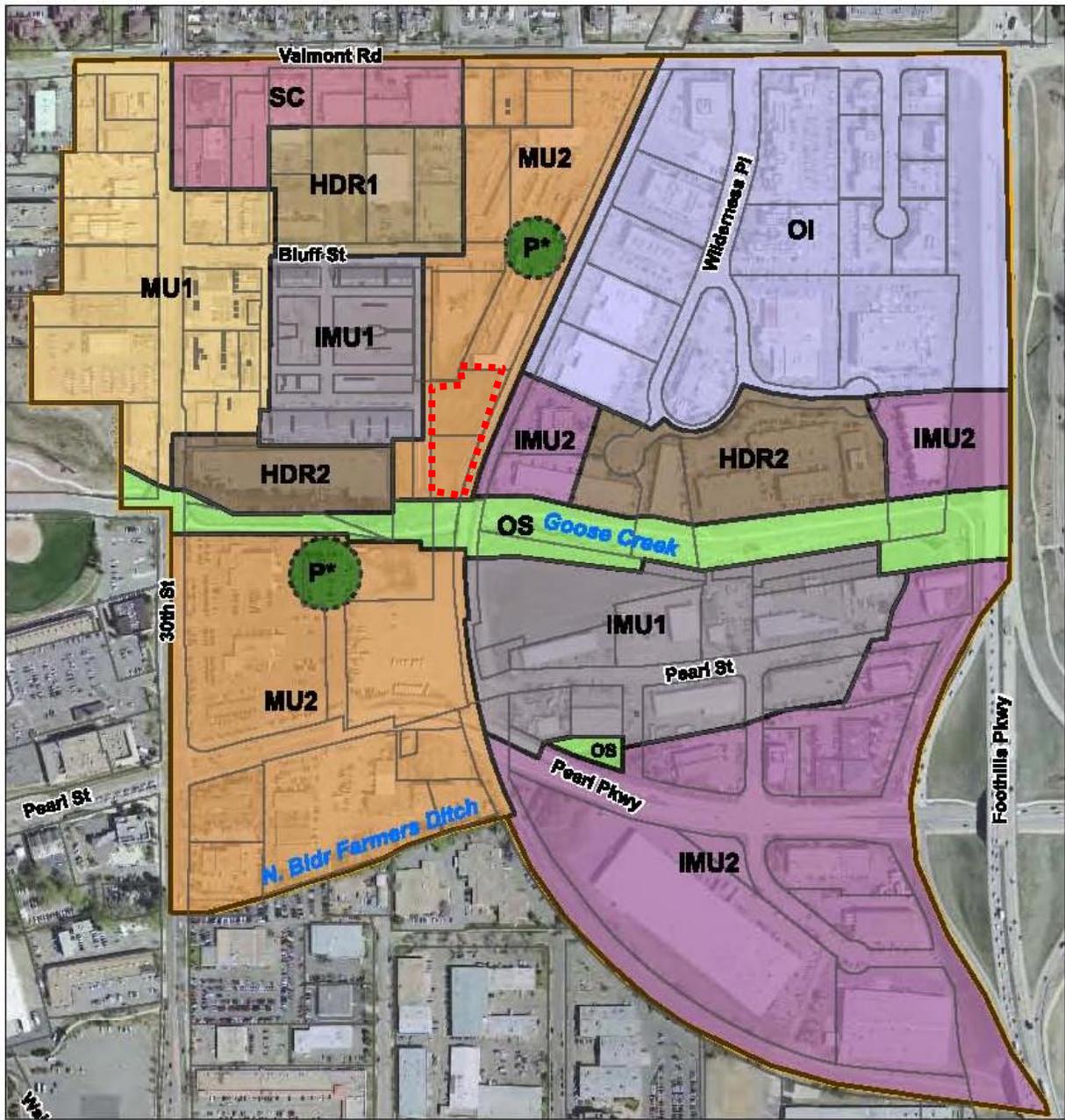
**Boulder Valley Comprehensive Plan (BVCP):** Pages 67 to 69 of the [BVCP](#) describe the purpose of Area Plans as a means to provide direction for specific geographic areas, and bridge the gap between the broad policies of the Comprehensive Plan and site specific project review. The Comprehensive Plan notes that issues that Area Plans address include appropriate character, scale and mix of uses and if regulatory changes are needed to ensure or encourage appropriate development. The Transit Village Area Plan (TVAP ) is one among four adopted area plans within the Comprehensive Plan, and includes the following stated purpose:

*“To describe the city’s vision for the future of the 160-acre Transit Village area and guide the long term development of the area. The area is defined as within walking distance to the future FasTracks transit services – commuter rail, bus rapid transit, and regional bus services.”*

**Transit Village Area Plan (TVAP):** The area overseen by the TVAP was renamed Boulder Junction, in reference to the area from decades ago as the junction of two major rail lines. The overall TVAP land use Plan is presented on the following page as **Figure 7**. Within TVAP, the land use designation for the site is MU-2 or Mixed Use -2, which anticipates three- to four-story mixed use buildings at a FAR of 1.5 to 2.0. See **Figure 7** below for the TVAP Land Use Map, and **Figure 6** for a description of the MU-2 Land Use designation with precedent development images.

<b>Mixed Use -2-</b>		<b>1.5 - 2.0 Floor Area*</b>
		
<p>Three- to four-story mixed-use buildings. Predominant use may be business or residential. Mostly structured or first-floor parking; may have some surface parking.</p>		
<b>Figure 6: Excerpt from TVAP: Intent of MU2 land use</b>		

Figure 7: TVAP Land Use Map



**Legend**

**Plan Land Use**

- |                            |                        |                       |
|----------------------------|------------------------|-----------------------|
| High Density Residential 1 | Industrial Mixed Use 1 | Service Commercial    |
| High Density Residential 2 | Industrial Mixed Use 2 | Park / Public Plaza   |
| Office Industrial          | Mixed Use 1            | Greenway / Open Space |
|                            | Mixed Use 2            |                       |



**Character Districts in TVAP.** Within TVAP, the Boulder Junction area was divided into eight character districts shown in Figure 8, primarily based on future land use and to promote a particular urban design character for each area. The project site is located in the area identified by TVAP as the “Rail Plaza District.” As noted on page 23 of TVAP, this is the area that

ultimately, “will host the Boulder stop on the new commuter rail service to Denver and Longmont.” The intent of the district is further defined, “The district will evolve into a high-density, commercial and residential mixed use area, with three- to five-story buildings.”

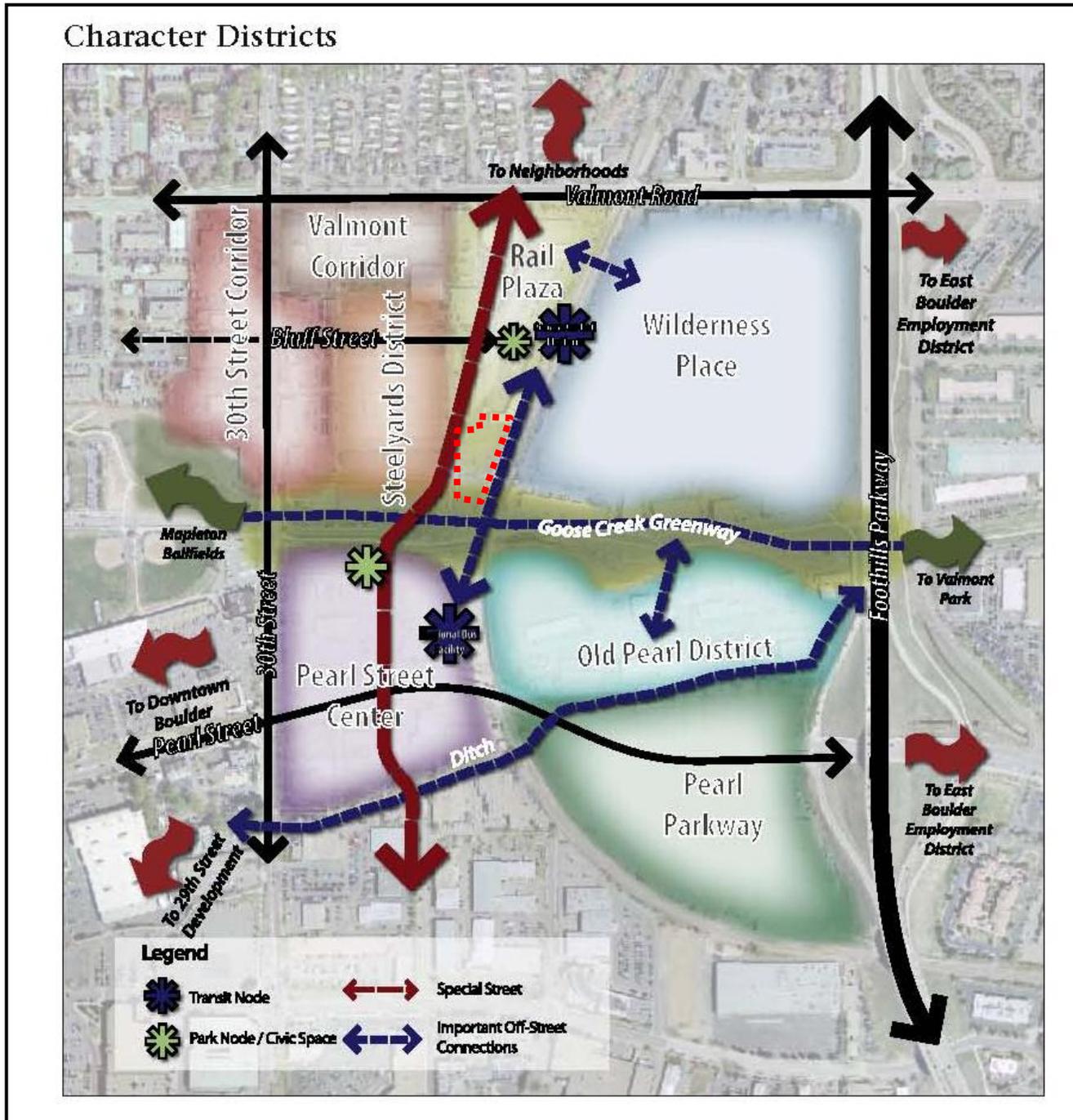


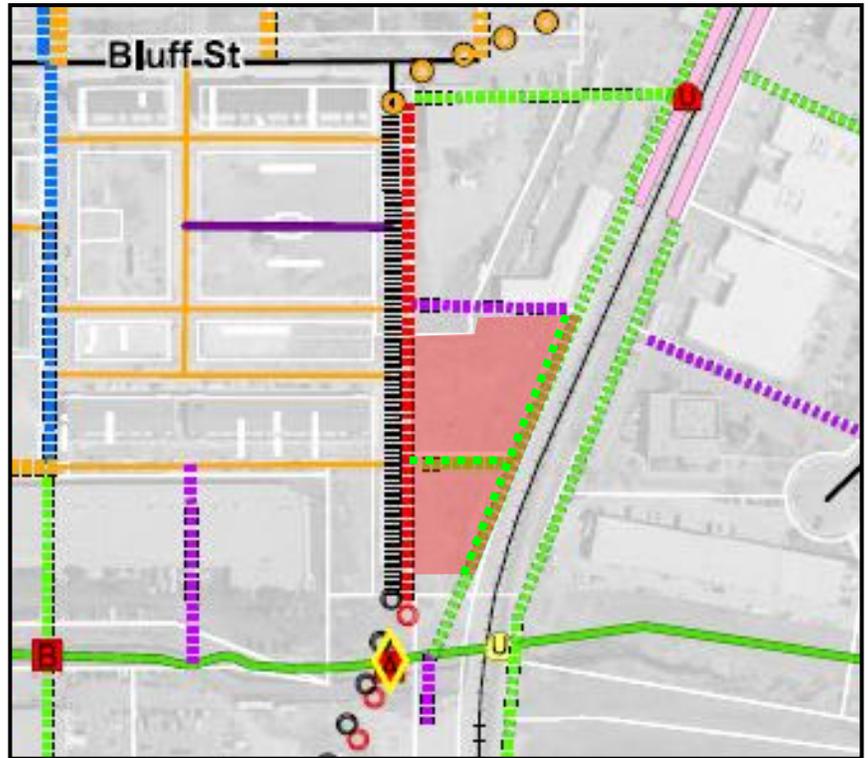
Figure 8: TVAP Character Districts Map

A connections plan was also adopted for TVAP that includes a number of connections through the site, as delineated in **Figure 9** (project site shown in red). Equally as important as Land Use, the connections plan is intended to:

*“Create walkable streets in a fine grain grid pattern, providing for walking, biking and possible car free zones. Provide multimodal connections within the area to adjacent neighborhoods and to key nearby destinations and activity areas.”*

The full text of the connections plan is found beginning on page 56 of TVAP, provided [here](#). As shown on the connections plan, Junction Place runs along the west side of the site and is anticipated to have on-street bike lanes (shown in red dashes). There are also multi-use path connections shown along the west side of the tracks to access the future rail plaza as well as running east-west through the center of the site to connect to Junction Place (shown in green dashes).

As mentioned above, the TVAP Connections Plan includes a number of connections through the site. Equally as important as Land Use, the connections plan is intended to *“create walkable streets in a fine grain grid pattern, providing for walking, biking and possible car free zones...(and to) Provide multimodal connections within the area to adjacent neighborhoods and to key nearby destinations and activity areas.”*



**Figure 9: Excerpt from TVAP Connections Plan showing connections through the site**

## **ANALYSIS OF KEY ISSUES**

### **Key Issue 1: Is the proposed project consistent with the Transit Village Area Plan (TVAP)?**

In keeping with the Concept Plan Review, overall the project has remained consistent with TVAP, especially in terms of building mass and scale, provision of required multi-use path connections, high quality open space features and activation of the Junction Place streetscape at the pedestrian level.

**Mass and Scale.** As noted on Pages 8 & 9 above, within [TVAP](#), the land use designation for the site is MU-2 or Mixed Use - 2, which anticipates three- to four-story mixed use buildings at a FAR of 1.5 to 2.0. In addition, the intent described in TVAP for the “Rail Plaza District” in which the project site is located is for the district to *“evolve into a high-density, commercial and residential mixed use area, with three- to five-story buildings.”* Because of the level of intensity anticipated to help support and enliven the rail plaza, overall the planned mass and scale appears to be consistent with TVAP.

**TVAP Connections Plan.** As shown on the TVAP connections plan and in **Figure 9** above, a 12’ wide multi-use path is anticipated along the west side of the tracks to access the rail platform, as well as a connecting E-W multi-use path generally along the lot line between the two parcels. The current proposal includes construction of both of the required multi-use paths. The proposal is also consistent with the Streetscape Guidelines in TVAP, which illustrate the anticipated layout of Junction Place from the Bus Facility in Depot Square to Bluff Street. This streetscape is described as “Segment 2” within the streetscape guidelines on Pg. 37 of TVAP as follows:

Segment 2 is the middle section from the northern extent of the bus facility, near Goose Creek, to Bluff Street from the northern extent of the bus facility, near Goose Creek, to Bluff Street. A majority of this section follows the existing 33rd Street, which will be widened to accommodate a shared-space street (where vehicles and bikes share the roadway) and wide pedestrian areas. On-street parking will be parallel or in pockets of diagonal parking, alternating with wider sidewalk space for outdoor seating, larger planting areas, or other amenities.

As part of this proposal, the applicant will be required to dedicate the following easements/right-of-way and to construct the following public improvements:

- ½ of the Junction Place (Segment 2) street cross-section
- The east – west multi-use path between lots 1 and 2
- The north – south multi-use path paralleling the BNSF railway right-of-way

As shown in **Figure 10**, the proposed Junction Place street section includes the following cross-sectional design elements, consistent with the street section requirements included in TVAP:

- 10' 6" Travel Lane (reduction from 11' as shown in TVAP approved by staff)
- 15' 6" combined on-street parking and landscape width (city staff has determined that parallel parking is preferred over the diagonal configuration shown in TVAP).
- 10' sidewalk

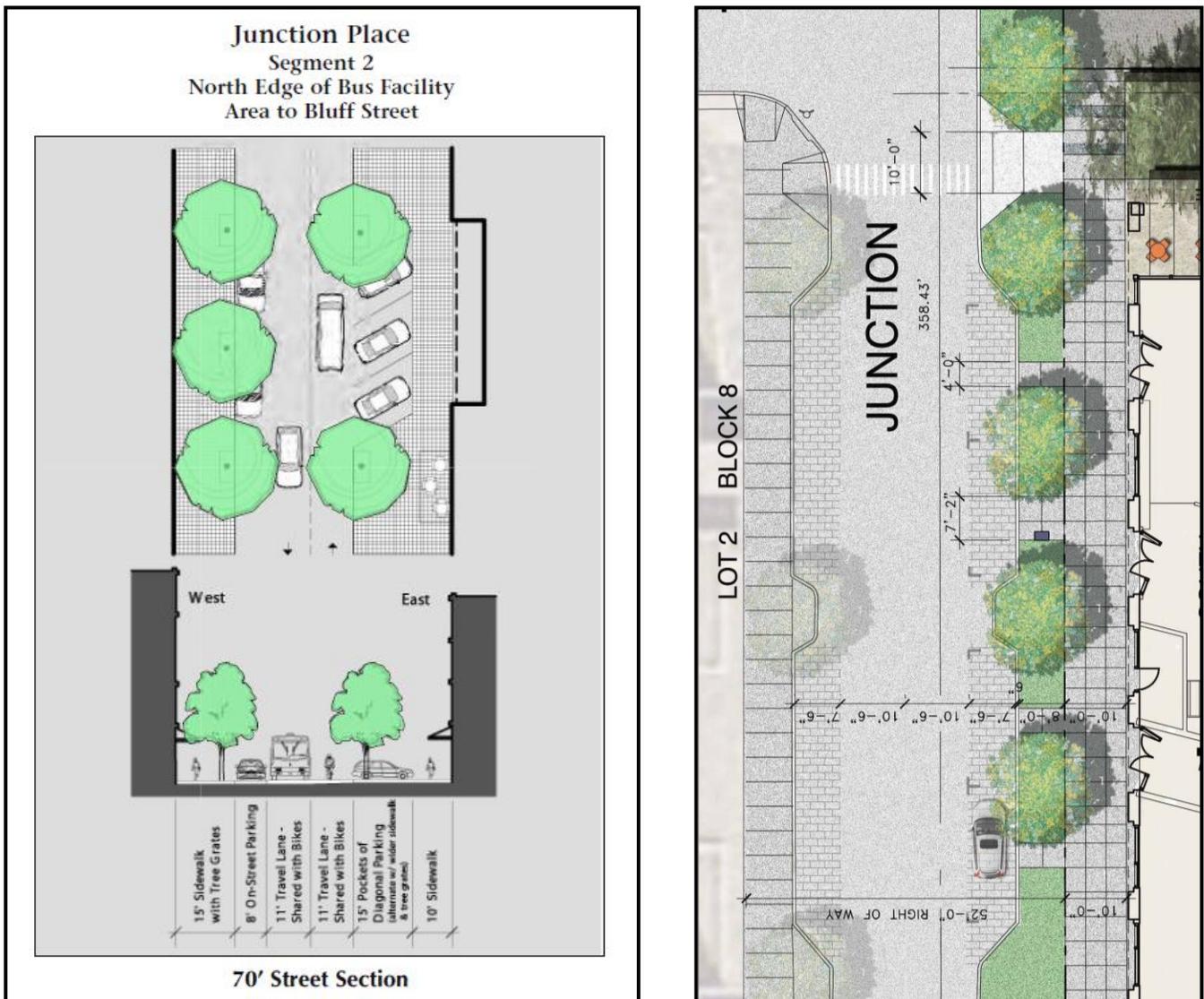


Figure 10: Illustrations comparing TVAP street section (left) with proposed project street section (right)

**TVAP Design Guidelines.** Overall, the project has improved its consistency with the TVAP Guidelines from the Concept Plan proposal. The following is a consistency analysis of the proposed project with the relevant TVAP Urban Design Guidelines.

### **General Urban Design Guidelines.**

- ***“Orient the main facade to the street and provide an entrance on the street side of the building.”***

Currently the proposed site plan meets this guideline, with the majority of the main entrances to the building located along Junction Pl. or from within the proposed central plaza.

- ***“Design buildings with pedestrian-scale materials and architectural articulation, particularly on the first floor. Avoid large blank walls. Along streets and sidewalks provide pedestrian interest including transparent windows and well-defined building entrances.”***

The proposed project meets the intent of this guideline by incorporating storefront windows and building entrances along Junction Pl, with brick as the primary building material accented by wood and metal elements. The north building also includes a small steel overhang that creates a pedestrian promenade.

- ***“Incorporate well-designed, functional open spaces with tree, quality landscaping and art, access to sunlight and places to sit comfortably. Where public parks or open spaces are not within close proximity, provide shared open spaces for a variety of activities. Where close to parks, open spaces provided by development may be smaller.”***

The project provides a total of nearly 20,000 sq. ft. of open space (nearly 33% of the site area where 20% is the minimum required per the MU-4 zone standards) through a variety of site design features. The main open space feature is a large public plaza in the center of the site, which provides access to the main building entries for the office tenants and community room/ flex space, and includes a multi-use path connection between Junction Pl. and multi-use path along the railroad to the east. The scale of the plaza is broken down through the incorporation of sculptural artwork, seating, landscape elements, patterned paving, trellised bike parking areas and architectural lighting, and recessed building corners on either side of the plaza entry help to create more intimate pocket plazas around the coffee shop patio and pedestrian garage entry. Additional open space amenities include a “pocket plaza” on the north side of the north building, and the restaurant patio area on the south end of the southern building.

In response to staff comments made during Concept Plan review, the applicant has modified the central plaza to be more proportional and welcoming; however, staff has concerns regarding the placement of the transformer boxes on the north edge of the plaza adjacent to the southern building. These concerns were echoed by BDAB at their July 15, 2015 discussion of the project. As a result, staff has included a recommended condition of approval requiring the applicant to relocate the transformers and to reduce their overall visibility.

### **Junction Place Guidelines.**

- ***“In addition to the street trees, sidewalks and bike facilities specified by the Junction Place streetscape section, provide seating, planters, art, special pavement and lighting along Junction Place. ”***

See open space comments directly above. The project plans include all of the required street trees, an 8 foot planting strip and a 10 foot sidewalk, and special goose neck light fixtures along the building facades along Junction Pl. The main plaza includes a proposed sculptural art element as well as string lights, seating and bike

parking.

- ***“Where feasible, place active uses, such as retail or commercial services on the first floor of buildings along Junction Place.”***

The applicant has indicated that the intent of the first floor spaces is to be as flexible as possible in terms of uses. In addition to the proposed restaurant, coffee shop and flex space/ community room, the applicant has indicated that the intent for the ground floor spaces is to allow the flexibility to have personal service and/or retail uses as the market allows. The first floors of the building have been designed with adaptability in mind such that overtime they could be used as ground floor retail rather than office. In addition, the sidewalk is wide enough to accommodate café seating areas adjacent to active uses as appropriate.

### **Rail Plaza District Guidelines:**

- ***“Locate buildings along the street with parking behind.”***

The current proposal represents an improvement from the Concept Plan submittal in terms of creating more of a consistent visual pattern along Junction Place. Staff noted that the plaza in the previous submittal was too large and therefore disrupted the building façade. In the current submittal, the gap has been reduced, and the west facades of the two buildings brought closer together to create more of a defined urban streetscape. The parking is located in an underground structure and is therefore not visible from the right-of-way. Overall, the streetscape is consistent with the adopted TVAP vision for that segment of Junction Place (shown on page 11 above).

- ***“Orient buildings to Junction Place (see Junction Place guidelines), as well as to the tracks. If feasible, place active uses on the first floor. Consider making the track-side frontage a car-free zone with pedestrian amenities.”***

The project fully meets this intent by placing the majority of entrances along Junction Place and orienting additional entrances toward the central plaza, which will be a focal point for pedestrians on Junction Place. The building addresses the track side of the site, although in a different manner. The proposal for the east frontages of both buildings is to create a solar façade consisting almost entirely of architecturally integrated solar panels. The panels are designed such that there will still be ample fenestration and pedestrian interest along the tracks, and two building entrances will also be provided. The southern restaurant patio area will also serve to create visual interest for people passing by the site on the multi-use path. The eastern portion of the site along the tracks is designated for the required multi-use path connection, and as a result will be a completely car-free zone.

### **Key Issue 2: Does the proposal meet Site Review Criteria, including BVCP policies?**

Overall, staff finds the proposed project to be an innovative project that sensitively utilizes an existing infill site to provide a high quality urban space which furthers the design and land use goals of TVAP while potentially serving as a flagship project in terms of environmentally sustainable building design. As a whole, the current proposal meets the Site Review criteria as set forth in section 9-2-14, B.R.C. 1981, including, on balance, the policies of the Boulder Valley Comprehensive Plan (BVCP). Specifically, the project has been found to meet the following BVCP policies:

- 2.01 Unique Community Identity
- 2.03 Compact Development Pattern
- 2.09 Neighborhoods as Building Blocks
- 2.14 Mix of Complementary Land Uses
- 2.17 Variety of Activity Centers
- 2.22 Improve Mobility Grid
- 2.30 Sensitive Infill and Redevelopment
- 2.32 Physical Design for People
- 2.37 Enhanced Design for Private Sector Projects
- 4.05 Energy-Efficient Building Design

Overall, the project has been found to meet all applicable Site Review criteria pertaining to Site Design, including those related to open space, landscaping, circulation, parking and building design. Please refer to [Attachment B](#) for staff's complete analysis of the review criteria.

**Key Issue 2: Does the proposed restaurant meet Use Review Criteria found in section 9-2-15(e), B.R.C. 1981?**

The proposed restaurant use has been found to meet the Use Review criteria found in section 9-2-15(e), B.R.C. 1981. Specifically, the proposed restaurant would provide a direct service to the surrounding uses by offering an eating establishment within walking distance of the proposed office uses as well as the existing Steel Yards residential uses. The location, design and operating characteristics of the proposed use are such that the use will be reasonably compatible with and have minimal negative impact on the use of nearby properties, and the use is in keeping with the predominant character of the surrounding area as established by TVAP. Staff's analysis of the Use Review criteria is also included as [Attachment B](#).

**PUBLIC COMMENT AND PROCESS:**

Required public notice was given in the form of written notification mailed to all property owners within 600 feet of the subject site and a sign posted on the property for at least 10 days. All notice requirements of section 9-4-3, B.R.C. 1981 have been met. Staff has received comments from one neighbor in the Steel Yards development expressing concern over the proposed restaurant use. Please refer to [Attachment C](#) for all correspondence received.

**STAFF FINDINGS AND RECOMMENDATION:**

Staff recommends that the Planning Board approve the Site Review application LUR2015-00048 and Use Review application LUR2014-00065, adopting the staff memorandum as findings of fact and subject to the recommended conditions of approval.

**RECOMMENDED CONDITIONS OF SITE REVIEW APPROVAL**

1. The Applicant shall ensure that the **development shall be in compliance with all plans prepared by the Applicant** on July 16, 2015 on file in the City of Boulder Planning Department, except to the extent that the development may be modified by the conditions of this approval.
2. The Applicant shall **comply with all previous conditions** contained in any previous approvals, except to the extent that any previous conditions may be modified by this approval, including, but not limited to, the following: the Development Agreement recorded at Reception No. 1960064 on July 14, 1999 and Subdivision Agreement recorded at Reception No. 2064118 on July 26, 2000 in the records of the

Boulder County Clerk and Recorder.

3. Prior to submittal of a Technical Document Review application for the easement dedications referenced in Condition No. 4 below and prior to a building permit application, the Applicant shall provide to the City written documentation, in a form acceptable to the City, from the Burlington Northern Santa Fe (BNSF) Railway **demonstrating the extinguishment of the 16-foot wide portion of the 25-foot wide Railroad Easement** recorded in the Records of the Boulder County Clerk and Recorder on Film 1598 at Reception No. 1007999 to allow for dedication of a 16-foot wide multi-use path easement. The document extinguishing the 16-foot wide easement area shall be recorded with the Boulder County Clerk and Recorder's Office prior to submittal of a Technical Document Review application.

4. Prior to building permit application, as part of Technical Document Review applications, the Applicant shall **dedicate to the City**, at no cost, the following easements, as shown on the approved plans, meeting the City of Boulder Design and Construction Standards, , the form and final location of which shall be subject to the approval of the City Manager:

a. **A ten-foot wide utility and public access easement** for the Junction Place utilities and sidewalk along the west property lines of both Lot 1 and Lot 2, Block 9, Steel Yards Subdivision.

b. **A sixteen-foot wide public access easement** for the east/west 12-foot wide multi-use path through the Plaza Area along the property line between Lot 1 and Lot 2, Block 9, Steel Yards Subdivision.

c. **A sixteen-foot wide drainage and public access easement** for the north/south multi-use path along the east property lines of both Lot 1 and Lot 2, Block 9, Steel Yards Subdivision.

d. **A thirteen-foot wide drainage easement** along the north property line of Lot 1, Block 9, Steel Yards Subdivision.

5. Prior to a building permit application, the Applicant shall provide to the City written documentation, in a form acceptable to the City, from the PUBLIC SERVICE CO. OF COLORADO ("PSCO") demonstrating **extinguishment of an easement identified as No. 14 on the PUBLIC SERVICE COMPANY OF COLORADO EASEMENT** recorded at Reception No. 2484658 on August 7, 2003 in the records of the Boulder County Clerk and Recorder which runs north/south through the center of both Lot 1 and Lot 2, Block 9, Steel Yards Subdivision. The extinguishment of the PSCO easement shall be recorded in the Records of the Boulder County Clerk and Recorder prior to a building permit application.

6. Prior to a building permit application, the Applicant shall submit an Administrative Review application, subject to the approval of the City Manager, to vacate **the drainage easement** dedicated to the City on the Steel Yards Subdivision plat along the south property line of Lot 2, Block 9, Steel Yards Subdivision and the west side of the 25-foot Railroad Easement located along the east property lines of both Lot 2 and Lot 1, Block 9, Steel Yards Subdivision.

7. Prior to a building permit application, the Applicant shall submit a Technical Document Review application for the following items, subject to the approval of the City Manager:

a. **Final architectural plans**, including material samples and colors, to insure compliance with the intent of this approval and compatibility with the surrounding area. The architectural intent shown on the approved plans dated July 16, 2015 is acceptable. The final architectural plans shall show the transformer boxes currently shown at the south end of the northern building relocated to a less visible location or

architecturally integrated into a building or other site feature. The final architectural plans shall also show the TDM elements described in Condition No. 9 below. Planning staff will review plans to assure that the architectural intent is performed.

- b. A **final site plan** which includes detailed floor plans and section drawings.
  - c. A **final utility plan** meeting the City of Boulder Design and Construction Standards.
  - d. A **final storm water report and plan** meeting the City of Boulder Design and Construction Standards.
  - e. **Final transportation plans** meeting the City of Boulder Design and Construction Standards and the Boulder Greenways Design Guidelines for all transportation improvements. The Final transportation plans must include, but are not limited to: multi-use path plan and profile drawings, Junction Place plan drawings, street cross-sectional drawings, signage and striping plans in conformance with Manual on Uniform Traffic Control Devices (MUTCD) standards, transportation detail drawings, geotechnical soils report, and pavement analysis.
  - f. A **detailed landscape plan**, including size, quantity, and type of plants existing and proposed; type and quality of non-living landscaping materials; any site grading proposed; and any irrigation system proposed, to insure compliance with this approval and the City's landscaping requirements. Removal of trees must receive prior approval of the Planning Department. Removal of any tree in City right of way must also receive prior approval of the City Forester.
  - g. A **detailed outdoor lighting plan** showing location, size, and intensity of illumination units, indicating compliance with section 9-9-16, B.R.C.1981.
  - h. A **detailed shadow analysis** to insure compliance with the City's solar access requirements of section 9-9-17, B.R.C. 1981.
8. Prior to a building permit application, the Applicant shall construct, remove, or reconstruct the public improvements as described below, subject to acceptance by the City, in conformance with the approved plans and with the City of Boulder Design and Construction Standards and provide a financial guarantee, as required under section 9-2-20, B.R.C. 1981 and in a form acceptable to the Director of Public Works, for the public improvements described below:
- a. Construct a north/south **12-foot wide multi-use path along the east property line**.
  - b. Construct an east/west **12-foot wide multi-use path through the plaza area using stone accent paving bands or other similar material as approved by staff**.
  - c. Remove and reconstruct an east/west **12-foot multi-use path south of the Property** to provide a pavement section that can support a 14-ton truck and include a two-foot wide colored concrete shoulder.
  - d. Remove the existing diagonal parking and curb-and-gutter north of the Property in the Junction Place right-of-way and replace it with **new curb-and-gutter and parallel parking**.
  - e. Construct **parallel parking, a landscape strip, and a 10-foot wide sidewalk along Junction Place** adjacent to the Property.

9. The **Applicant shall provide the TDM elements described in Table 2 of the TDM Plan** (except for the EcoPasses and subsidized Bike Share and carshare membership which will be provided by the Boulder Junction General Improvement District), including but not limited to the following: 20 short-term bicycle parking spaces; 40 long-term bicycle parking spaces along with a security camera monitoring in the parking garage; a B-Cycle station; a minimum of three carpool spaces and three car-share spaces; and changing facilities on first level of both buildings.

### **RECOMMENDED CONDITIONS OF USE REVIEW APPROVAL**

1. The Applicant shall ensure that the **development shall be in compliance with all plans prepared by the Applicant** on July 16, 2015 and the Applicant's written statement dated July 24, 2015 on file in the City of Boulder Planning Department, except to the extent that the development may be modified by the conditions of this approval. Further, the Applicant shall ensure that the approved use is operated in compliance with the following restrictions:

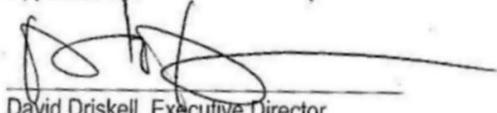
a. The Applicant shall operate the business in accordance with the management plan dated June 15, 2015 which is attached to this Notice of Disposition.

b. The approved use shall be closed from 2:00 a.m. to 11:00 a.m. seven days per week.

c. Size of the approved use shall be limited to 1,759 square feet. The total number of indoor seats for the approved use shall not exceed the lesser of 85 seats or the maximum occupant load per IBC section 1004. The patio area will not exceed 900 square feet. All trash located within the outdoor dining area, on the restaurant property, and adjacent streets, sidewalks and properties shall be picked up and properly disposed of immediately after closing.

2. The Applicant **shall not expand or modify the approved use**, except pursuant to subsection 9-2-15(h), B.R.C. 1981.

Approved By:



David Driskell, Executive Director  
Department of Community Planning and Sustainability

### **ATTACHMENTS:**

**A: Applicant's Proposed Plans and Written Statement**

**B: Staff's Analysis of Review Criteria**

**C: Public Comments Received**

# Boulder Commons

2440 - 2490 JUNCTION PL. BOULDER, CO

ATTACHMENT A



## Project Information

<b>OWNER</b> MCV CAPITAL 1215 SPRUCE ST, STE 300 BOULDER, CO 80302 P: 303.444.9200 F: 303.633.2740	<b>MEP ENGINEER</b> INTEGRAL GROUP 580 CALIFORNIA ST, 12TH FL SAN FRANCISCO, CA 94104 P: 510.663.2070
<b>ARCHITECT OF RECORD</b> COBURN DEVELOPMENT INC. 3020 CARBON PLACE #203 BOULDER, CO P: 303.442.3351 F: 303.447.3933	<b>LANDSCAPE ARCHITECT</b> GLS ARCH 2677 MISSION ST #200 SAN FRANCISCO, CA 94110 P: 415.285.3614
<b>CIVIL ENGINEER</b> JVA ENGINEERS 1319 SPRUCE ST BOULDER, CO 80302 P: 303.444.1951 F: 303.444.1957	<b>CONSULTING ARCHITECT</b> EHDD. 500 TREAT AVE #201 SAN FRANCISCO, CA 94110 P: 415.285.9193
<b>STRUCTURAL ENGINEER</b> STUDIO NYL STRUCTURAL ENGINEERS 2995 BASELINE RD, STE 314 BOULDER, CO 80303 P: 303.558.3145 F: 303.440.8536	

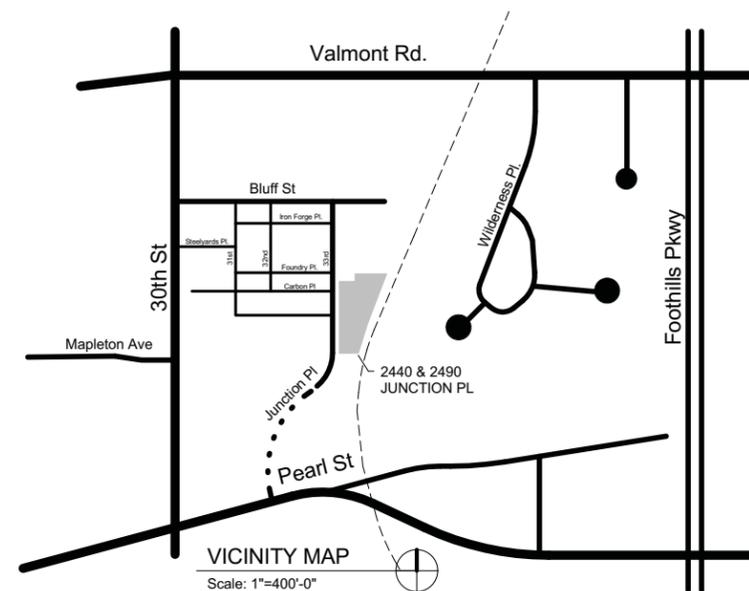
PARCEL AREA (GROSS)	1.34 ACRES
CURRENT ZONING	MU-4
PROPOSED USE	RETAIL, BUSINESS, RESTAURANT
ZONING	MU-4
PROPOSED NET LOT AREA	1.34 ACRES
PROPOSED F.A.R.	1.74
PROPOSED BUILDING AREA	101,570 SF
PROPOSED BUILDING HT.	55'-0"

## Drawing Index - Site Review

Sheet No.	Sheet Name
T0.0	SR - TITLE SHEET
SD1.0	SITE DEVELOPMENT PLAN
C1.0	PRELIMINARY STORMWATER PLAN
C1.0A	PRELIMINARY STORMWATER PLAN - ALT.
C2.0	PRELIMINARY UTILITY PLAN
C3.0	HORIZONTAL CONTROL PLAN
L1.0	LANDSCAPE - KEY PLAN
L2.0	LAYOUT AND GRADING PLAN
L3.0	SECTIONS AND ELEVATIONS
L3.1	SECTIONS AND ELEVATIONS
L4.0	DETAILS
L5.0	PLANTING PLAN AND SCHEDULE
E-100	SITE LIGHTING ANALYSIS
A1.1	GARAGE PLAN
A1.2	NORTH BUILDING LEVEL 1
A1.3	NORTH BUILDING LEVEL 2
A1.4	NORTH BUILDING LEVEL 3
A1.5	NORTH BUILDING LEVEL 4
A1.6	SOUTH BUILDING LEVEL 1
A1.7	SOUTH BUILDING LEVEL 2
A1.8	SOUTH BUILDING LEVEL 3
A1.9	SOUTH BUILDING LEVEL 4

## Drawing Index - Site Review

Sheet No.	Sheet Name
A1.10	ROOF PLAN
A1.11	AREA PLANS
A1.12	AREA PLANS
A2.1	ARCHITECTURAL PERSPECTIVE
A2.2	ARCHITECTURAL PERSPECTIVE
A2.3	ARCHITECTURAL PERSPECTIVE
A2.4	ARCHITECTURAL PERSPECTIVE
A2.5	EXTERIOR ELEVATIONS
A2.6	EXTERIOR ELEVATIONS
A2.7	EXTERIOR ELEVATIONS
A2.8	EXTERIOR ELEVATIONS
A3.1	SHADOW STUDIES
A3.2	SHADOW STUDIES
A3.3	PRINCIPAL BUILDING HEIGHT



**SITE REVIEW  
CORRECTIONS  
07/16/2015**

SHEET No.  
**T0.0**

Agenda Item 5A SR Page 18 of 73

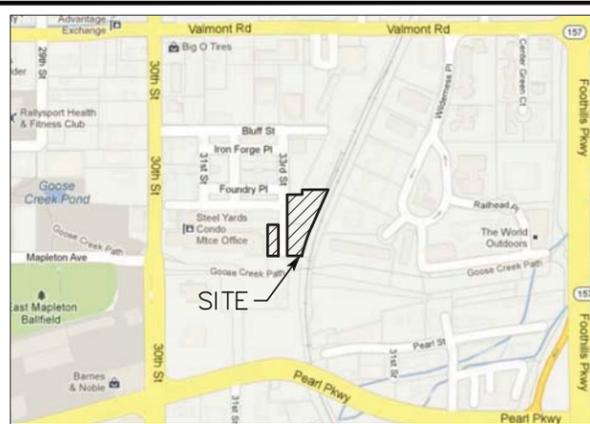
**BOULDER COMMONS**

2440 + 2490 Junction Pl. Boulder, CO

# ALTA/ACSM LAND TITLE SURVEY

LOT 2 OF BLOCK 8, AND LOTS 1 & 2 OF BLOCK 9, STEEL YARDS SUBDIVISION, LOCATED IN THE NORTHEAST QUARTER OF SECTION 29, TOWNSHIP 1 SOUTH, RANGE 70 WEST OF THE 6TH P.M., CITY OF BOULDER, COUNTY OF BOULDER, STATE OF COLORADO

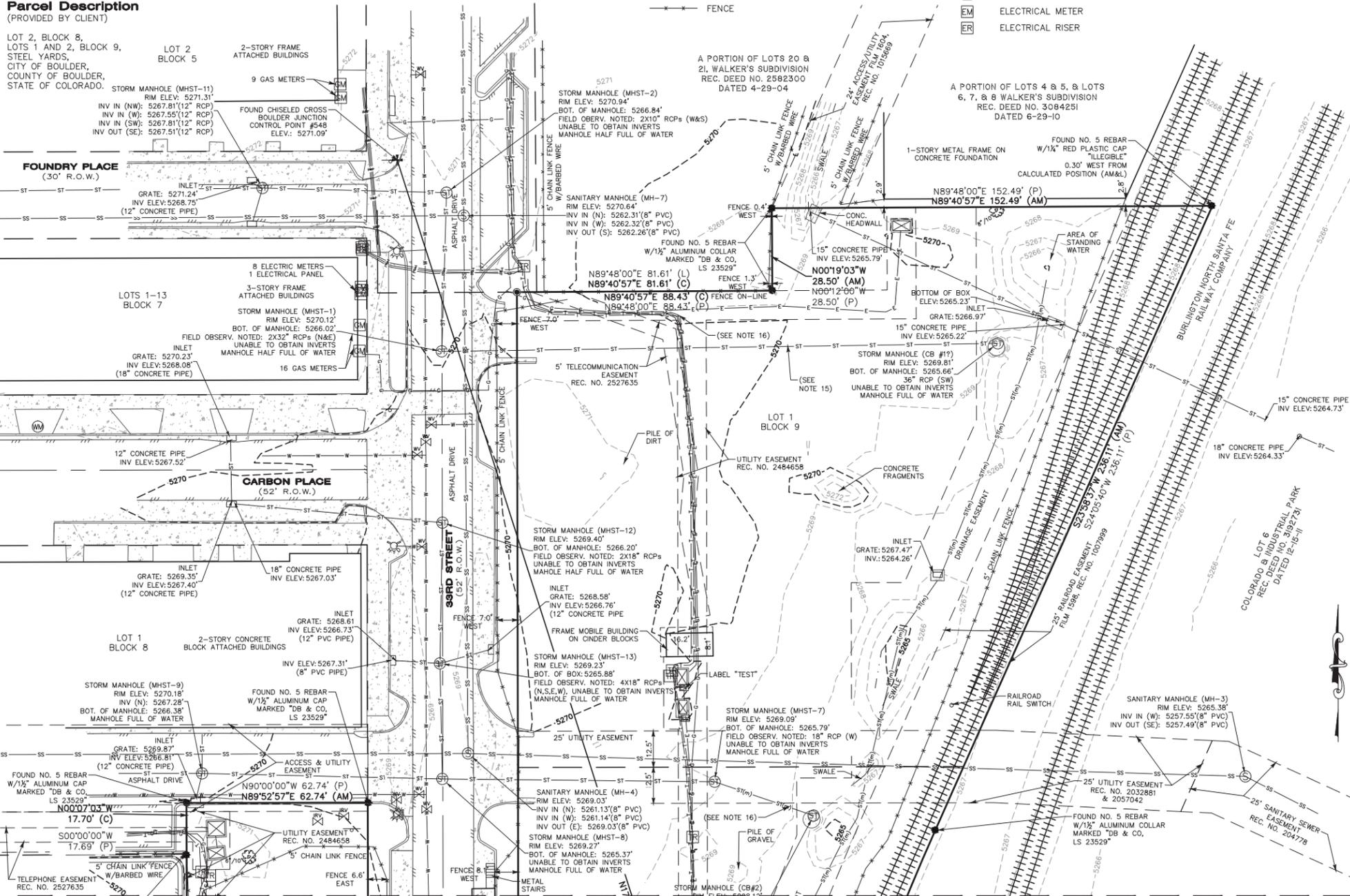
SHEET 1 OF 2



Vicinity Map  
NOT TO SCALE

### Parcel Description (PROVIDED BY CLIENT)

LOT 2, BLOCK 8,  
LOTS 1 AND 2, BLOCK 9,  
STEEL YARDS,  
CITY OF BOULDER,  
COUNTY OF BOULDER,  
STATE OF COLORADO.



### Legend

- ✚ FOUND CHISELED CROSS
- FOUND MONUMENT AS DESCRIBED
- SET #5 REBAR WITH 1 1/2" ALUMINUM CAP "FLATIRON'S SURV 16406"
- (AM) AS MEASURED AT TIME OF SURVEY
- (C) CALCULATED FROM RECORD AND AS MEASURED INFORMATION
- (P) AS PER THE PLAT OF STEEL YARDS
- (L) AS PER IMPROVEMENT SURVEY PLAT BY DREXEL BARRELL & CO., LS-98-0210, DATED 12/17/98
- ▭ CONCRETE
- ▨ EDGE OF ASPHALT
- FENCE
- ⊕ ELECTRICAL PANEL
- ⊖ ELECTRIC VAULT
- OW— OVERHEAD UTILITY LINE
- UTILITY POLE
- TELEPHONE LINE
- TELEPHONE RISER
- GAS LINE
- GAS METER
- SIGN
- GUARDRAIL
- ST— STORM DRAINAGE LINE SCALED FROM MAPS
- W— WATER LINE
- WATER VALVE
- WATER METER
- FIRE HYDRANT
- IRRIGATION VALVE
- SS— SANITARY SEWER LINE
- S— SANITARY SEWER MANHOLE
- ST— STORM DRAINAGE LINE
- ST— STORM DRAINAGE MANHOLE
- E— ELECTRICAL LINE
- ELECTRICAL TRANSFORMER
- ELECTRICAL METER
- ELECTRICAL RISER

### Notes

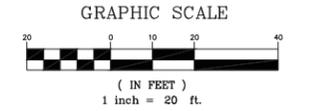
- FIDELITY NATIONAL TITLE COMPANY COMMITMENT NUMBER 515-F0436078-170-RR0, DATED JANUARY 4, 2013 AT 7:00 A.M. (LOT 1, BLOCK 8), AND COMMITMENT NUMBER 515-F0448297-170-RR0, AMENDMENT NO. 1, DATED MAY 29, 2013 AT 7:00 A.M. (LOT 2, BLOCK 8), WERE ENTIRELY RELIED UPON FOR RECORDED INFORMATION REGARDING RIGHTS-OF-WAY, EASEMENTS AND ENCUMBRANCES IN THE PREPARATION OF THIS SURVEY. THE PROPERTY SHOWN AND DESCRIBED HEREON IS MORE THAN THE PROPERTY DESCRIBED IN SAID TITLE COMMITMENT.
- LOT 2 OF BLOCK 9, ON THIS SURVEY WAS PREPARED WITHOUT THE BENEFIT OF A CURRENT TITLE COMMITMENT AND DOES NOT CONSTITUTE A TITLE SEARCH BY FLATIRON'S, INC. TO DETERMINE TITLE OR EASEMENTS OF RECORD. THIS SURVEY DOES NOT PURPORT TO REFLECT ANY OF THE FOLLOWING WHICH MAY BE APPLICABLE TO THE SUBJECT REAL ESTATE: EASEMENTS, OTHER THAN POSSIBLE EASEMENTS THAT WERE VISIBLE AT THE TIME OF MAKING THIS SURVEY; BUILDING SETBACK LINES; RESTRICTIVE COVENANTS; SUBDIVISION RESTRICTIONS; ZONING OR OTHER LAND-USE REGULATIONS, AND ANY OTHER FACTS THAT AN ACCURATE AND CURRENT TITLE SEARCH MAY DISCLOSE.
- ACCORDING TO COLORADO LAW YOU MUST COMMENCE ANY LEGAL ACTION BASED UPON ANY DEFECT IN THIS SURVEY WITHIN THREE YEARS AFTER YOU FIRST DISCOVER SUCH DEFECT. IN NO EVENT MAY ANY ACTION BASED UPON ANY DEFECT IN THIS SURVEY BE COMMENCED MORE THAN TEN YEARS FROM THE DATE OF THE CERTIFICATION SHOWN HEREON.
- THIS ALTA/ACSM LAND TITLE SURVEY WAS PREPARED FOR THE EXCLUSIVE USE OF COBURN DEVELOPMENT AND JVA INC., NAMED IN THE STATEMENT HEREON. SAID STATEMENT DOES NOT EXTEND TO ANY UNNAMED PERSON WITHOUT AN EXPRESS STATEMENT BY THE SURVEYOR NAMING SAID PERSON.
- THIS SURVEY IS VALID ONLY IF PRINT HAS ORIGINAL SEAL AND SIGNATURE OF SURVEYOR.
- BASIS OF BEARINGS: A LINE BETWEEN BOULDER JUNCTION LAND SURVEY CONTROL DIAGRAM CONTROL POINT 1050 (NOT ACCEPTED AS PROPERTY CORNER) BEING A NO. 4 REBAR WITH ALLOY COLLAR AND CONTROL POINT 548, BEING A CHISELED CROSS WITH A CALCULATED BEARING OF N15°09'16"W AS SHOWN HEREON. ALL BEARINGS SHOWN HEREON ARE RELATIVE THERETO.
- THE LOCATIONS FOR UNDERGROUND UTILITIES ARE BASED UPON VISIBLE SURFACE EVIDENCE. MAPS PROVIDED BY UTILITY LOCATES AND CLIENT. LOCATIONS OF UNDERGROUND UTILITIES AND STRUCTURES MAY VARY FROM LOCATIONS SHOWN HEREON. ADDITIONAL BURIED UTILITIES AND STRUCTURES MAY BE ENCOUNTERED. NO EXCAVATIONS WERE MADE DURING THE PROGRESS OF THIS SURVEY TO LOCATE BURIED UTILITIES AND STRUCTURES. ALL UNDERGROUND UTILITIES MUST BE FIELD LOCATED BY THE APPROPRIATE AGENCY OR UTILITY COMPANY PRIOR TO ANY EXCAVATION, PURSUANT TO C.R.S. SEC. 9-1.5-103.
- ANY PERSON WHO KNOWINGLY REMOVES, ALTERS OR DEFACES ANY PUBLIC LAND SURVEY MONUMENT AND/OR BOUNDARY MONUMENT OR ACCESSORY, COMMITS A CLASS TWO (2) MISDEMEANOR PURSUANT TO STATE STATUTE C.R.S.
- THE DISTANCE MEASUREMENTS SHOWN HEREON ARE U.S. SURVEY FOOT.
- THE CONTOURS REPRESENTED HEREON WERE INTERPOLATED BY AUTOCAD CIVIL 3D (DIGITAL TERRAIN MODELING) SOFTWARE BASED UPON ACTUAL MEASURED SPOT ELEVATIONS. DEPENDING ON THE DISTANCE FROM A MEASURED SPOT ELEVATION AND LOCAL VARIATIONS IN TOPOGRAPHY, THE CONTOUR SHOWN MAY NOT BE AN EXACT REPRESENTATION OF THE SITE TOPOGRAPHY. THE PURPOSE OF THIS TOPOGRAPHIC MAP IS FOR SITE EVALUATION AND TO SHOW SURFACE DRAINAGE FEATURES. ADDITIONAL TOPOGRAPHIC OBSERVATIONS MAY BE NECESSARY IN SPECIFIC AREAS OF DESIGN. TOPOGRAPHY SHOWN HEREON COMPLIES WITH NATIONAL MAP ACCURACY STANDARDS.
- ELEVATIONS BASED ON BOULDER JUNCTION LAND SURVEY CONTROL DIAGRAM POINT C-1050 (NOT ACCEPTED AS PROPERTY CORNER), WITH A PUBLISHED ELEVATION OF 5266.48 FEET (NAVD83), BEING A NO. 4 REBAR WITH ALLOY COLLAR LOCATED NEAR THE SOUTHEAST CORNER OF LOT 2, BLOCK 9.
- SUBSURFACE BUILDINGS, IMPROVEMENTS OR STRUCTURES ARE NOT NECESSARILY SHOWN. BUILDINGS AND OTHER IMPROVEMENTS OR STRUCTURES ON ADJACENT PROPERTIES THAT ARE MORE THAN FIVE (5) FEET FROM ANY OF THE PROPERTY LINES OF THE SUBJECT PROPERTY ARE NOT NECESSARILY SHOWN.
- DATES OF FIELD WORK: APRIL 10, 2013
- THE FOLLOWING DOCUMENTS ARE MENTIONED IN THE ABOVE REFERENCED TITLE COMMITMENT AND APPEAR TO AFFECT THE SUBJECT PROPERTY BUT CANNOT BE SHOWN GRAPHICALLY. THE FOLLOWING LIST CONTAINS THE TITLE COMMITMENT EXCEPTION NUMBER, DATE RECORDED, RECEPTION NUMBER AND/OR BOOK AND PAGE.

COMMITMENT #	DATE	BOOK	PAGE	DESCRIPTION	
#515-F0436078-170-RR0	APR. 15, 1916	BK. 395,	PG. 124	TELEPHONE EASEMENT (EXACT LOCATION IS NOT SET FORTH)	
	APR. 15, 1916	BK. 395,	PG. 125	TELEPHONE EASEMENT (EXACT LOCATION IS NOT SET FORTH)	
	#13 JUL. 26, 2000	REC. NO. 2064118		SUBDIVISION AGREEMENT	
	#14 APR. 23, 2001	REC. NO. 2140577		COVENANTS & DISCLOSURE	
	#15 DEC. 20, 2001	REC. NO. 2234145		CONDOMINIUM DECLARATION	
	JAN. 29, 2002	REC. NO. 2248063		AMENDMENTS	
	MAR. 01, 2002	REC. NO. 2260157		AMENDMENTS	
	AUG. 20, 2002	REC. NO. 2321011		AMENDMENTS	
	DEC. 02, 2002	REC. NO. 2364645		AMENDMENTS	
	APR. 07, 2003	REC. NO. 2421544		AMENDMENTS	
	APR. 02, 2004	REC. NO. 2527256		RESOLUTION FOR HAZARD INSURANCE	
	JUL. 19, 2004	REC. NO. 2608639		RESOLUTION FOR HAZARD INSURANCE	
	#515-F0448297-170-RR0, AMENDMENT NO. 1	APR. 15, 1916	BK. 395,	PG. 124	TELEPHONE EASEMENT (EXACT LOCATION IS NOT SET FORTH)
		APR. 15, 1916	BK. 395,	PG. 125	TELEPHONE EASEMENT (EXACT LOCATION IS NOT SET FORTH)
		#11 JUL. 26, 2000	REC. NO. 2064118		SUBDIVISION AGREEMENT
#12 APR. 23, 2001		REC. NO. 2140577		COVENANTS & DISCLOSURE	
#13 DEC. 20, 2001		REC. NO. 2234145		CONDOMINIUM DECLARATION	
JAN. 29, 2002		REC. NO. 2248063		AMENDMENTS	
MAR. 01, 2002		REC. NO. 2260157		AMENDMENTS	
AUG. 20, 2002		REC. NO. 2321011		AMENDMENTS	
DEC. 02, 2002		REC. NO. 2364645		AMENDMENTS	
APR. 07, 2003		REC. NO. 2421544		AMENDMENTS	
APR. 02, 2004		REC. NO. 2527256		RESOLUTION & POLICY FOR HAZARD INSURANCE	
JUL. 19, 2004		REC. NO. 2608639		RESOLUTION FOR HAZARD INSURANCE	
#16 APR. 12, 2013		REC. NO. 3304418		PILOT AGREEMENT FOR BOULDER JUNCTION ACCESS	

**Depositing Certificate**  
SUBMITTED TO BOULDER COUNTY LAND USE FOR RECORDING ON THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 20\_\_\_\_.

MATCHLINE (SEE SHEET 2)

**Surveyor's Certificate**  
TO COBURN DEVELOPMENT, AND JVA INC.  
THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2011 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/ACSM LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS 1 AND 5 OF TABLE A THEREOF. THE FIELD WORK WAS COMPLETED ON APRIL 18, 2013.  
PURSUANT TO COLORADO STATE BOARD OF LICENSURE FOR PROFESSIONAL LAND SURVEYORS RULE 6.2.2 THE UNDERSIGNED FURTHER CERTIFIES THAT THIS MAP OR PLAT WAS PREPARED BY ME OR UNDER MY RESPONSIBLE CHARGE, IS ACCURATE TO THE BEST OF MY KNOWLEDGE, INFORMATION AND BELIEF, IS IN ACCORDANCE WITH APPLICABLE STANDARDS OF PRACTICE AND IS NOT A GUARANTEE OR WARRANTY, EITHER EXPRESSED OR IMPLIED.  
JOHN B. GUYTON COLORADO P.L.S. #16406  
CHAIRMAN & CEO, FLATIRON'S, INC.



REVISION	DATE	DESCRIPTION
1	04-26-13	REVISIONS PER JVA COMMENTS
2	07-26-13	REVISIONS PER CITY COMMENTS
3		
4		
5		
6		
7		
8		

ALTA/ACSM LAND TITLE SURVEY  
PREPARED FOR  
COBURN DEVELOPMENT & Others (See Note 3)  
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**Flatiron's, Inc.**  
Surveying, Engineering & Geomatics  
www.FlatironsInc.com  
3660 DOWNING ST  
BOULDER, CO 80501  
DENVER, CO 80205  
655 FOURTH AVE  
LONGMONT, CO 80501  
PH: (303) 443-7001  
PH: (303) 443-7001  
FAX: (303) 443-9830  
FAX: (303) 443-9830

JOB NUMBER:  
13-61,369  
DATE:  
04-18-2013  
DRAWN BY:  
E. DAVIS  
CHECKED BY:  
WW/NV/ETB

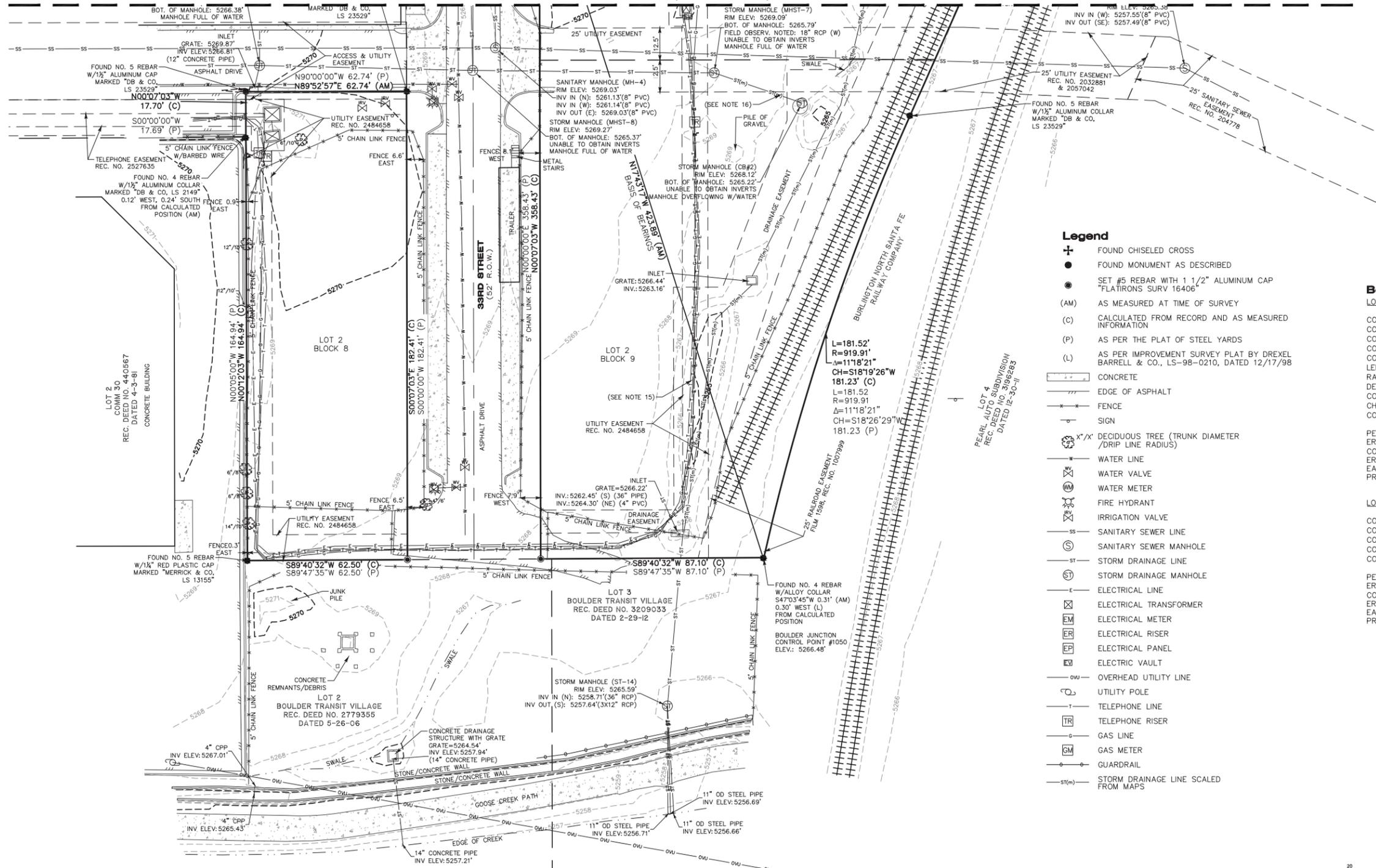
SHEET 1 OF 2

# ALTA/ACSM LAND TITLE SURVEY

LOT 2 OF BLOCK 8, AND LOTS 1 & 2 OF BLOCK 9, STEEL YARDS SUBDIVISION, LOCATED IN THE  
NORTHEAST QUARTER OF SECTION 29, TOWNSHIP 1 SOUTH, RANGE 70 WEST OF THE 6TH P.M.,  
CITY OF BOULDER, COUNTY OF BOULDER, STATE OF COLORADO

SHEET 2 OF 2

MATCHLINE (SEE SHEET 1)



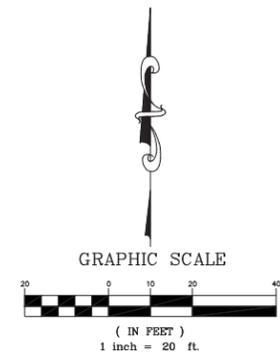
- Legend**
- + FOUND CHISELED CROSS
  - FOUND MONUMENT AS DESCRIBED
  - SET #5 REBAR WITH 1 1/2" ALUMINUM CAP  
FLATRONS SURV 16406
  - (AM) AS MEASURED AT TIME OF SURVEY
  - (C) CALCULATED FROM RECORD AND AS MEASURED INFORMATION
  - (P) AS PER THE PLAT OF STEEL YARDS
  - (L) AS PER IMPROVEMENT SURVEY PLAT BY DREXEL BARRELL & CO., LS-98-0210, DATED 12/17/98
  - CONCRETE
  - EDGE OF ASPHALT
  - FENCE
  - SIGN
  - X/X'X' DECIDUOUS TREE (TRUNK DIAMETER / DRIP LINE RADIUS)
  - WATER LINE
  - WATER VALVE
  - WATER METER
  - FIRE HYDRANT
  - IRRIGATION VALVE
  - SANITARY SEWER LINE
  - SANITARY SEWER MANHOLE
  - STORM DRAINAGE LINE
  - STORM DRAINAGE MANHOLE
  - ELECTRICAL LINE
  - ELECTRICAL TRANSFORMER
  - ELECTRICAL METER
  - ELECTRICAL RISER
  - ELECTRICAL PANEL
  - ELECTRIC VAULT
  - OVERHEAD UTILITY LINE
  - UTILITY POLE
  - TELEPHONE LINE
  - TELEPHONE RISER
  - GAS LINE
  - GAS METER
  - GUARDRAIL
  - STORM DRAINAGE LINE SCALED FROM MAPS

**Boundary Closure Report**  
LOTS 1 & 2, BLOCK 9

COURSE: N00°07'03"W	LENGTH: 358.43'
COURSE: N89°40'57"E	LENGTH: 88.43'
COURSE: N00°19'03"W	LENGTH: 28.50'
COURSE: N89°40'57"E	LENGTH: 152.49'
COURSE: S23°58'37"W	LENGTH: 236.11'
LENGTH:	181.52'
RADIUS:	919.91'
DELTA:	01°18'21"
COURSE:	S18°19'26"W
CHORD:	181.23'
COURSE: S89°40'32"W	LENGTH: 87.10'
PERIMETER:	1132.57'
ERROR CLOSURE:	0.00
COURSE:	S31°51'31"E
ERROR NORTH:	-0.002
EAST:	0.001
PRECISION 1:	1132580000.00

**LOT 2, BLOCK 8**

COURSE: N00°07'03"W	LENGTH: 17.70'
COURSE: N89°52'57"E	LENGTH: 62.74'
COURSE: S00°07'03"E	LENGTH: 182.41'
COURSE: S89°40'32"W	LENGTH: 62.50'
COURSE: N00°12'03"W	LENGTH: 164.94'
PERIMETER:	490.30'
ERROR CLOSURE:	0.00
COURSE:	N07°01'50"E
ERROR NORTH:	0.004
EAST:	0.001
PRECISION 1:	490290000.00



REVISION	DATE
1 - REVISIONS PER JVA COMMENTS	04-26-13
2 - REVISIONS PER CITY COMMENTS	07-26-13
3 - REVISIONS PER JVA COMMENTS	
4 - REVISIONS PER CITY COMMENTS	
5 - REVISIONS PER JVA COMMENTS	
6 - REVISIONS PER CITY COMMENTS	
7 - REVISIONS PER JVA COMMENTS	
8 - REVISIONS PER CITY COMMENTS	

ALTA/ACSM LAND TITLE SURVEY  
PREPARED FOR  
JVA &  
Others (See Note 3)  
COPYRIGHT 2013 FLATRONS, INC.

**Flatrons, Inc.**  
Surveying, Engineering & Geomatics  
www.FlatronsInc.com  
3825 IRIS AVE, STE. 395 3660 DOWNING ST  
BOULDER, CO 80501 DENVER, CO 80205  
PH: (303) 443-7001 PH: (303) 443-7001  
PH: (303) 778-1733 PH: (303) 443-9830  
FAX: (303) 778-4355 FAX: (303) 443-9830



JOB NUMBER:  
13-61,369  
DATE:  
04-17-2013  
DRAWN BY:  
E. DAVIS  
CHECKED BY:  
WW/NV/ETB

SHEET 2 OF 2

BY: DAVIS FILE: 1369-ALTA(C13).DWG DATE: 7/26/2013 2:48 PM



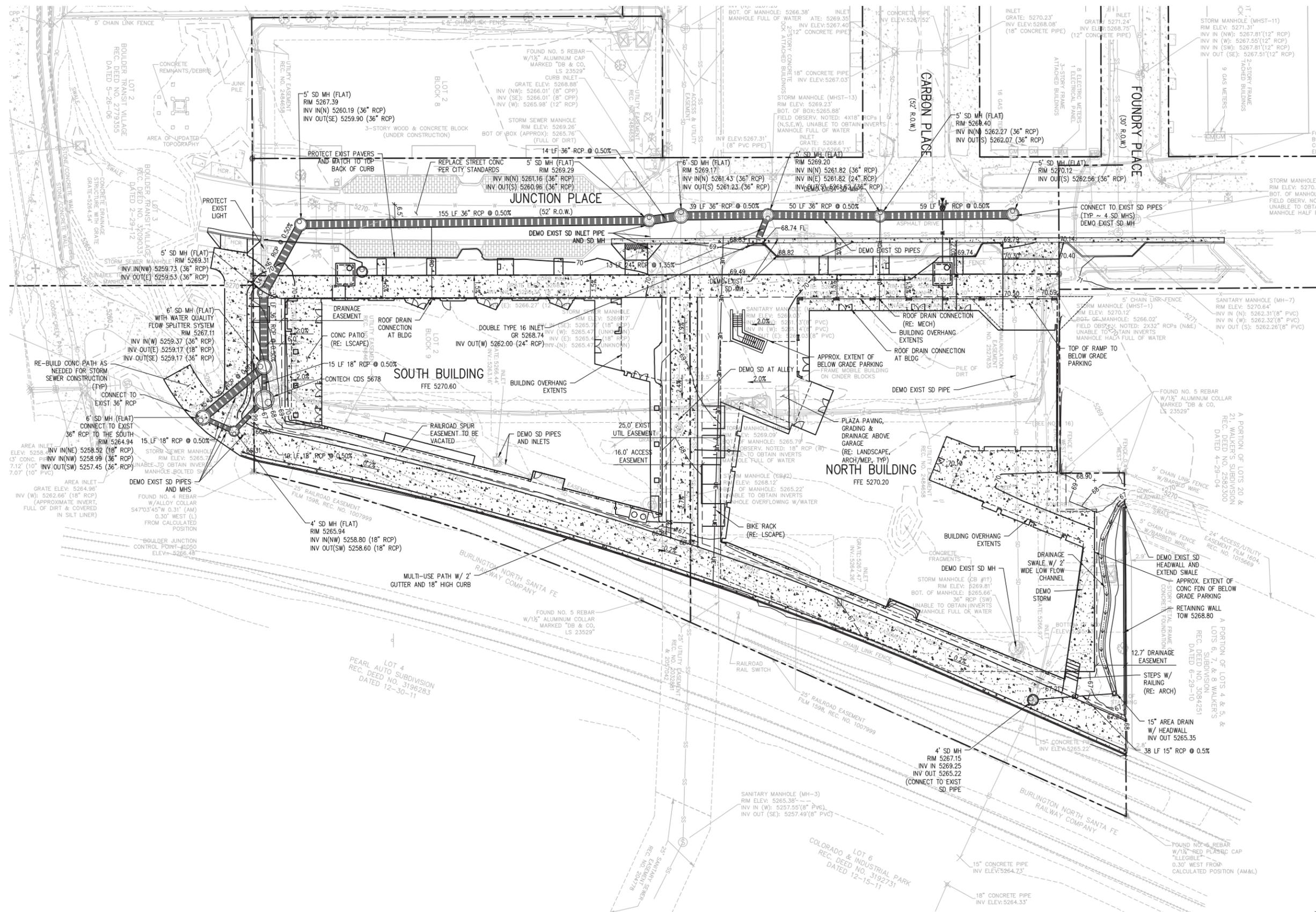


**COBURN ARCHITECTURE**

3020 Carbon Place #203  
Boulder, Colorado  
p: 303-442-3351  
f: 303-447-3933

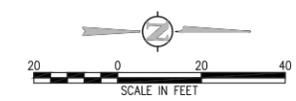


JVA, Incorporated 1319 Spruce Street  
Boulder, CO 80302 Phone: 303.444.1951  
Fax: 303.444.1957 Email: info@jva.com



**SITE REVIEW CORRECTIONS**  
07/16/2015

SHEET NO. **C1.0**  
**PRELIMINARY**  
 Agenda Item 5A Page 2 of 73  
**STORMWATER PLAN**

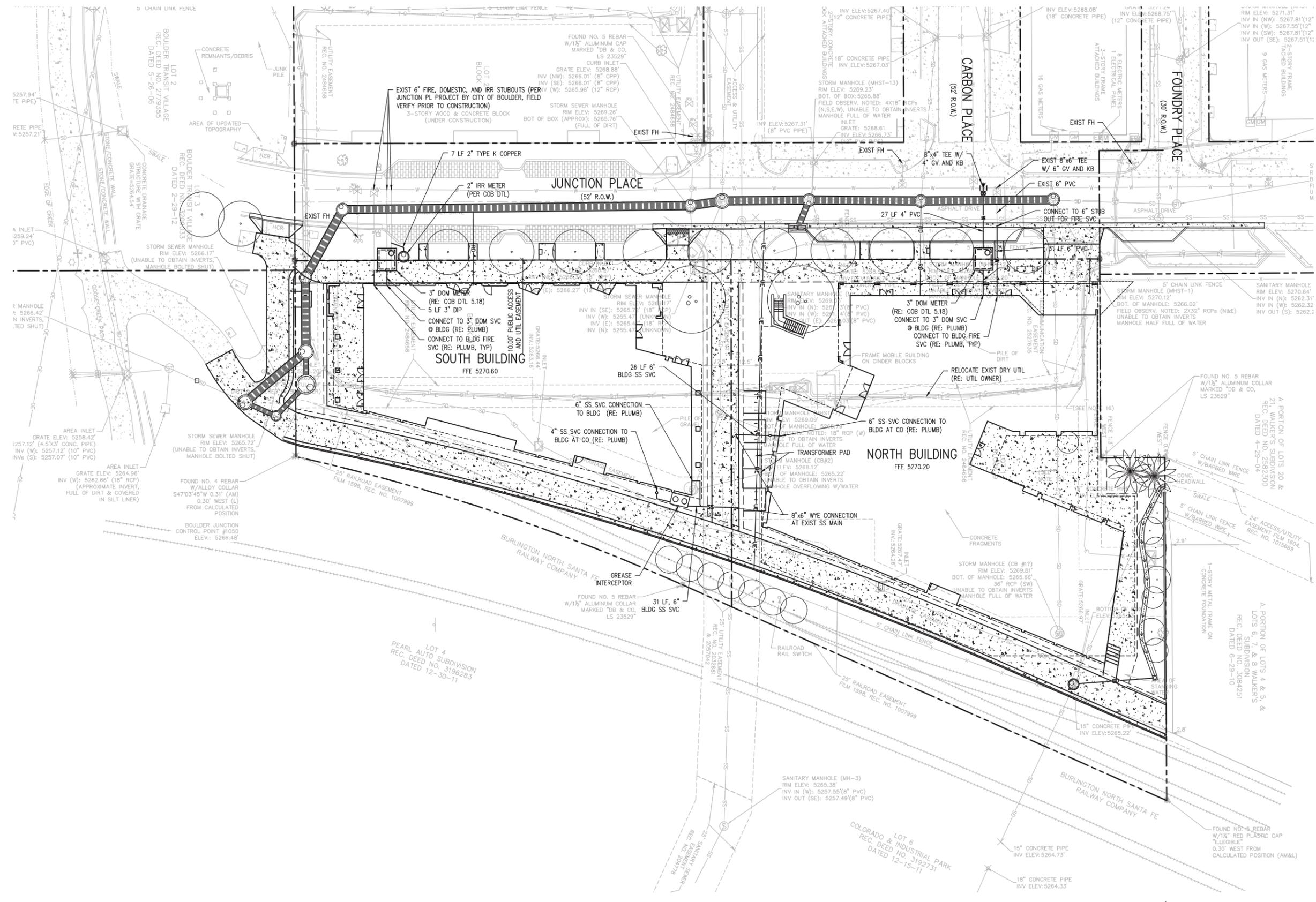




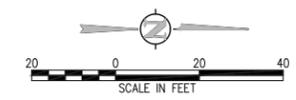
3020 Carbon Place #203  
Boulder, Colorado  
p: 303-442-3351  
f: 303-447-3933



JVA, Incorporated 1319 Spruce Street  
Boulder, CO 80302 Phone: 303.444.1951  
Fax: 303.444.1957 E-mail: info@jva.com



**SITE REVIEW  
CORRECTIONS**  
07/16/2015

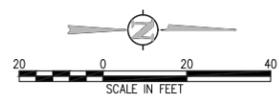
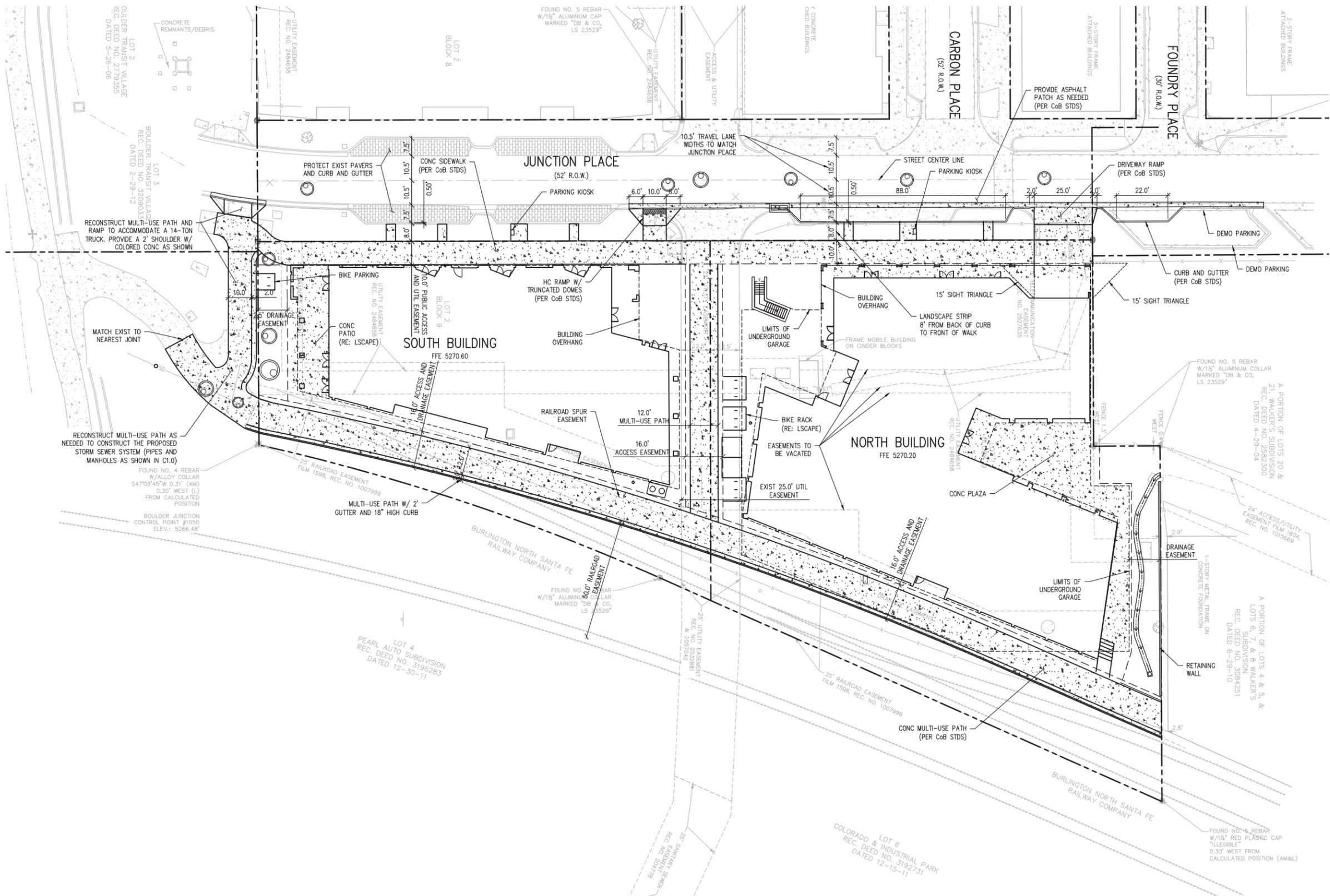


SHEET No.  
**C2.0**  
PRELIMINARY  
UTILITY PLAN

Agenda Item 5A

**BOULDER COMMONS**

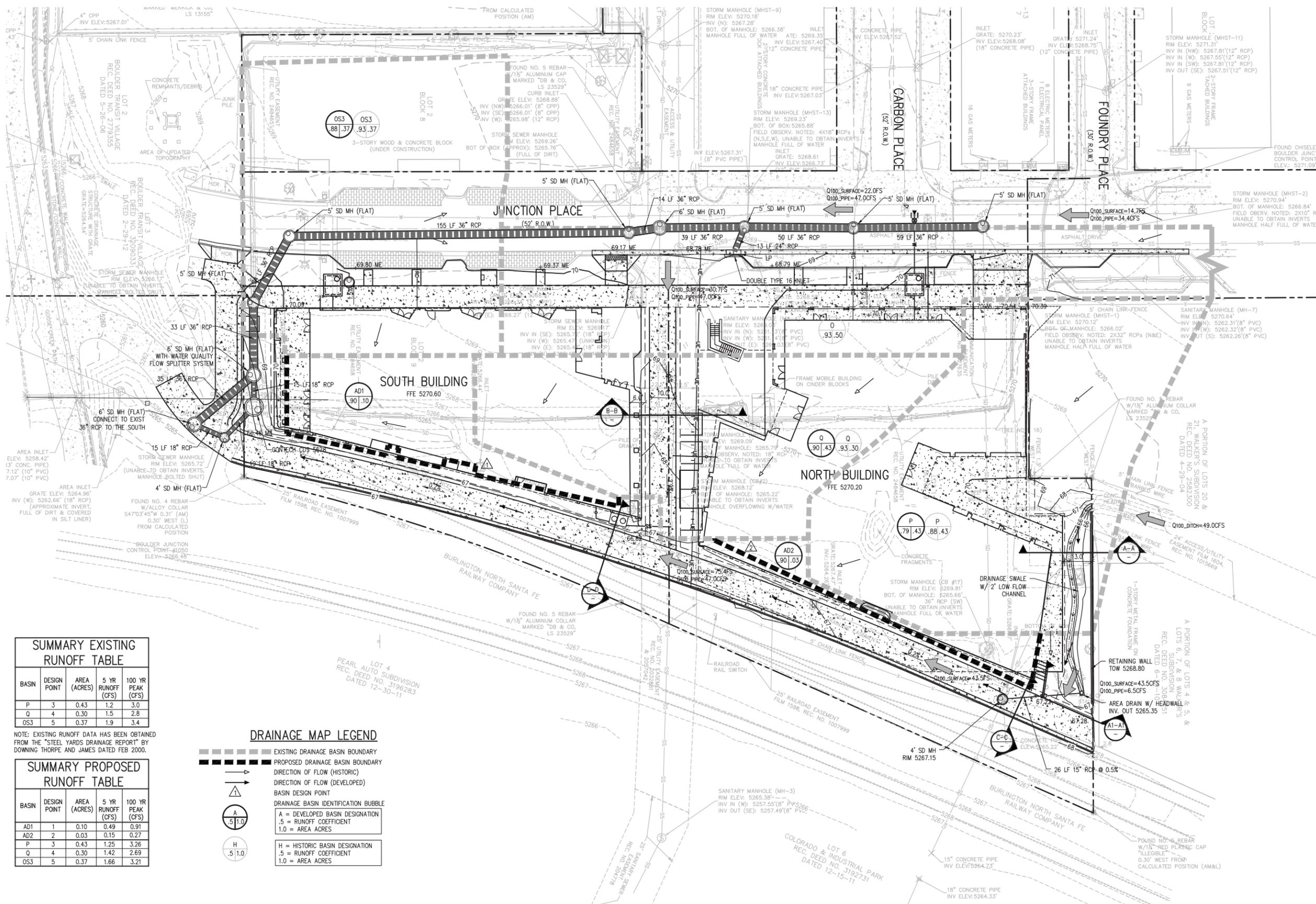
2440 - 2490 Junction Pl. Boulder, CO



**SITE REVIEW  
CORRECTIONS**  
07/16/2015

SHEET No.  
**C3.0**  
PRELIMINARY  
HORIZONTAL CONTROL PLAN

Agenda Item 5A



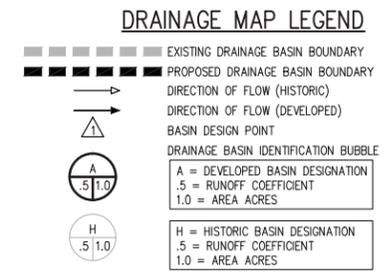
**SUMMARY EXISTING RUNOFF TABLE**

Basin	Design Point	Area (Acres)	5 Yr Runoff (CFS)	100 Yr Peak (CFS)
P	3	0.43	1.2	3.0
Q	4	0.30	1.5	2.8
OS3	5	0.37	1.9	3.4

NOTE: EXISTING RUNOFF DATA HAS BEEN OBTAINED FROM THE "STEEL YARDS DRAINAGE REPORT" BY DOWNING THORPE AND JAMES DATED FEB 2000.

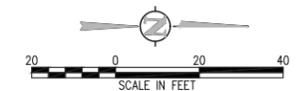
**SUMMARY PROPOSED RUNOFF TABLE**

Basin	Design Point	Area (Acres)	5 Yr Runoff (CFS)	100 Yr Peak (CFS)
AD1	1	0.10	0.49	0.91
AD2	2	0.03	0.15	0.27
P	3	0.43	1.25	3.26
Q	4	0.30	1.42	2.69
OS3	5	0.37	1.66	3.21



**SITE REVIEW CORRECTIONS**  
07/16/2015

SHEET NO.  
**FIG-1**  
PRELIMINARY  
DEVELOPED DRAINAGE MAP





STONE ACCENT PAVING BANDS

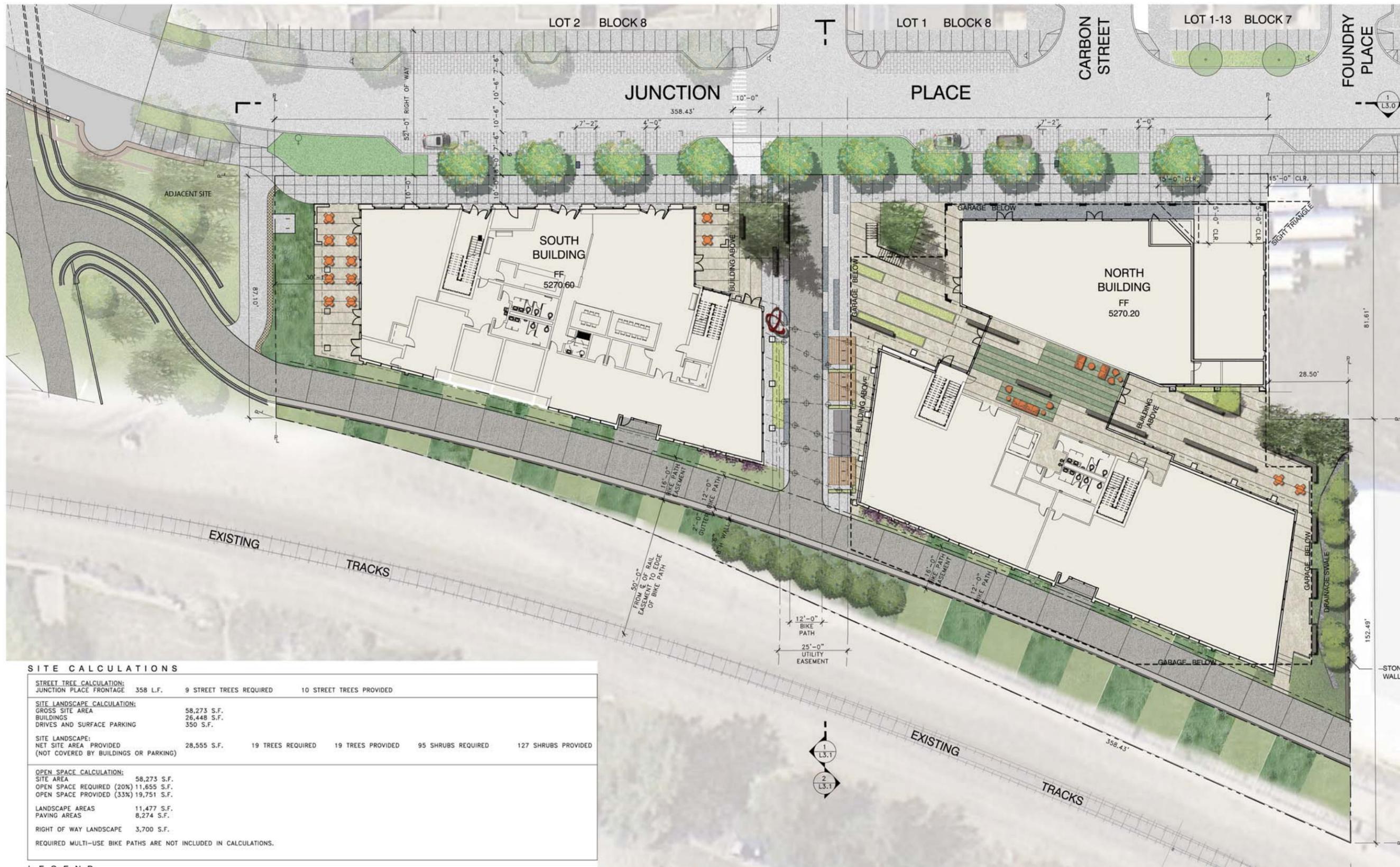


BIKE RACKS



STONE SLAB BENCHES





**SITE CALCULATIONS**

<b>STREET TREE CALCULATION:</b>					
JUNCTION PLACE FRONTAGE	358 L.F.	9 STREET TREES REQUIRED	10 STREET TREES PROVIDED		
<b>SITE LANDSCAPE CALCULATION:</b>					
GROSS SITE AREA	58,273 S.F.				
BUILDINGS	26,448 S.F.				
DRIVES AND SURFACE PARKING	350 S.F.				
<b>SITE LANDSCAPE:</b>					
NET SITE AREA PROVIDED (NOT COVERED BY BUILDINGS OR PARKING)	28,555 S.F.	19 TREES REQUIRED	19 TREES PROVIDED	95 SHRUBS REQUIRED	127 SHRUBS PROVIDED
<b>OPEN SPACE CALCULATION:</b>					
SITE AREA	58,273 S.F.				
OPEN SPACE REQUIRED (20%)	11,655 S.F.				
OPEN SPACE PROVIDED (33%)	19,751 S.F.				
LANDSCAPE AREAS	11,477 S.F.				
PAVING AREAS	8,274 S.F.				
RIGHT OF WAY LANDSCAPE	3,700 S.F.				
REQUIRED MULTI-USE BIKE PATHS ARE NOT INCLUDED IN CALCULATIONS.					

**LEGEND**

CONCRETE SIDEWALK PAVING	GRAVEL DRAINAGE SWALE	STONE SLAB BENCHES	GROUND COVER PLANTING TYPE 1	EXISTING TREE TO REMAIN	PARKING KIOSK
BIKE PATH CONCRETE PAVING (STANDARD)	RETAINING WALL	(28) BIKE PARKING	GROUND COVER PLANTING TYPE 2	EXISTING FIRE HYDRANT	LOBBY CARPET
STONE PAVING	TURF BLOCK	BUILDING TRANSFORMER	FLUSH PLANTER ON STRUCTURE	EXISTING STREET SIGNAGE	TRELLIS
STONE ACCENT PAVING BANDS	LARGE SCALE ART OR STONE MONOLITH	VINE PLANTINGS	RAILING	SUSPENDED CYLINDER LIGHTS	
PROPOSED TREES					

**SITE REVIEW CORRECTION**  
07/16/2015

SHEET No.  
**L2.0**



JUNCTION PLACE STREET FRONTAGE SECTION LOOKING EAST

SCALE: 3/8" = 1'-0"

2



JUNCTION PLACE STREET FRONTAGE SECTION LOOKING EAST

SCALE: 3/8" = 1'-0"

1

**SITE REVIEW**  
**CORRECTION**  
07/16/2015

SHEET No.  
**L3.0**  
SECTIONS AND ELEVATIONS



**COBURN**  
ARCHITECTURE

3020 Carbon Place #203  
Boulder, Colorado  
p: 303-442-3351  
f: 303-447-3933



Landscape/ Architecture  
2677 Mission Street, #200  
San Francisco, CA  
p: 415-285-3614  
gls@glsarch.com



7'-6" SIDEWALK   7'-6" PARKING   10'-6" DRIVE LANE   10'-6" DRIVE LANE   7'-1" PARKING   7'-6" PLANTING 6" CURB   10'-0" SIDEWALK   19'-4" GARAGE PARKING ENTRY   10'-0" TRELLIS   3'-0" PLANTER   10'-0" TRELLIS   3'-0" PLANTER   17'-4" TRANSFORMER   10'-0" TRELLIS   3'-0" PLANTER   12'-0" BIKE PATH   CONCRETE PAVING   PLANTING AREA   BIKE PATH AND PLANTING BEYOND   EXISTING TRACKS

52'-0" JUNCTION PLACE RIGHT OF WAY   50'-0" EASEMENT

**SECTION THROUGH MID-BLOCK PASEO LOOKING NORTH**

SCALE: 1/8" = 1'-0"

2



PLANTING AREA   CONCRETE PAVING   12'-0" BIKE PATH   CONCRETE PAVING   STONE PAVERS   CONCRETE PAVING   STONE PAVERS   10'-0" SIDEWALK   7'-6" PLANTING 6" CURB   7'-1" PARKING   10'-6" DRIVE LANE   10'-6" DRIVE LANE   7'-6" PARKING   7'-6" SIDEWALK

52'-0" JUNCTION PLACE RIGHT OF WAY

**SECTION THROUGH MID-BLOCK PASEO LOOKING SOUTH**

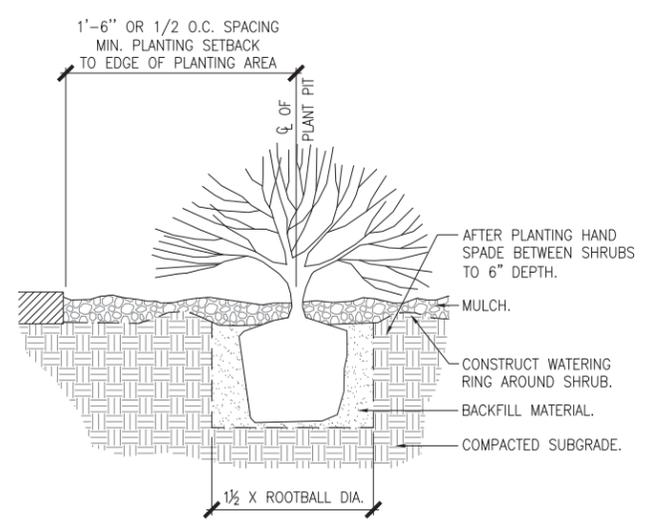
SCALE: 1/8" = 1'-0"

1

**SITE REVIEW CORRECTION**  
07/16/2015

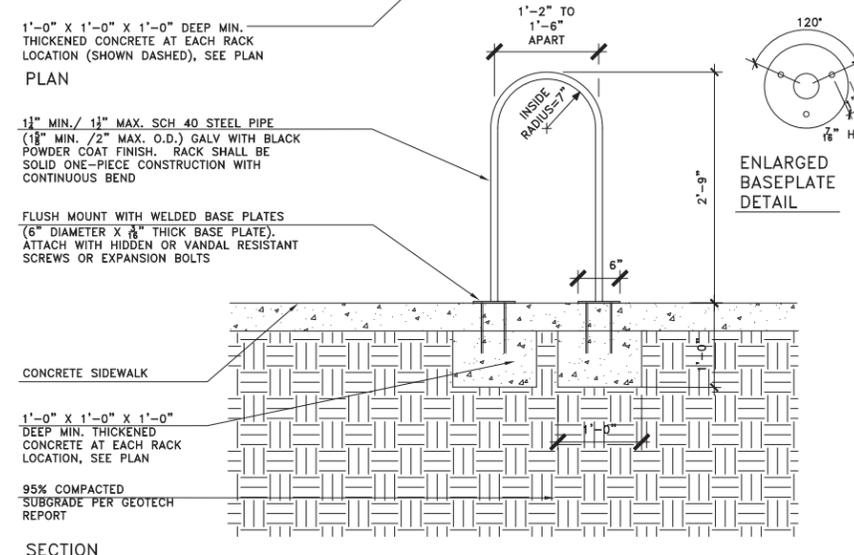
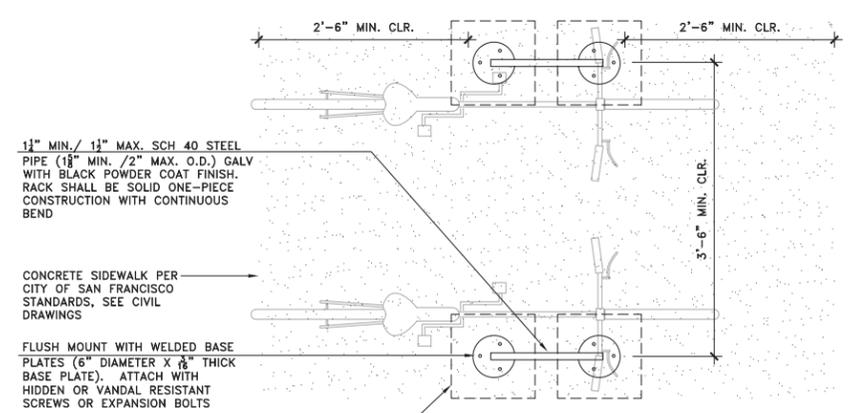
SHEET No.  
**L3.1**

Agenda Item SECTION 2 AND 3



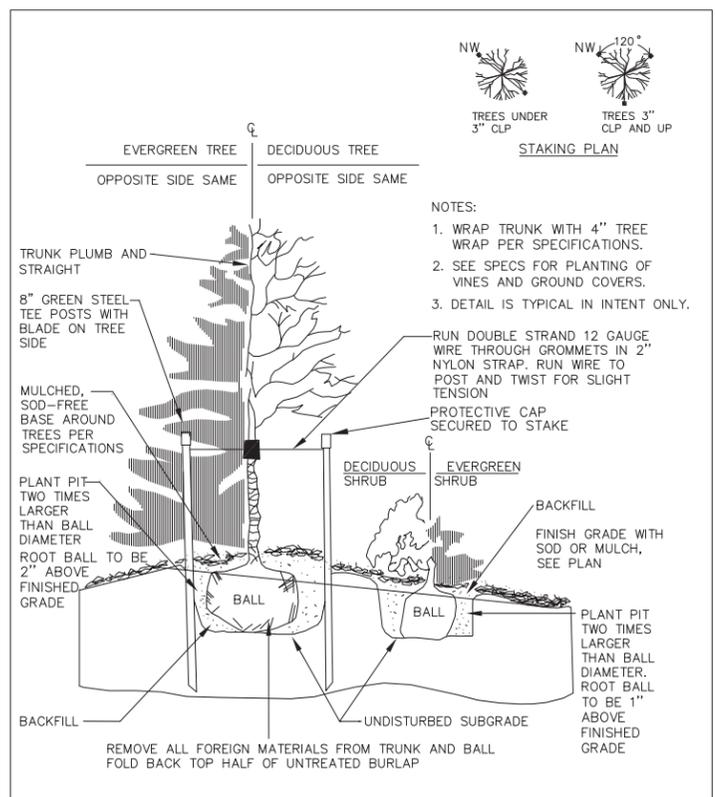
**SHRUB PLANTING DETAIL**  
SCALE: N.T.S.

7



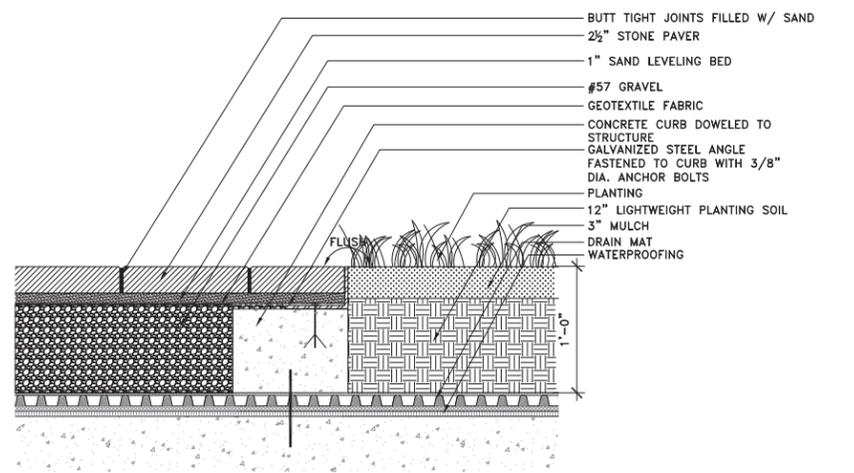
**BIKE RACK**  
SCALE: 1" = 1'-0"

5



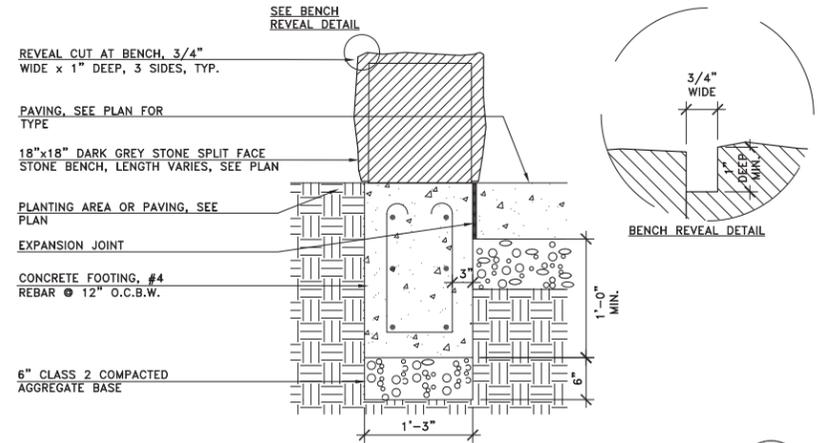
**TREE PLANTING DETAIL**  
SCALE: N.T.S.

6



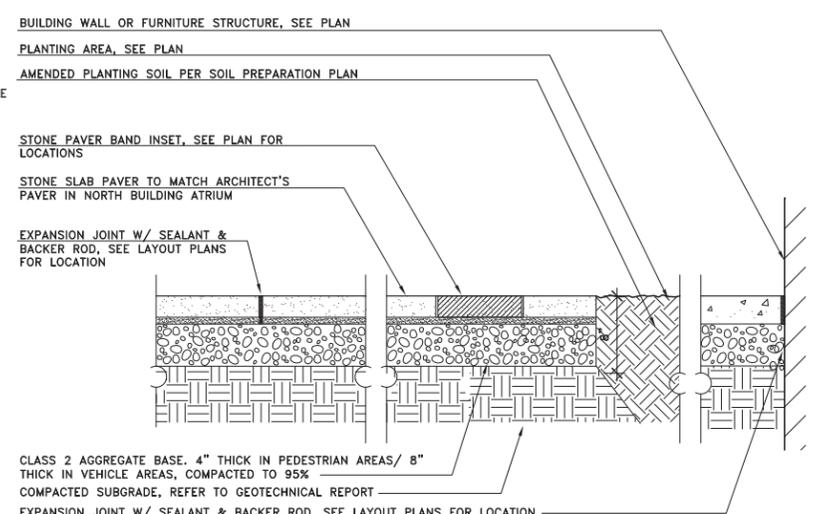
**FLUSH PLANTING ON STRUCTURE**  
SCALE: 1 1/2" = 1'-0"

4



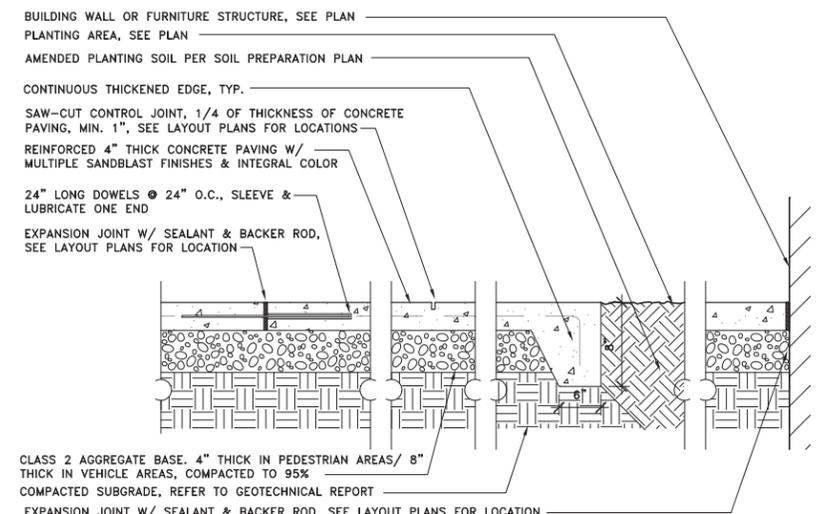
**STONE SLAB BENCH**  
SCALE: 1" = 1'-0"

3



**STONE PAVING WITH STONE ACCENT BANDS**  
SCALE: 1" = 1'-0"

2



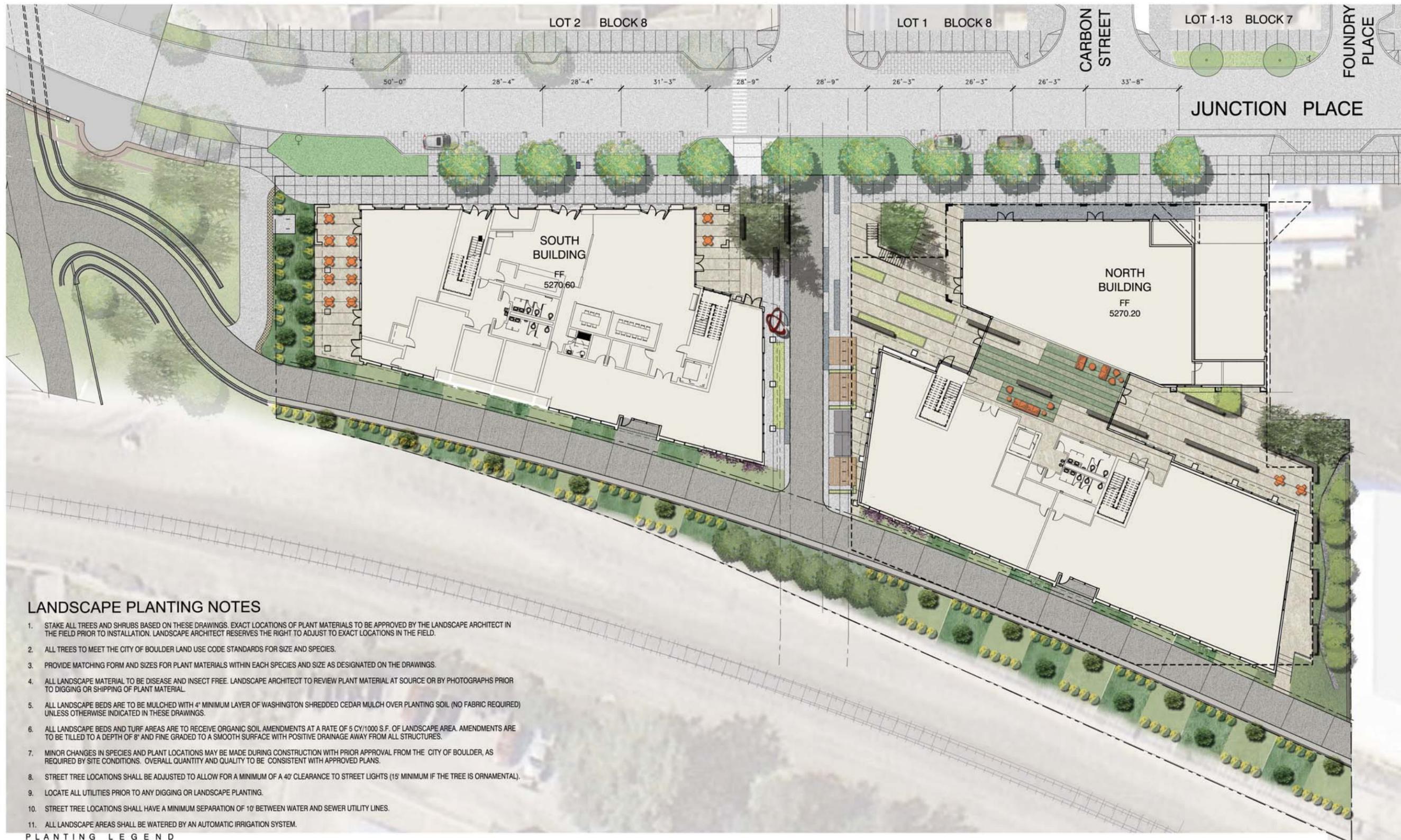
**CONCRETE PAVING/ BIKE PATH PAVING**  
SCALE: 1" = 1'-0"

1

DRAWN BY: JSH	CITY OF BOULDER, COLORADO <b>TREES AND SHRUBS</b> PLANTING DETAIL	ISSUED: JULY 2, 1998
CHECKED BY: SRW		REVISED: OCT. 17, 2000
APPROVED BY: DIRECTOR OF PUBLIC WORKS		DRAWING NO. 3.02

**SITE REVIEW CORRECTION**  
07/16/2015

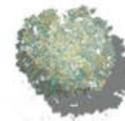
SHEET No.  
**L4.0**



**LANDSCAPE PLANTING NOTES**

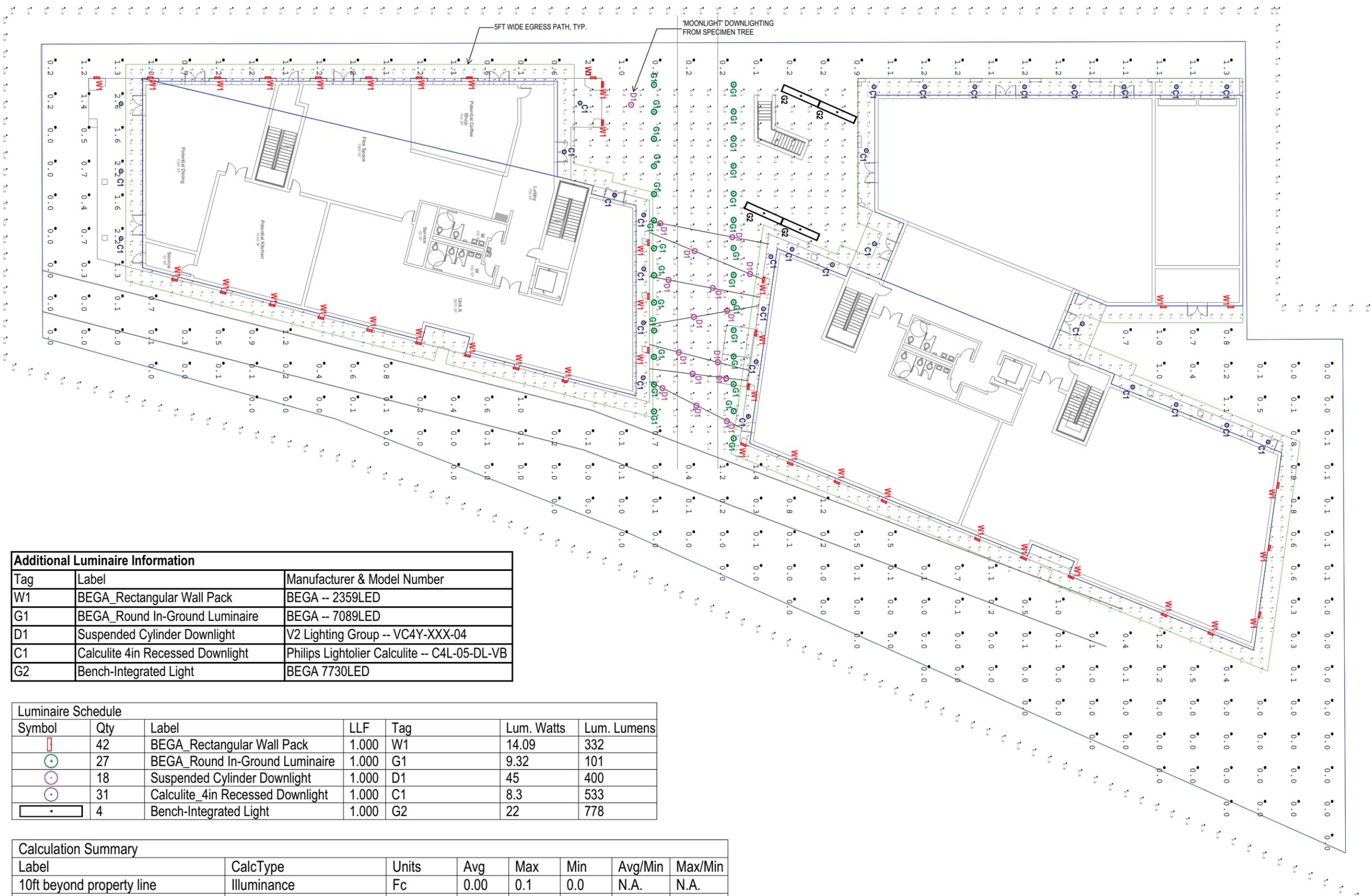
1. STAKE ALL TREES AND SHRUBS BASED ON THESE DRAWINGS. EXACT LOCATIONS OF PLANT MATERIALS TO BE APPROVED BY THE LANDSCAPE ARCHITECT IN THE FIELD PRIOR TO INSTALLATION. LANDSCAPE ARCHITECT RESERVES THE RIGHT TO ADJUST TO EXACT LOCATIONS IN THE FIELD.
2. ALL TREES TO MEET THE CITY OF BOULDER LAND USE CODE STANDARDS FOR SIZE AND SPECIES.
3. PROVIDE MATCHING FORM AND SIZES FOR PLANT MATERIALS WITHIN EACH SPECIES AND SIZE AS DESIGNATED ON THE DRAWINGS.
4. ALL LANDSCAPE MATERIAL TO BE DISEASE AND INSECT FREE. LANDSCAPE ARCHITECT TO REVIEW PLANT MATERIAL AT SOURCE OR BY PHOTOGRAPHS PRIOR TO DIGGING OR SHIPPING OF PLANT MATERIAL.
5. ALL LANDSCAPE BEDS ARE TO BE MULCHED WITH 4" MINIMUM LAYER OF WASHINGTON SHREDDED CEDAR MULCH OVER PLANTING SOIL. (NO FABRIC REQUIRED) UNLESS OTHERWISE INDICATED IN THESE DRAWINGS.
6. ALL LANDSCAPE BEDS AND TURF AREAS ARE TO RECEIVE ORGANIC SOIL AMENDMENTS AT A RATE OF 5 CY/1000 S.F. OF LANDSCAPE AREA. AMENDMENTS ARE TO BE TILLED TO A DEPTH OF 8" AND FINE GRADED TO A SMOOTH SURFACE WITH POSITIVE DRAINAGE AWAY FROM ALL STRUCTURES.
7. MINOR CHANGES IN SPECIES AND PLANT LOCATIONS MAY BE MADE DURING CONSTRUCTION WITH PRIOR APPROVAL FROM THE CITY OF BOULDER, AS REQUIRED BY SITE CONDITIONS. OVERALL QUANTITY AND QUALITY TO BE CONSISTENT WITH APPROVED PLANS.
8. STREET TREE LOCATIONS SHALL BE ADJUSTED TO ALLOW FOR A MINIMUM OF A 40' CLEARANCE TO STREET LIGHTS (15' MINIMUM IF THE TREE IS ORNAMENTAL).
9. LOCATE ALL UTILITIES PRIOR TO ANY DIGGING OR LANDSCAPE PLANTING.
10. STREET TREE LOCATIONS SHALL HAVE A MINIMUM SEPARATION OF 10' BETWEEN WATER AND SEWER UTILITY LINES.
11. ALL LANDSCAPE AREAS SHALL BE WATERED BY AN AUTOMATIC IRRIGATION SYSTEM.

**PLANTING LEGEND**

 <p>CELTIS OCCIDENTALIS HACKBERRY WATER USE - LOW ALTERNATE: QUERCUS BICOLOR SWAMP WHITE OAK WATER USE - MEDIUM 4" CALIPER B&amp;B HEIGHT - 18' QUANTITY - 10</p>	 <p>PICEA ABIES 'CUPRESSINS' NORWAY SPRUCE WATER USE - MEDIUM 8" CALIPER B&amp;B SPECIMEN HEIGHT - 20' QUANTITY - 1</p>	 <p>BETULA PLATYPHYLLA 'WHITESPIRE' MOUNTAIN BIRCH WATER USE - LOW 2 1/2" CALIPER B&amp;B HEIGHT - 15' QUANTITY - 8</p>	 <p>CEPHALOTAXUS HARRINGTONIA 'PROSTRATA' JAPANESE PLUM YEW WATER USE - MEDIUM HEIGHT - 2.5' B&amp;B QUANTITY - 104</p>	 <p>MAHONIA REPENS/ CREEPING HOLLY PERSICARIA AFFINS / HIMALAYAN BORDER JEWELL AJUGA REPTANS / CARPET BUGLE ANEMONE SYLVESTRIS / SNOW DROP ANEMONE 1 GALLON @ 12" ON CENTER HEIGHT - 1.5' WATER USE - LOW</p>	 <p>MISCANTHUS SINENSIS 'MORNING LIGHT' JAPANESE SILVER GRASS ALTERNATIVE: CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER; FEATHER REED GRASS 1 GALLON @ 3'-0" ON CENTER HEIGHT - 3'-4' WATER USE - MEDIUM</p>	 <p>FESTUCA IDAHOENSIS 'SISKIYOU BLUE' BLUE FESCUE ALTERNATIVE: HELICTOTRCHON SPMPREVIRENS BLUE AVENA 1 GALLON @ 12" ON CENTER HEIGHT - 1.5-2" WATER USE - LOW</p>
 <p>ABIES CONCOLOR WHITE FIR WATER USE - HIGH 2 3/4" CALIPER B&amp;B HEIGHT - 15' QUANTITY - 3</p>	 <p>CORNUS STOLONIFERA 'ISANTI' REDTWIG DOGWOOD WATER USE - HIGH HEIGHT - 3'-4' B&amp;B QUANTITY - 23</p>	 <p>JUNIPERUS SCOPULORUM 'WHICHITA BLUE' JUNIPER WATER USE - LOW 10 GALLON HEIGHT - 10' QUANTITY - 7</p>	 <p>STREET TREE UNDERSTORY: RIBES ALPINUM 'GREEN MOUND' GREEN MOUND CURRANT 1 GALLON @ 2'-0" ON CENTER HEIGHT - 2' WATER USE - MEDIUM</p>			

**SITE REVIEW  
CORRECTION**  
07/16/2015

SHEET No.  
**L5.0**



**Additional Luminaire Information**

Tag	Label	Manufacturer & Model Number
W1	BEGA_Rectangular Wall Pack	BEGA -- 2359LED
G1	BEGA_Round In-Ground Luminaire	BEGA -- 7089LED
D1	Suspended Cylinder Downlight	V2 Lighting Group -- VC4Y-XXX-04
C1	Calculite 4in Recessed Downlight	Philips Lightolier Calculite -- C4L-05-DL-VB
G2	Bench-Integrated Light	BEGA 7730LED

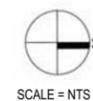
**Luminaire Schedule**

Symbol	Qty	Label	LLF	Tag	Lum. Watts	Lum. Lumens
W1	42	BEGA_Rectangular Wall Pack	1.000	W1	14.09	332
G1	27	BEGA_Round In-Ground Luminaire	1.000	G1	9.32	101
D1	18	Suspended Cylinder Downlight	1.000	D1	45	400
C1	31	Calculite_4in Recessed Downlight	1.000	C1	8.3	533
G2	4	Bench-Integrated Light	1.000	G2	22	778

**Calculation Summary**

Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
10ft beyond property line	Illuminance	Fc	0.00	0.1	0.0	N.A.	N.A.
Bike Path & Plaza	Illuminance	Fc	2.30	8.6	0.0	N.A.	N.A.
Egress Path & Entries_North Bldg	Illuminance	Fc	2.66	8.2	0.1	26.60	82.00
Egress Path & Entries_South Bldg	Illuminance	Fc	2.23	6.9	0.1	22.30	69.00
Site_Other Common Areas	Illuminance	Fc	0.39	2.6	0.0	N.A.	N.A.

**SITE REVIEW  
CORRECTIONS**  
07/16/2015



SCALE = NTS

SHEET No.  
**E1.0**

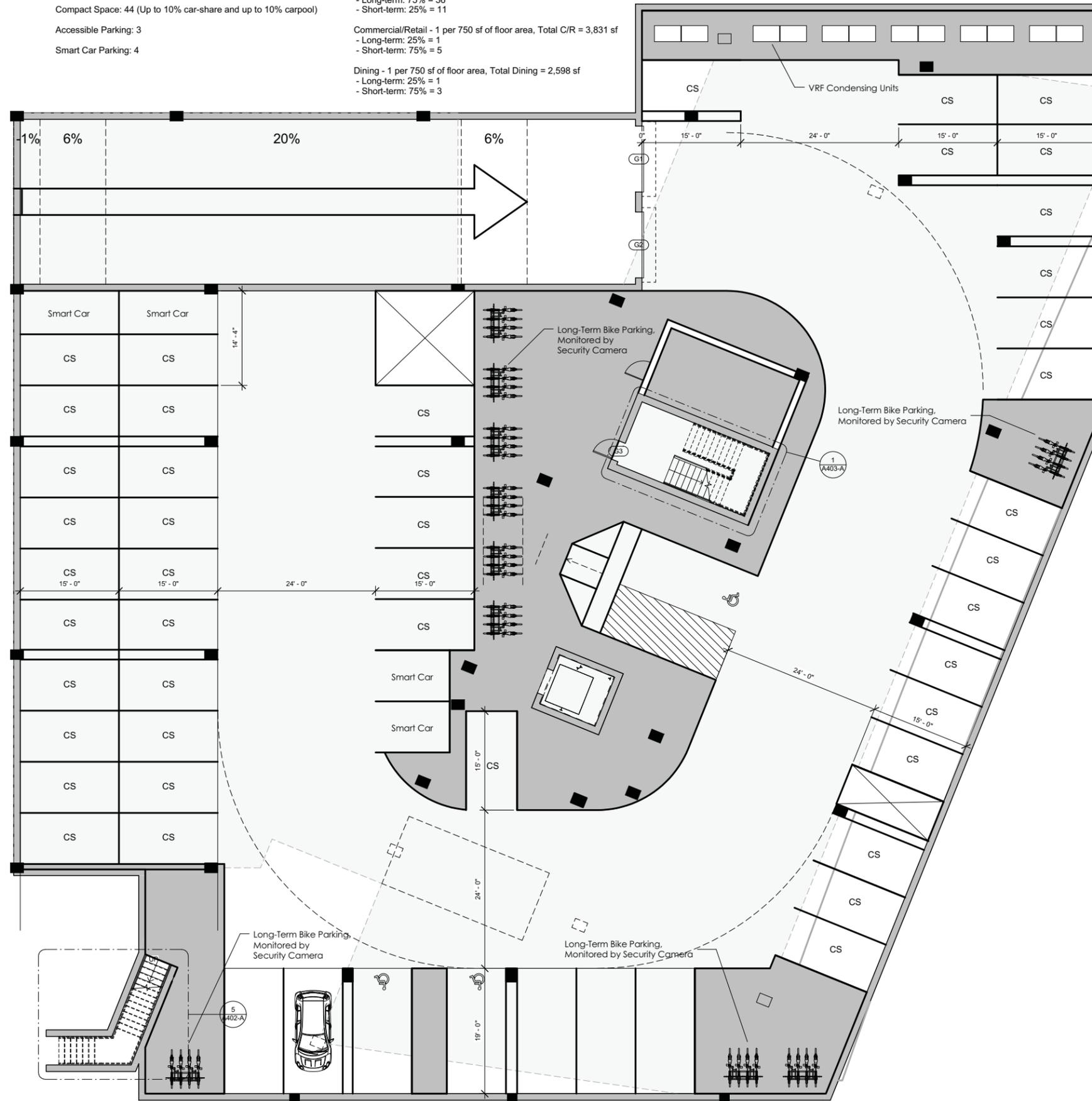
Agenda Items 5A LIGHTING METRIC ANALYSIS

# 52 Total Spaces

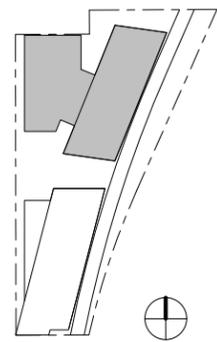
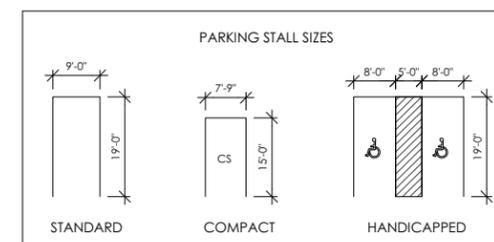
- Standard Spaces: 5
- Compact Space: 44 (Up to 10% car-share and up to 10% carpool)
- Accessible Parking: 3
- Smart Car Parking: 4

# Bike Parking Requirements

- Office - 1 per 1,500 sf of floor area, Total Office = 69,516 sf
  - Long-term: 75% = 36
  - Short-term: 25% = 11
- Commercial/Retail - 1 per 750 sf of floor area, Total C/R = 3,831 sf
  - Long-term: 25% = 1
  - Short-term: 75% = 5
- Dining - 1 per 750 sf of floor area, Total Dining = 2,598 sf
  - Long-term: 25% = 1
  - Short-term: 75% = 3



PARKING INFORMATION	
TOTAL SPACES REQUIRED PER 9-3	0
STANDARD SPACES	5
COMPACT SPACES	44
ACCESSIBLE SPACES	3
SMART CAR SPACES	4
TOTAL SPACES PROVIDED	56
TOTAL LT BICYCLE SPACES REQUIRED	38
TOTAL ST BICYCLE SPACES REQUIRED	19
LT BICYCLE PARKING PROVIDED	40
ST BICYCLE PARKING PROVIDED	20
CARPPOOL SPACES	3, UP TO 10%
CAR-SHARE SPACES	3, UP TO 10%



**SITE REVIEW CORRECTIONS**  
07/16/2015

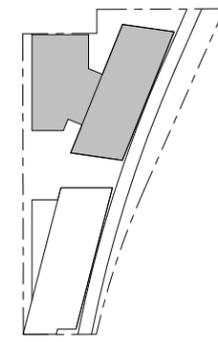
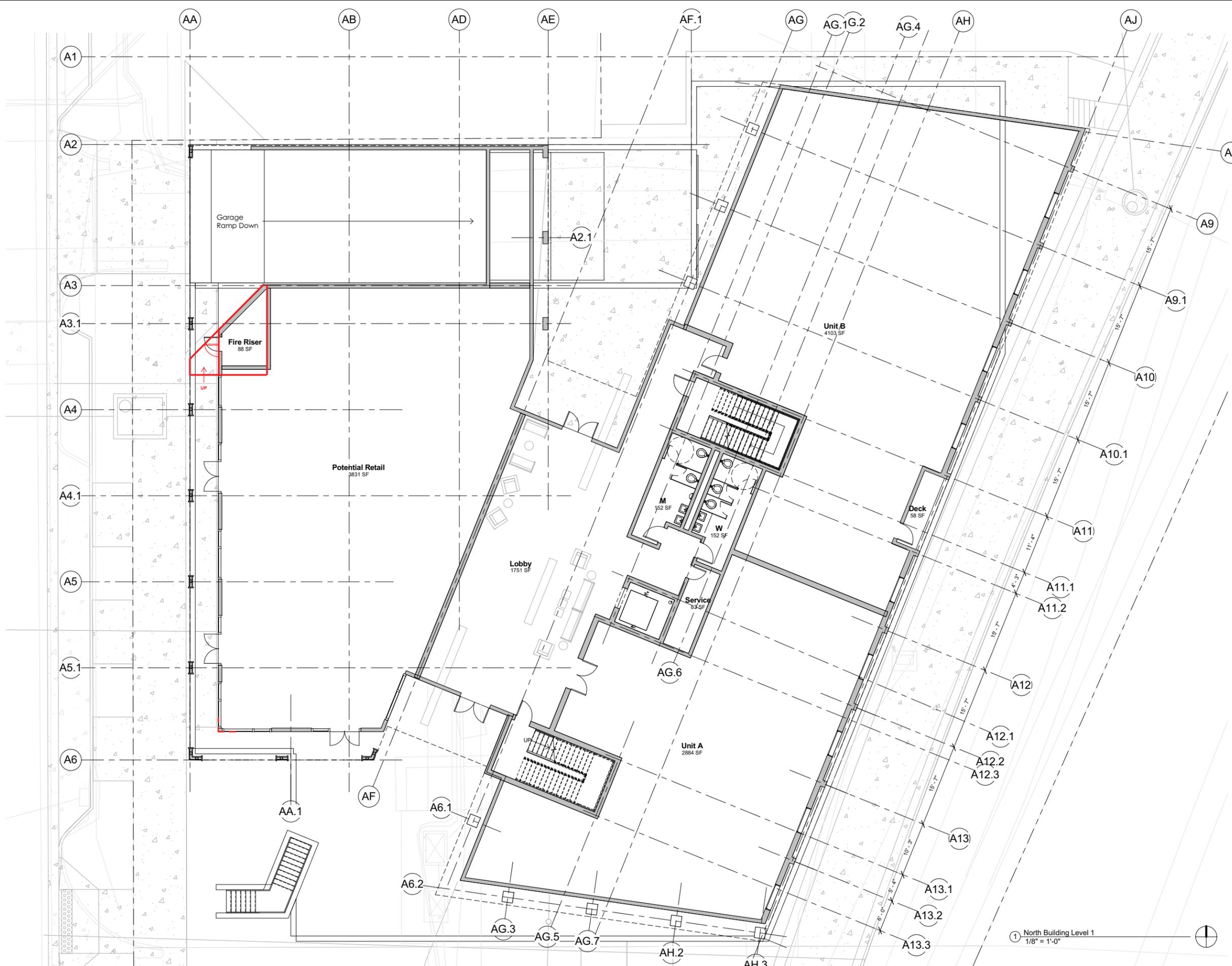
SHEET No.  
**A1.1**  
GARAGE PLAN

1 Garage Floor Plan - Dependent 1  
1/8" = 1'-0"



**COBURN**  
ARCHITECTURE

3020 Carbon Place #203  
Boulder, Colorado  
p: 303-442-3351  
f: 303-447-3933



KEY PLAN

**SITE REVIEW  
CORRECTIONS**  
07/16/2015

SHEET No.  
**A1.2**

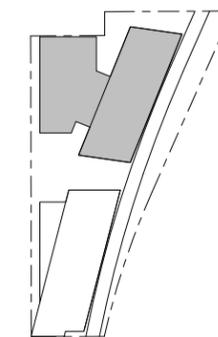
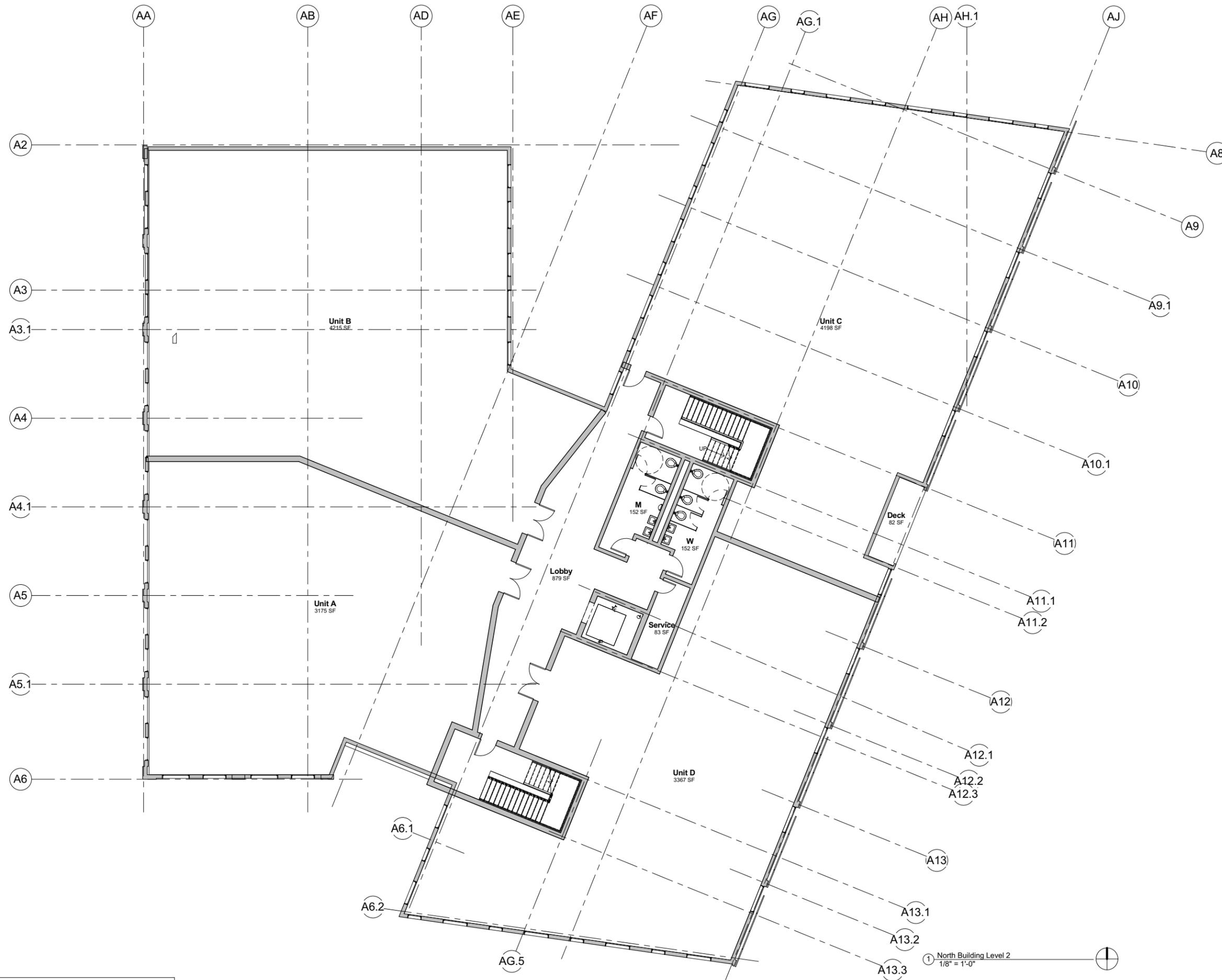
Agenda Item 5A Page 34 of 73  
**NORTH BUILDING LEVEL 1**

1 North Building Level 1  
1/8" = 1'-0"



**COBURN**  
ARCHITECTURE

3020 Carbon Place #203  
Boulder, Colorado  
p: 303-442-3351  
f: 303-447-3933



KEY PLAN

**SITE REVIEW**  
**CORRECTIONS**  
07/16/2015

SHEET No.  
**A1.3**

Agenda Item 5A Page 35 of 73

**NORTH BUILDING LEVEL 2**

**BOULDER COMMONS**

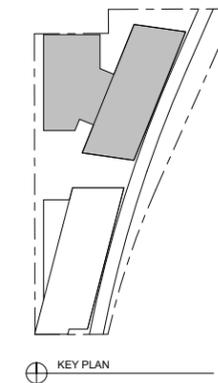
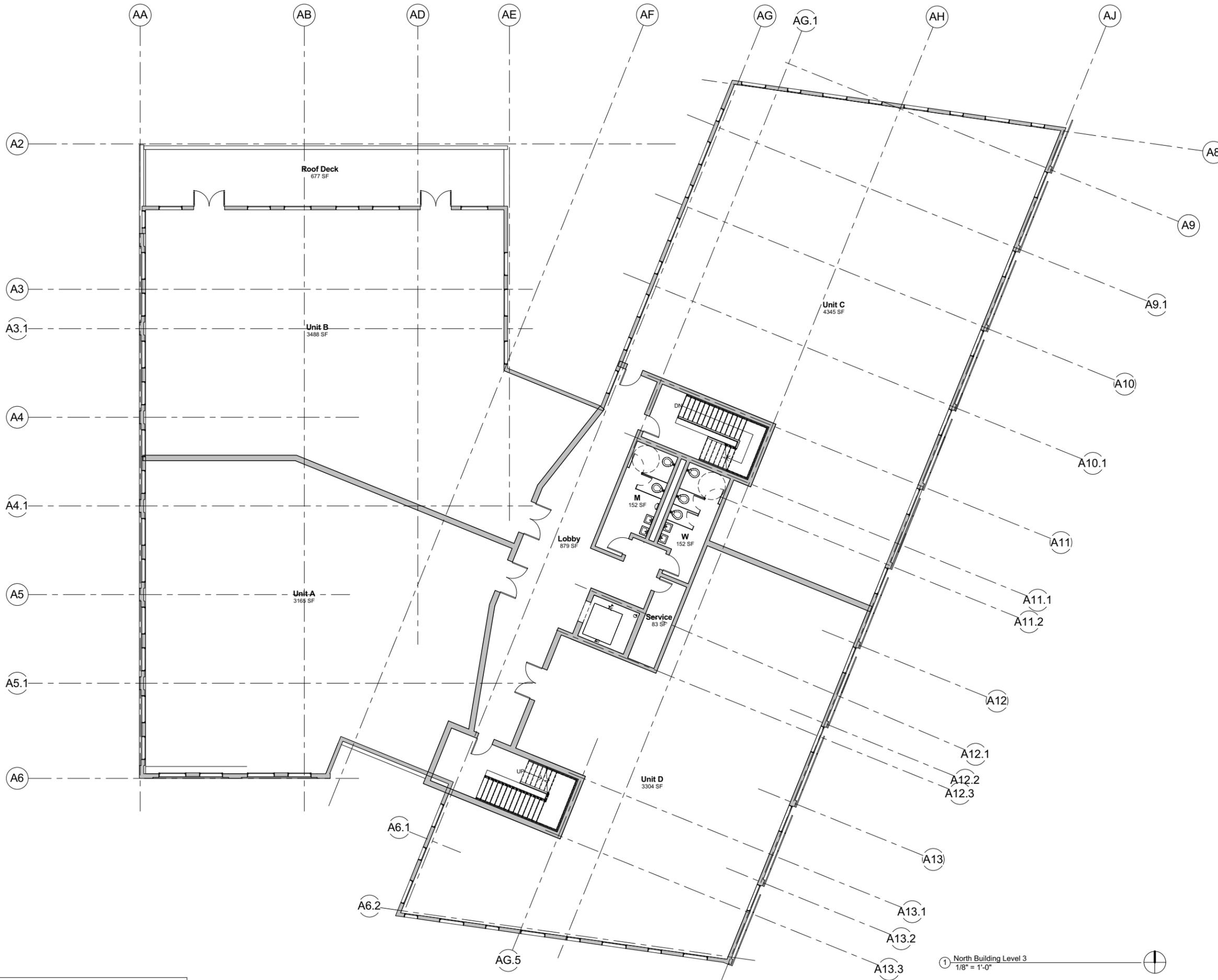
2440 + 2490 Junction Pl. Boulder, CO

1 North Building Level 2  
1/8" = 1'-0"



**COBURN**  
ARCHITECTURE

3020 Carbon Place #203  
Boulder, Colorado  
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**SITE REVIEW**  
**CORRECTIONS**  
07/16/2015

SHEET No.  
**A1.4**  
NORTH BUILDING LEVEL 3

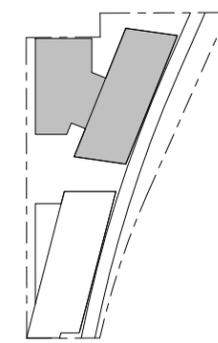
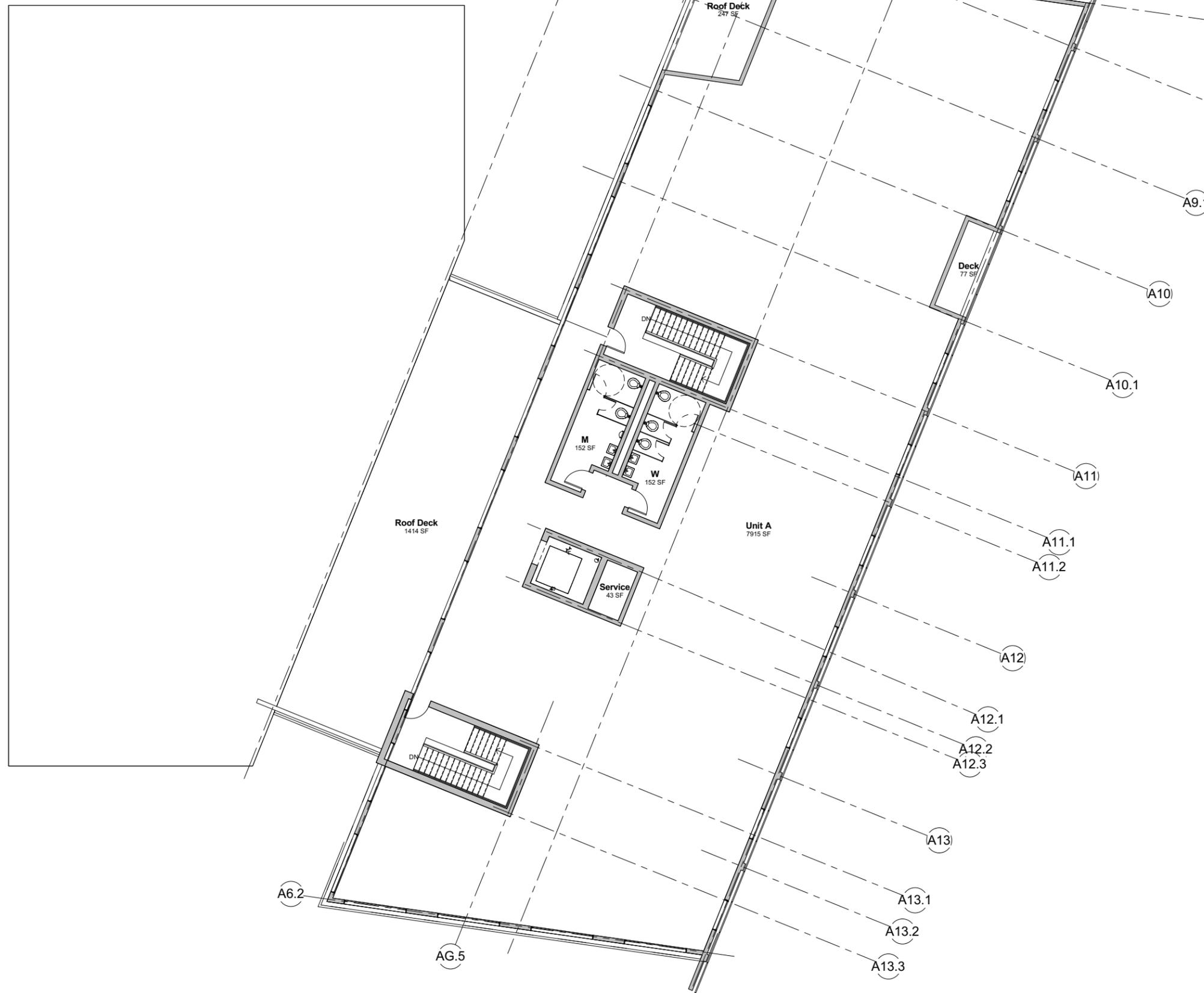
1 North Building Level 3  
1/8" = 1'-0"

Agenda Item 5A Page 36 of 73



**COBURN**  
ARCHITECTURE

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KEY PLAN

**SITE REVIEW  
CORRECTIONS**  
07/16/2015

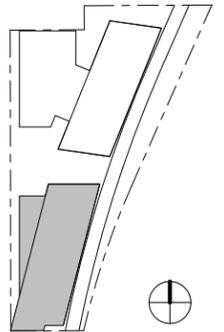
SHEET No.  
**A1.5**

Agenda Item 5A Page 37 of 73  
**NORTH BUILDING LEVEL 4**



**COBURN**  
ARCHITECTURE

3020 Carbon Place #203  
Boulder, Colorado  
p: 303-442-3351  
f: 303-447-3933



**SITE REVIEW  
CORRECTIONS**  
07/16/2015

SHEET No.  
**A1.6**

Agenda Item 5A Page 28 of 73  
**SOUTH BUILDING LEVEL 1**

1 South Building Level 1  
1/8" = 1'-0"



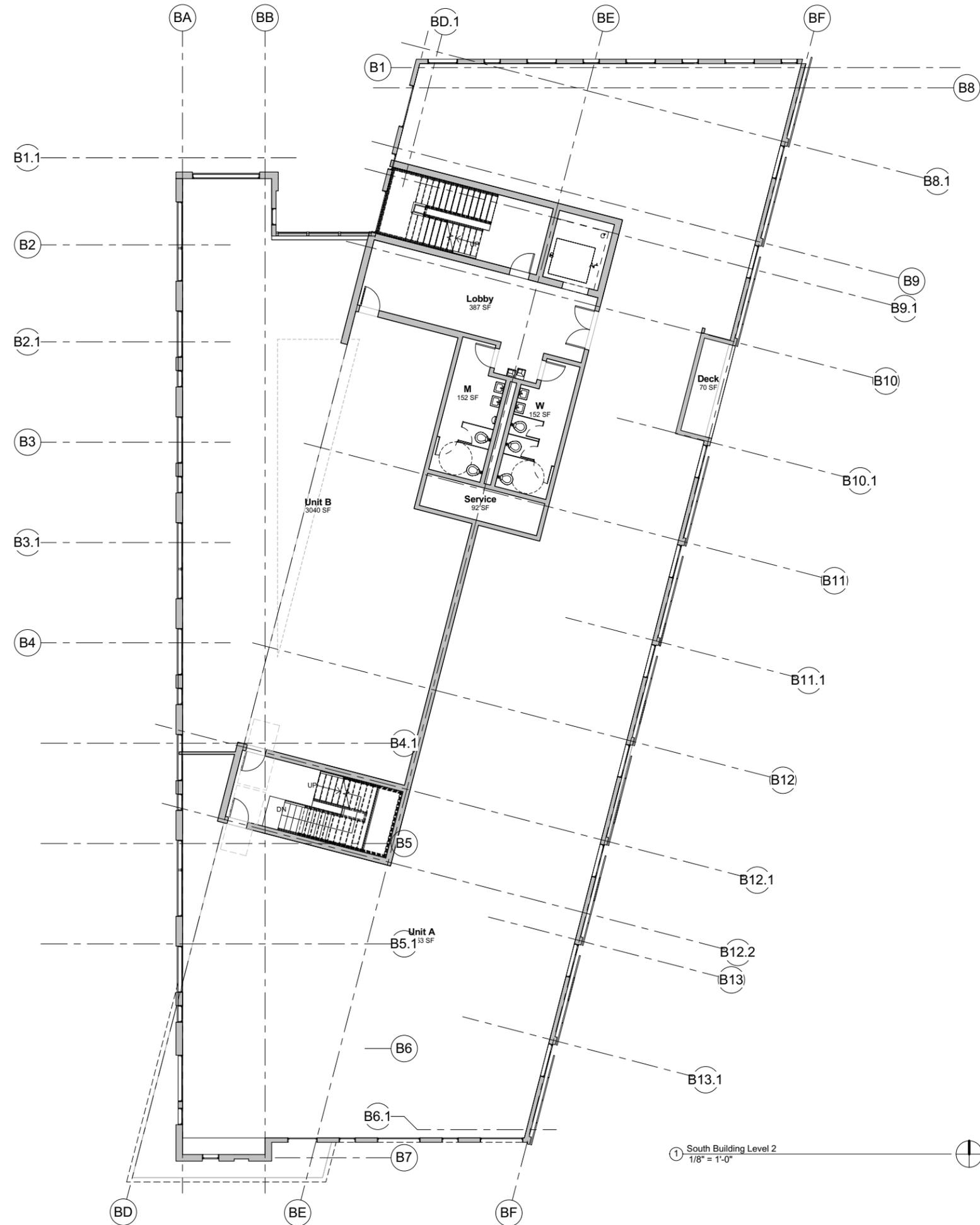
**BOULDER COMMONS**

2440 + 2490 Junction Pl. Boulder, CO

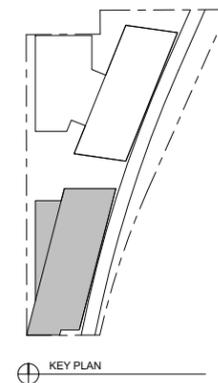


**COBURN**  
ARCHITECTURE

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Boulder, Colorado  
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① South Building Level 2  
1/8" = 1'-0"



KEY PLAN

**SITE REVIEW  
CORRECTIONS**  
07/16/2015

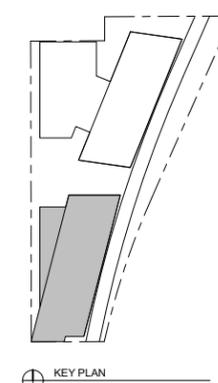
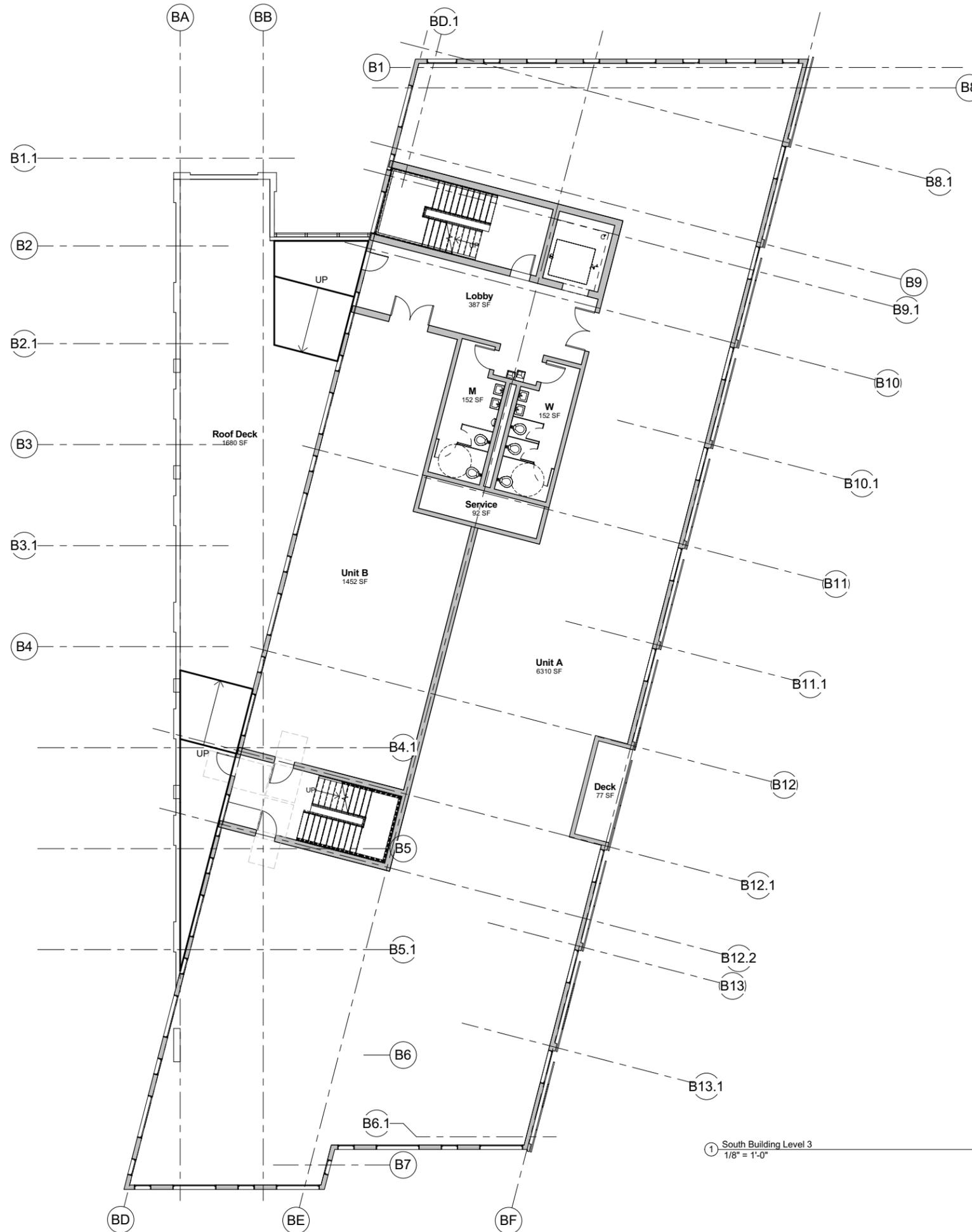
SHEET No.  
**A1.7**

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**SOUTH BUILDING LEVEL 2**



**COBURN**  
ARCHITECTURE

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**SITE REVIEW**  
**CORRECTIONS**  
07/16/2015

SHEET No.  
**A1.8**

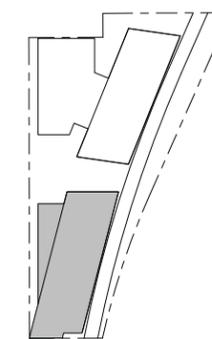
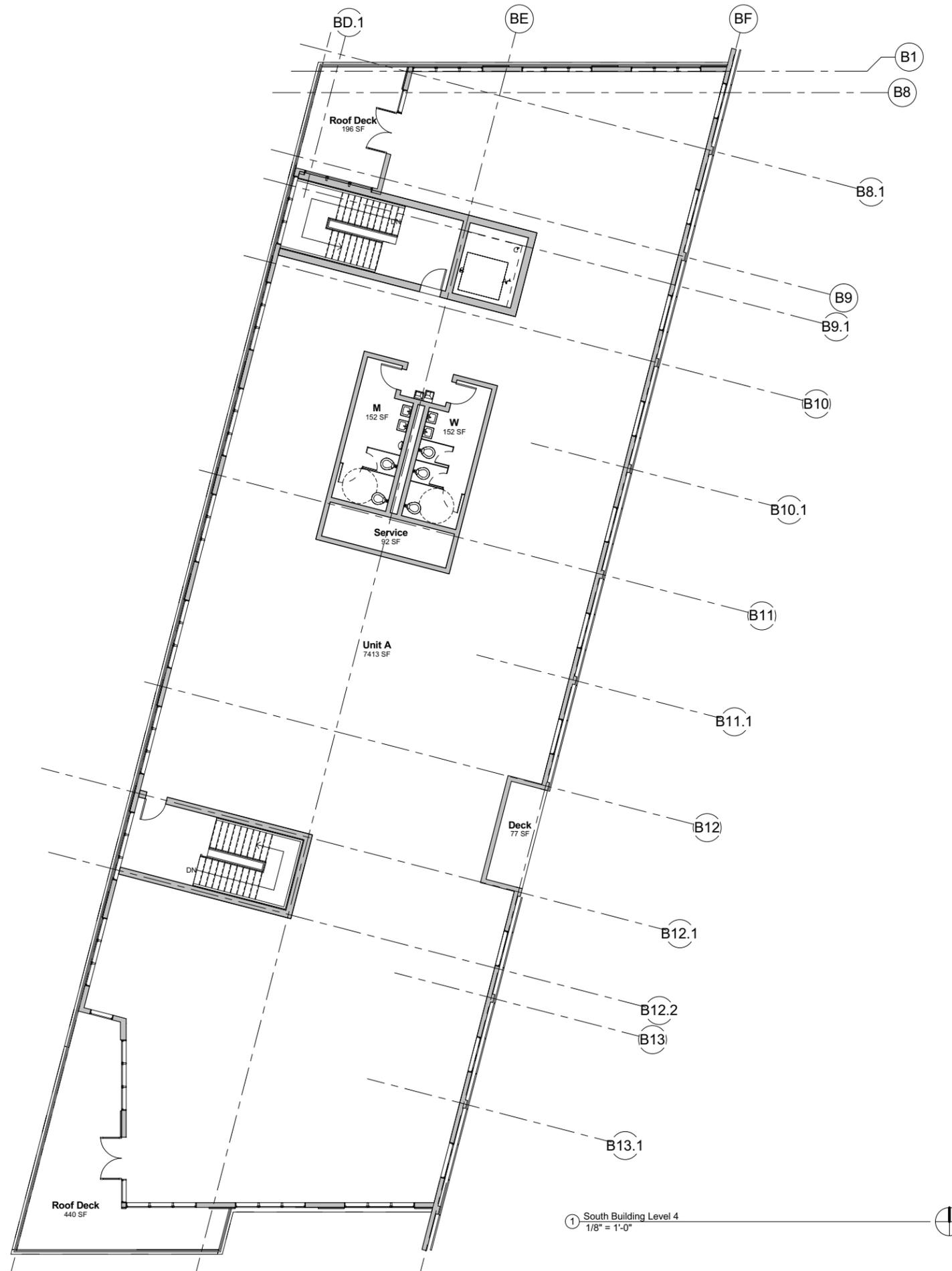
Agenda Item 5A Page 10 of 73  
**SOUTH BUILDING LEVEL 3**

1 South Building Level 3  
1/8" = 1'-0"



**COBURN**  
ARCHITECTURE

3020 Carbon Place #203  
Boulder, Colorado  
p: 303-442-3351  
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KEY PLAN

**SITE REVIEW  
CORRECTIONS**  
07/16/2015

SHEET No.  
**A1.9**  
Agenda Item 5A Page 41 of 73  
**SOUTH BUILDING LEVEL 4**



**COBURN**  
ARCHITECTURE

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1 ROOF PLAN  
1" = 20'-0"



**SITE REVIEW  
CORRECTIONS**  
07/16/2015

SHEET No.

**A1.10**

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ROOF PLAN

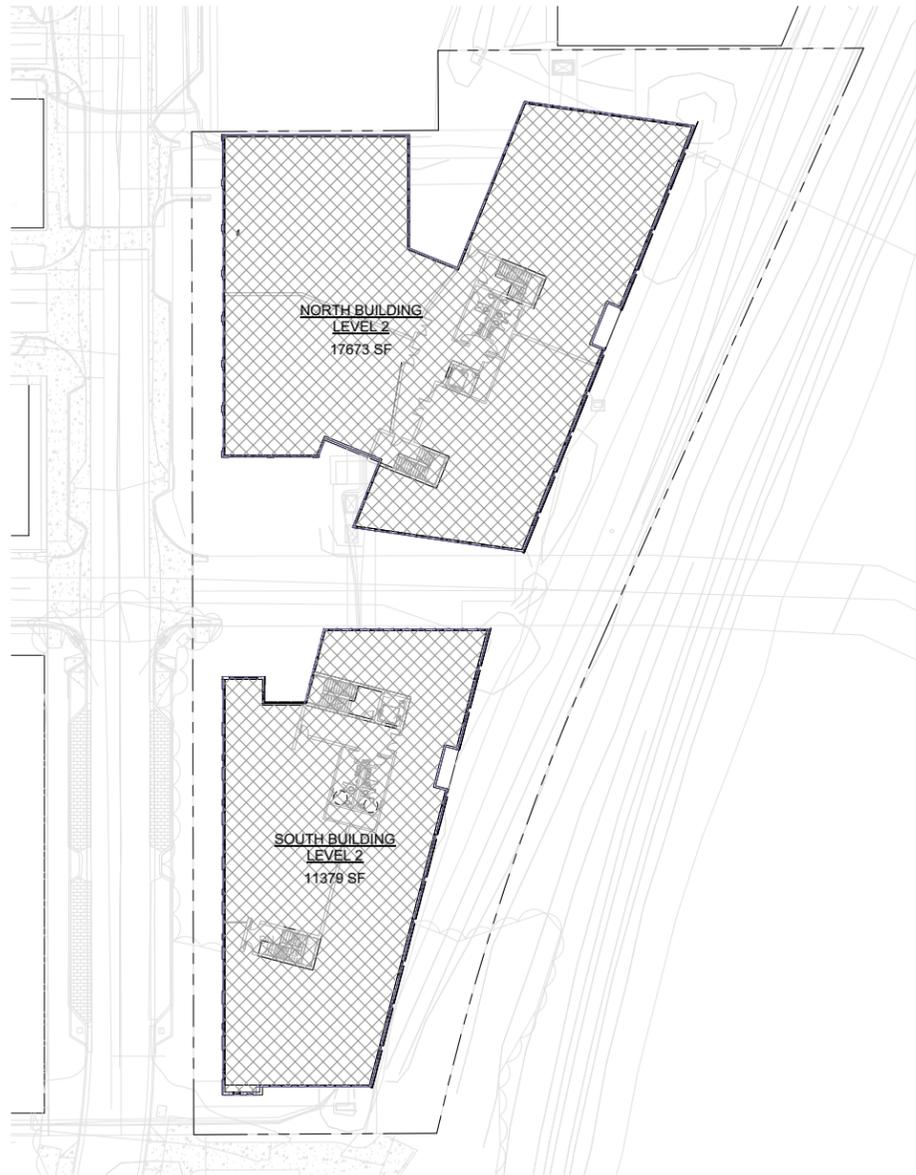
**BOULDER COMMONS**

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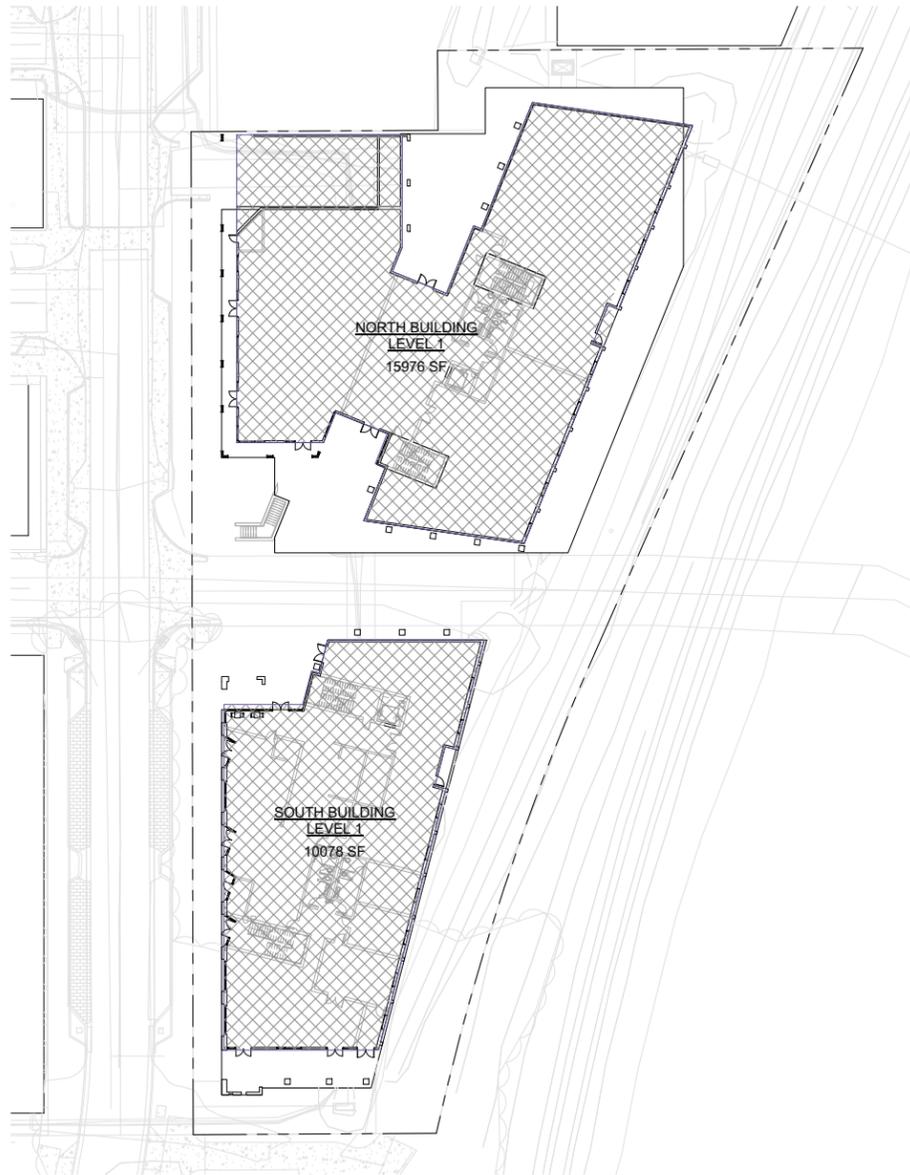


**COBURN**  
ARCHITECTURE

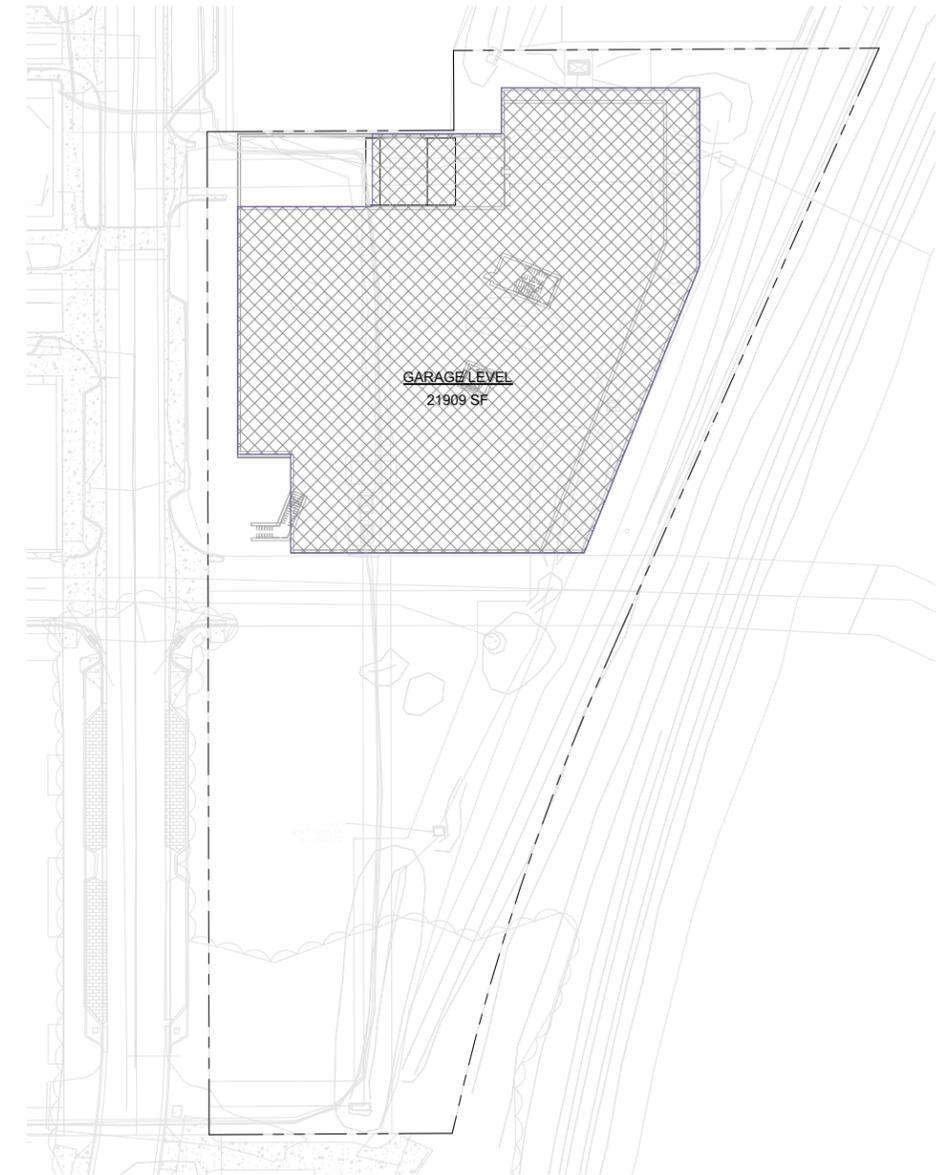
3020 Carbon Place #203  
Boulder, Colorado  
p: 303-442-3351  
f: 303-447-3933



② LEVEL 2  
1" = 30'-0"



① LEVEL 1  
1" = 30'-0"



③ GARAGE LEVEL  
1" = 30'-0"

GROSS BUILDING AREA SCHEDULE	
LEVEL	AREA
GARAGE LEVEL	21909 SF
NORTH BUILDING LEVEL 1	15976 SF
NORTH BUILDING LEVEL 2	17673 SF
NORTH BUILDING LEVEL 3	17032 SF
NORTH BUILDING LEVEL 4	9317 SF
SOUTH BUILDING LEVEL 1	10078 SF
SOUTH BUILDING LEVEL 2	11379 SF
SOUTH BUILDING LEVEL 3	9805 SF
SOUTH BUILDING LEVEL 4	8968 SF

**SITE REVIEW  
CORRECTIONS**  
07/16/2015

SHEET No.  
**A1.11**

Agenda Item 5A Area Plans Page 43 of 73

**BOULDER COMMONS**

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**COBURN**  
ARCHITECTURE

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② LEVEL 4  
1" = 30'-0"



① LEVEL 3  
1" = 30'-0"

GROSS BUILDING AREA SCHEDULE	
LEVEL	AREA
GARAGE LEVEL	21909 SF
NORTH BUILDING LEVEL 1	15976 SF
NORTH BUILDING LEVEL 2	17673 SF
NORTH BUILDING LEVEL 3	17032 SF
NORTH BUILDING LEVEL 4	9317 SF
SOUTH BUILDING LEVEL 1	10078 SF
SOUTH BUILDING LEVEL 2	11379 SF
SOUTH BUILDING LEVEL 3	9805 SF
SOUTH BUILDING LEVEL 4	8968 SF

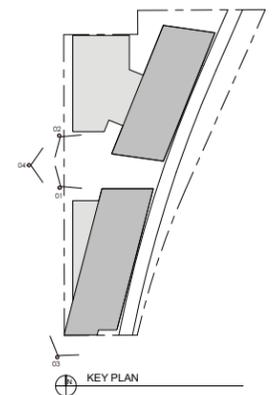
**SITE REVIEW  
CORRECTIONS**  
07/16/2015

SHEET No.  
**A1.12**

Agenda Item 5A AREA PLANS Page 44 of 73



# 01/ North Building Entrance

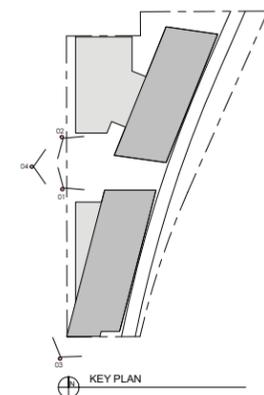


**SITE REVIEW  
CORRECTIONS**  
07/16/2015

SHEET No.  
**A2.1**  
ARCHITECTURAL  
PERSPECTIVE



# 02/ South Building Entrance

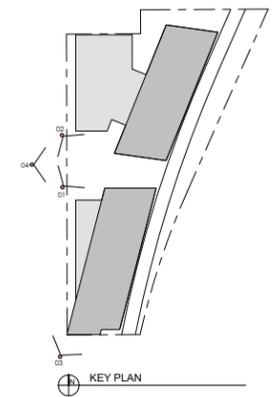


**SITE REVIEW  
CORRECTIONS**  
07/16/2015

SHEET No.  
**A2.2**  
ARCHITECTURAL  
PERSPECTIVE



03/ View from Goose Creek Bridge



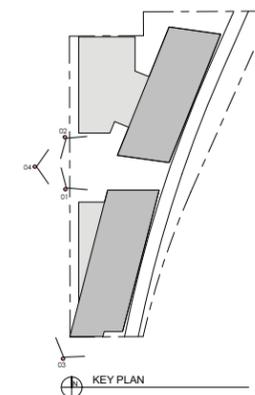
**SITE REVIEW  
CORRECTIONS**  
07/16/2015

SHEET No.  
**A2.3**  
ARCHITECTURAL  
PERSPECTIVE

Agenda Item 5A



## 04/ View From Steel Yards



**SITE REVIEW  
CORRECTIONS**  
07/16/2015

SHEET No.  
**A2.4**  
ARCHITECTURAL  
PERSPECTIVE

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**COBURN**  
ARCHITECTURE

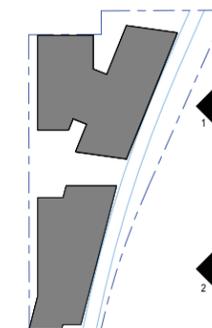
3020 Carbon Place #203  
Boulder, Colorado  
p: 303-442-3351  
f: 303-447-3933

**ehdd.**

ARCHITECTURE  
500 Treat Ave. #201  
San Francisco, CA  
p: 415-285-9193  
f: 415-285-3866

KEY

1. Standing Seam Metal System
2. Wood or Similar
3. Flat Lock Metal Panel
4. Glazed Entry Doors
5. Brick Rain Screen System (Color A)
6. Brick Rain Screen System (Color B)
7. Fixed Metal Sunshade
8. Painted Window Mullion
9. Structural Glass Guardrail
10. Solar Panel Rain Screen - Black
11. HPL - High Pressure Laminate - Integral Color
12. Painted Steel Structure
13. Laser Cut Metal Louvers



KEY PLAN

**SITE REVIEW  
CORRECTIONS**  
07/16/2015

SHEET No.  
**A2.7**  
Agenda Item 5A Page 49 of 73  
**EAST ELEVATIONS**



**1 East Elevation North Building**  
SCALE: 1/8" = 1'-0"



**2 East Elevation South Building**  
SCALE: 1/8" = 1'-0"



**COBURN**  
ARCHITECTURE

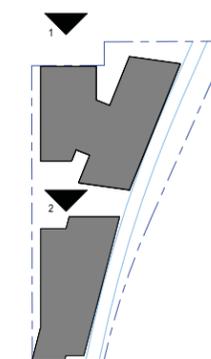
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Boulder, Colorado  
p: 303-442-3351  
f: 303-447-3933



ARCHITECTURE  
500 Treat Ave. #201  
San Francisco, CA  
p: 415-285-9193  
f: 415-285-3866

KEY

1. Standing Seam Metal System
2. Wood or Similar
3. Flat Lock Metal Panel
4. Glazed Entry Doors
5. Brick Rain Screen System (Color A)
6. Brick Rain Screen System (Color B)
7. Fixed Metal Sunshade
8. Painted Window Mullion
9. Structural Glass Guardrail
10. Solar Panel Rain Screen - Black
11. HPL - High Pressure Laminate - Integral Color
12. Painted Steel Structure



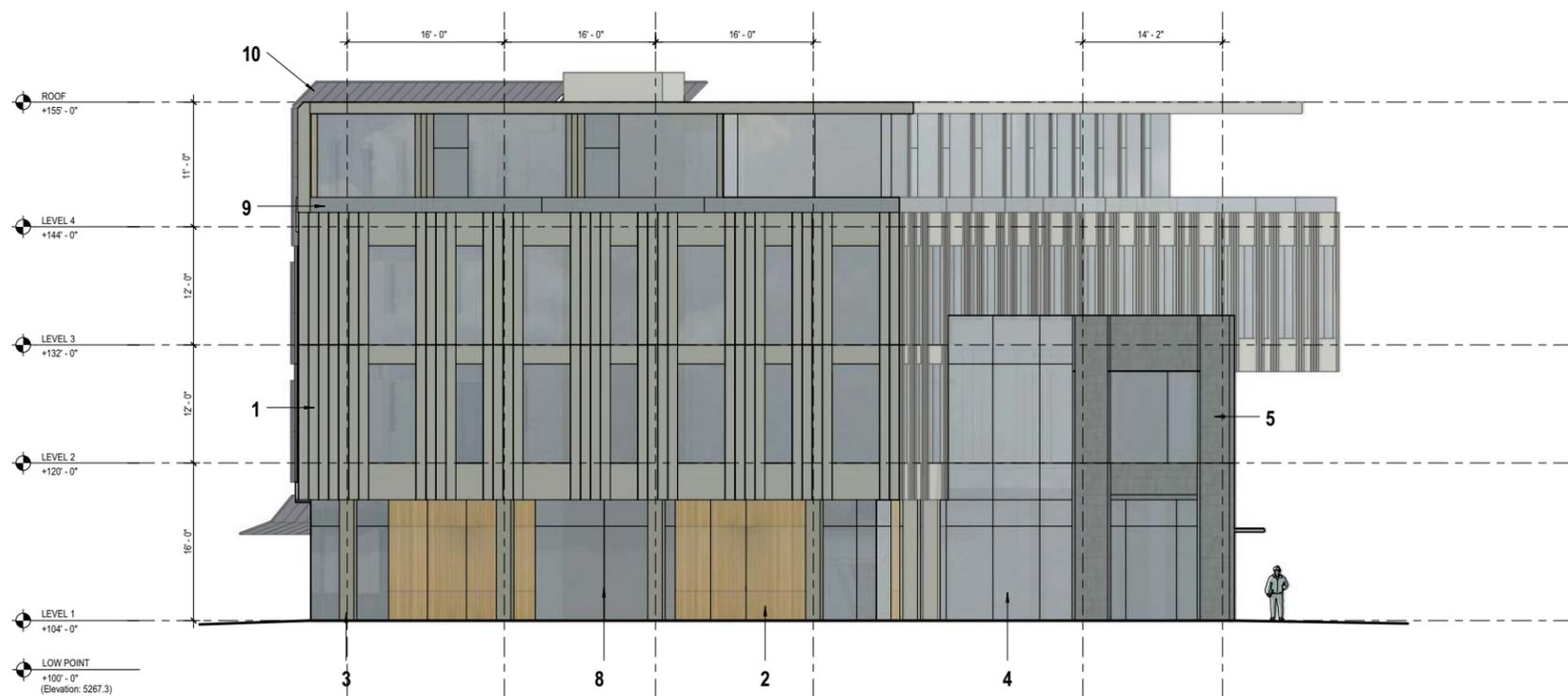
KEY PLAN

**SITE REVIEW**  
**CORRECTIONS**  
07/16/2015

SHEET No.  
**A2.8**  
Agenda Item 5A Page 50 of 73  
**NORTH ELEVATIONS**



**1 North Elevation North Building**  
SCALE: 1/8" = 1'-0"



**2 North Elevation South Building**  
SCALE: 1/8" = 1'-0"

**BOULDER COMMONS**

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**COBURN**  
ARCHITECTURE

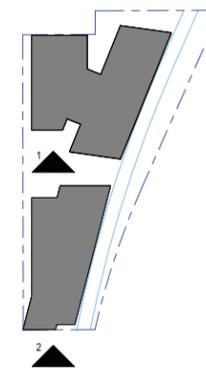
3020 Carbone Place #203  
Boulder, Colorado  
p: 303-442-3351  
f: 303-447-3933

**ehdd.**

ARCHITECTURE  
500 Treat Ave. #201  
San Francisco, CA  
p: 415-285-9193  
f: 415-285-3866

KEY

1. Standing Seam Metal System
2. Wood or Similar
3. Flat Lock Metal Panel
4. Glazed Entry Doors
5. Brick Rain Screen System (Color A)
6. Brick Rain Screen System (Color B)
7. Fixed Metal Sunshade
8. Painted Window Mullion
9. Structural Glass Guardrail
10. Solar Panel Rain Screen - Black
11. HPL - High Pressure Laminate - Integral Color
12. Painted Steel Structure



KEY PLAN

**SITE REVIEW  
CORRECTIONS**  
07/16/2015

SHEET No.  
**A2.6**  
Agenda Item 5A Page 51 of 73  
**SOUTH ELEVATIONS**



**1** South Elevation North Building  
A2.6 SCALE: 1/8" = 1'-0"



**2** South Elevation South Building  
A2.6 SCALE: 1/8" = 1'-0"



**COBURN**  
ARCHITECTURE

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f: 303-447-3933



ARCHITECTURE  
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San Francisco, CA  
p: 415-285-9193  
f: 415-285-3866

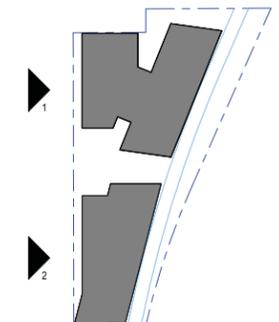
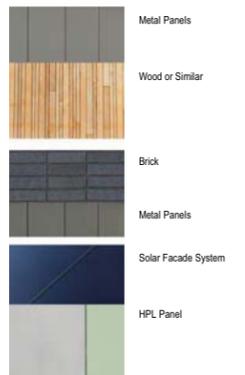


1 West Elevation North Building  
A2.5 SCALE: 1/8" = 1'-0"



2 West Elevation South Building  
A2.5 SCALE: 1/8" = 1'-0"

- KEY
1. Standing Seam Metal System
  2. Wood or Similar
  3. Flat Lock Metal Panel
  4. Glazed Entry Doors
  5. Brick Rain Screen System (Color A)
  6. Brick Rain Screen System (Color B)
  7. Fixed Metal Sunshade
  8. Painted Window Mullion
  9. Structural Glass Guardrail
  10. Solar Panel Rain Screen - Black
  11. HPL - High Pressure Laminate - Integral Color
  12. Painted Steel Structure



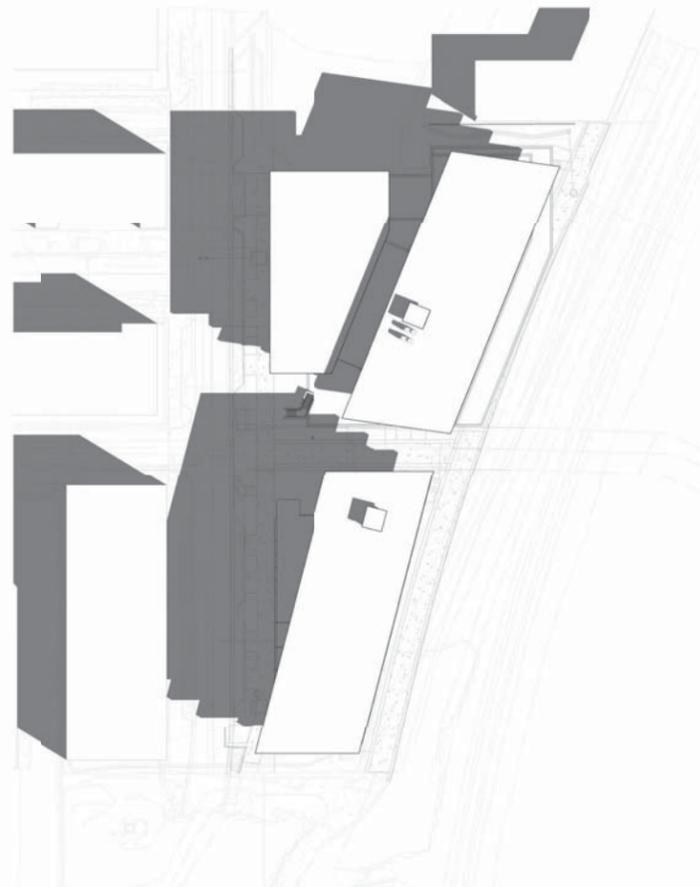
KEY PLAN  
**SITE REVIEW CORRECTIONS**  
07/16/2015

SHEET No.  
**A2.5**  
Page 52 of 73  
**WEST ELEVATIONS**

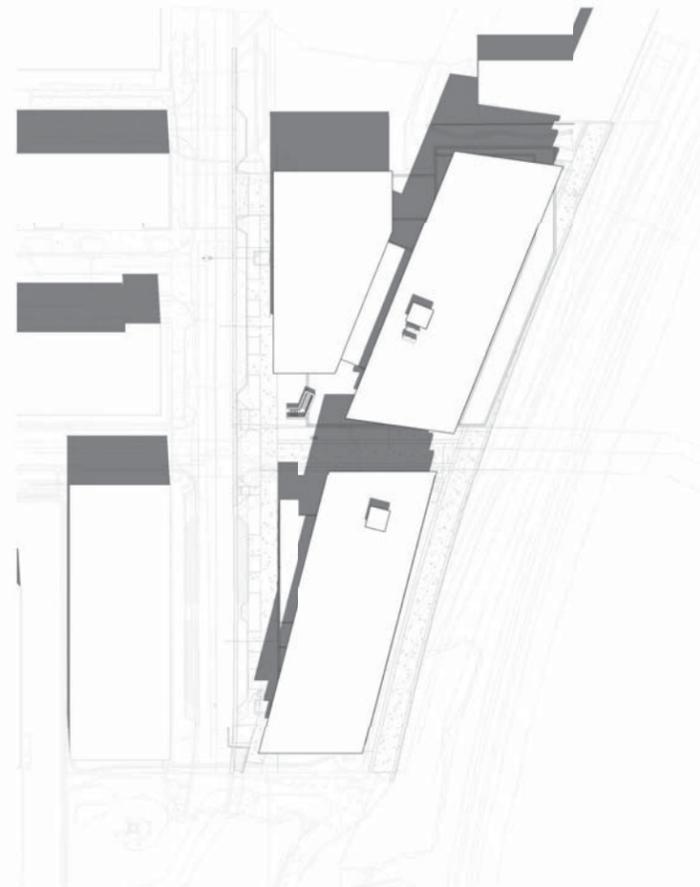


**COBURN**  
ARCHITECTURE

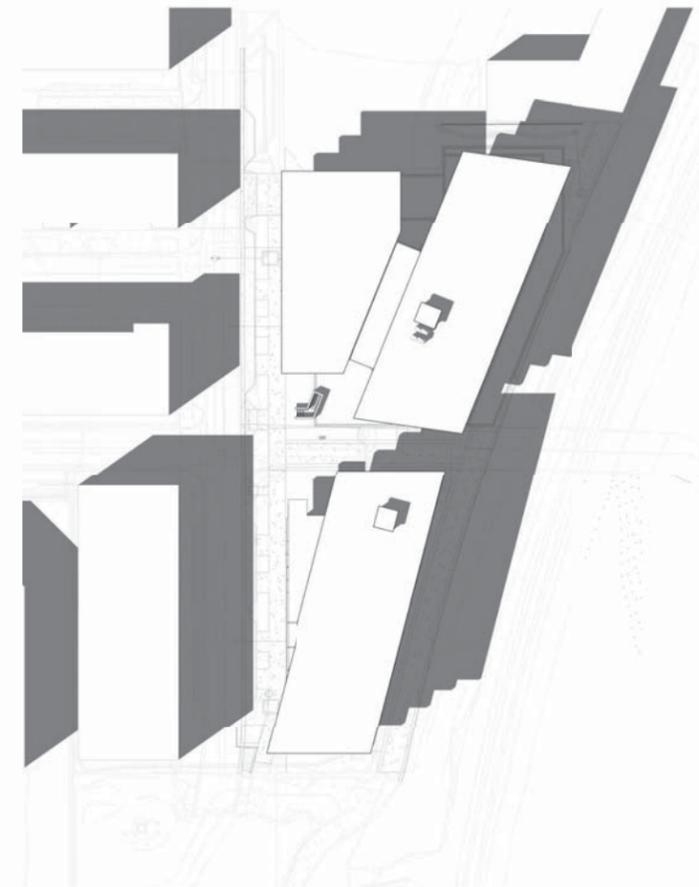
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Boulder, Colorado  
p: 303-442-3351  
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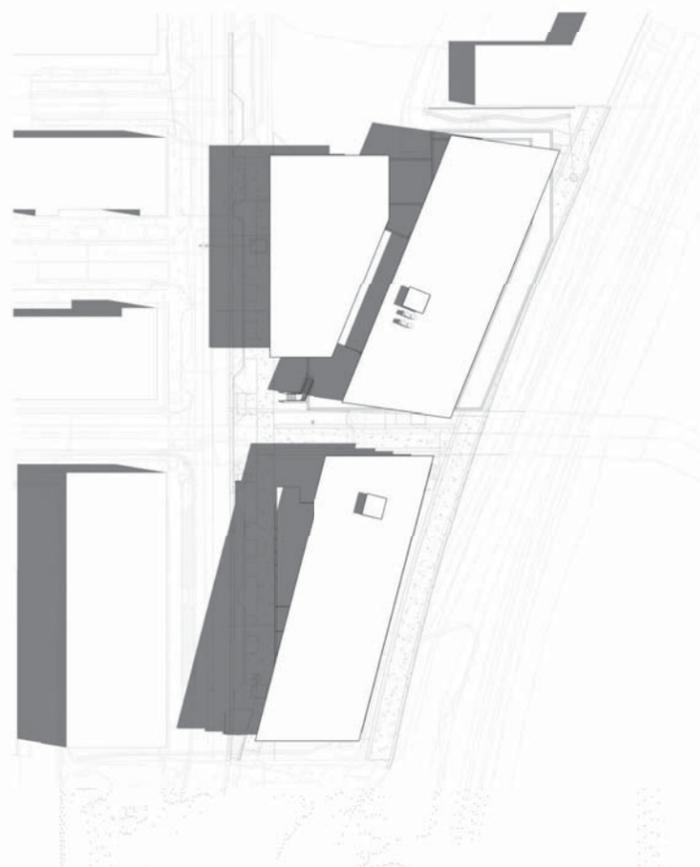
① Shadow Study - March 20th @ 9am  
1" = 50'-0"



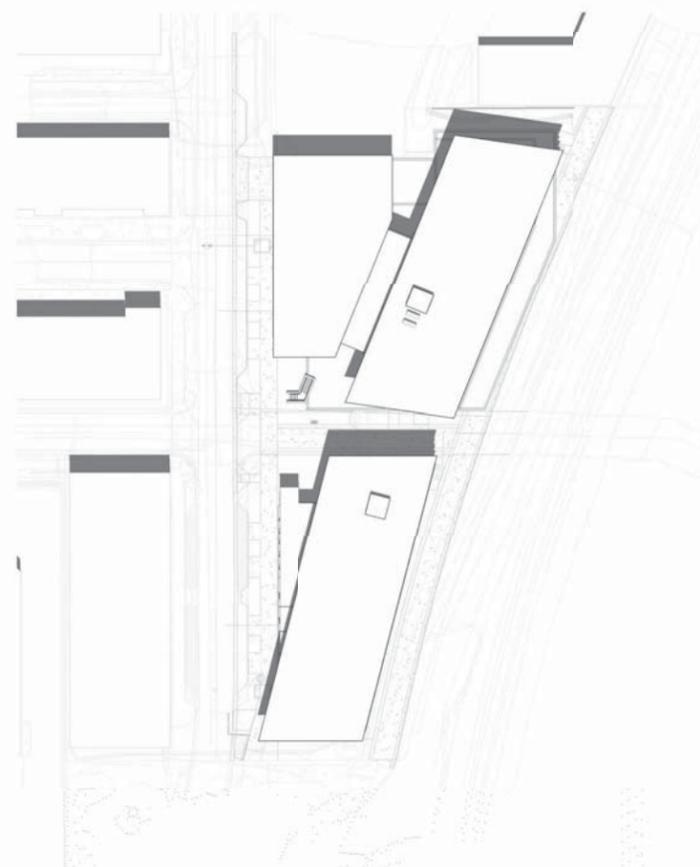
② Shadow Study - March 20th @ 12pm  
1" = 50'-0"



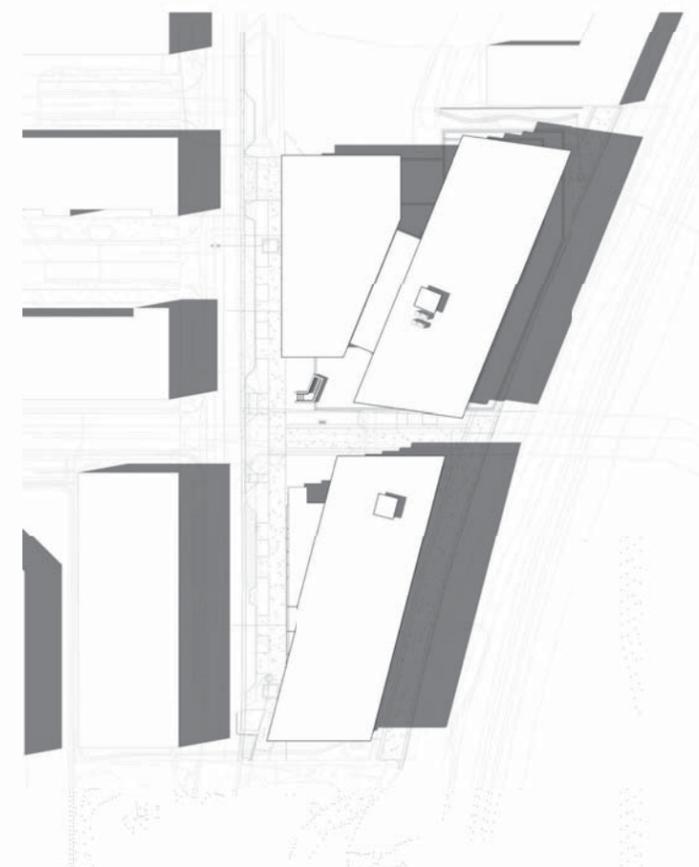
③ Shadow Study - March 20th @ 3pm  
1" = 50'-0"



④ Shadow Study - June 21st @ 9am  
1" = 50'-0"



⑤ Shadow Study - June 21st @ 12pm  
1" = 50'-0"



⑥ Shadow Study - June 21st @ 3pm  
1" = 50'-0"



**BOULDER COMMONS**

2440 + 2490 Junction Pl. Boulder, CO

**SITE REVIEW  
CORRECTIONS**  
07/16/2015

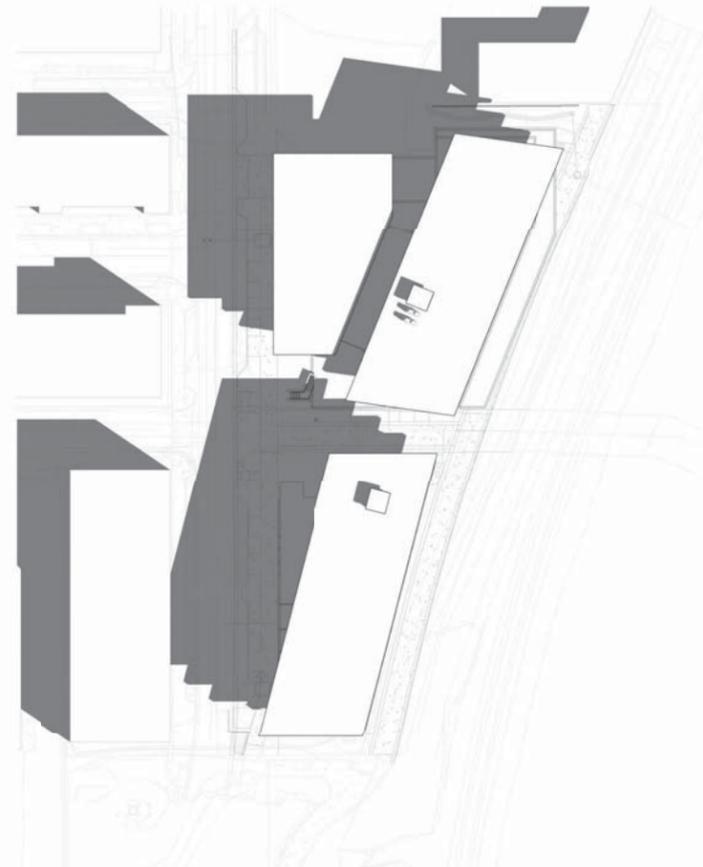
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**A3.1**

Agenda Item 5A Page 53 of 73  
**SHADOW STUDIES**

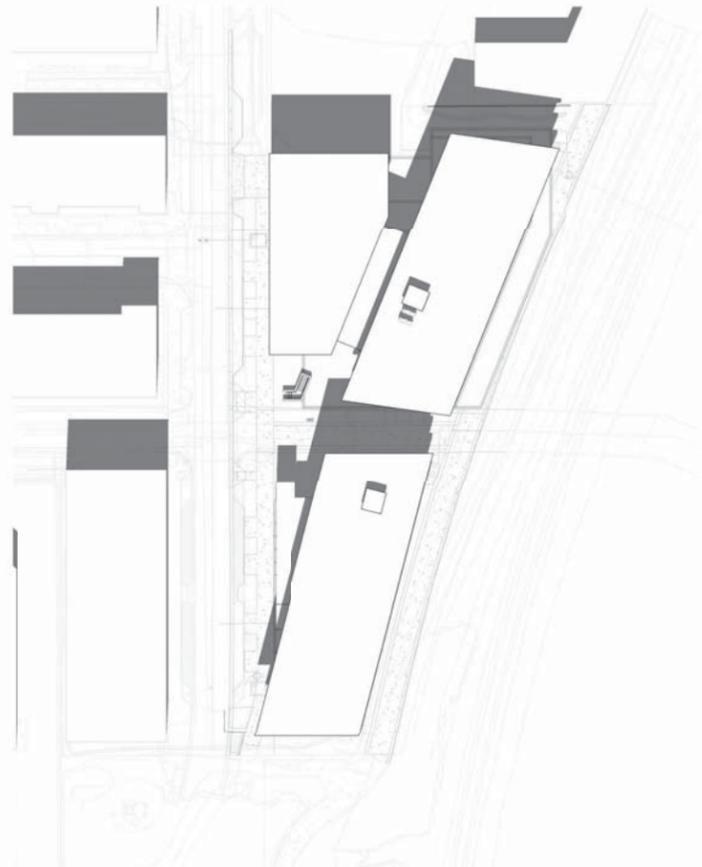


**COBURN**  
ARCHITECTURE

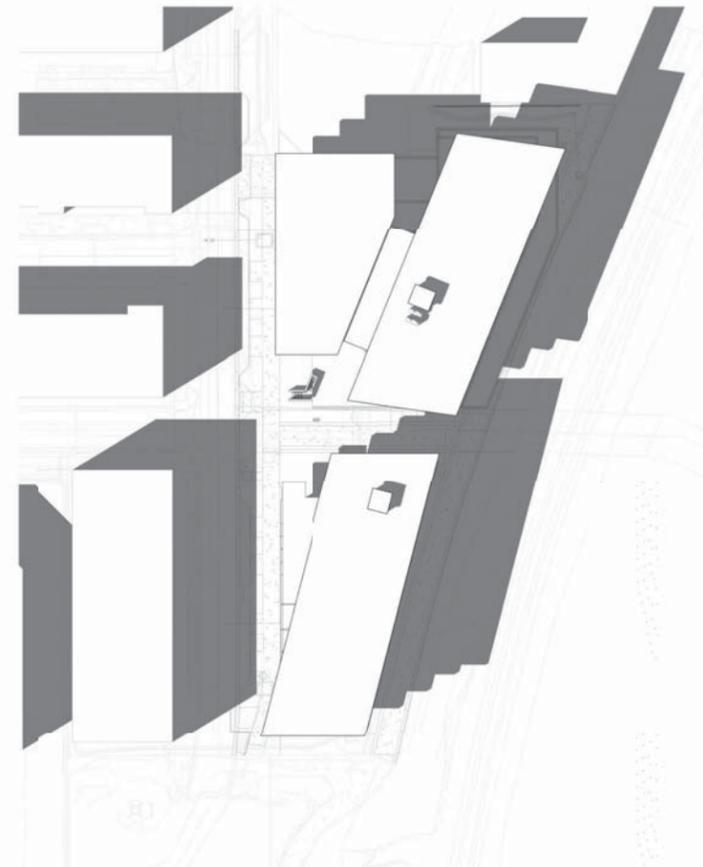
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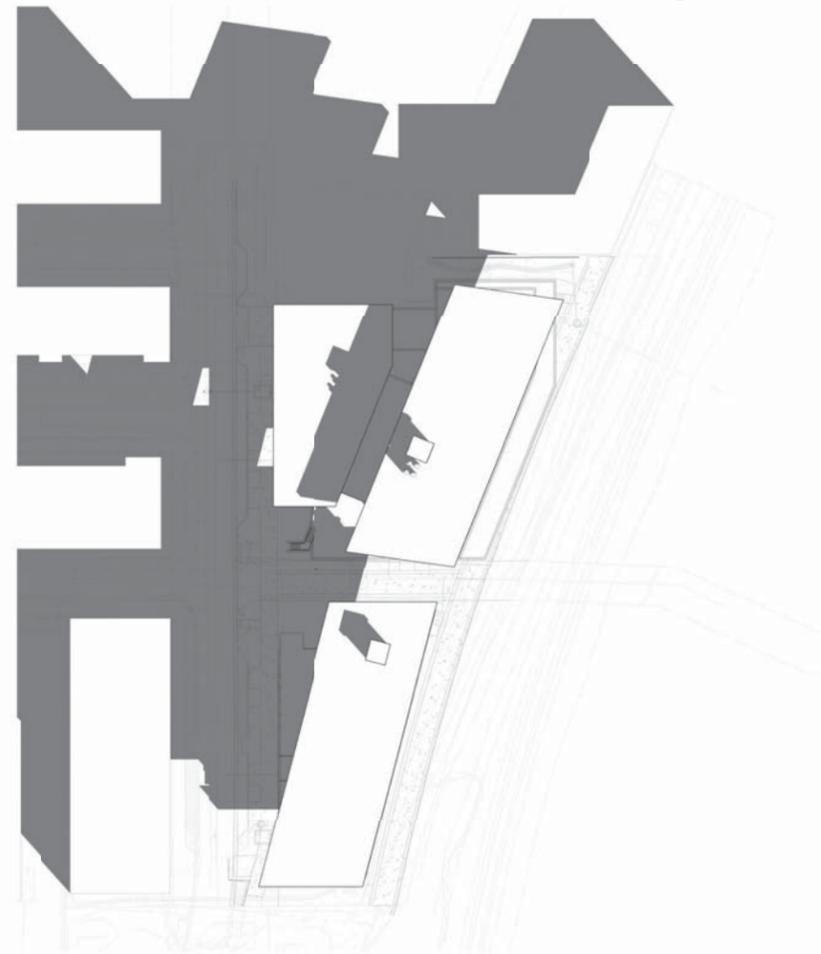
① Shadow Study - September 22nd @ 9am  
1" = 50'-0"



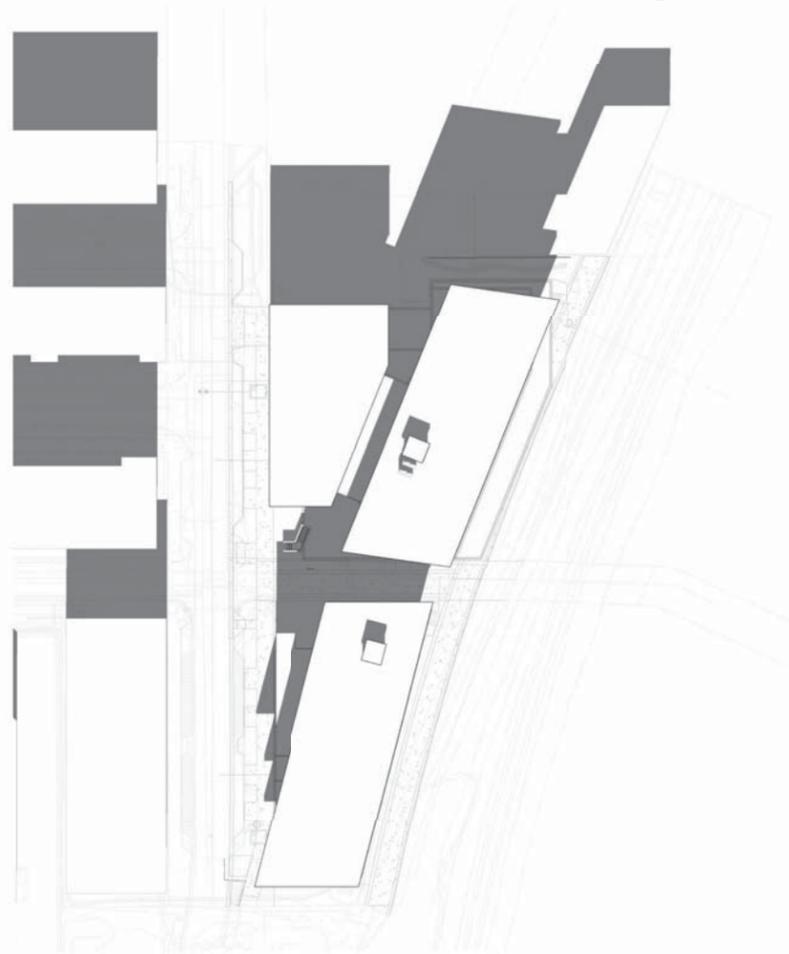
② Shadow Study - September 22nd @ 12pm  
1" = 50'-0"



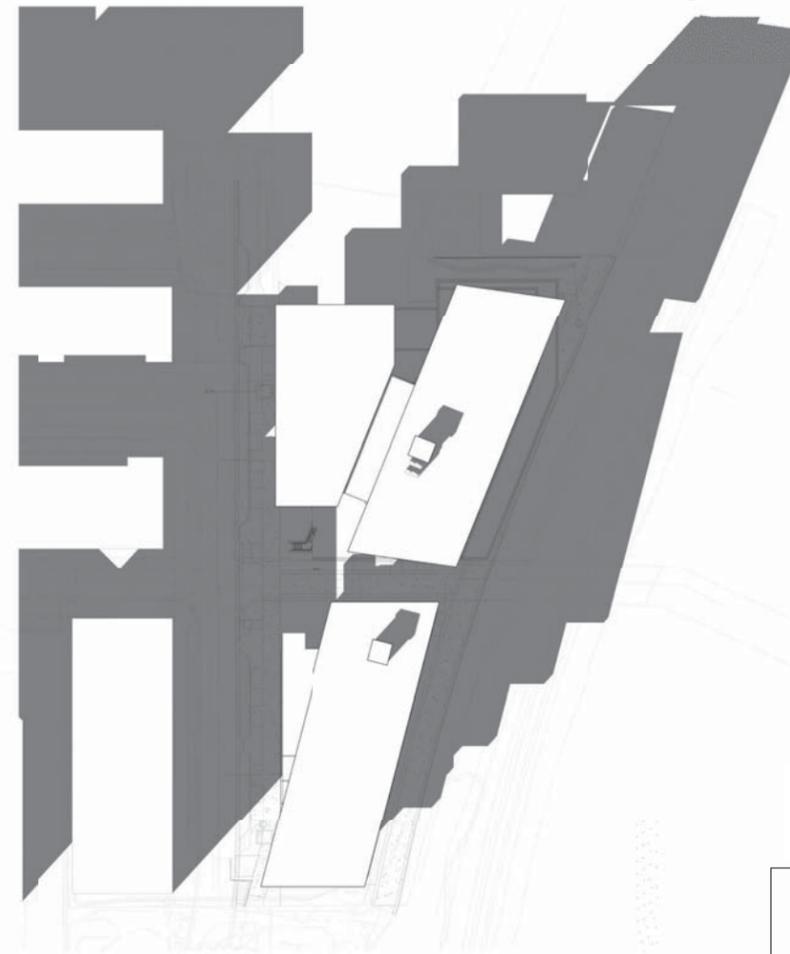
③ Shadow Study - September 22nd @ 3pm  
1" = 50'-0"



④ Shadow Study - December 21st @ 9am  
1" = 50'-0"



⑤ Shadow Study - December 21st @ 12pm  
1" = 50'-0"



⑥ Shadow Study - December 21st @ 3pm  
1" = 50'-0"



**BOULDER COMMONS**

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**SITE REVIEW  
CORRECTIONS**  
07/16/2015

SHEET No.  
**A3.2**

Agenda Item 5A Page 54 of 73  
**SHADOW STUDIES**



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ARCHITECTURE

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**SITE REVIEW  
CORRECTIONS**  
07/16/2015

SHEET No.  
**A3.3**  
PRINCIPAL BUILDING  
HEIGHT

Agenda Item 5A Page 95 of 110

**BOULDER COMMONS**

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① Principal Building Heights  
1" = 30'-0"





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Boulder, CO 80301  
[coburnpartners.com](http://coburnpartners.com)

July 24, 2015

**The Commons**

Lots 1 & 2, Block 9, Steel Yards Replat  
Concept Review and Comment - Written Statement

**Introduction:**

The proposed project, The Commons, consists of two commercial buildings on the remaining two vacant lots in the Steel Yards to the east of Junction Place. This project will create a commercial development that is highly sustainable, provides engaging active uses along the ground level, and incorporates mobility connections appropriate to the transit-rich Boulder Junction. The form of the building creates a buffer for the rail lines to the east, a transition to the residential and mixed-use buildings to the west, and is optimized for solar and daylighting opportunities. The northern building will be approximately 60,000 sf with up to 56 underground parking spaces, including designated car share spaces; and the southern building will be approximately 40,000 sf. The project is intended to be designed to a LEED platinum standard as a minimum, but will likely exceed that metric.

The basic form and mass of the building has arisen simply from the program, context, and sustainability goals. The main volumes of the structures are kept to the east side of each site, creating a buffer between the railway tracks and the residential units in the Steel Yards, as well as protecting the Junction Place pedestrian space from noise and train activity. This also allows for uninterrupted solar access on the roof and east facade of the buildings. The buildings step down toward the street on the west side, creating an appropriate streetscape whose human scale helps to create a transition between the larger format projects to the south of Goose Creek and the finer scale of the Steel Yards to the west and north. A variety of public spaces, ranging from full plazas to simple extended thresholds and widened sidewalks, help to create the complexity and interest that form a quality pedestrian experience. A public plaza in the center of the project creates an entry location for both buildings: an outdoor gathering space with a variety of seating; car and bike share access; shade structures and landscaping; trees; and a location for outdoor events, both during the day and in the evening.

The buildings will be highly sustainable. To achieve this, there are a number of strategies that are designed into the project from the outset. In addition to the solar array tuned to the building's aspect on the site, the project will seek to reduce its demand loads on an annualized basis. The buildings provide optimum widths for daylighting, and to this end, incorporate a light well on the northern building which doubles as an internal public plaza, reducing the need for artificial light. There will be venting, allowing air flow through the building to provide natural cooling at night, and thermal mass in the floors to help carry that cooling throughout the day. A responsive HVAC strategy, such as a variable refrigerant system, will allow the building to respond to the actual unit needs rather than conditioning on a full-building scale, and a variety of tenant-based ideas are being considered to reduce plug loads. A highly advanced building envelope, consisting of thermally broken windows, tuned window-to-glass ratios per facade, and architectural designed shades will complete the package, reducing heat gain in summer and heat loss in winter.

CREATING  
GREAT  
PLACES™



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The project will incorporate a variety of community based elements including a mobility hub, a shared flex meeting space, a planned coffee shop with corresponding coffee club, and a restaurant focused to the south. In order to extend the transit connections to the next level in Boulder Junction, the project incorporates its own "mobility hub" (including a car share program and B-Cycle station), routes the eastern bike path through the center of the project to join with the multi-modal Junction Place, and is compatible with future bus and rail stop connections. The lobby will double as a community gathering space which can be reserved and utilized by community groups, and is also available for large scale meetings for the tenants of the buildings. The vision for the planned coffee shop has been adapted from a San Francisco-based precedent. The coffee shop component would be available to the general public, while the catered coffee club space would be available only to members. Both spaces would be controlled by the coffee shop manager. The club would be equipped with a variety of furniture configurations to serve the needs of professional groups of all sizes, as well as a AV capabilities and a robust wifi network. Members would be able to reserve a single workstation on a daily, weekly, or monthly basis. The south end of the building, abutting the Goose Creek Greenway, creates a prime location for public interaction and gathering, and is planned to be a restaurant space. This will capitalize on the axis created by the diagonal bridge over Goose Creek, creating a visual termination with public interest as Junction Place turns north.

**Transportation Demand Management Toolkit:**

**Core Elements:**

1. The on-street parking at Boulder Commons will be managed by the TDM parking district.
2. Per MU-4, Boulder Commons is not required to provide off-street parking, however, the applicant intends to provide a parking garage with up to 56 total parking spaces.
3. According to Boulder Revised Code, Boulder Commons generates the need for 39 long-term bicycle parking spaces. These spaces will be provided in the secured parking garage and will be monitored via security camera.
4. According to Boulder Revised Code, Boulder Commons generates the need for 19 short-term bicycle parking spaces. The majority of these will be adjacent to the plaza. Dedicated racks will be provided adjacent to the restaurant and retail spaces.
5. Travel choices will be increase by including a mobility hub. Up to 10% of off-street parking will be dedicated to carpool and carshare.
6. Bike and pedestrian enhancements include connectivity to the the city-planned Goose Creek trail via multi-use path that runs north-south along the east side of the Boulder Commons parcels. A pedestrian plaza will route the multi-use path through the center of project to join with multi-modal Junction Place corridor. This will be compatible with future bus and rail stop connections.
7. Changing facilities will be accommodated via ADA-compliant public restrooms on level 01 of both buildings.
8. Mandatory employee orientation will be facilitated by the applicant/building management to encourage alternative means of transportation.

**Package Elements:**

1. Boulder Commons will implement car-sharing, carpooling and a B-cycle.





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3020 Carbon Place #203  
Boulder, CO 80301  
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2. A business eco-pass program will be implemented.

**Changes from Concept Plan:**

Planning Board and Staff comment: Shrink the plaza area in the center of the site. This has been incorporated into the plan.

Planning Board comment: Add northern plaza area, and connect that to the bike path to the east. This has been incorporated into the plan.

Planning Board comment: expression of architecture should use lasting materials and be non-traditional in nature, but still preserve human scale. This has been incorporated into the plan.

Staff comment: provide more green landscaping in the plaza. This has been incorporated into the plan.

Staff comment: remove 45 degree angled parking north of the garage entry to avoid vehicular conflicts. This has been incorporated into the plan.

Staff comment: remove patterning in bike path concrete. This has been removed.

Staff comment: southwest corner of south building projects into the right of way. This has been reduced both to the south and the west and no longer projects beyond property line.

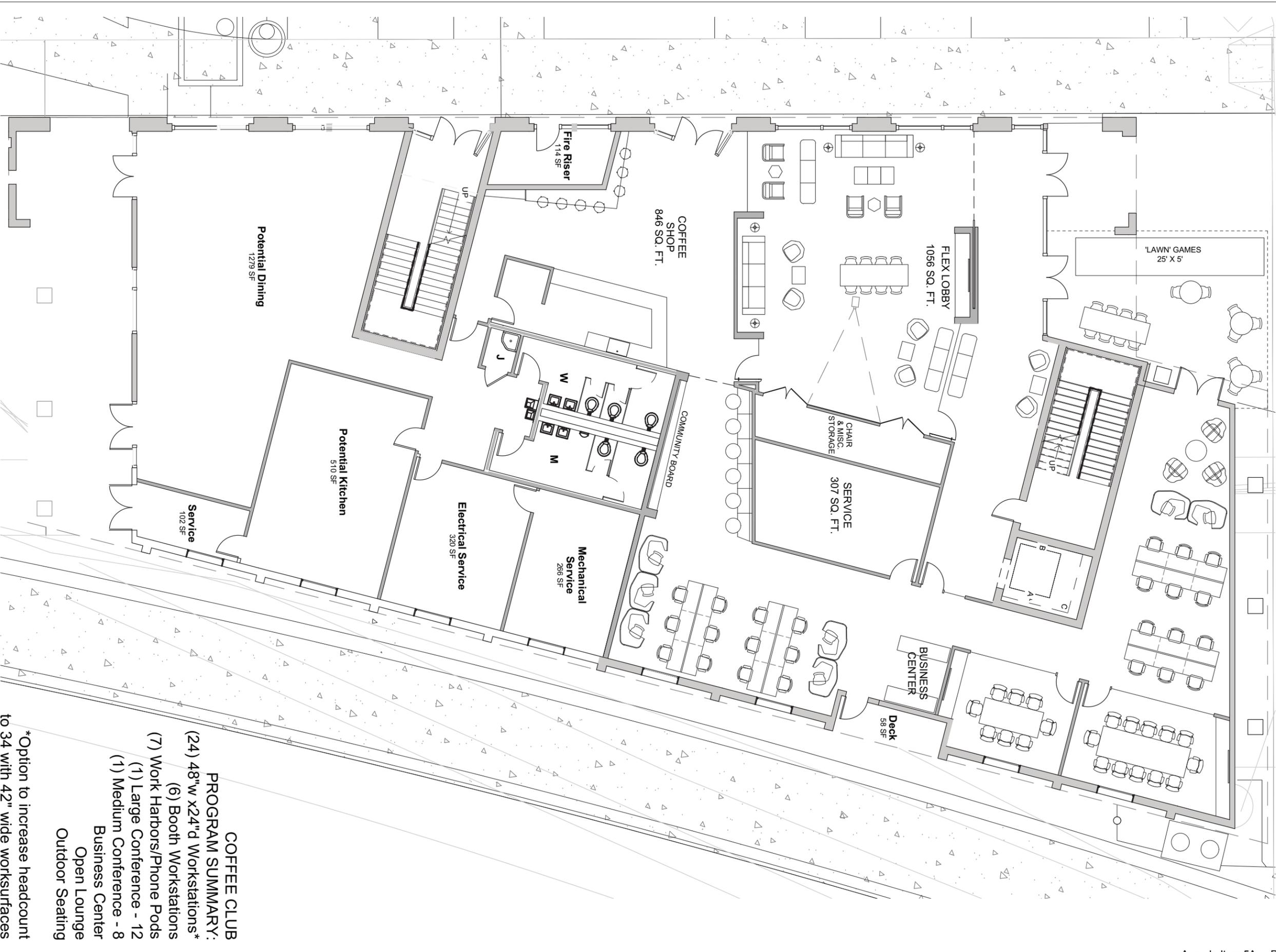
**Statement of Ownership:**

This site is currently owned and operated by Morgan Creek Ventures.

**Changes to standards that are requested:**

This project is requesting some minor variances to the code standards. They include variances to minimum front yard landscaped setback requirements, minimum front yard setback from a street 3<sup>rd</sup> story and above, maximum stories, maximum building height, maximum floor area of any principal building, and restaurant use over 1500 sf.





\*Option to increase headcount to 34 with 42" wide worksurfaces

- COFFEE CLUB PROGRAM SUMMARY:**
- (24) 48" w x24" d Workstations\*
  - (6) Booth Workstations
  - (7) Work Harbors/Phone Pods
  - (1) Large Conference - 12
  - (1) Medium Conference - 8
- Business Center  
Open Lounge  
Outdoor Seating



**HUNTSMAN**

Drawing No.: **SP-1**

Drawing Title:  
GROUND FLOOR  
SOUTH BUILDING  
SPACEPLAN

Project Name:  
THE COMMONS  
BOULDER, CO

North Date: 07.08.15

Scale: N.T.S.

Proj. No.: 15027.00

Attachment

June 15, 2015

**Boulder Commons  
Management Plan  
2440+2490 Junction Place**

**Background:** 2440 and 2490 Junction Place are vacant parcels of land located directly east of the Steelyards Development. We would like to create a highly-sustainable commercial development that encourages active uses along the ground level, and incorporates mobility connections appropriate to the transit-rich Boulder Junction. We plan to construct two buildings that total around 100,000 sf. The 40,000 sf south building will include a restaurant/kitchen, coffee shop, retail frontage, and a flex space on the ground level. The restaurant will serve the Indoor/Outdoor Dining Spaces and will offer catering services to the adjacent flex space. The flex space will be available for tenant and reserved public use.

**Hours of Operation:** 11 am to 2am

**Parking:** See site plan and parking information on Sheets C2.0 and A1.1 of submittal drawings.

**Deliveries:** Daily deliveries – food and beverage distribution companies

**Trash and Recycling:** Daily scheduled pick-ups by Western Disposal Company. We will have one small dumpster for waste and another for recyclables.

**Security:** We will have interior and exterior surveillance cameras running at all times to prevent loitering and ensure safety.

**Drug and Alcohol Policy:** Boulder Commons will be a drug and tobacco-free facility.

**Neighborhood Outreach and Methods of Future Communication:** We will keep all neighbors informed of our construction process and operation schedules once these buildings open.

**Methods of dispute resolution with the surrounding neighborhood:** Boulder Commons will maintain a friendly relationship with all neighbors. Should a dispute arise, the owner/manager will discuss the issue and promptly find a resolution. Any decisions made that will affect future operations will be portrayed to all facility staff.

**Outdoor Lighting:** All pedestrian walks will be illuminated via street lights and building emergency lighting. Reference Sheet E1.0.

Criteria for Review: No site review application shall be approved unless the approving agency finds that:

(1) Boulder Valley Comprehensive Plan:

- (A) The proposed site plan is consistent with the land use map and the service area map and, on balance, the policies of the Boulder Valley Comprehensive Plan.

*Pages 67 to 69 of the BVCP describe the purpose of Area Plans as a means to provide direction for specific geographic areas, and bridge the gap between the broad policies of the Comprehensive Plan and site specific project review. The Comprehensive Plan notes that issues that Area Plans address include appropriate character, scale and mix of uses and if regulatory changes are needed to ensure or encourage appropriate development. The Transit Village Area Plan (TVAP) is one among four adopted area plans within the Comprehensive Plan with the stated purpose of the being:*

*“To describe the city’s vision for the future of the 160-acre Transit Village area and guide the long term development of the area. The area is defined as within walking distance to the future FasTracks transit services – commuter rail, bus rapid transit, and regional bus services.”*

**Transit Village Area Plan (TVAP):** *The area overseen by the TVAP was renamed Boulder Junction, in reference to the area from decades ago as the junction of two major rail lines. Within TVAP, the land use designation for the site is MU-2 or Mixed Use -2, which anticipates three- to four-story mixed use buildings at a FAR of 1.5 to 2.0. As discussed in the attached staff memorandum, the proposed 3-4 story buildings with a total FAR of 1.71 are consistent with the intent of the MU-2 land use designation set forth in TVAP.*

*In addition, the proposed project is consistent with a number of BVXCP Policies, including:*

- **2.01 Unique Community Identity**
  - *By meeting the goals of TVAP, the project will further the unique community identity and sense of place anticipated therein.*
- **2.03 Compact Development Pattern**
  - *The proposed project utilizes an infill site that is part of a planned redevelopment area regulated through TVAP.*
- **2.09 Neighborhoods as Building Blocks**
  - *As a mixed use commercial area, the proposed project offers unique physical elements of neighborhood character and identity, and compliments the existing Steel Yards neighborhood to the west as well as the new Boulder Junction area developing to the south.*
- **2.14 Mix of Complementary Land Uses**
  - *The proposed development adds significant commercial square footage to a mixed-use area that is anticipated to develop further with additional residential and non-residential uses.*
- **2.17 Variety of Activity Centers**
  - *The proposed development helps to achieve the desired land use pattern set forth in TVAP, and as such helps move the TVAP area closer to becoming the transit-oriented urban city center it is intended to become.*
- **2.22 Improve Mobility Grid**

- *The project incorporates required multi-use path connections to improve accessibility within and around the site, and also includes completion of the east side of Junction Pl. in accordance with TVAP.*
- **2.30 Sensitive Infill and Redevelopment**
  - *The project sensitively utilizes an urban infill site to create a project that fully meets the intent of TVAP in terms of both land use and design quality.*
- **2.32 Physical Design for People**
  - *The project provides coordinated facilities for pedestrians and bicyclists, provides functional landscaping and open space, and is designed at an appropriate scale for nearby buildings.*
- **2.37 Enhanced Design for Private Sector Projects**
  - *The project is compatible with the existing context of the area as well as the context set forth in TVAP. The project provides a number of features which will enhance the public realm, including open space and transportation connections, and is designed at a human scale with both permeability and visual interest in mind.*
- **4.05 Energy-Efficient Building Design**
  - *The proposed project is striving to meet or exceed a LEED Platinum rating, and incorporate numerous state-of-the-art design features intended to improve the building's efficiency and generate as close to a net zero development as possible. These features are discussed further in the staff memorandum.*

(B) The proposed development shall not exceed the maximum density associated with the Boulder Valley Comprehensive Plan residential land use designation. Additionally, if the density of existing residential development within a three-hundred-foot area surrounding the site is at or exceeds the density permitted in the Boulder Valley Comprehensive Plan, then the maximum density permitted on the site shall not exceed the lesser of:

*The proposed project is for a mixed-use commercial development that does not include any residential units; therefore, the proposed development does not exceed the maximum density associated with the BVCP land use designation.*

(i) The density permitted in the Boulder Valley Comprehensive Plan, or

*Not applicable.*

(ii) The maximum number of units that could be placed on the site without waiving or varying any of the requirements of chapter 9-8, "Intensity Standards," B.R.C. 1981.

*Not applicable.*

(C) The proposed development's success in meeting the broad range of BVCP policies considers the economic feasibility of implementation techniques required to meet other site review criteria.

*The proposed project is subject to the adopted Transit Village Area Plan (TVAP) and as such is required to provide public improvements in accordance with the adopted plan. Required public improvements to be provided by the applicant as part of this project include a new 12' multi-use path along the west side of the*

railroad tracks as well as a multi-use path running east-west across the project site connecting the path to Junction Place. The project also includes completion of the east side of Junction Place. Overall, this project includes a number of improvements that will provide economic benefit to the community as a whole. Further, the project includes numerous sustainability and building efficiency features which are design to save money in the long-term, meaning that the project has been designed with long-term economic feasibility in mind.

(2) Site Design: Projects should preserve and enhance the community's unique sense of place through creative design that respects historic character, relationship to the natural environment, multi-modal transportation connectivity and its physical setting. Projects should utilize site design techniques which are consistent with the purpose of site review in Subsection (a) of this section and enhance the quality of the project. In determining whether this subsection is met, the approving agency will consider the following factors:

(A) Open Space: Open space, including, without limitation, parks, recreation areas and playgrounds:

- (i) Useable open space is arranged to be accessible and functional and incorporates quality landscaping, a mixture of sun and shade and places to gather;

*The project incorporates a variety of usable open spaces, including a large central plaza, a pocket plaza on the north side of the north building, and a restaurant patio area on the south side of the southern building. The central plaza is easily accessed from either Junction Place or the multi-use path along the east side of the site via a multi-use path connection running east-west through the plaza, and includes seating, public art, and bike parking. The central plaza also provides the primary building entrances for the first floor tenant spaces, as well as pedestrian access to the underground parking. The restaurant patio is situated just off Goose Creek, and will be visible to passers-by. The northern pocket plaza is more private, and is intended primarily for building tenants. Overall, the open space fully meets the intent of this criterion.*

- (ii) Private open space is provided for each detached residential unit;

*Not applicable, as there are no residential units proposed.*

- (iii) The project provides for the preservation of or mitigation of adverse impacts to natural features, including, without limitation, healthy long-lived trees, significant plant communities, ground and surface water, wetlands, riparian areas, drainage areas and species on the federal Endangered Species List, "Species of Special Concern in Boulder County" designated by Boulder County, or prairie dogs (*Cynomys ludovicianus*), which is a species of local concern, and their habitat;

*Not applicable, as the site is currently a graded dirt lot with no significant natural features.*

- (iv) The open space provides a relief to the density, both within the project and from surrounding development;

*The open space features will provide a relief from the intensity of the project, and will also help to provide relief from the density of the existing Steel Yards development to the west. The central plaza creates an open sight line through the middle of the site and aligns with the existing street connection the east, which will provide a relief from the building massing for both users of the plaza as well as*

*people approaching the site from the west. The central plaza also includes amenities meant to make it feel more comfortable, including seating, string lights, trees and transparent building materials along the ground floors. The pocket plaza on the north side of the site will also help provide a relief to tenants by providing them with a somewhat secluded, quieter open space area.*

- (v) Open space designed for active recreational purposes is of a size that it will be functionally useable and located in a safe and convenient proximity to the uses to which it is meant to serve;

*The open spaces are intended primarily for passive recreational usage, and are sized appropriately. The central plaza will include a multi-use path connection, which will be appropriately signed and decorated with colored pavers in order to ensure that bicyclists and pedestrians are safely separated.*

- (vi) The open space provides a buffer to protect sensitive environmental features and natural areas; and

*Not applicable, as there are no existing sensitive environmental features or natural areas on or near the site.*

- (vii) If possible, open space is linked to an area- or city-wide system.

*The proposal includes construction of a new multi-use path along the west side of the railroad tracks, which will connect the Goose Creek trail to the future Rail Plaza to the north. The project also includes a new east-west connection through the site which will connect the multi-use path along the tracks to Junction Pl. to the west.*

(B) Open Space in Mixed Use Developments (Developments That Contain a Mix of Residential and Nonresidential Uses):

*Not applicable, as the proposed project does not include any residential uses.*

(C) Landscaping:

- (i) The project provides for aesthetic enhancement and a variety of plant and hard surface materials, and the selection of materials provides for a variety of colors and contrasts and the preservation or use of local native vegetation where appropriate;

*The landscaping within the project primarily supports the Junction Place streetscape, and as such the majority of the landscaping is either street trees or hardy ground cover, both of which help to soften an otherwise urban development. Additional trees are provided in the central plaza, and planters are provided in the northern pocket plaza.*

- (ii) Landscape design attempts to avoid, minimize or mitigate impacts on and off site to important native species, healthy, long lived trees, plant communities of special concern, threatened and endangered species and habitat by integrating the existing natural environment into the project;

*Not applicable, as there are no existing sensitive environmental features or natural areas on or near the site.*

- (iii) The project provides significant amounts of plant material sized in excess of the landscaping requirements of Sections 9-9-12, "Landscaping and Screening Standards," and 9-9-13, "Streetscape Design Standards," B.R.C. 1981; and

*The landscape plan includes two large mature trees within the central plaza, and provides turf areas both within the plaza and along the multi-use path that are larger than required by the code.*

- (iv) The setbacks, yards and useable open space along public rights of way are landscaped to provide attractive streetscapes, to enhance architectural features and to contribute to the development of an attractive site plan.

*The streetscape along Junction Pl. is designed in accordance with the TVAP street design guidelines, and is landscaped with planting strips containing street trees.*

(D) Circulation: Circulation, including, without limitation, the transportation system that serves the property, whether public or private and whether constructed by the developer or not:

- (i) High speeds are discouraged or a physical separation between streets and the project is provided;

*A physical separation between streets and the project is provided in the form of a landscaped planting strip and 10' sidewalk. The project includes completion of the eastern side of Junction Pl. in accordance with the adopted TVAP guidelines, which are intended to discourage high speeds.*

- (ii) Potential conflicts with vehicles are minimized;

*The project minimizes the potential for conflicts between vehicles and bikers/ pedestrians by utilizing a single garage access at the northern end of the site, and by providing bollards at the bicycle entrance to the central plaza.*

- (iii) Safe and convenient connections are provided that support multi-modal mobility through and between properties, accessible to the public within the project and between the project and the existing and proposed transportation systems, including, without limitation, streets, bikeways, pedestrianways and trails;

*The project provides numerous connections supporting multi-modal mobility, including a multi-use path running along the east side of the site along the railroad tracks which will eventually connect the rail plaza to the Goose Creek path, an east-west connection providing a connection between that path and Junction Place to the west, and a 10' sidewalk running along Junction Place and providing direct access to the Goose Creek bridge.*

- (iv) Alternatives to the automobile are promoted by incorporating site design techniques, land use patterns and supporting infrastructure that supports and encourages walking, biking and other alternatives to the single-occupant vehicle;

*The proposed project includes several design and programmatic features intended to promote alternative transportation, including the aforementioned multi-use path connections, short- and long-*

*term bike parking spaces and changing facilities for tenants. In addition, the proposed “mobility hub” concept includes providing 10% of the off-street parking for carpool and carshare vehicles.*

- (v) Where practical and beneficial, a significant shift away from single-occupant vehicle use to alternate modes is promoted through the use of travel demand management techniques;

*The applicant has provided a travel demand management plan and is also part of the TDM parking district.*

- (vi) On-site facilities for external linkage are provided with other modes of transportation, where applicable;

*The project includes short- and long-term bike parking and also provides spaces for carpool and carshare vehicles.*

- (vii) The amount of land devoted to the street system is minimized; and

*The street system has been designed and implemented through the transit Village Area Plan.*

- (viii) The project is designed for the types of traffic expected, including, without limitation, automobiles, bicycles and pedestrians, and provides safety, separation from living areas and control of noise and exhaust.

*The site is designed to accommodate bicycle and pedestrian traffic as well as automobiles. The automobile parking is located in an underground parking area to control noise and exhaust. In addition, street traffic will be separated from the development by a landscaped strip and 10’ sidewalk.*

#### (E) Parking:

- (i) The project incorporates into the design of parking areas measures to provide safety, convenience and separation of pedestrian movements from vehicular movements;

*The parking garage under the north building will remove automobiles from plain view and create a level of separation between cars and pedestrians. Having only one access point that is not central to the project will also help to reduce interactions between vehicles and pedestrians.*

- (ii) The design of parking areas makes efficient use of the land and uses the minimum amount of land necessary to meet the parking needs of the project;

*By placing the parking underground, the amount of land required for parking is minimized.*

- (iii) Parking areas and lighting are designed to reduce the visual impact on the project, adjacent properties and adjacent streets; and

*By placing the parking garage underground, the visual impact of the parking area and associated lighting on adjacent streets and properties are minimized.*

- (iv) Parking areas utilize landscaping materials to provide shade in excess of the requirements in Subsection 9-9-6(d), and Section 9-9-14, "Parking Lot Landscaping Standards," B.R.C. 1981.

*Not applicable, as the parking is located underground.*

(F) Building Design, Livability and Relationship to the Existing or Proposed Surrounding Area:

- (i) The building height, mass, scale, orientation, architecture and configuration are compatible with the existing character of the area or the character established by adopted design guidelines or plans for the area;

*The site is comprised of of the two remaining undeveloped lots in the Steel Yards Subdivision. Surrounding uses include the new Nickel Flats residential building immediately across Junction Pl. and the mixed residential and live-work Steelyards development further to the west; the former Sutherlands site (currently under review for the S'Park development) immediately to the north, and mixed office and light industrial uses to the east across the railroad tracks. The Goose Creek Greenway runs just south of the site, with the mixed use Depot Square development and residential Solana development abutting the west side of the railroad tracks further to the south.*

*The site is located in the northern portion of Boulder Junction. The Boulder Junction area is guided by the Transit Village Area Plan and is anticipated to redevelop as several new urban, mixed-use, transit-oriented neighborhoods. Consistency of the project with TVAP is discussed in Key Issue #1 below.*

*The proposed northern building is 59,998 sq. ft. in size and consists of a roughly "H" shaped design that steps down the massing from four stories (55') along the rail road (east) side of the site to three stories along the Junction Pl. frontage to the west. The southern building totals 40,230 sq. ft. in size, and incorporates a similar stepped design that places a 55', four-story building element along the railroad tracks which steps down to three stories along Junction Pl. to transition to the smaller scale of the existing Steel Yards buildings to the west. Overall, the height, mass and scale of the buildings is in keeping with the adopted TVAP vision for the Mixed-Use 2 area and also fits in with the existing and planned larger building to the north and south while providing a buffer and a transition in scale to the smaller residential and mixed use buildings to the west.*

- (ii) The height of buildings is in general proportion to the height of existing buildings and the proposed or projected heights of approved buildings or approved plans or design guidelines for the immediate area;

*As noted on Pages 8 & 9 of the staff memorandum, within TVAP, the land use designation for the site is MU-2 or Mixed Use -2, which anticipates three- to four-story mixed use buildings at a FAR of 1.5 to 2.0. In addition, the intent described in TVAP for the "Rail Plaza District" in which the project site is located is for the district to "evolve into a high-density, commercial and residential mixed use area, with three- to five-story buildings." Because of the level of intensity anticipated to help support and enliven the rail plaza, overall the planned mass and scale appears to be consistent with TVAP.*

- (iii) The orientation of buildings minimizes shadows on and blocking of views from adjacent properties;

*The orientation of the buildings is dictated by the urban lot and the Junction Place street orientation. The street preserves views as you move south over Goose Creek bridge, and the train tracks to the east mean there are no immediate neighbors whose views could be affected. In addition, because of the unique shape of the property, the proposed buildings are narrow from east to west and long from north to south, meaning that the overall width of shadows crossing onto the S'Park property to the north are minimized. Overall, given the planned intensity to the north of the project site to be realized via the S'Park development as well as the fact that there are essentially open space buffers to the east and south of the site (the rail road tracks and Goose Creek, respectively), the buildings will minimize shadows and will not significantly affect existing viewsheds from other properties.*

- (iv) If the character of the area is identifiable, the project is made compatible by the appropriate use of color, materials, landscaping, signs and lighting;

*The character of the area is determined both by the existing Steel Yards development to the west as well as the intent of the adopted TVAP Character Districts. As discussed in Key Issue #1 in the staff memorandum, the project is in keeping with the Rail Plaza District design guidelines. While the signage and lighting has not yet been finalized for the project, the applicant will be required to provide final architectural plans for Technical Document Review, which will ensure consistency with the architectural intent of the project as memorialized in the approved plan set.*

- (v) Projects are designed to a human scale and promote a safe and vibrant pedestrian experience through the location of building frontages along public streets, plazas, sidewalks and paths, and through the use of building elements, design details and landscape materials that include, without limitation, the location of entrances and windows, and the creation of transparency and activity at the pedestrian level;

*Both buildings incorporate a 10-foot setback along Junction Pl., with the ground level frontages containing primary entrances to several tenant spaces and consisting primarily of brick storefronts with metal and wood accents and a mix of overhangs and recessed windows and doorways to maintain visual interest. The north and south elevations of each building consist primarily of metal paneling with high -pressure laminate and wood siding. As will be discussed in further detail below, the eastern facades along the railroad tracks will consist almost entirely of integrated solar panels. Overall, the frontages along Junction Pl., combined with the central open space plaza and the associated amenities therein, will create a genuinely welcoming and visually interesting pedestrian realm that will help to enliven Junction Pl. in accordance with the intent of the Junction Place design guidelines found in TVAP.*

- (vi) To the extent practical, the project provides public amenities and planned public facilities;

*As discussed in the Open Space criteria analysis above, the project provides two required multi-use path connections in accordance with the TVAP Connections Plan, one running north-south along the railroad tracks and one running east west across the site. The project also includes the completion of the east side of Junction Place.*

- (vii) For residential projects, the project assists the community in producing a variety of housing types, such as multifamily, townhouses and detached single family units, as well as mixed lot sizes, number of bedrooms and sizes of units;

*Not applicable, as the project is commercial only and does not include any residential units.*

- (viii) For residential projects, noise is minimized between units, between buildings and from either on-site or off-site external sources through spacing, landscaping and building materials;

*Not applicable, as the project is commercial only and does not include any residential units.*

- (ix) A lighting plan is provided which augments security, energy conservation, safety and aesthetics;

*A preliminary lighting plan has been provided. A final lighting plan will be required during Technical Document Review in order to ensure compliance with the city's lighting standards.*

- (x) The project incorporates the natural environment into the design and avoids, minimizes or mitigates impacts to natural systems;

*The project primarily supports the urban street, and as such the majority of the landscaping is either street trees or hardy ground cover, both focused on providing a sense of green and organic material in an urban space. The project also provides immediate adjacency to the Goose Creek greenway. As previously discussed, there are no existing sensitive natural systems on-site, so overall the project will enhance the urban open space network in the area.*

- (xi) Buildings minimize or mitigate energy use; support on-site renewable energy generation and/or energy management systems; construction wastes are minimized; the project mitigates urban heat island effects; and the project reasonably mitigates or minimizes water use and impacts on water quality;

*The proposed project will be required to meet the city's recently adopted energy code (International Energy Efficiency Code (IECC) plus 30 percent additional efficiency). These standards are considered to be very aggressive with regard to energy efficiency in building design. In addition, the proposed project includes a variety of sustainability features intended to achieve a LEED platinum standard at a minimum. The design and orientation of the buildings allows for optimal daylighting and uninterrupted solar access on the roof and east facade of the buildings and the project integrates solar panels into the architectural design of these elements. The fenestration patterning and materiality are intended to create a building envelope consisting of thermally broken windows, tuned window-to-glass ratios per facade, and architectural designed shades to reduce heat gain in summer and heat loss in winter. Per the applicant's written statement, the project will also seek to reduce its demand loads on an annualized basis. The project design also includes venting to provide air-flow through the building for natural cooling at night, and thermal mass in the floors to help carry that cooling throughout the day. The applicant is also exploring responsive HVAC strategies such as a variable refrigerant system to allow the building to respond to the actual unit needs rather than conditioning on a full-building scale, and a variety of tenant-based ideas are being considered to reduce plug loads.*

- (xii) Exteriors of buildings present a sense of permanence through the use of authentic materials such as stone, brick, wood, metal or similar products and building material detailing;

*The exterior palette is comprised of dark, stackbond brick and warm grey zinc panels, run vertically. Occasionally, painted steel columns and beams will be exposed. Also, a vertically-oriented wood or composite wood product is planned for a few walls on level 1 and most of the exposed soffits on level 1. This palette fits the aesthetic of the Boulder Junction, and the high quality materials will ensure that the building presents a sense of permanence throughout its life.*

- (xiii) Cut and fill are minimized on the site, the design of buildings conforms to the natural contours of the land, and the site design minimizes erosion, slope instability, landslide, mudflow or subsidence, and minimizes the potential threat to property caused by geological hazards;

*As an existing, graded urban lot, the lot needs little cut or fill except for the underground parking structure.*

- (xiv) In the urbanizing areas along the Boulder Valley Comprehensive Plan boundaries between Area II and Area III, the building and site design provide for a well-defined urban edge; and

*Not applicable, as the lot is not located within one of the urbanizing areas along the Boulder Valley Comprehensive Plan boundaries between Area II and Area III.*

- (xv) In the urbanizing areas located on the major streets shown on the map in Appendix A to this title near the Boulder Valley Comprehensive Plan boundaries between Area II and Area III, the buildings and site design establish a sense of entry and arrival to the City by creating a defined urban edge and a transition between rural and urban areas.

*Not applicable, as the lot is not located within one of the urbanizing areas along the Boulder Valley Comprehensive Plan boundaries between Area II and Area III.*

## USE REVIEW CRITERIA

**Criteria for Review:** No use review application will be approved unless the approving agency finds all of the following:

✓ (1) Consistency with Zoning and Non-Conformity: The use is consistent with the purpose of the zoning district as set forth in Section 9-5-2(c), "Zoning Districts Purposes," B.R.C. 1981, except in the case of a non-conforming use;

*The project site is zoned MU-4 (Mixed Use – 4) , defined in the land use code as: “Mixed use residential areas generally intended for residential uses with neighborhood-serving retail and office uses; and where complementary uses may be allowed. It is anticipated that development will occur in a pedestrian-oriented pattern, with buildings built up to the street.” (section 9-5-2(c)(2)(D)). For the purposes of applying zoning, the proposed use is considered a restaurant outside of the University Hill general improvement district that is over 1,500 square feet in floor area and which closes after 11 p.m., which requires a Use Review to operate in the MU-4 zone.*

\_\_\_\_\_ (2) Rationale: The use either:

- (A) Provides direct service or convenience to or reduces adverse impacts to the surrounding uses or neighborhood;

*The proposed restaurant and tavern will provide a direct service the surrounding uses and neighborhood by adding to the mix of uses along Junction Place in accordance with the adopted vision of TVAP. With 90 residential units in the Steel Yards, the new affordable units and hotel to the south, and the new residential units proposed at S\*Park, the coffee shop and restaurant will serve a much needed function for the area. The restaurant will provide both indoor and outdoor dining spaces which will serve both adjacent tenants as well as the broader public. The location of the restaurant on the southern side of the south building places it away from the existing residential uses to the west in Steel Yards and also takes advantage of the Goose Creek Greenway to act as a buffer between the outdoor seating and the nearby Boulder Junction development across the creek to the south..*

- (B) Provides a compatible transition between higher intensity and lower intensity uses;

- (C) Is necessary to foster a specific city policy, as expressed in the Boulder Valley Comprehensive Plan, including, without limitation, historic preservation, moderate income housing, residential and non-residential mixed uses in appropriate locations, and group living arrangements for special populations; or

- (D) Is an existing legal non-conforming use or a change thereto that is permitted under subsection (e) of this section;

- 3) Compatibility: The location, size, design, and operating characteristics of the proposed development or change to an existing development are such that the use will be reasonably compatible with and have minimal negative impact on the use of nearby properties or for residential uses in industrial zoning districts, the proposed development reasonably mitigates the potential negative impacts from nearby properties;

*The proposed restaurant is part of a larger, 2-building, 11,000 sq. ft. development known as "the Commons," and as such will contribute to the overall planned mix of uses in that area. The proposed hours of operation are from 11:00 am to 2:00 am, which will allow the restaurant to provide a broad range of food services. The proposed project is currently in the Site Review process, and is evaluated for consistency with the adopted goals and vision of TVAP in the staff memorandum. Overall, given TVAP's stated intent to place "active uses, such as retail or commercial services on the first floor of buildings along Junction Place," and to have the Rail Plaza District become a "high-density, commercial and residential mixed-use area," that also serves as "a lively gathering place inviting to a broad spectrum of the community," the proposed restaurant will help further these goals and to make Junction Place a more active urban area. In addition, as discussed above, the location of the restaurant on the southern side of the south building places it away from the existing residential uses to the west in Steel Yards and also takes advantage of the Goose Creek Greenway to act as a buffer between the outdoor seating and the nearby Boulder Junction development across the creek to the south..*

- (4) Infrastructure: As compared to development permitted under Section 9-6-1, "Schedule of Permitted Uses of Land," B.R.C. 1981, in the zone, or as compared to the existing level of impact of a non-conforming use,

the proposed development will not significantly adversely affect the infrastructure of the surrounding area, including, without limitation, water, wastewater, and storm drainage utilities and streets;

*The proposed restaurant is included as part of a larger redevelopment proposal for the site, and has been taken into consideration in the utility and right-of-way requirements for the development. The proposed project is of a size and intensity anticipated by TVAP, and as such is in keeping with the planned infrastructure improvements for the area.*

✓ (5) Character of Area: The use will not change the predominant character of the surrounding area or the character established by adopted design guidelines or plans for the area; and

*Within TVAP, the land use designation for the site is MU-2 or Mixed Use -2, which anticipates three- to four-story mixed use buildings at a FAR of 1.5 to 2.0. The project site is located in the area identified by TVAP as the "Rail Plaza District." As noted on page 23 of TVAP, this is the area that ultimately, "will host the Boulder stop on the new commuter rail service to Denver and Longmont." The intent of the district is further defined, "The district will evolve into a high-density, commercial and residential mixed use area, with three- to five-story buildings." As discussed in the staff memorandum, overall the project has remained consistent with the Concept Plan Review and with TVAP, especially in terms of building mass and scale, provision of required multi-use path connections, high quality open space features and activation of the Junction Place streetscape at the pedestrian level. As part of the larger proposed project, the proposed restaurant is in keeping with the character established for the area by TVAP.*

N/A (6) Conversion of Dwelling Units to Non-Residential Uses: There shall be a presumption against approving the conversion of dwelling units in the residential zoning districts set forth in Subsection 9-5-2(c)(1)(a), B.R.C. 1981, to non-residential uses that are allowed pursuant to a use review, or through the change of one non-conforming use to another non-conforming use. The presumption against such a conversion may be overcome by a finding that the use to be approved serves another compelling social, human services, governmental, or recreational need in the community including, without limitation, a use for a day care center, park, religious assembly, social service use, benevolent organization use, art or craft studio space, museum, or an educational use.

*Not applicable, as the subject proposal does not include any conversion of existing dwelling units to non-residential uses.*

**Van Schaack, Chandler**

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**From:** Sally Laventure [sallylaventure@gmail.com]  
**Sent:** Monday, June 22, 2015 11:35 AM  
**To:** Van Schaack, Chandler  
**Subject:** The Commons

As a resident of The Steelyards, with a bedroom and a garden area that will face The Commons, I am very concerned about the restaurant concept, open from 11-2AM, with outdoor seating. Possible music, outdoor talking and noise until 2 am is unacceptable to this quiet residential area. The train is enough. Please reconsider the impact this will have on The Steelyard residents. I am in favor of a restaurant, but one that is open until 2 am is not a concept I am willing to live with quietly!

Thanks for your consideration.

Sally LaVenture  
3260 Iron Forge Pl. #105  
Boulder 80301  
971-409-0083