

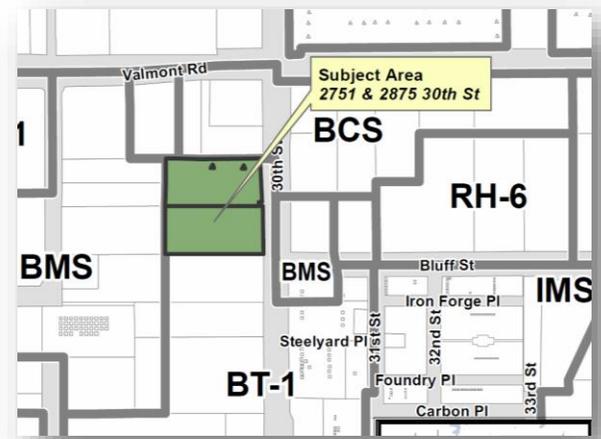
MEMORANDUM

TO: Planning Board
FROM: Elaine McLaughlin, Case Manager
DATE: September 27, 2016
SUBJECT: **Call Up Item:** Site Review: Redevelopment of a vacant lot, formerly occupied by a Recreational Vehicle (RV) dealership and repair facility located at 2751 30th Street. Proposed are 32 townhomes and four small corner retail spaces with below grade parking, a central open space area and a parking reduction of 25 percent or 60 spaces where 80 spaces are standard.

ADDRESS: 2751 30th Street
PROJECT NAME: Boulder Junction Rowhomes
CASE NUMBER: LUR2016-00008

Background

The 1.85-acre project site is located on the west side of 30th Street, just south of the intersection of 30th and Valmont Rd. The site is comprised of two parcels, one of which is currently undeveloped and the other of which currently contains an RV repair store. Surrounding uses include the mixed residential and live-work Steelyards development across 30th St. to the southeast as well as a variety of service industrial and retail uses along the 30th Street corridor running north and south of the project site. The site is located within the area identified as Phase 2 of the adopted Transit Village Area Plan (TVAP). TVAP Phase 2 includes the areas east of the railroad tracks and west of 30th Street, wherein regulatory changes necessary to implement the vision of TVAP, including land use map and zoning changes, are anticipated to occur after substantial redevelopment of the areas in Phase 1 is complete. The application was reviewed as a Concept Plan by the Planning Board on [October 15, 2015](#). The project plans responded to Concept Plan comments by providing retail spaces on the ground floor along 30th Street on the north and south ends of both buildings; response to access and site layout issues to better respond to future TVAP connections; as well as provision of greater permeability through the site. In addition, working with the Design Advisory Board and staff through the process, the building fenestration patterns and material choices improved.



Proposed Project

The applicant is proposing 32 three-story brick townhomes and a central open space area over a podium parking structure with both passive and active recreational amenities. Also proposed are four small retail spaces on the ends of the buildings located along 30th Street. A total of 12 units will face 30th Street in two separate buildings (example shown on following page), and a total of 12 units will face west and an alley on the west side of the property. The applicant is proposing rooftop photovoltaic panels to address the city's energy efficiency standards. The applicant is also proposing a 25 percent parking reduction: 60 spaces are proposed where 80 are standard. The parking reduction is discussed in more detail below. The proposed bedroom count per unit and parking requirements are summarized as follows:

parking required / provided				
count	unit	spaces	required	unit location(s)
16	3 bed-rm	2	32	29 1/2 Street
12	4 bed-rm	3	36	30th Street
4	2 bed-rm	1.5	6	above commercial
2	comm	2	4	north and south ends
2	comm	1	2	either side - Paseo
			80	spaces required
			60	spaces required
			60	provided



Analysis

The application was found to meet the Site Review Criteria along with the requirements of the Business Transition – 1 (BT-1) zoning for form, bulk and setbacks. Townhomes are considered a by-right use within the BT-1 zoning which is defined in section 9-5-2 of the Land Use Code as follows:

Transitional business areas which generally buffer a residential area from a major street and are primarily used for commercial and complementary residential uses, including without limitation, temporary lodging and office uses.

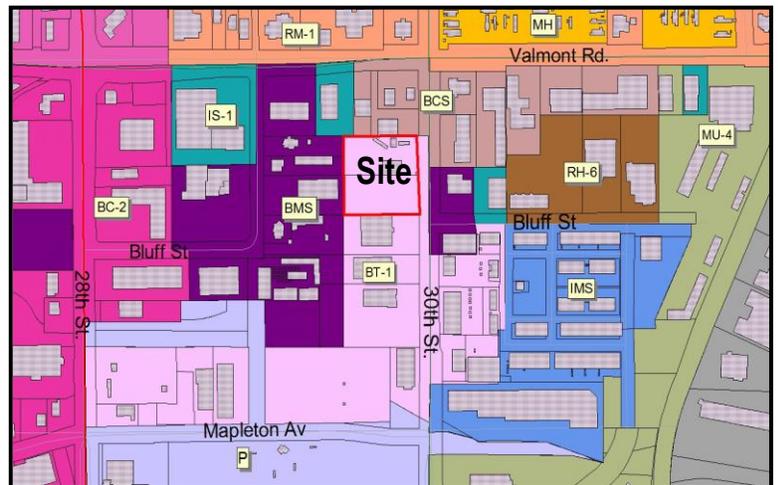
Because the site is located within the area designated as Phase II of the Transit Village Area Plan (TVAP), the future zoning for the project site (under the TVAP land use designation of MU1) is Business Main Street (BMS), defined per section 9-5-2(c)(2)(F), B.R.C. 1981 as:

“Business areas generally anchored around a main street that are intended to serve the surrounding residential neighborhoods. It is anticipated that development will occur in a pedestrian-oriented pattern, with buildings built up to the street; retail uses on the first floor; residential and office uses above the first floor; and where complementary uses may be allowed.”

The provision of 32 townhomes constructed of brick with stone lintels in an urban configuration along 30th Street near services and transit meets a number of Site Review criteria including section 9-2-14(h)(2)(F)

“(vii) For residential projects, the project assists the community in producing a variety of housing types, such as multifamily, townhouses and detached single family units, as well as mixed lot sizes, number of bedrooms and sizes of units.”

In particular, the provision of townhomes that include three- and four-bedrooms will provide an important residential option particularly appealing to families. In addition, the enclosed open space central to the site has both active and passive recreational amenities that can serve as a neighborhood gathering space. The provision of small retail spaces will help to establish a vertical mix of uses for the buildings along 30th Street that is in keeping with the anticipated MU1 land use of Boulder Junction and are of sufficient size given the significant amount of retail within a ¼ mile radius of the site.



The parking reduction of 25 percent is considered consistent with the Site Review criteria in that the property uniquely located near a number of services and amenities including retail, restaurants, access to the Goose Creek Multi-Use Path, and the RTD station at Depot Square. There's also a significant number of bus stops in the immediate area as shown on the bus map:

Alternatives to the automobile are being promoted through a Transportation Demand Management Plan that includes: installation of bike racks, covered bike storage, Eco-Passes for a period of three years; unbundled parking: parking spaces will not be designated to specific units during business hours. This will aid in parking congestions as residents who do not have a vehicle will not have a parking space sitting unoccupied and residents with the need for additional parking spaces can obtain them. This will also aid in the use of parking spaces for commercial units in the daytime as parking spaces will be reserved for after hour use. The applicant is illustrating 27 EV charging stations or roughly half of the number of vehicle spaces devoted to the option of Electric Vehicle charging station. This, combined with the rooftop photovoltaic panels will help to meet the city's rigorous energy efficiency standards along with the Site Review criteria for on-site renewable energy generation and management.



Public Comment. Required public notice was given in the form of written notification mailed to all property owners within 600 feet of the subject property and a sign posted on the property for at least 10 days. All notice requirements of [section 9-4-3](#), "Public Notice Requirements," B.R.C. 1981 have been met. No comments were received on this application.

Conclusion. The proposal was *approved* by staff on September 27, 2016 and the decision may be called up before Planning Board on or before **October 11, 2016** and there is one Planning Board hearing scheduled during the required 14 day call-up period on **October 6, 2016**. Questions about the project or decision should be directed to the Case Manager, Elaine McLaughlin at (303) 441-4130 or at the following email address: mclaughline@bouldercolorado.gov.

Attachments

- A. [Signed Disposition](#)
- B. [Site Review](#)
- C. [Project Plans/Written Statement](#)



**CITY OF BOULDER
Planning and Development Services**

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phone 303-441-1880 • fax 303-441-3241 • email plandevlop@bouldercolorado.gov
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**CITY OF BOULDER PLANNING DEPARTMENT
NOTICE OF DISPOSITION**

You are hereby advised that the following action was taken by the Planning Department based on the standards and criteria of the Land Use Regulations as set forth in Chapter 9-2, B.R.C. 1981, as applied to the proposed development.

DECISION: **APPROVED WITH CONDITIONS**
 PROJECT NAME: **BOULDER JUNCTION ROWHOUSES**
 DESCRIPTION: **SITE REVIEW for 32 townhomes and four retail/commercial spaces with interior open space and below grade parking.**
 LOCATION: **2751 30TH ST**
 COOR: **N04W04**
 LEGAL DESCRIPTION: **Refer to Exhibit A attached**
 APPLICANT: **Jason Lewiston**
 OWNER: **Boulder Junction Rowhomes LLC**
 APPLICATION: **Site Review, LUR2016-00008**
 ZONING: **BT-1**
 CASE MANAGER: **Elaine McLaughlin**
 VESTED PROPERTY RIGHT: **NO; the owner has waived the opportunity to create such right under Section 9-2-19, B.R.C. 1981.**

APPROVED MODIFICATIONS FROM THE LAND USE REGULATIONS:
 Section 9-9-6, "Parking Standards," B.R.C. 1981: 25 percent parking reduction to provide 60 spaces provided where 80 are standard.

FOR CONDITIONS OF APPROVAL, SEE THE FOLLOWING PAGES OF THIS DISPOSITION.

Approved On: _____
 Date: 9/27/16

By: _____
 David Driskell, Executive Director of Community Planning and Sustainability

This decision may be appealed to the Planning Board by filing an appeal letter with the Planning Department within two weeks of the decision date. If no such appeal is filed, the decision shall be deemed final fourteen days after the date above mentioned.

Appeal to Planning Board expires: 10/11/16
 Final Approval Date: 10/12/16

Address: 2751 30TH ST

IN ORDER FOR A BUILDING PERMIT APPLICATION TO BE PROCESSED FOR THIS PROJECT, A SIGNED DEVELOPMENT AGREEMENT AND FINAL PLANS FOR CITY SIGNATURE MUST BE SUBMITTED TO THE PLANNING DEPARTMENT WITH DISPOSITION CONDITIONS AS APPROVED SHOWN ON THE FINAL PLANS, IF THE DEVELOPMENT AGREEMENT IS NOT SIGNED WITHIN NINETY (90) DAYS OF THE FINAL DECISION DATE, THE PLANNING DEPARTMENT APPROVAL AUTOMATICALLY EXPIRES.

Pursuant to Section 9-2-12 of the Land Use Regulations (Boulder Revised Code, 1981), the applicant must begin and substantially complete the approved development within three years from the date of final approval. Failure to "substantially complete" (as defined in Section 9-2-12) the development within three years shall cause this development approval to expire.

CONDITIONS OF APPROVAL

1. The Applicant shall be responsible for ensuring that the **development shall be in compliance with all approved plans prepared by the Applicant on August 30, 2016** and the Transportation Demand Management (TDM) Plan dated July 13, 2016 on file in the City of Boulder Planning Department except to the extent that the development may be modified by these conditions of approval.
2. Prior to a building permit application, the Applicant shall submit, and obtain City Manager approval of, a Technical Document Review application for the following items:
 - a. **Final architectural and site plans**, which include detailed floor plans and section drawings including material samples and colors, to insure compliance with the intent of this approval and compatibility with the surrounding area. The final plans shall illustrate the building to be prewired for future photovoltaic systems, from the roof-top to the primary electrical panel for the building. The architectural intent shown on the approved plans prepared by the Applicant on August 30, 2016 is acceptable. Planning staff will review the plans to ensure that the architectural intent is performed.
 - b. **A final utility plan** meeting the City of Boulder Design and Construction Standards.
 - c. **A final storm water report and plan** meeting the City of Boulder Design and Construction Standards.
 - d. **Final transportation plans** meeting the City of Boulder Design and Construction Standards and the CDOT State Highway Access Code Standards for all transportation improvements. These plans must include, but are not limited to: plan and profile drawings for the north/south alley; plan and profile drawings for 30th Street where the cross-section of the road is to be widened; profile drawings for the flow-line of the curb-and-gutter to be constructed or re-constructed along 30th Street and extending twenty-five feet beyond the existing curb-and-gutter to ensure positive drainage; plan and profile drawings for the 30th Street sidewalk, cross-sections at twenty-five foot intervals or where required by staff for 30th Street and the new alley; typical sections for the streets and alley; signage and striping plans in conformance with Manual on Uniform Traffic Control Devices (MUTCD) standards, street lighting, driveway ramp and transportation detail drawings, geotechnical soils report and pavement analysis.
 - e. **A detailed landscape plan**, including size, quantity, and type of plants existing and proposed; type and quality of non-living landscaping materials; any site grading proposed; and any irrigation system proposed, to insure compliance with this approval and the City's landscaping requirements. Removal of trees must receive prior approval of the Planning Department. Removal of any tree in the City right of way must also receive prior approval of the City Forester.

- f. **A detailed outdoor lighting plan** showing location, size and intensity of illumination units, indicating compliance with section 9-9-16, B.R.C. 1981.
3. Prior to a building permit application, the Applicant shall submit an application for and receive approval of a **Preliminary Plat** and submit a Technical Document Review application and receive approval for a **Final Plat**, subject to the review and approval of the City Manager and execute a subdivision agreement meeting the requirements of chapter 9-12, "Subdivision," B.R.C. 1981 and which provides, without limitation and at no cost to the City, for the following, unless otherwise approved by the City Manager:
 - a. The dedication, to the City, of all right-of-way and easements necessary to serve the development.
 - b. The construction of all public improvements necessary to serve the development, including but not limited to: the new alley running north/south; the widening of 30th Street to accommodate a northbound left-turn lane; five-foot wide bike lane, two-foot wide curb-and-gutter; landscape strip and detached sidewalk; relocation of existing street lighting; stormwater detention facilities; and water and sanitary sewer mains.
 - c. A financial guarantee, in a form acceptable to the Director of Public Works, in an amount equal to the cost of constructing all public improvements necessary to serve the development.
4. Prior to a building permit application and as part of a Technical Document Review application, the Applicant shall ensure that the owner of the property known as 2691 30th Street dedicates to the City, at no cost, **a 20-foot wide temporary emergency access easement** which will connect to 30th Street to be generally located along the westerly and southerly property lines of the 2691 30th Street property, meeting City of Boulder Design and Construction standards, the form and final location of which shall be subject to the approval of the City Manager. This temporary emergency access easement shall extinguish once another code-compliant emergency access easement is provided subject to City Manager approval.
5. Prior to issuance of a building permit, the Applicant shall submit a **financial guarantee**, in a form acceptable to the Director of Public Works, in an amount equal to the cost of providing eco-passes to the residents and employees of the development for three years after the issuance of a certificate of occupancy for each dwelling unit and commercial / retail unit as proposed in the Applicant's TDM Plan.

Exhibit A: Legal Description

LEGAL DESCRIPTION

A PART OF THE NW 1/4 OF SECTION 29, TOWNSHIP 1 NORTH, RANGE 70 WEST OF THE 6TH P.M., DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT 417.06 FEET SOUTH AND 20 FEET WEST OF THE NORTH QUARTER CORNER OF SAID SECTION; THENCE WEST 309.55 FEET; THENCE SOUTH 140.72 FEET; THENCE EAST 309.535 FEET; THENCE NORTH 140.72 FEET TO THE POINT OF BEGINNING, COUNTY OF BOULDER, STATE OF COLORADO,

LESS AND EXCEPT ANY PORTION OF THE SUBJECT PROPERTY LYING WITHIN THE 30TH STREET RIGHT OF WAY.

CRITERIA FOR REVIEW:

No site review application shall be approved unless the approving agency finds that:

(1) Boulder Valley Comprehensive Plan:

√ (A) **The proposed site plan is consistent with the land use map and the service area map and, on balance, the policies of the Boulder Valley Comprehensive Plan.**

The site is located within the service area of the city and is being proposed consistent with the BVCP land use map designation: Mixed Use Industrial. On page 67 of the Comprehensive Plan, the Mixed Use Industrial land use is defined as follows:

“Mixed Use-Industrial development may be deemed appropriate and will be encouraged in some industrial areas where industrial character will predominate. Housing compatible with and appropriate to the industrial character will be encouraged and may be required. Neighborhood retail and service uses may be allowed. Specific zoning and other regulations will be adopted which define the desired intensity, mix, location and design characteristics of these uses.”

While the site is located within Phase II of the area guided by the Transit Village Area Plan, the phased approach to implementation of TVAP requires the city’s funding of key public improvements and the regulatory aspects of the plan involving land use and zoning changes, TDM and transportation connections. Because of this, a BVCP Land Use Map change and Rezoning of the project site to Business Main Street (per phase II of TVAP) would not be warranted at this time. The criteria for city initiation of Phase 2 land use and zoning changes are the following:

- Substantial redevelopment of Phase 1;
- Plan in place for providing public improvements to Phase 2; and
- Market support for Phase 2 land uses

The policies of the BVCP also encourage a compact form of development and promote higher density development along existing and future multi-modal corridors, in compatible surroundings. Policies within the BVCP also aim to mitigate the increasingly significant in-commuting trend due to the current jobs-to-housing imbalance by requiring development projects to provide affordable housing. The development pattern established by the relationship of the transit facility to the proposed (and future) residential, in concert with the mix of uses that include retail and nearby office industrial directly fulfills a number of the BVCP policies including:

- 2.16 Mixed Use and Higher Density Development*
- 2.17 Variety of Activity Centers*
- 2.21 Commitment to a Walkable and Accessible Community*
- 2.23 Trail Corridors/Linkages*
- 2.30 Sensitive Infill and Redevelopment*
- 2.31 Design of Newly-Developing Areas*
- 2.32 Physical Design for People*
- 2.33 Environmentally Sensitive Urban Design*
- 2.34 Importance of Street Trees and Streetscapes*
- 2.37 Enhanced Design for Private Sector Projects*

The following are responses specific to several key policies:

2.16 Mixed Use and Higher Density Development

The city will encourage well-designed mixed use and higher density development that incorporates a substantial amount of affordable housing in appropriate locations, including in some commercial centers and industrial areas and in proximity to multimodal corridors and transit centers.

The provision of townhomes, considered higher density residential, mixed with some commercial space is a unique unit type in the context. The townhomes would likely be appealing to families and are within proximity to the BRT station at Depot Square. The Inclusionary Housing requirement for the site is being met through cash-in-lieu. While there will not be “a substantial amount of affordable housing” on the site, the application meets the IH requirement and the contribution will allow for affordable housing to be built.

2.31 Design of Newly-Developing Areas

The city will encourage a neighborhood concept for new development that includes a variety of residential densities, housing types, sizes and prices, opportunities for shopping, nearby support services and conveniently sited public facilities, including roads and pedestrian connections, parks, libraries and schools.

The proposed project offers a unique unit type within the context: a townhome that would be appealing to families and in a location where there is an opportunity for shopping and other services. The site is located 600 feet from both the Goose Creek Multi-Use Path as well as the Mapleton Ballfields and the YMCA.

2.37 Enhanced Design for Private Sector Projects

Through its policies and projects, the city will encourage or require quality architecture and urban design in private sector development that encourages alternative modes of transportation, provides a livable environment and addresses the elements listed below:

b) Relationship to the public realm. Projects should relate positively to public streets, plazas, sidewalks, paths, ditches and natural features. Buildings and landscaped areas—not parking lots—should present a well-designed face to the public realm, should not block access to sunlight, and should be sensitive to important public view corridors. Future strip commercial development will be discouraged.

The proposed townhomes are planned to address the street in an urban configuration in keeping with the existing Steelyards development across 30th Street. The well designed buildings along 30th Street will provide pedestrian interest through the use of well-defined entries; durable, attractive materials including brick and stone; and through the “punctuation marks” of commercial spaces at the corners of the buildings along 30th Street.



√ (B) The proposed development shall not exceed the maximum density associated with the Boulder Valley Comprehensive Plan residential land use designation. Additionally, if the density of existing residential development within a three-hundred-foot area surrounding the site is at or exceeds the density permitted in the Boulder Valley Comprehensive Plan, then the maximum density permitted on the site shall not exceed the lesser of:

The BVCP does not specify density requirements for the Mixed Use Industrial land use designations and therefore this part of the criterion doesn't apply to that area.

The existing residential development within 300 feet of the site includes the residential units at Steelyards which has densities that meet the density permitted in the BVCP.

n/a (i) The density permitted in the Boulder Valley Comprehensive Plan, or,

√ (ii) The maximum number of units that could be placed on the site without waiving or varying any of the requirements of [chapter 9-8](#), "Intensity Standards," B.R.C. 1981.

The land use designation on the site is consistent with the intensity standards of the equivalent zoning district BT-1 zoning district. The application is well within the maximum density permitted in the BT-1 zoning of 1,200 square feet of open space per dwelling unit (in this case: 38,400 square feet of open space is required for 32 units and there is 43,670 square feet of open space provided on the plans).

√ (C) The proposed development's success in meeting the broad range of BVCP policies considers the economic feasibility of implementation techniques require to meet other site review criteria.

The proposed project incorporates high quality and durable building materials on attractive buildings along with well designed open space areas. The development would not be rendered infeasible in meeting the BVCP polices or the Site Review criteria.

(2) Site Design: Projects should preserve and enhance the community's unique sense of place through creative design that respects historic character, relationship to the natural environment, multi-modal transportation connectivity and its physical setting. Projects should utilize site design techniques which are consistent with the purpose of site review in subsection (a) of this section and enhance the quality of the project. In determining whether this subsection is met, the approving agency will consider the following factors:

(A) Open Space: Open space, including, without limitation, parks, recreation areas, and playgrounds:

√ (i) Useable open space is arranged to be accessible and functional and incorporates quality landscaping, a mixture of sun and shade and places to gather;

The project plans illustrates both individual balcony spaces as well as a central courtyard space that includes opportunities for both passive and active recreation.

n/a (ii) Private open space is provided for each detached residential unit;

There are no detached residential units planned on the site.

n/a (iii) The project provides for the preservation of or mitigation of adverse impacts to natural features, including, without limitation, healthy long-lived trees, significant plant communities, ground and surface water, wetlands, riparian areas, drainage areas and species on the federal Endangered Species List, "Species of Special Concern in Boulder County" designated by Boulder County, or prairie dogs (*Cynomys ludovicianus*), which is a species of local concern, and their habitat;

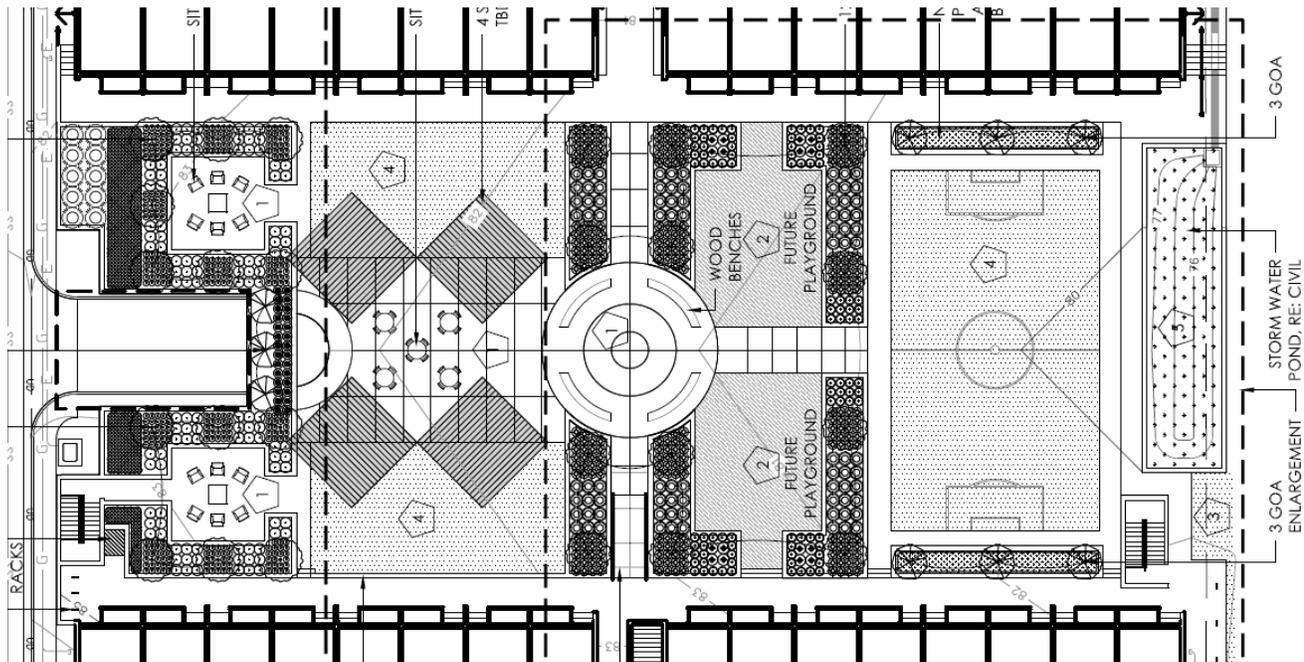
There are no known special status plant or animal species on the project site.

√ (iv) The open space provides a relief to the density, both within the project and from surrounding development;

The interior courtyard space provides a relief to density in a communal gathering area.

√ (v) Open space designed for active recreational purposes is of a size that it will be functionally useable and located in a safe and convenient proximity to the uses to which it is meant to serve;

Configured with an internal courtyard space with units opening to the space, the active recreational areas shown on the landscape plan will provide a useable, and functional space that would be attractive to families.



n/a (vi) The open space provides a buffer to protect sensitive environmental features and natural areas;

There are no sensitive environmental features or natural areas on the site or adjacent to the site.

√ (vii) If possible, open space is linked to an area- or city-wide system.

The planned detached walkway will link to the existing urbanized street system, and the walkway links to the nearby Goose Creek Multi-Use Path (600 feet to the south).

(B) Open Space in Mixed Use Developments (Developments that contain a mix of residential and non-residential uses)

√ (i) The open space provides for a balance of private and shared areas for the residential uses and common open space that is available for use by both the residential and non-residential uses that will meet the needs of the anticipated residents, occupants, tenants, and visitors of the property;

The internal courtyard space creates a common gathering space for the residential units; there are also private 3rd story balcony spaces proposed for each townhome. There are small patio spaces planned to serve the non-residential spaces proposed.

√ (ii) The open space provides active areas and passive areas that will meet the needs of the anticipated residents, occupants, tenants, and visitors of the property and are compatible with the surrounding area or an adopted plan for the area.

There's variety in the open space area (courtyard space with play areas) that offers both passive and active recreational amenities. The active recreation includes a ballfield and a playground; along with passive amenities such as outdoor seating and tables with shade structure. There is an adopted plan for the area: TVAP but it is not in effect for the site which falls under Phase II of TVAP.

(C) Landscaping

√ (i) The project provides for aesthetic enhancement and a variety of plant and hard surface materials, and the selection of materials provides for a variety of colors and contrasts and the preservation or use of local native vegetation where appropriate;

The proposed landscape plan illustrates both hard surface courtyard spaces along with a variety of planting areas. The plant schedule indicates mixes of perennials, ornamental grasses, shrubs and vines; along with a variety of trees both understory/ornamental trees and larger canopy and evergreen trees.

√ (ii) Landscape design attempts to avoid, minimize, or mitigate impacts to important native species, plant communities of special concern, threatened and endangered species and habitat by integrating the existing natural environment into the project;

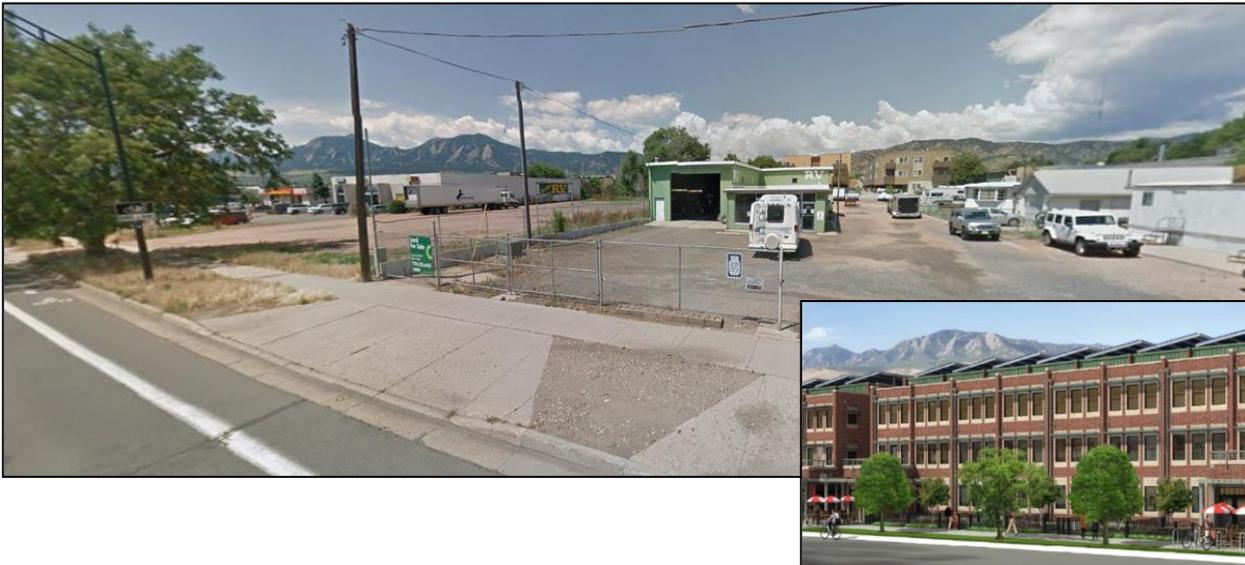
There are no known special status species within the project site.

√ (iii) The project provides significant amounts of plant material sized in excess of the landscaping requirements of [sections 9-9-12](#), "Landscaping and Screening Standards" and [9-9-13](#), "Streetscape Design Standards," B.R.C. 1981;

There are 42 trees required per the land use code and there are 64 trees provided; and there are 237 shrubs where 140 are required.

√ (iv) The setbacks, yards, and useable open space along public rights-of-way are landscaped to provide attractive streetscapes, to enhance architectural features, and to contribute to the development of an attractive site plan.

The public rights of way along both 30th Street and the west alley are planned to be well landscaped and will contribute to an attractive streetscape, particularly along 30th Street where today there are few plant materials. Shown below is the existing site and a rendering of the proposed buildings inset in approximately the same location.



(D) Circulation: Circulation, including, without limitation, the transportation system that serves the property, whether public or private and whether constructed by the developer or not:

√ (i) High speeds are discouraged or a physical separation between streets and the project is provided;

The site is designed to have a single access off of 30th Street per the Design and Construction Standards; and vehicles have specific well-defined access into the below grade parking and the alley. In addition, the applicant is illustrating the alley that is identified in the TVAP connections plan, although the site is not yet subject to TVAP, the connection will be put in place.

√ (ii) Potential conflicts with vehicles are minimized;

There is limited vehicular access within the site, primarily it is intended to access the below grade parking or the alley that is anticipated to connect with other properties in the future.

√ (iii) Safe and convenient connections are provided that support multi-modal mobility through and between properties, accessible to the public within the project and between the project and the existing and proposed transportation systems, including, without limitation, streets, bikeways, pedestrianways and trails;

Along with the proposed detached walkway, there is a connection to a proposed alley on the north side of the site that will ultimately connect to other properties in the future. The site is located 600 feet north of the Goose Creek Multi-use Path that has regional connectivity to other paths within the city, both to the east and west.

√ (iv) Alternatives to the automobile are promoted by incorporating site design techniques, land use patterns, and supporting infrastructure that supports and encourages walking, biking, and other alternatives to the single-occupant vehicle;

The applicant is proposing a moderate (25 percent) parking reduction given the close proximity to the RTD BRT station at Depot Square and the immediate access to a number of bus lines and bus stops as shown in the RTD map to the right. The applicant will be required to provide EcoPasses for a period of three years to occupants. In addition, the applicant is proposing 62 short term bike parking spaces and 56 long term bike storage spaces within the garage.

√ (v) Where practical and beneficial, a significant shift away from single-occupant vehicle use to alternate modes is promoted through the use of travel demand management techniques;

Refer to response to (iv) above.

√ (vi) On-site facilities for external linkage are provided with other modes of transportation, where applicable;

An existing bus stop is located approximately 100 feet to the north and the proposed detached walkway will provide direct access to the bus stop. As shown to the right, the site is located along or very close to a number of bus lines including the Bound, the Hop, the 206 and the 208 and is located approximately ¼ mile from the regional RTD bus rapid transit (BRT) facility.

√ (vii) The amount of land devoted to the street system is minimized; and

See response to (i) above.

√ (viii) The project is designed for the types of traffic expected, including, without limitation, automobiles, bicycles, and pedestrians, and provides safety, separation from living areas, and control of noise and exhaust.

See response to (i) above.



(E) Parking

√ (i) **The project incorporates into the design of parking areas measures to provide safety, convenience, and separation of pedestrian movements from vehicular movements;**

The below grade parking structure is designed consistent with the city's Design and Construction Standards to provide safety and convenience for pedestrians. The parking structure is below grade and therefore the following criteria: (E)(ii)(iii) and (iv) are not applicable.

(ii) The design of parking areas makes efficient use of the land and uses the minimum amount of land necessary to meet the parking needs of the project;

This criteria is met due to the parking that is proposed below grade.

(iii) Parking areas and lighting are designed to reduce the visual impact on the project, adjacent properties, and adjacent streets; and

This criteria is met due to the parking that is proposed below grade.

(iv) Parking areas utilize landscaping materials to provide shade in excess of the requirements in Subsection 9-9-6 (d), "Parking Area Design Standards," and Section 9-9-14, "Parking Lot Landscaping Standards," B.R.C. 1981.

This criteria is met due to the parking that is proposed below grade.

(F) Building Design, Livability, and Relationship to the Existing or Proposed Surrounding Area

√ (i) **The building height, mass, scale, orientation, and configuration are compatible with the existing character of the area or the character established by an adopted plan for the area;**

The buildings do not exceed the 35 foot height limit and with three stories planned, they are consistent with the immediate context of Steelyards across 30th Street as shown below. The building-forward design is consistent both with the configuration of buildings in Steelyards along 30th Street they are proposed to be in keeping with the configuration anticipated in TVAP, although the application is not required to be consistent with the area plan as it is located within Phase 2 of the plan.



√ (ii) The height of buildings is in general proportion to the height of existing buildings and the proposed or projected heights of approved buildings or approved plans for the immediate area;

As noted above, the three story and 35-foot heights proposed are consistent with the two and three story heights within Steelyards as well as the Goose Creek condominiums located to the south; and the recently constructed permanently affordable apartments (the Ledges) located on 29th Street to the west of the site. There are some existing single story buildings nearby within the service commercial areas nearby that include service stations and warehouses. However, the residential buildings in close proximity are primarily two and three stories.

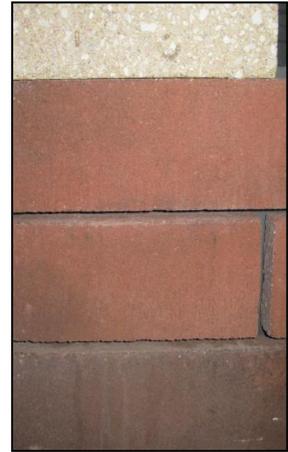


√ (iii) The orientation of buildings minimizes shadows on and blocking of views from adjacent properties;

The densities anticipated for this area are urban and within Solar Access Area III where, per land use code section 9-9-17(c)(3), B.R.C.1981: *“because of planned densities, topography or lot configurations or orientations, uniform solar access protection for the south yards and walls or for rooftops may unduly restrict permissible development.”* Today, the site is developed as a surface parking lot with a single story building set far back from 30th Street. With an urban configuration planned for the project, the existing broad views of the Flatirons across the parking lot will not be seen along 30th Street. This would be true if the building were two stories as well. Like most of the urbanized areas of Boulder, the views toward the Flatirons in particular are ephemeral, that is, as one moves along a street or walkway views toward the Flatirons open back up and become visible, then are obscured by a building. That condition is true whether a building is single story or five stories when one is walking, biking or driving adjacent to a building that is in the foreground of a mountain or Flatirons view.

√ (iv) **If the character of the area is identifiable, the project is made compatible by the appropriate use of color, materials, landscaping, signs, and lighting;**

The Steelyards established the most recent character of the area, with use of red and blond brick and with fenestration that is traditional in scale and proportion. The project is proposed with brick and traditional scale and proportion and would be compatible in this context.



√ (v) **Projects are designed to a human scale and promote a safe and vibrant pedestrian experience through the location of building frontages along public streets, plazas, sidewalks and paths, and through the use of building elements, design details and landscape materials that include, without limitation, the location of entrances and windows, and the creation of transparency and activity at the pedestrian level;**

The buildings are designed to a human scale with well-defined fenestration and patterning. The corners of the buildings along 30th Street are punctuated by commercial spaces that also provide pedestrian interest as one moves through the space adjacent to the building. The buildings are located close to the 30th Street right of way and there are building elements, design details and landscaping designed to enhance pedestrian interest.

√ (vi) **To the extent practical, the project provides public amenities and planned public facilities;**

The public realm in this relatively small site is realized in the improvements to the public right of way on 30th Street. The applicant is proposing a new streetscape that will include a detached walkway and street trees where today little landscaping exists.

√ (vii) **For residential projects, the project assists the community in producing a variety of housing types, such as multifamily, townhouses and detached single family units, as well as mixed lot sizes, number of bedrooms and sizes of units;**

The proposed project will provide 32 townhomes in an area where there are few such units that would likely be attractive to families. Recent buildout and approvals in the area include a number of one and two bedroom stacked flat apartments, the townhomes will provide a unique housing option and add variety to the existing residential context.

√ (viii) **For residential projects, noise is minimized between units, between buildings, and from either on-site or off-site external sources through spacing, landscaping, and building materials;**

The applicant has placed the recycling and trash service areas within the below grade parking structure which will help to screen noise. Similarly, commercial spaces that could generate noise are placed on the corners of the buildings along the 30th Street where there is an existing noise environment.

√ (ix) **A lighting plan is provided which augments security, energy conservation, safety, and aesthetics;**

A lighting plan will be evaluated at technical document review for consistency with the city's dark skies ordinance.

√ (x) **The project incorporates the natural environment into the design and avoids, minimizes, or mitigates impacts to natural systems;**

There are no natural systems on the project site that today is a broad parking lot for RVs.

√ (xi) **Buildings minimize or mitigate energy use; support on-site renewable energy generation and/or energy management systems; construction wastes are minimized; the project mitigates urban heat island effects; and the project reasonably mitigates or minimizes water use and impacts on water quality.**

The project plans illustrate rooftop photovoltaic panels for each unit. Similarly, the applicant is proposing 27 electric vehicle charging stations within the parking garage. Heat island effect is minimized by moving parking to below grade and placing a park like space above the parking structure. The existing heat island effect from the broad parking lot will be eliminated with the proposed project.

√ (xii) **Exteriors or buildings present a sense of permanence through the use of authentic materials such as stone, brick, wood, metal or similar products and building material detailing;**

The applicant has designed four sides of the buildings with brick, stone and limited metal panels. This will help to establish a sense of permanence.

√ (xiii) **Cut and fill are minimized on the site, the design of buildings conforms to the natural contours of the land, and the site design minimizes erosion, slope instability, landslide, mudflow or subsidence, and minimizes the potential threat to property caused by geological hazards;**

The existing surface parking lot will be removed and a single story parking structure will be excavated into the site to establish below grade parking. The townhomes will also have basements for which site excavation will be required.

n/a (xiv) **In the urbanizing areas along the Boulder Valley Comprehensive Plan boundaries between Area II and Area III, the building and site design provide for a well-defined urban edge; and**

n/a (xv) **In the urbanizing areas located on the major streets shown on the map in Appendix A of this title near the Boulder Valley Comprehensive Plan boundaries between Area II and Area III, the buildings and site design establish a sense of entry and arrival to the City by creating a defined urban edge and a transition between rural and urban areas.**

(G) Solar Siting and Construction: For the purpose of ensuring the maximum potential for utilization of solar energy in the City, all applicants for residential site reviews shall place streets, lots, open

spaces, and buildings so as to maximize the potential for the use of solar energy in accordance with the following solar siting criteria:

√ (i) **Placement of Open Space and Streets:** Open space areas are located wherever practical to protect buildings from shading by other buildings within the development or from buildings on adjacent properties. Topography and other natural features and constraints may justify deviations from this criterion.

The site is consistent with the terms of Solar Access Area III defined in the land use code section 9-9-17, B.R.C. 1981 as follows,

“includes areas where, because of planned densities, topography or lot configurations or orientations, uniform solar access protection for south yards and walls or for rooftops may unduly restrict permissible development.”

√ (ii) **Lot Layout and Building Siting:** Lots are oriented and buildings are sited in a way which maximizes the solar potential of each principal building. Lots are designed to facilitate siting a structure which is unshaded by other nearby structures. Wherever practical, buildings are sited close to the north lot line to increase yard space to the south for better owner control of shading.

The buildings have been configured to align 30th Street (a north-west arterial) with a center courtyard. This will permit light, air and solar access into the site. The roof tops are aligned with PV panels.

√ (iii) **Building Form:** The shapes of buildings are designed to maximize utilization of solar energy. Buildings shall meet the solar access protection and solar siting requirements of [section 9-9-17](#), "Solar Access," B.R.C. 1981.

The building form is intended to balance the utilization of solar energy, ensuring solar access into the communal courtyard space along with adherence to principles of good urban design where buildings align and address 30th Street and the future alley.

√ (iv) **Landscaping:** The shading effects of proposed landscaping on adjacent buildings are minimized.

The proposed planting of 64 deciduous trees throughout the site will provide shade during hot summer months and allow solar access when branches are denuded of leaves during the winter months. Within the site, the landscaping has been designed to balance both sun and shade within the open spaces, along the streets, and within the setbacks. Soil volumes proposed for trees exceeds recommended volumes for example for a 10-foot canopy tree, the recommended volume is 157 cubic feet where 180 to 191 cubic feet of soil volume is proposed. Similarly, for a 12-foot canopy tree the recommended soil volume is 226 cubic feet where 308- to 514 cubic feet of soil volume is proposed.

The following criteria is not applicable to the proposed project.

 (H) **Additional Criteria for Poles Above the Permitted Height:** No site review application for a pole above the permitted height will be approved unless the approving agency finds all of the following:

___(i) The light pole is required for nighttime recreation activities, which are compatible with the surrounding neighborhood, or the light or traffic signal pole is required for safety, or the electrical utility pole is required to serve the needs of the City; and

___(ii) The pole is at the minimum height appropriate to accomplish the purposes for which the pole was erected and is designed and constructed so as to minimize light and electromagnetic pollution.

___(l) *Land Use Intensity Modifications:*

___(i) *Potential Land Use Intensity Modifications:*

(a) The density of a project may be increased in the BR-1 district through a reduction of the lot area requirement or in the Downtown (DT), BR-2, or MU-3 districts through a reduction in the open space requirements.

(b) The open space requirements in all Downtown (DT) districts may be reduced by up to one hundred percent.

(c) The open space per lot requirements for the total amount of open space required on the lot in the BR-2 district may be reduced by up to fifty percent.

(d) Land use intensity may be increased up to 25 percent in the BR-1 district through a reduction of the lot area requirement.

___(ii) *Additional Criteria for Land Use Intensity Modifications:* A land use intensity increase will be permitted up to the maximum amount set forth below if the approving agency finds that the criteria in paragraph (h)(1) through subparagraph (h)(2)(H) of this section and following criteria have been met:

(a) *Open Space Needs Met:* The needs of the project's occupants and visitors for high quality and functional useable open space can be met adequately;

(b) *Character of Project and Area:* The open space reduction does not adversely affect the character of the development or the character of the surrounding area; and

(c) *Open Space and Lot Area Reductions:* The specific percentage reduction in open space or lot area requested by the applicant is justified by any one or combination of the following site design features not to exceed the maximum reduction set forth above:

(i) Close proximity to a public mall or park for which the development is specially assessed or to which the project contributes funding of capital improvements beyond that required by the parks and recreation component of the development excise tax set forth in chapter 3-8, "Development Excise Tax," B.R.C. 1981: maximum one hundred percent reduction in all Downtown (DT) districts and ten percent in the BR-1 district;

(ii) Architectural treatment that results in reducing the apparent bulk and mass of the structure or structures and site planning which increases the openness of the site: maximum five percent reduction;

(iii) A common park, recreation, or playground area functionally useable and accessible by the development's occupants for active recreational purposes and sized for the number of inhabitants of the development, maximum five percent reduction; or developed facilities within the project designed to meet the active recreational needs of the occupants: maximum five percent reduction;

(iv) Permanent dedication of the development to use by a unique residential population whose needs for conventional open space are reduced: maximum five percent reduction;

(v) The reduction in open space is part of a development with a mix of residential and non-residential uses within an BR-2 zoning district that, due to the ratio of residential to non-residential uses and because of the size, type, and mix of dwelling units, the need for open space is reduced: maximum reduction fifteen percent; and

(vi) The reduction in open space is part of a development with a mix of residential and non-residential uses within an BR-2 zoning district that provides high quality urban design elements that will meet the needs of anticipated residents, occupants, tenants, and visitors of the property or will accommodate public gatherings, important activities, or events in the life of the community and its people, that may include, without limitation, recreational or cultural amenities, intimate spaces that foster social interaction, street furniture, landscaping, and hard surface treatments for the open space: maximum reduction 25 percent.

 (J) *Additional Criteria for Floor Area Ratio Increase for Buildings in the BR-1 District:*

 (i) *Process:* For buildings in the BR-1 district, the floor area ratio ("FAR") permitted under table 8-2, section 9-8-2, "Floor Area Ratio Requirements," B.R.C. 1981, may be increased by the city manager under the criteria set forth in this subparagraph.

 (ii) *Maximum FAR Increase:* The maximum FAR increase allowed for buildings thirty-five feet and over in height in the BR-1 district shall be from 2:1 to 4:1.

 (iii) *Criteria for the BR-1 District:* The FAR may be increased in the BR-1 district to the extent allowed in subparagraph (h)(2)(J)(ii) of this section if the approving agency finds that the following criteria are met:

(a) Site and building design provide open space exceeding the required useable open space by at least ten percent: an increase in FAR not to exceed 0.25:1.

(b) Site and building design provide private outdoor space for each office unit equal to at least ten percent of the lot area for buildings 25 feet and under and at least 20 percent of the lot area for buildings above 25 feet: an increase in FAR not to exceed 0.25:1.

(c) Site and building design provide a street front facade and an alley facade at a pedestrian scale, including, without limitation, features such as awnings and windows, well-defined building entrances, and other building details: an increase in FAR not to exceed 0.25:1.

(d) For a building containing residential and non-residential uses in which neither use comprises less than 25 percent of the total square footage: an increase in FAR not to exceed 1:1.

(e) The unused portion of the allowed FAR of historic buildings designated as landmarks under chapter 9-11, "Historic Preservation," B.R.C. 1981, may be transferred to other sites in the same zoning district. However, the increase in FAR of a proposed building to which FAR is transferred under this paragraph may not exceed an increase of 0.5:1.

(f) For a building which provides one full level of parking below grade, an increase in FAR not to exceed 0.5:1 may be granted.

___(K) Additional Criteria for Parking Reductions: The off-street parking requirements of section 9-9-6, "Parking Standards," B.R.C. 1981, may be modified as follows:

___(i) Process: The city manager may grant a parking reduction not to exceed fifty percent of the required parking. The planning board or city council may grant a reduction exceeding fifty percent.

The applicant is proposing a 25 percent parking reduction. The following findings were made for the staff level approval of the parking reduction.

___(ii) Criteria: Upon submission of documentation by the applicant of how the project meets the following criteria, the approving agency may approve proposed modifications to the parking requirements of section 9-9-6, "Parking Standards," B.R.C. 1981 (see tables 9-1, 9-2, 9-3 and 9-4), if it finds that:

(a) For residential uses, the probable number of motor vehicles to be owned by occupants of and visitors to dwellings in the project will be adequately accommodated;

There are 60 spaces for 32 townhome units. While three- and four-bedroom units predominate on the site, it is likely that these units will be occupied by families, many of whom would not have driving age children. The parking reduction is adequate for the numbers of units, the anticipated residents, and the proximity to transit.

(b) The parking needs of any non-residential uses will be adequately accommodated through on-street parking or off-street parking;

There is no on-street parking proposed on 30th Street, however, there are on-street spaces along the alley within the site. The small commercial spaces planned in the building are of a reasonable size to not create a large parking demand and will likely be oriented to pedestrians, bicyclists and residents.

(c) A mix of residential with either office or retail uses is proposed, and the parking needs of all uses will be accommodated through shared parking;

The parking on the site is proposed to be unbundled. Therefore, if a resident does not want to have a parking space, one of the commercial tenants could lease the space instead. This establishes an inherent shared parking configuration.

(d) If joint use of common parking areas is proposed, varying time periods of use will accommodate proposed parking needs;

See response to (c) above.

(e) If the number of off-street parking spaces is reduced because of the nature of the occupancy, the applicant provides assurances that the nature of the occupancy will not change.

The nature of occupancy is predominately residential townhomes with four small retail spaces. This type of occupancy is unlikely to change to any other uses – particularly on

the ground floor. Live-work units are not a use that is permitted in the BT-1 zoning district and therefore, there would not be a change of use on the ground level that would increase parking demand.

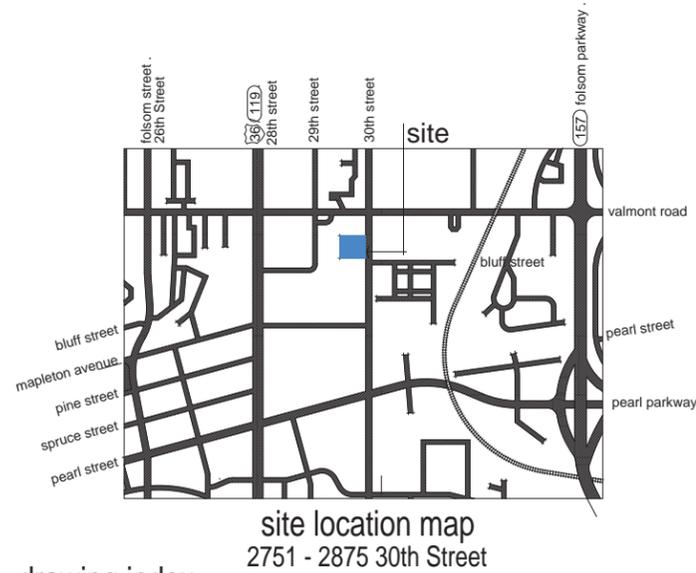
n/a (L) *Additional Criteria for Off-Site Parking:* The parking required under section 9-9-6, "Parking Standards," B.R.C. 1981, may be located on a separate lot if the following conditions are met:

n/a i) The lots are held in common ownership;

n/a (ii) The separate lot is in the same zoning district and located within three hundred feet of the lot that it serves; and

n/a (iii) The property used for off-site parking under this Subsection continues under common ownership or control.

**Boulder Junction
rowhouses**
2751 - 2875 30th Street



site location map
2751 - 2875 30th Street



drawing index

drawing number	subject
T1.0	Drawing Index . Project Team
C1.0	Civil Engineering Cover
C2.0	Demolition Plan
C3.0	Civil Site Plan (Ground Level)
C3.1	Civil Site Plan (Garage Level)
C4.0	Overall Utility Plan
C5.0	Grading Plan (Ground Level)
C5.1	Grading Plan (Garage Level)
C6.0	Storm Water Management Plan
C7.0	R.O.W. Improvement Section
C8.0	Section Grading Plan
L1.0	Landscape Plan
L2.0	Commons Area Landscape Planting Plan
L3.0	Front Yard Planting Plan
L4.0	Landscape Details
L5.0	Landscape Details
A1.0	Energy Features
A2.0	Inspirations
A3.0	Rendered Site Plan
A4.0	Rendered 30th Street Elevation
A5.0	Rendered 29 1/2 Alley Elevation
A6.0	Rendered Commons View 1
A7.0	Rendered Commons View 2
A8.0	Materials
A9.0	Zoning Chart plus Open Space and Parking Calculations
A10.0	Typical Wall Section (1/2"=1'-0")
A11.0	Typical Window Details (3/8"=1'-0")
A12.0	Typical Building Elevation(s): 30th Street and 29 1/2 Alley . plus: Building Height Graphics
A13.0	partial Building Elevation(s) - Commons
A14.0	Typical Side Elevations and Building Section
A15.0	Floor Plans . (typ.) End Unit Commercial and 2 Bedroom Units
A16.0	Floor Plans . (typ.) Paseo Unit Commercial and 2 Bedroom Units
A17.0	Floor Plans . (typ.) 30th Street 4 Bedroom Units
A18.0	Floor Plans . (typ.) 29 1/2 Alley 3 bedroom Units

Project Team
Developer . Owner

The Boulder Junction Rowhouses . LLC

Civil Engineer



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30 August 2016

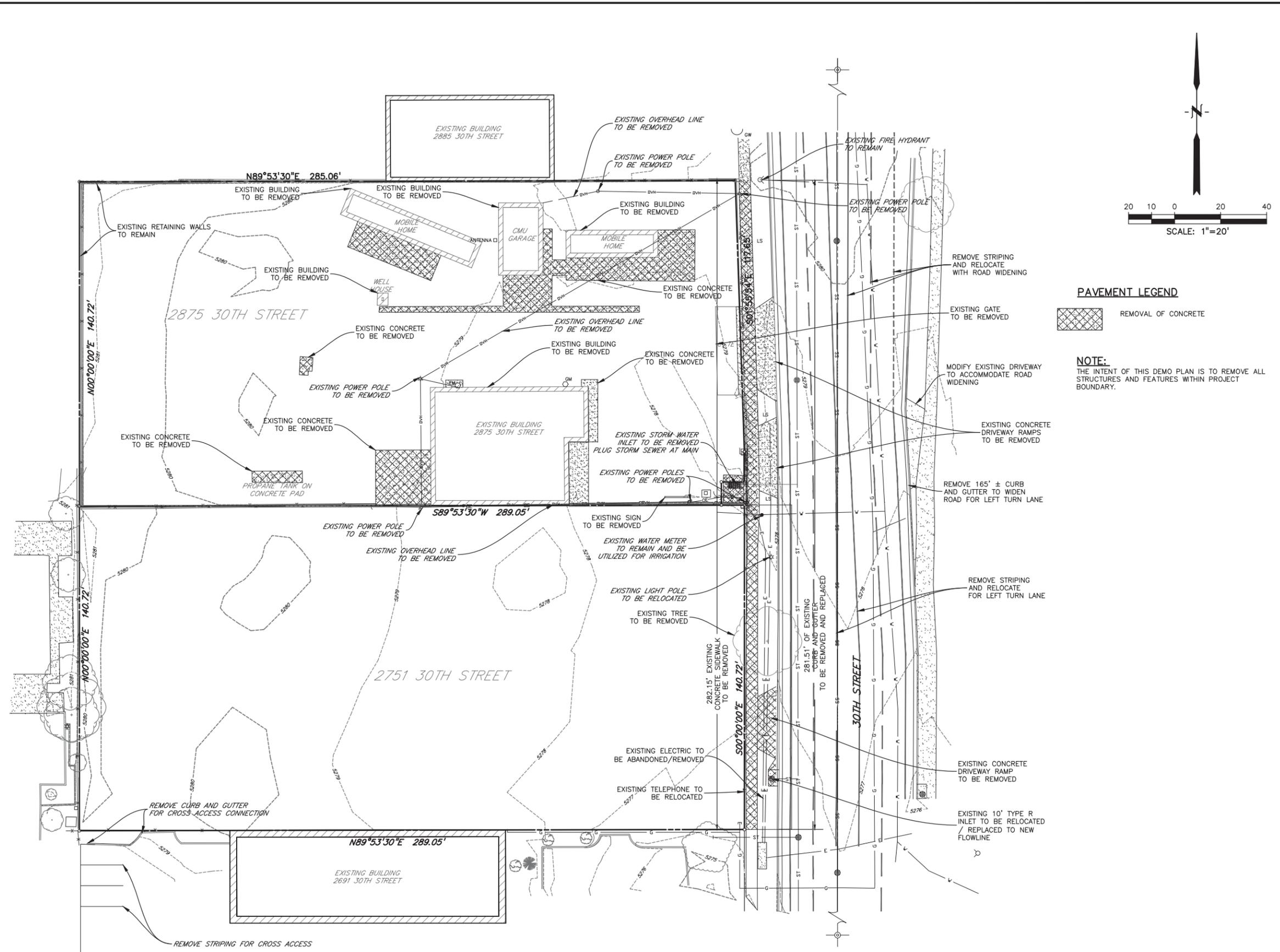
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T1.0



NOTE:
 THE INTENT OF THIS DEMO PLAN IS TO REMOVE ALL STRUCTURES AND FEATURES WITHIN PROJECT BOUNDARY.

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OWNER/CLIENT:
 THE BOULDER JUNCTION
 ROWHOUSES, LLC

**BOULDER JUNCTION
 ROWHOUSES**
 2751 AND 2875 30TH STREET
 BOULDER, COLORADO

ISSUE	DATE
LAND USE REVIEW	8/30/2016
DESIGNED BY:	SKG
DRAWN BY:	SKG
CHECKED BY:	GJL
FILE NAME:	20545_C2.0

NOT FOR CONSTRUCTION

DRAWING SCALE:
 HORIZONTAL: 1" = 20'
 VERTICAL: N/A

**DEMOLITION
 PLAN**

PROJECT NO. 20545-00BLCV

C2.0

SHEET: 2 OF 10

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BOULDER JUNCTION ROWHOUSES

2751 AND 2875 30TH STREET
BOULDER, COLORADO

ISSUE	DATE
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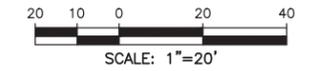
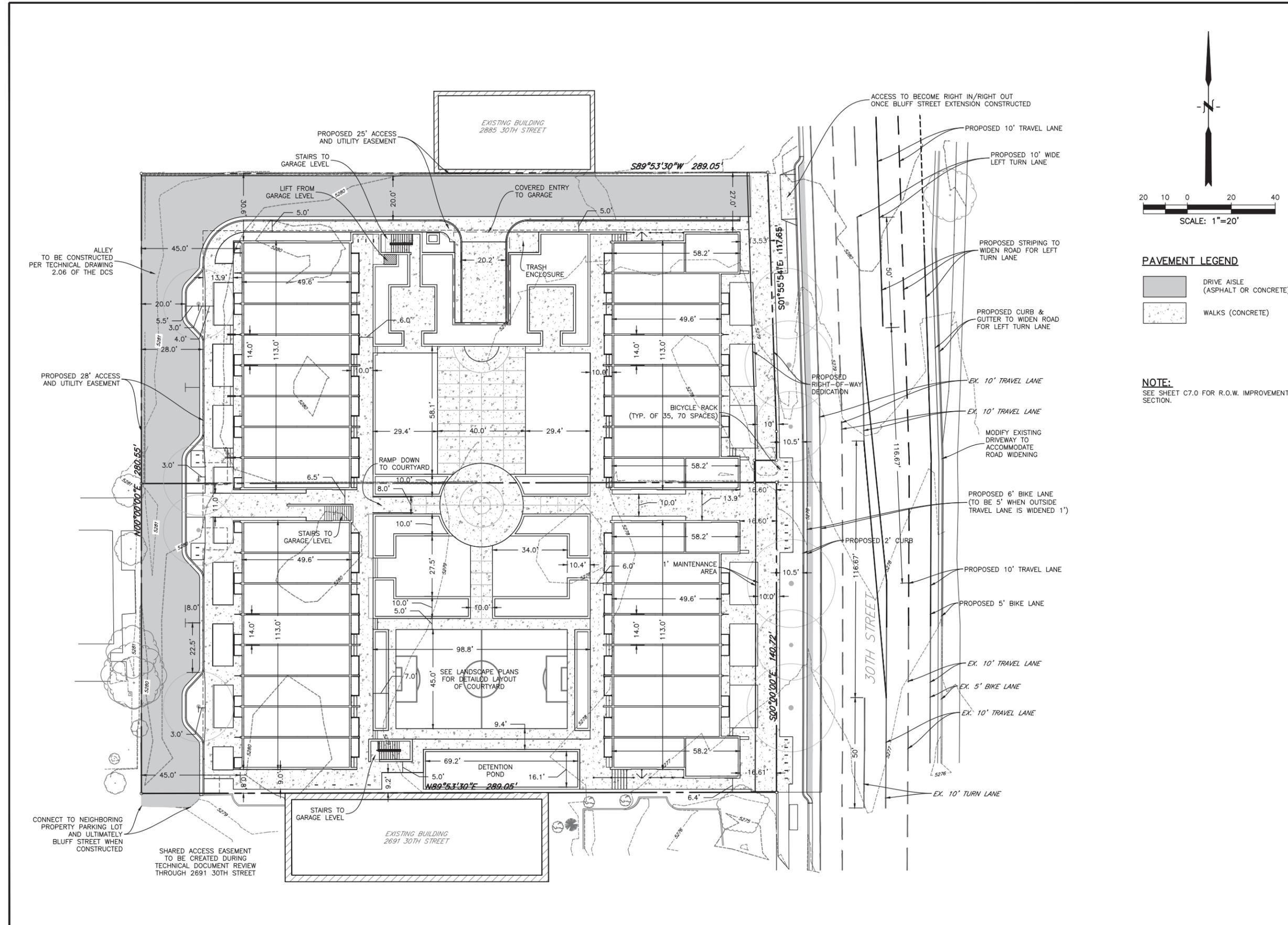
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VERTICAL: N/A

CIVIL
SITE PLAN
(GROUND LEVEL)

PROJECT NO. 20545-00BLCV

C3.0

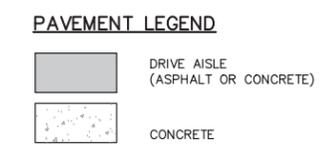
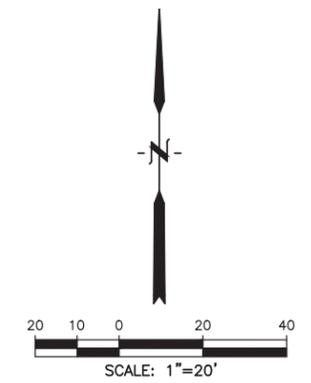
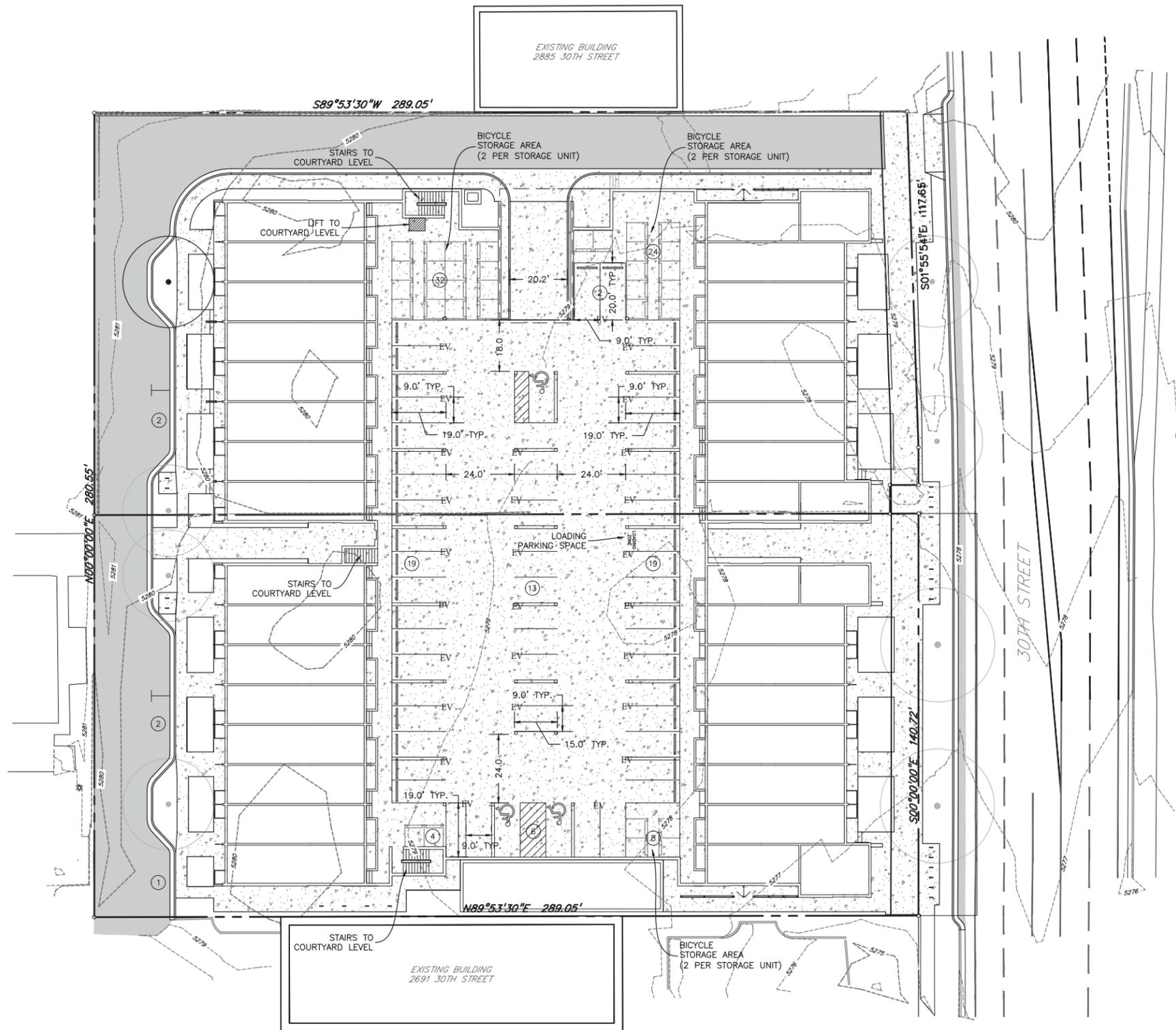
SHEET: 3 OF 10



PAVEMENT LEGEND

- DRIVE AISLE (ASPHALT OR CONCRETE)
- WALKS (CONCRETE)

NOTE:
SEE SHEET C7.0 FOR R.O.W. IMPROVEMENTS SECTION.



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**BOULDER JUNCTION
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 2751 AND 2875 30TH STREET
 BOULDER, COLORADO

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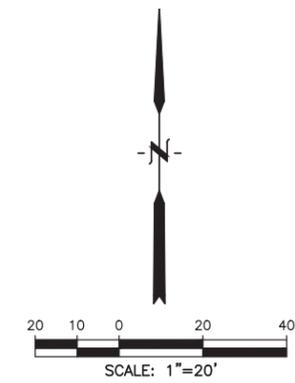
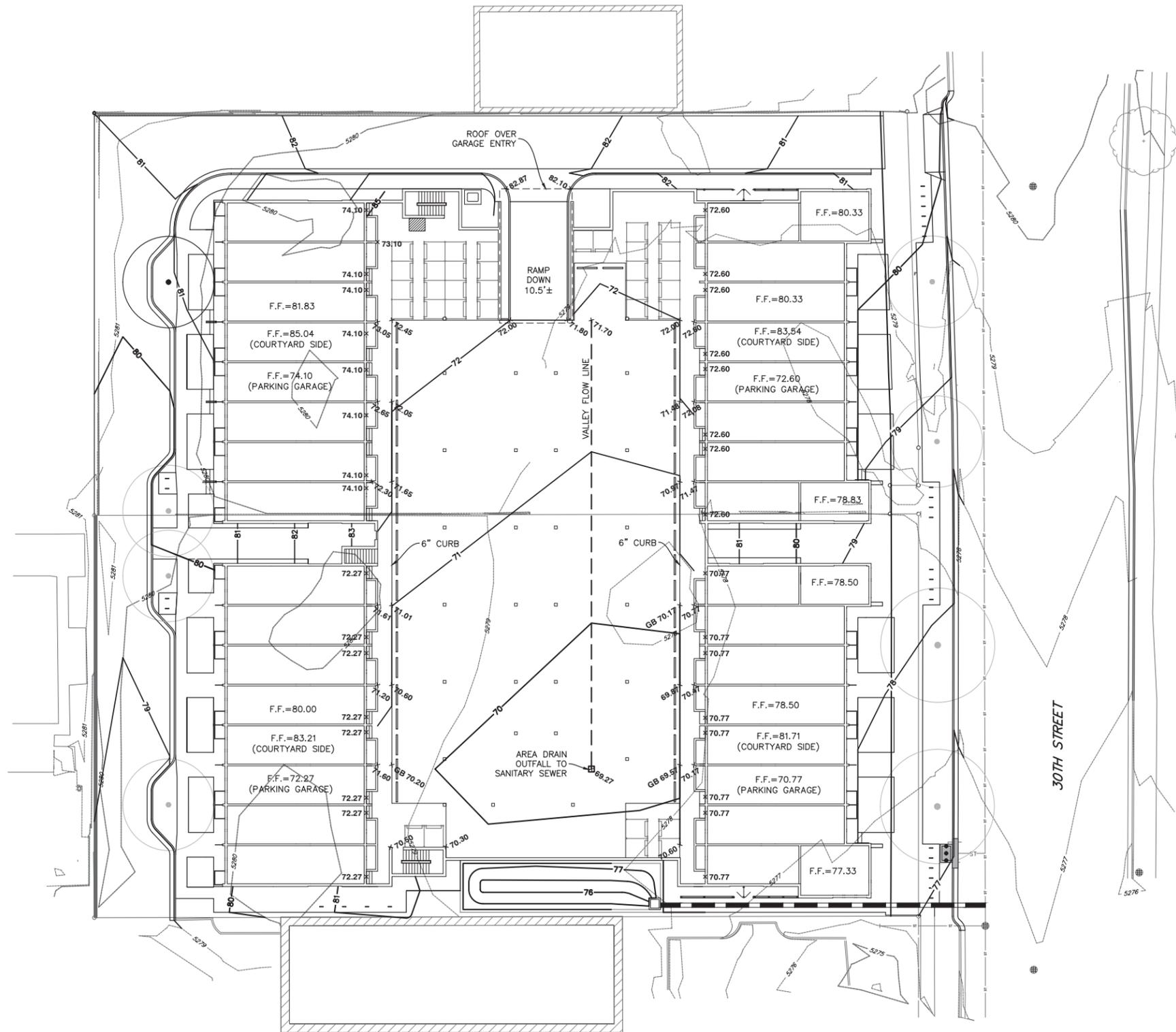
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 VERTICAL: N/A

CIVIL
 SITE PLAN
 (GARAGE LEVEL)

PROJECT NO. 20545-00BLCV

C3.1

SHEET: 4 OF 10



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 2751 AND 2875 30TH STREET
 BOULDER, COLORADO

ISSUE	DATE
LAND USE REVIEW	8/30/2016
DESIGNED BY:	SKG
DRAWN BY:	SKG
CHECKED BY:	GUL
FILE NAME:	20545_C5.1

NOT FOR CONSTRUCTION

DRAWING SCALE:
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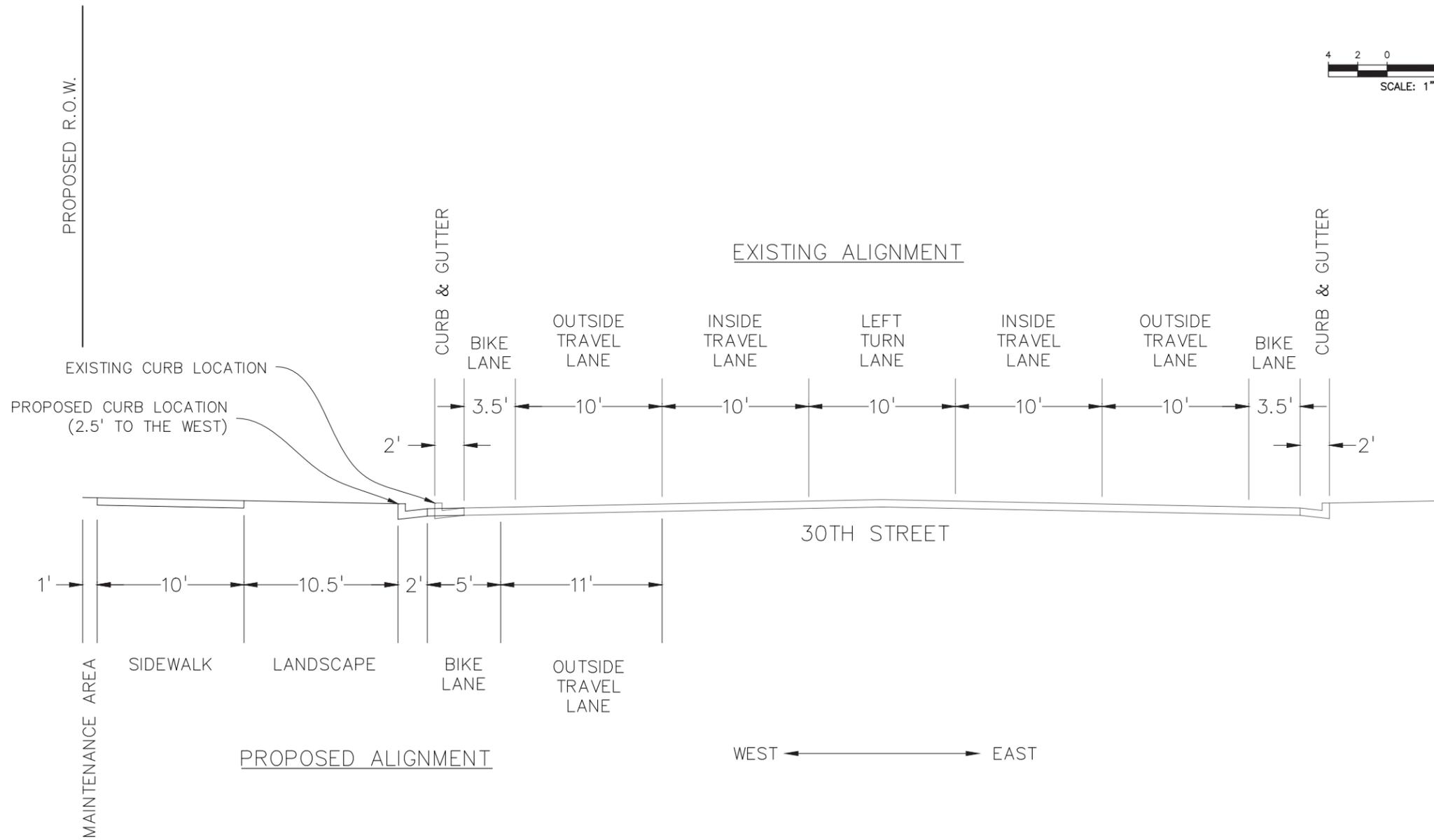
**GRADING
 PLAN
 (GARAGE LEVEL)**

PROJECT NO. 20545-00BLCV

C5.1

SHEET: 7 OF 10

BOULDER JUNCTION ROWHOUSES



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**BOULDER JUNCTION
ROWHOUSES**
2751 AND 2875 30TH STREET
BOULDER, COLORADO

ISSUE	DATE
LAND USE REVIEW	8/30/2016
DESIGNED BY:	SKG
DRAWN BY:	SKG
CHECKED BY:	GJL
FILE NAME:	20545_C7.0

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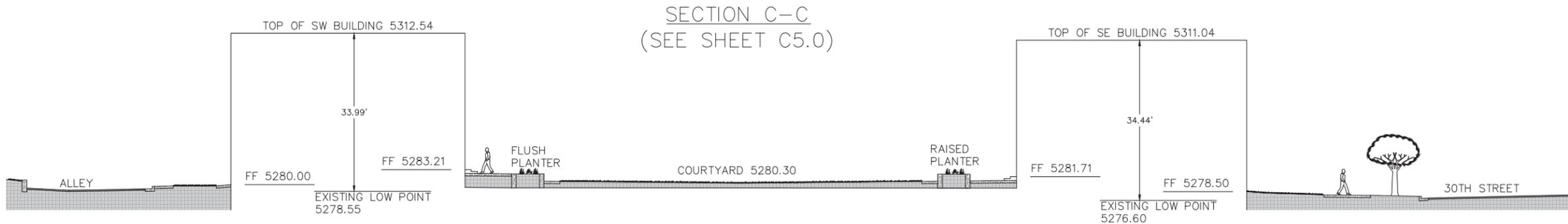
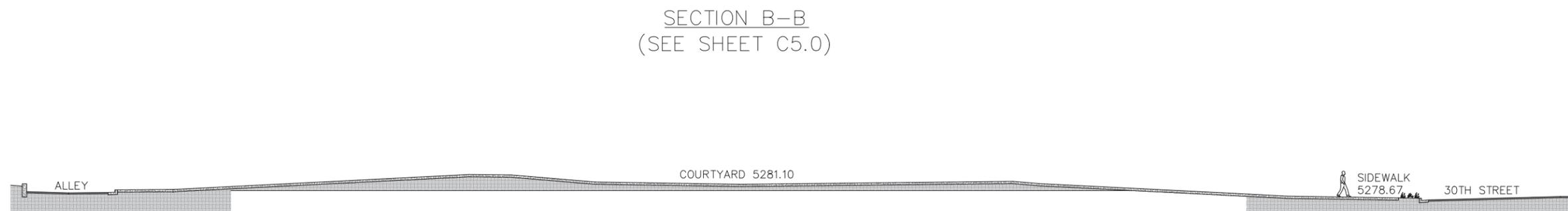
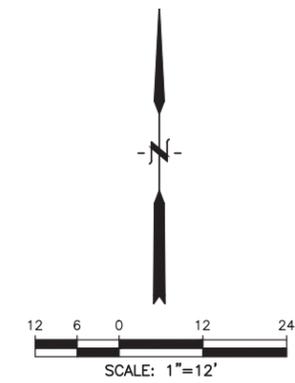
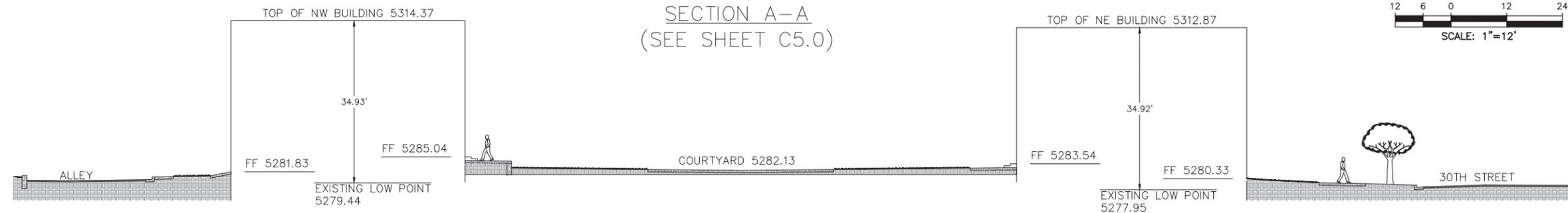
DRAWING SCALE:
HORIZONTAL: 1" = 4'
VERTICAL: 1" = 4'

R.O.W.
IMPROVEMENTS
SECTION

PROJECT NO. 20545-00BLCV

C7.0

SHEET: 9 OF 10



PREPARED BY:



DREXEL, BARRELL & CO.
 Engineers & Surveyors
 1800 38TH STREET
 BOULDER, COLORADO 80301
 CONTACT: GARRETT LINGREEN, P.E.
 (303) 442-4338
 BOULDER
 COLORADO SPRINGS
 GREELEY

OWNER/CLIENT:

THE BOULDER JUNCTION
 ROWHOUSES, LLC

**BOULDER JUNCTION
 ROWHOUSES**
 2751 AND 2875 30TH STREET
 BOULDER, COLORADO

ISSUE	DATE
LAND USE REVIEW	8/30/2016
DESIGNED BY:	SKG
DRAWN BY:	BMC
CHECKED BY:	GJL
FILE NAME:	20545_C8.0

NOT FOR CONSTRUCTION

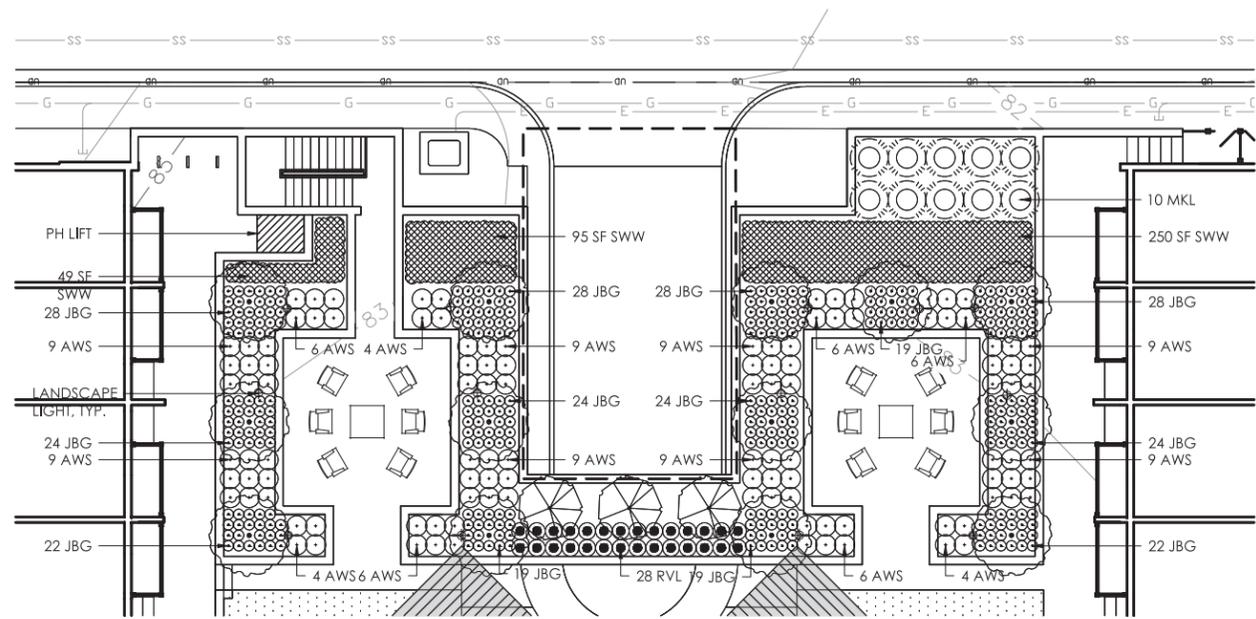
DRAWING SCALE:
 HORIZONTAL: 1" = 12'
 VERTICAL: N/A

**SECTIONS
 GRADING PLAN
 (GROUND LEVEL)**

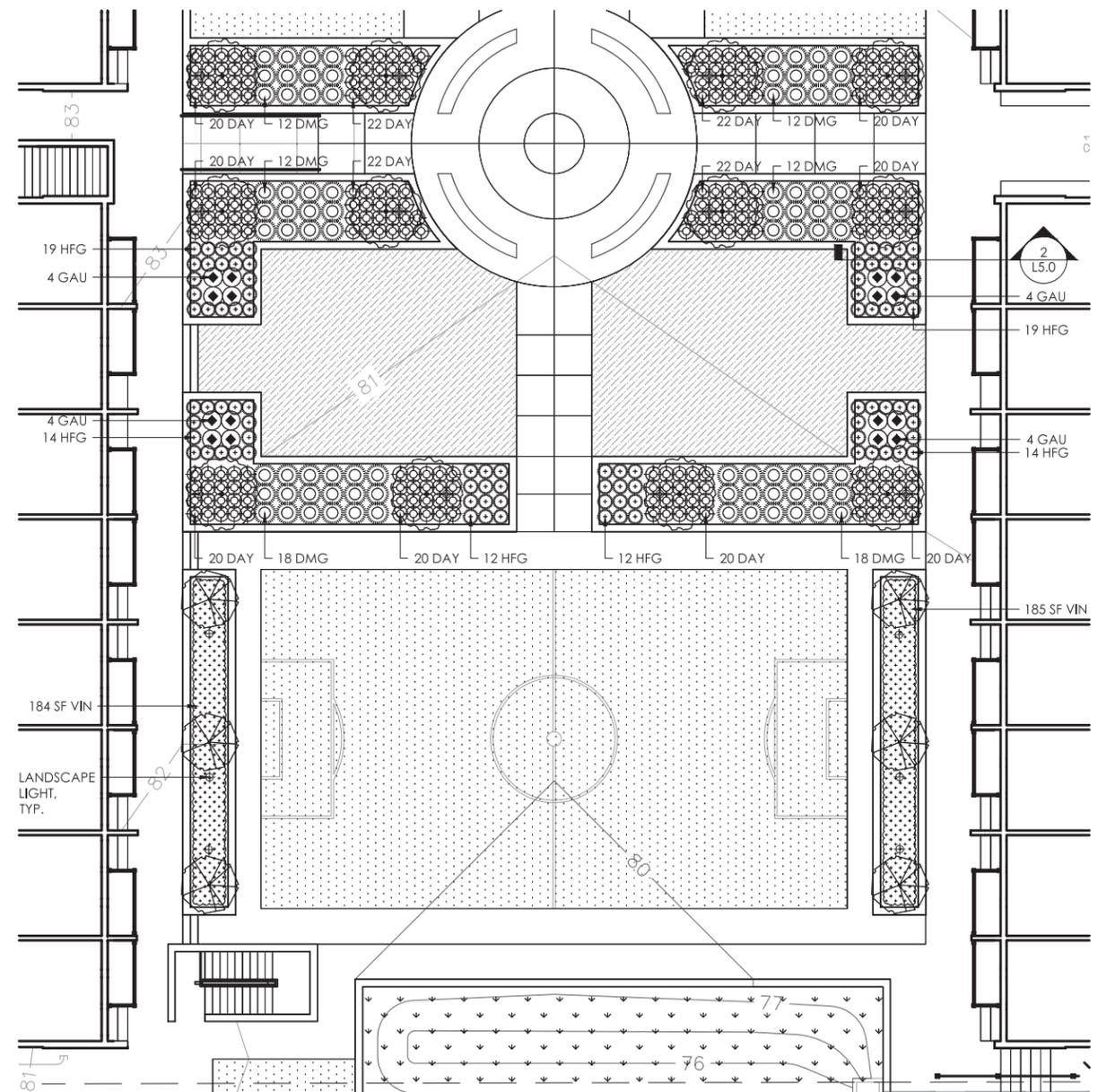
PROJECT NO. 20545-00BLCV

C8.0

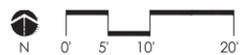
SHEET: 10 OF 10



1 LANDSCAPE LAYOUT PLAN
SCALE: 1"=10'-0"



2 LANDSCAPE LAYOUT PLAN
SCALE: 1"=10'-0"



commons area landscape planting plan

1" = 10'-0"

2751 and 2875 30th Street

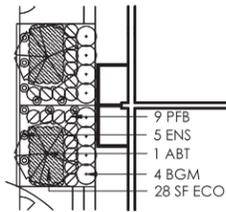
Boulder, Colorado

30 August 2016

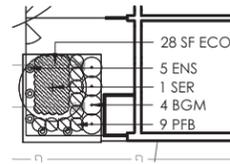


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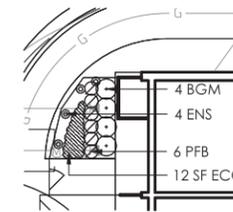
L2.0



1 TYP. FRONT YARD LANDSCAPE
SCALE: 1"=10'-0"



2 SOUTHWEST UNIT FRONT YARD LANDSCAPE
SCALE: 1"=10'-0"



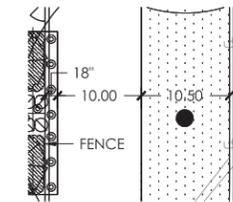
3 NORTHWEST UNIT FRONT YARD LANDSCAPE
SCALE: 1"=10'-0"

GENERAL NOTES:

- CONSTRUCTION SHALL CONFORM TO ALL APPLICABLE STATE AND LOCAL CODES AND REGULATIONS.
- CONTRACTOR TO CONTACT THE UTILITY NOTIFICATION CENTER OF COLORADO BEFORE DIGGING, INCLUDING BUT NOT LIMITED TO, TRENCHING, SHRUB AND TREE PLANTING PITS. IF UTILITIES OCCUR WITHIN (3) THREE FEET OF PROPOSED FOOTINGS, THE CONTRACTOR SHALL REPORT SUCH CONDITIONS TO THE OWNER.
- CONTRACTOR IS RESPONSIBLE FOR FULL ON-SITE INVESTIGATIONS AS NEEDED IN ORDER TO GAIN A FULL UNDERSTANDING OF EXISTING CONDITIONS PRIOR TO CONSTRUCTION. FAILURE TO INSPECT THE SITE PRIOR TO CONSTRUCTION SHALL NOT BE CAUSE FOR REQUESTING ADDITIONAL MONIES BY THE CONTRACTOR.
- THE CONTRACTOR SHALL OBTAIN, AT HIS EXPENSE, ALL PERMITS WHICH ARE NECESSARY TO PERFORM THE PROPOSED WORK.
- THE LANDSCAPE ARCHITECT IS NOT RESPONSIBLE FOR THE CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OR FOR SAFETY PRECAUTIONS OR PROBLEMS UTILIZED IN CONNECTION WITH THE WORK, AND HE/SHE WILL NOT BE RESPONSIBLE FOR THE CONTRACTOR'S FAILURE TO CARRY OUT THE WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS.
- VERIFY ALL CONDITIONS AT THE JOB SITE AND NOTIFY LANDSCAPE ARCHITECT AND THE OWNER'S REP. IMMEDIATELY OF ANY DIMENSIONAL ERRORS, OMISSIONS OR DISCREPANCIES PRIOR TO ANY DEMOLITION OR CONSTRUCTION.

LANDSCAPE NOTES:

- PLANT MATERIAL AND BED LOCATIONS TO BE STAKED BY THE LANDSCAPE CONTRACTOR FOR REVIEW BY THE OWNER'S REPRESENTATIVE OR LANDSCAPE ARCHITECT. ALL ADJUSTMENTS SHALL BE MADE BY THE CONTRACTOR. THE CONTRACTOR SHALL NOT DIG PLANT PITS UNTIL LOCATIONS ARE APPROVED.
- ROUGH GRADING TO PLUS OR MINUS ONE TENTH OF A FOOT BY GENERAL CONTRACTOR. LANDSCAPE CONTRACTOR TO PROVIDE FINISH GRADING IN ALL LANDSCAPE AREAS. LANDSCAPE CONTRACTOR TO APPROVE ROUGH GRADES PRIOR TO MOBILIZATION. MOBILIZATION ON THE PART OF THE LANDSCAPE CONTRACTOR WILL INDICATE THAT ROUGH GRADING IS ACCEPTABLE TO THE LANDSCAPE CONTRACTOR, AND THEREFORE BE RESPONSIBLE FOR PROVIDING ALL FINISHED GRADES TO MEET THE CIVIL GRADING PLANS.
- NOTIFY LANDSCAPE ARCHITECT IN ORDER TO TAG TREES AT THE NURSERY MINIMUM EARLY SPRING PRIOR TO INSTALLATION. SHRUBS WILL BE INSPECTED ON-SITE. LANDSCAPE PLANT MATERIALS MAY BE REJECTED AT ANY TIME DUE TO ISSUES OF QUALITY.
- ALL NEW LANDSCAPE AREAS SHALL BE IRRIGATED BY A COMMERCIAL GRADE, FULLY AUTOMATIC UNDERGROUND IRRIGATION SYSTEM. CONTRACTOR IS RESPONSIBLE FOR GENERATING AN IRRIGATION PLAN FOR REVIEW AND AN AS BUILT PLAN AS REQUIRED.
- ALL PLANT MATERIAL SHALL MEET OR EXCEED CURRENT AMERICAN STANDARD FOR NURSERY STOCK ANSI Z60.1 AND THE COLORADO NURSERY ACT AND ACCOMPANYING RULE AND REGULATIONS.
- TRUNK-WRAP TAPE: TWO LAYERS OF CRINKLED PAPER CEMENTED TOGETHER WITH BITUMINOUS MATERIAL, 4 INCHES (102 MM) WIDE MIN.
- WRAPPING TREE TRUNKS: WRAP TREES WITH TRUNK-WRAP TAPE. START AT BASE OF TRUNK AND SPIRAL COVER TRUNK TO HEIGHT OF FIRST BRANCHES. OVERLAP WRAP, EXPOSING HALF THE WIDTH, AND SECURELY ATTACH WITHOUT CAUSING GIRDLING. FINAL WRAP SHALL BE SECURED WITH TAPE IN AT LEAST THREE PLACES. DO NOT USE STAPLES. INSPECT TREE TRUNKS FOR INJURY, IMPROPER PRUNING, AND INSECT INFESTATION AND TAKE CORRECTIVE MEASURES REQUIRED BEFORE WRAPPING. DO NOT WRAP ROUGH BARK, POPULUS OR GLEDITSIA TREES. REMOVE WRAP IN SPRING.
 - ALL DECIDUOUS TREES SHALL BE WRAPPED BY THE CONTRACTOR NOVEMBER 1 - 15 OF THE YEAR IN WHICH THEY ARE PLANTED.
 - THE CONTRACTOR SHALL RETURN TO THE SITE AND REMOVE THE WRAP FROM APRIL 1 - 15 OF THE FOLLOWING SPRING. THE CONTRACTOR SHALL NOTIFY THE CITY AT LEAST 1 WEEK PRIOR TO WRAP REMOVAL.
 - CONTRACTOR SHALL BE RESPONSIBLE FOR WRAPPING AND UNWRAPPING TREES DURING THE WARRANTY PERIOD.
- WOOD CHIP MULCH SHALL BE CLEAN WOOD CHIPS FREE OF SOIL OR MAN MADE DEBRIS SHREDDED INTO COARSE PIECES RANGING IN SIZE FROM 1 - 3 INCHES. ALL PLANTING BED LANDSCAPE PLANTINGS SHALL BE MULCHED WITH 4 - 6 INCHES OF WOOD CHIPS AND NO WEED BARRIER FABRIC SHALL BE USED.



4 WALK & ROW AT 30th STREET
SCALE: 1"=10'-0"

front yard planting plan

1" = 10'-0"

2751 and 2875 30th Street

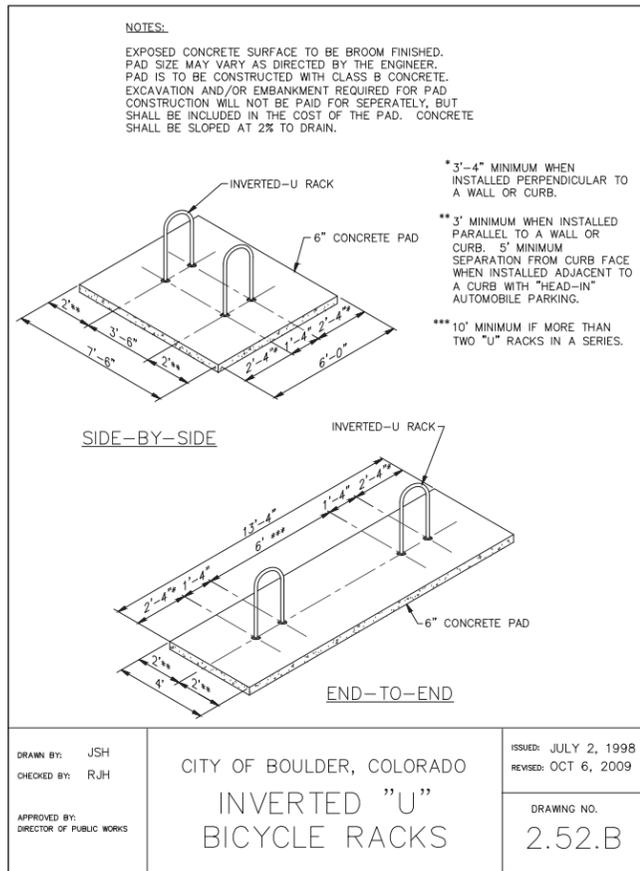
Boulder, Colorado

30 August 2016

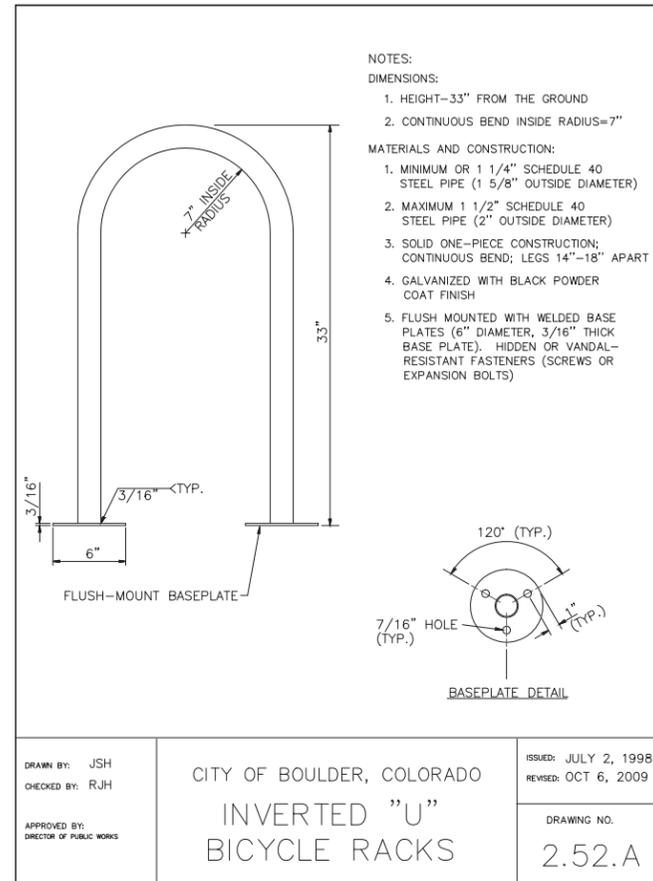


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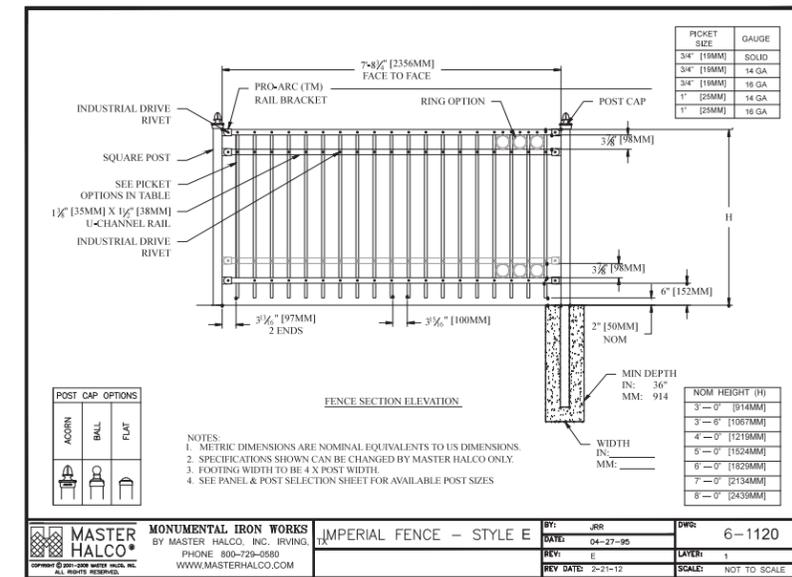
L3.0



1 BIKE RACK SPACING
 SCALE: N.T.S.



2 BIKE RACK
 SCALE: N.T.S.



3 ORNAMENTAL FENCE
 SCALE: N.T.S.



4 VINES ON WIRE CABLES
 SCALE: N.T.S.

landscape details

n.t.s.
 2751 and 2875 30th Street
 Boulder, Colorado
 30 August 2016



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Architectural Documents

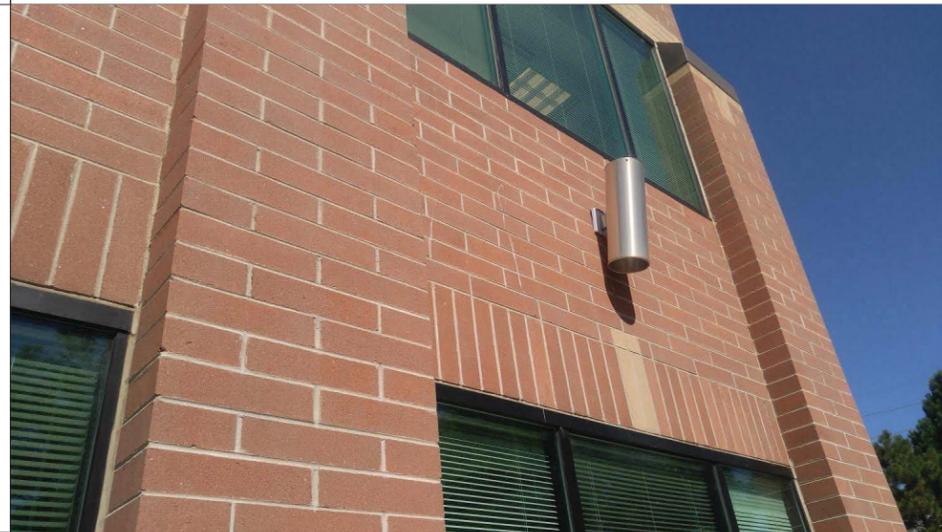
Boulder Junction rowhouses

Energy Features

Energy efficient design, including solar panels, is about saving money through lower utility bills. It is a fantastic coincidence that in saving money, you are also doing a great deal to help the environment by causing far less pollution, and increasing your comfort, health, safety, and even our national security.



NRG Block
same face dimensions as
'MONARCH' size brick



Our rowhouse dwellers will save hundreds of dollars per year on their utility bills due to several features, including:
NRG Block: Our exteriors will be built out of an insulated block, some in brick size shapes, which is far superior to any lumber/insulation combination. There will be virtually no air loss or heat exchange through our walls.

Greenius Screens: We will install our own interior magnetic window screens (patent pending) which will block most air loss and heat exchange at the windows.

Solar Panels: A PV system will provide much of the electricity for our units. The units will still be "grid" tied and will draw electricity from the grid whenever necessary.

Terrazzo concrete floors: You really don't want to know what you breath in from your carpeting or coated hardwood everyday. Our floors will be time tested, eternal poured concrete. Residents can place area rugs atop and replace them when needed if they like, but our flooring will not significantly off-gas or retain dirt.

Ceiling Fans: These use a lot less energy than air conditioners in the summer. User friendly Programmable Thermostats.

LED and CFL lighting: Some of our light bulbs will last ten years or more and use 1/15 the energy of yesterdays bulbs.

Front loading washing machines: These use a lot less water than top loading washers.

4770 and 4800 Baseline Road . Boulder
same size block as proposed for Boulder Junction Rowhouses
use photographs for referencing block size only
see ELEVATIONS - Boulder Junction Rowhouses for details including proposed window installation details



solar electric



2751 and 2875 30th Street
Boulder . Colorado

30 August 2016

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EV Outlets
EV outlets throughout garage for electric cars

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Architectural Documents

Boulder Junction
rowhouses

inspirations

used for the concept and design of Boulder Junction Rowhouses



... variety of housing types from urban townhomes (rowhouses) to stacked flats ...
Transit Village Area Plan . "land Use"

Rowhouses

a well loved urban home style found throughout history ...

an alternative to apartment living where residents have a sense of home and access to the outdoors
start with the historic design . massing . rhythm . details
add . energy efficiency . healthy construction methods and materials . and excellent planning and design



Front Yards

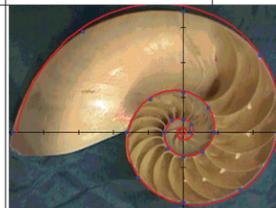
a pause between the public sidewalk and the private residence . a green space enjoyed by all

Hotel Boulderado

an architectural gem . an icon of Boulder Colorado

A 'Central park' . A 'Town Square' . A "Commons" accessible from each residence . accessible to the community
... usable open space . well-designed, functional open spaces with trees, quality landscaping and art, access to sunlight and places to sit comfortably ...
Transit Village Area Plan . "land Use"

a pleasant place to pass through or to spend time . an outdoor space to spend time . to pause
amenities for all ages . play field and play ground for children . seating areas . public art ... a park within the townhomes . within the City



A 'break' between buildings . A Paseo .
a welcoming entrance to the 'Commons' . a connection between the storefronts and the park

Paseo . between Row Houses

Golden Ratio

Site Planning . Building 'footprint' and 'elevation' . window size . 'Commons' planning



provide enough distance between the street and the buildings to allow for public uses . sitting . dining . meeting friends for coffee
Ample pedestrian space, storefronts, furnishings and street trees can make streets living spaces.
Transit Village Area Plan . "land Use"

provide space for small food service . coffee . confections . ice cream ...

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30 August 2016

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Architectural Documents
 Boulder Junction
 rowhouses



rendering . site plan

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 30 August 2016

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A3.0

Please see other pages for specific materials and dimensions. Also, the commercial spaces are not yet leased and we do not know at this time who the tenants will be. Any commercial signage / floor plan / tables shown in this submittal is for graphic presentation only. We will seek all necessary approvals for retail uses / signage / outdoor tables at the proper time. The signage / tables shown are not part of this site review approval.

Architectural Documents
Boulder Junction
rowhouses



rendering . 30th street elevation

Please see other pages for specific materials and dimensions. Also, the commercial spaces are not yet leased and we do not know at this time who the tenants will be. Any commercial signage / floor plan / tables shown in this submittal is for graphic presentation only. We will seek all necessary approvals for retail uses / signage / outdoor tables at the proper time. The signage / tables shown are not part of this site review approval.

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Architectural Documents
Boulder Junction
rowhouses



rendering . 29 1/2 - alley elevation

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Architectural Documents
Boulder Junction
rowhouses



rendering . commons . view 1

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A6.0

The center sculpture has yet to be commissioned. The
artwork shown is for graphic representation only.

The playground equipment is for graphic representation only.
We have yet to choose specific pieces of equipment.

Architectural Documents
Boulder Junction
rowhouses



rendering . commons . view 2

The playground equipment is for graphic representation only. We have yet to choose specific pieces of equipment.

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Architectural Documents

Boulder Junction rowhouses

materials and plants

	PV panels	Metals coping and wall panels	Leader Box downspout	Solar Canopy tie back	Solar Canopy	NRG- energy efficient appearance of brick	same size brick Boulder Library - Meadow Branch			
Window Cast Stone Surround		Inspiration masonry building dark window frame limestone surround	fiberglass Window Bronze	Door Canopy Integral Address	Proposed Signage pending necessary approvals	inspirations	decorative fence			
trees										
	ABT Autumn Brilliance Serviceberry	CBC Coralburst Crab	EOA English Oak	GOA Gambel Oak	HAC Common Hackberry	LTR Japanese Lilac Tree	NEW Newport Plum	WHO White Oak		
shrubs										
	AWS Anthony Waterer Spirea	MKL Miss Kim Lilac	SER Saskatoon Serviceberry	BGM Green Mountain Boxwood	DMG Dwarf Maiden Grass	HFG Hameln Fountain Grass	Blue Fescue Sod	concrete landscape wall note: walls will include 1" chamfer along a edges		
groundcover										
	ECO Purple Leaf Wintercreeper	ENS English Ivy	SAA Autumn Amber Sumac	SWW Sweet Woodruff	VIN Vinca Minor	DAY Daylily	GAU Whirling Butterflies	JBG Johnson's Blue Geranium	PFB Scabiosa Caucasia	RVL Royal Velvet Lavender

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Boulder . Colorado

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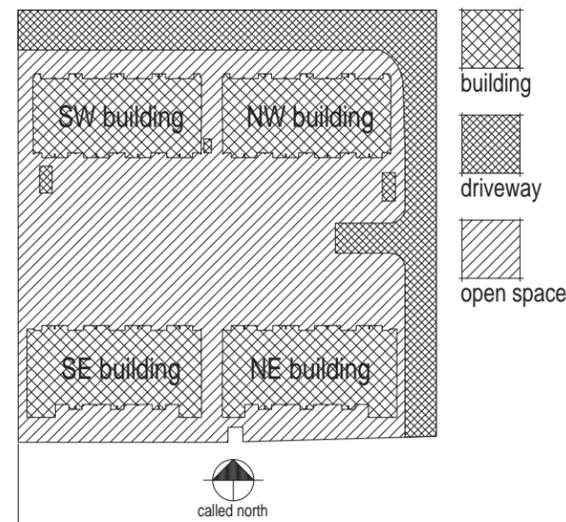
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Architectural Documents

Boulder Junction rowhouses

open space required / provided



site plan at grade

site plan at grade 80,704 square feet (typical)

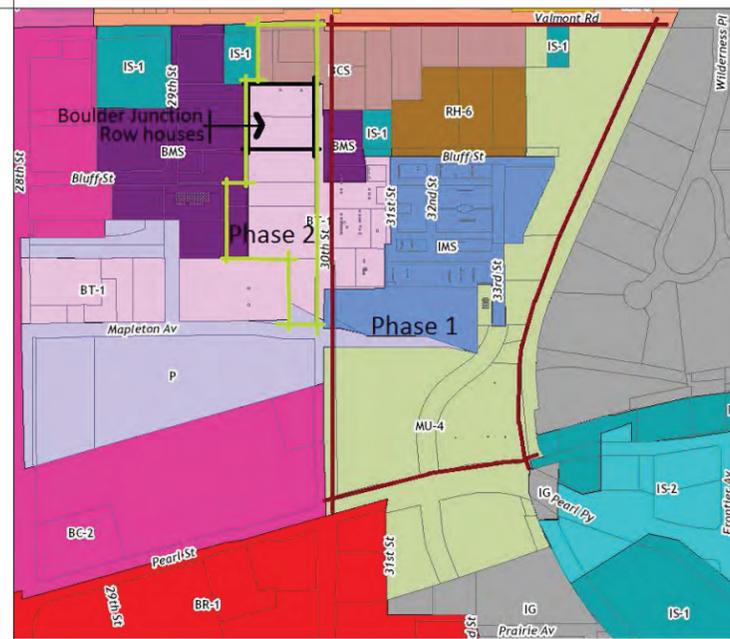
exclude:	
driveway	-14,052
building footprint(s)*	-25,142
*including exterior stairs	
*including window wells	
after exclusions:	41,510

add:	
balconies	+2,160
total open space provided:	43,670

required open space:	
32 units x 1,200 sq.ft. per =	38,400
provided:	1.14% required

Zoning

City of Boulder Colorado Zoning Map



Phasing

Transit Village (Boulder Junction) Area Plan



Our site, 2751 30th and 2875 30th, is zoned BT-1. As Mr. Ferro stated in the Concept Plan hearing (please see Minutes) the language of the TVAP Implementation Plan is intentional and designed to emphasize the build out of Phase I before Phase II is implemented.

And, as the TVAP Implementation Plan states, Phase II properties that wish to redevelop could do so under current zoning.

Therefore we are applying for Site Plan approval under the current BT-1 zoning.

BT-1 Zoning:

Per Table 6-1 Use Table:

Attached dwellings are an allowed use.
Offices are an allowed use.

Restaurants and brewpubs and taverns (no larger than 1,500 square feet in area, which may have meal service on an outside patio not more than 1/2 the floor area, and which close no later than 11:00 pm), are allowed with a use review.

Per Table 7-1 Form and Bulk Standards:

- Front setback: 20'*
- Side Yards: Floors 1 and 2: 0'
- 3rd Floor: 12'***
- Rear setback: 20'
- Height: 35'
- Stories: 3

* Please note: Per our Concept Plan Hearing, we added retail spaces along 30th Street.

In order to place these 'on the sidewalk', we are asking for an exception to the front setback requirement.

** Also, again per our Concept Plan Hearing, we have minimized the distance on the South property line, so our setback for the third floor is less than the 12' required.

Per Table 8-1 Intensity Standards:

Open Space Per Dwelling Unit: 1,200 square feet
32 rowhouses x 1,200 sf = 38,400 sf of open space required.
We meet (and exceed) the open space requirement.

Per Table 9-1 Residential Parking Requirements By Zoning District and Unit Type:

Minimum number of off street parking spaces for an attached dwelling unit:
1.5 for a 2 bedroom unit
2 for a 3 bedroom unit
3 for a 4 or more bedroom unit

Per Table 9-3 Nonresidential Parking requirements by Zoning District:

Minimum number of off street parking spaces per square foot of floor area for nonresidential uses and their accessory uses:
1 per 300 square feet

We are required to have 80 spaces, and provide 60*.

We are asking for a 25% reduction (please see calculations).

*pH ('handicap') and 'loading' spaces (a total of 3 pH and 1 loading = 4 spaces) are not included in the 'provided' count of 60.

parking required / provided

count	unit	spaces	required	unit location(s)
16	3 bed-rm	2	32	29 1/2 Street
12	4 bed-rm	3	36	30th Street
4	2 bed-rm	1.5	6	above commercial
2	comm	2	4	north and south ends
2	comm	1	2	either side - Paseo
			80	spaces required
			60	spaces required
			60	provided



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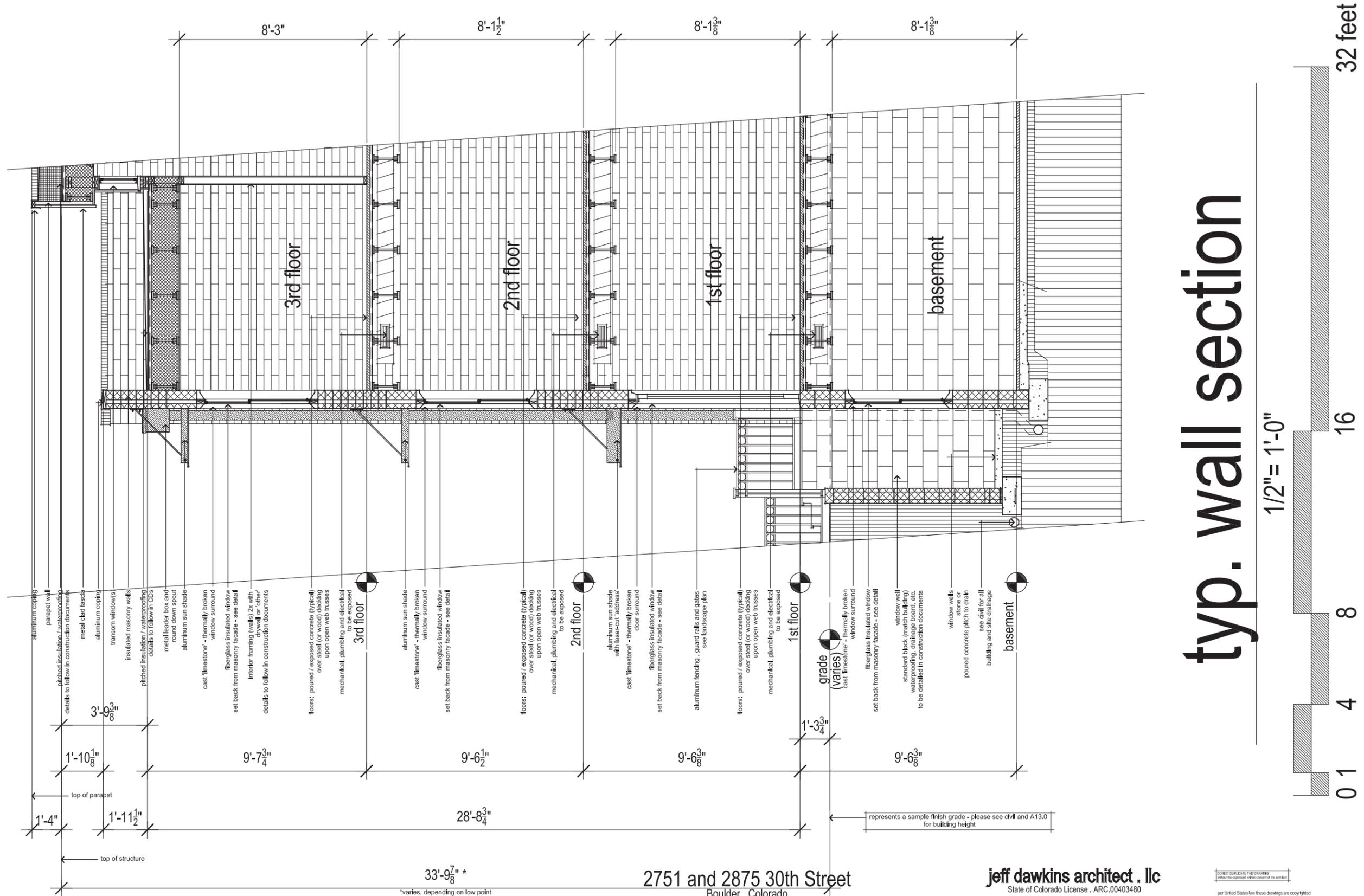
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A9.0

Architectural Documents
Boulder Junction
rowhouses



typ. wall section

*varies, depending on low point measurement for each building

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Boulder, Colorado
30 August 2016

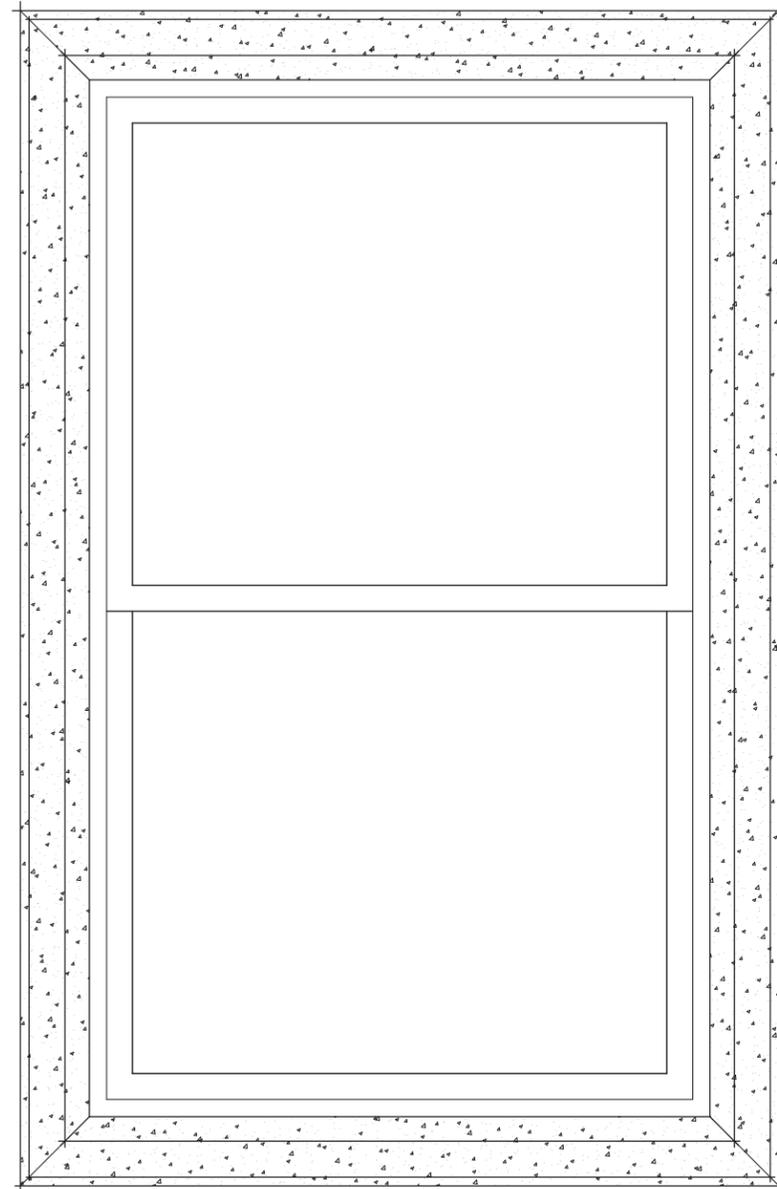
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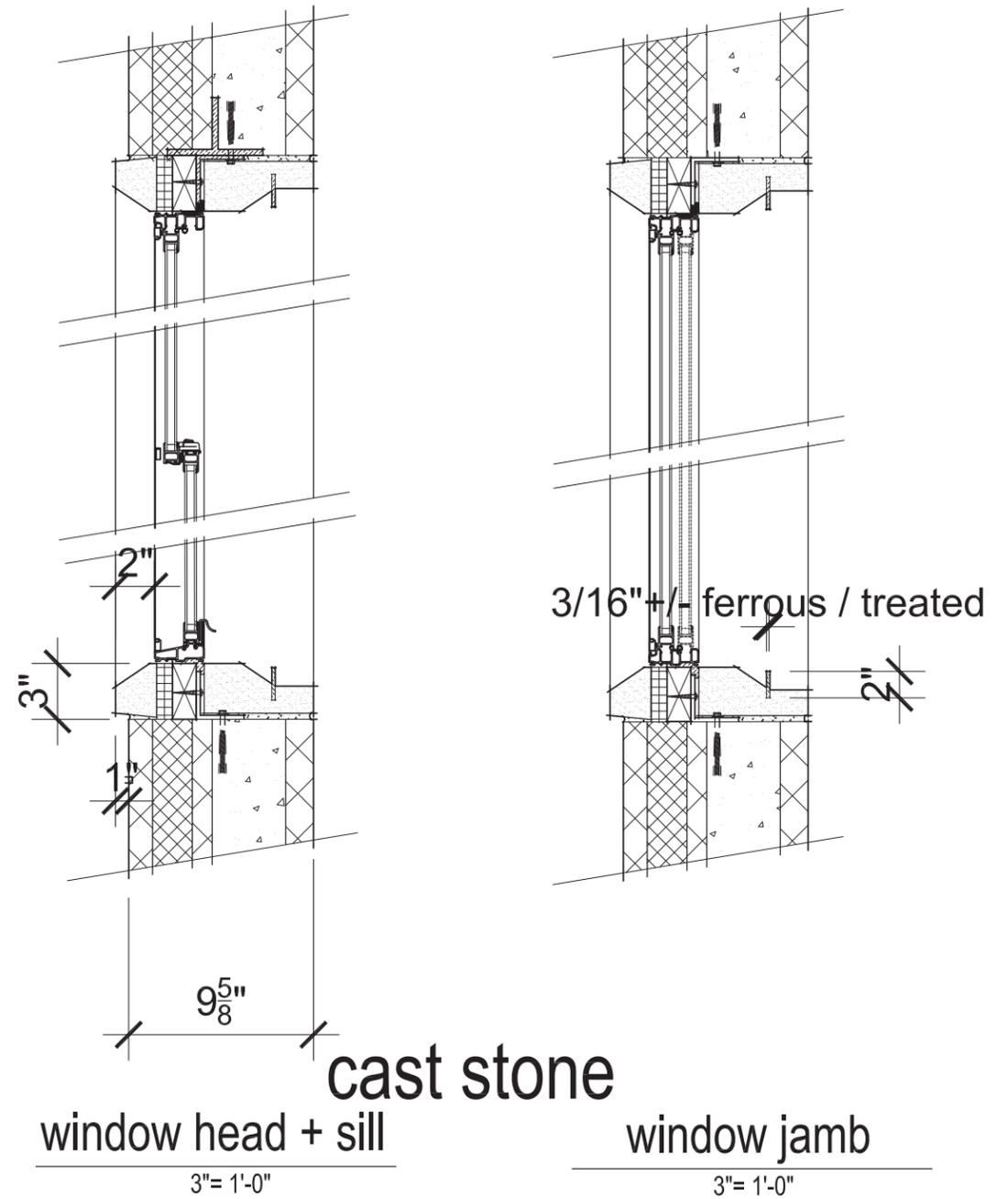
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A10.0

Architectural Documents
Boulder Junction
rowhouses



cast stone
window surround



cast stone
window head + sill
3"= 1'-0"

3/16" +/- ferrrous / treated
2"
3"= 1'-0"

typical window
installation details

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Boulder, Colorado
30 August 2016

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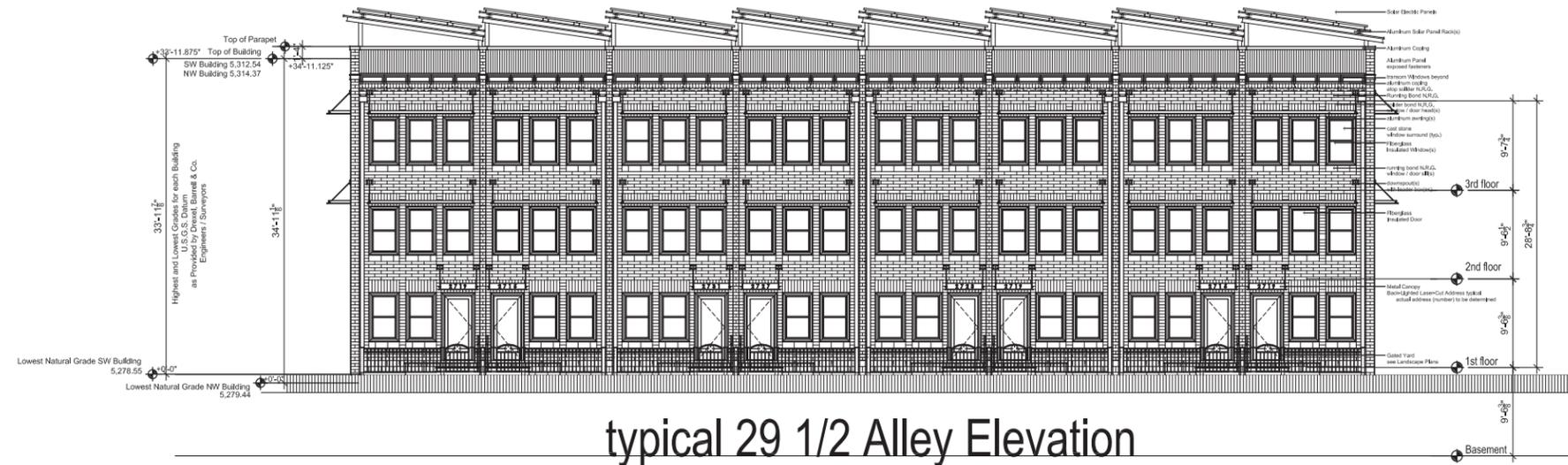
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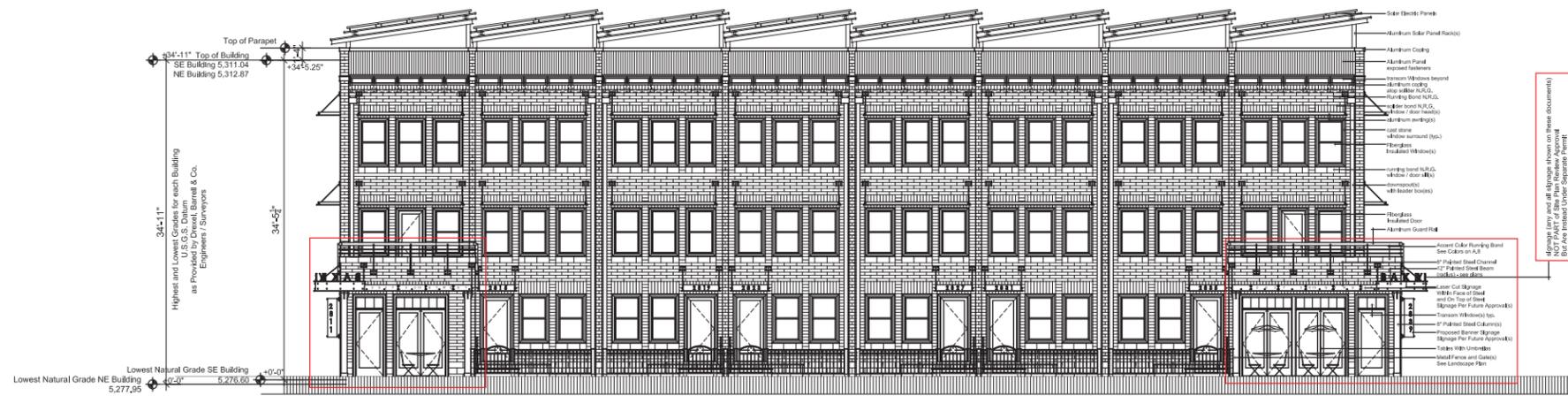
Architectural Documents

Boulder Junction rowhouses

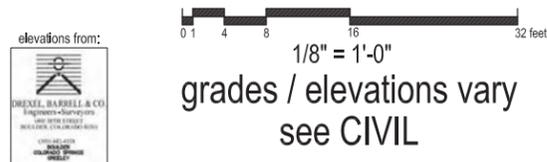
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typical 29 1/2 Alley Elevation



typical 30th Street Elevation



typical Street and Alley Elevations

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Boulder, Colorado
30 August 2016

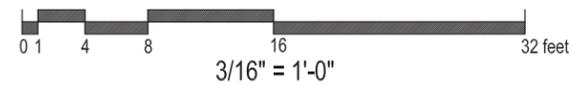
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grades / elevations vary
 see CIVIL

typical rear elevation and underground parking section

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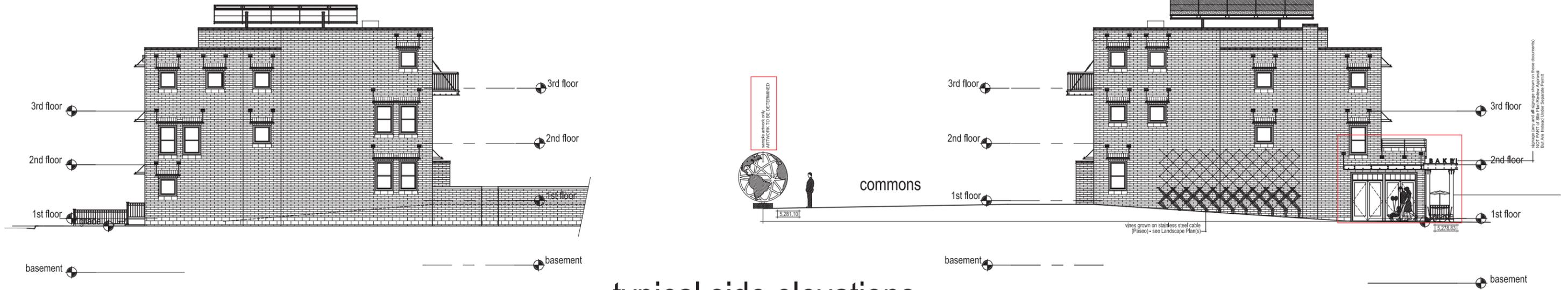
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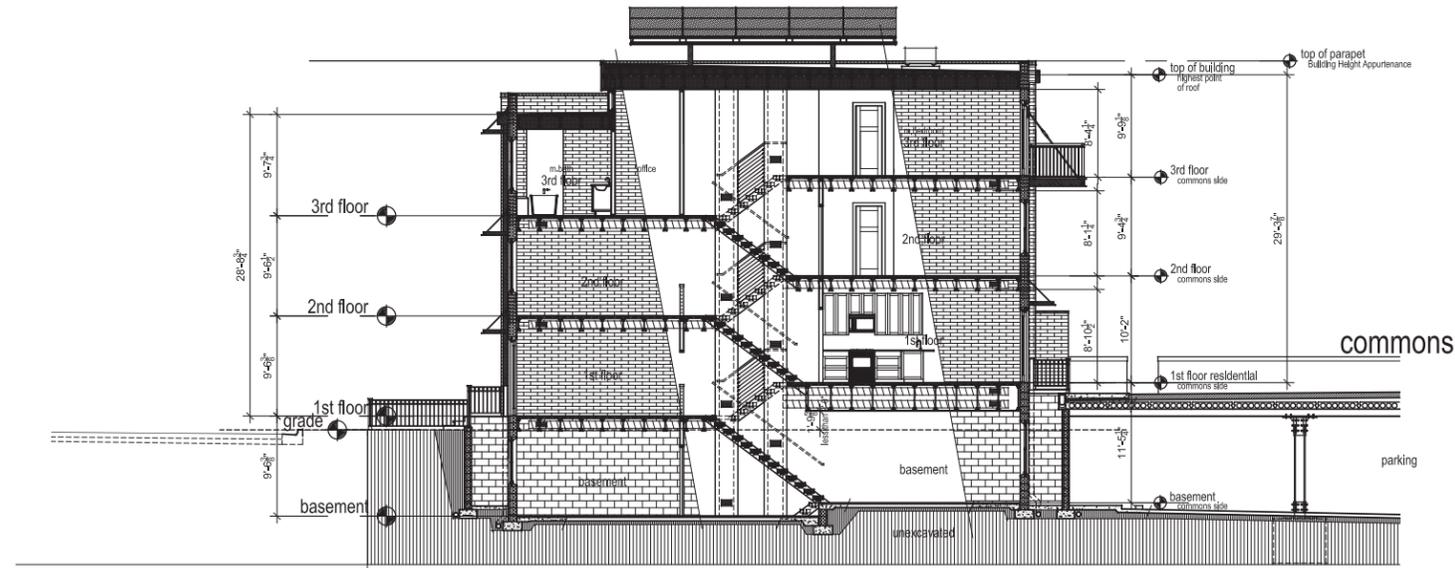
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typical side elevations



typical site section

0 1 4 8 16 32 feet
1/8" = 1'-0"

grades / elevations vary
see CIVIL

typical side elevation(s) and building section

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Boulder Junction rowhouses

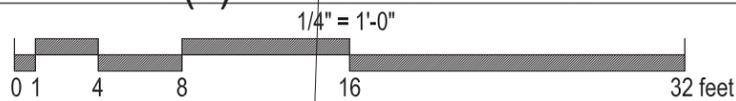
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area
typ. 2 bedroom unit
above grade
1,916 square feet
basement
482 square feet

area
commercial - end unit
above grade
473 square feet
(355 s.f. net)
basement
473 square feet

typical floor plans . end unit(s) . 2 bedroom . + commercial 30th . 2 such



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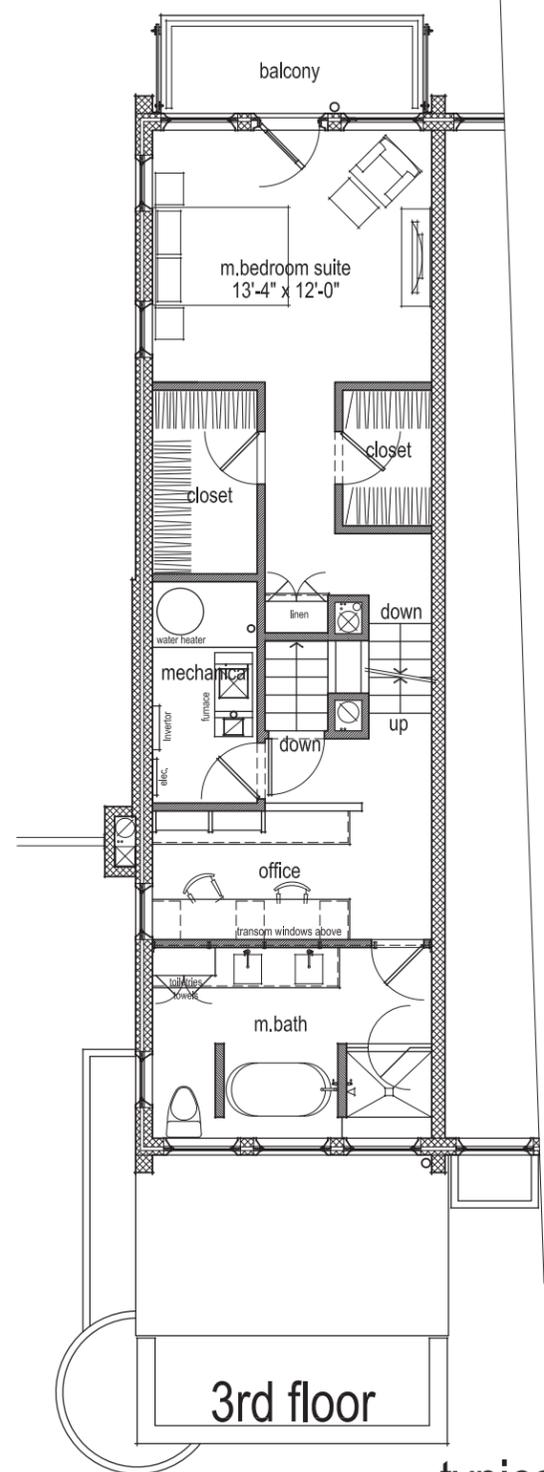
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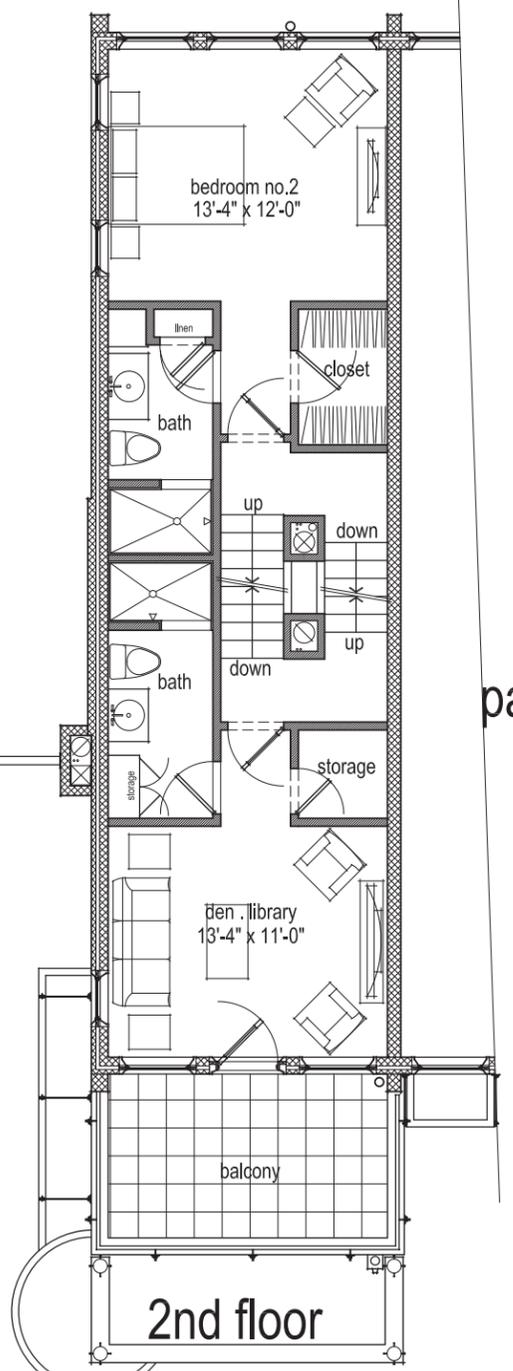
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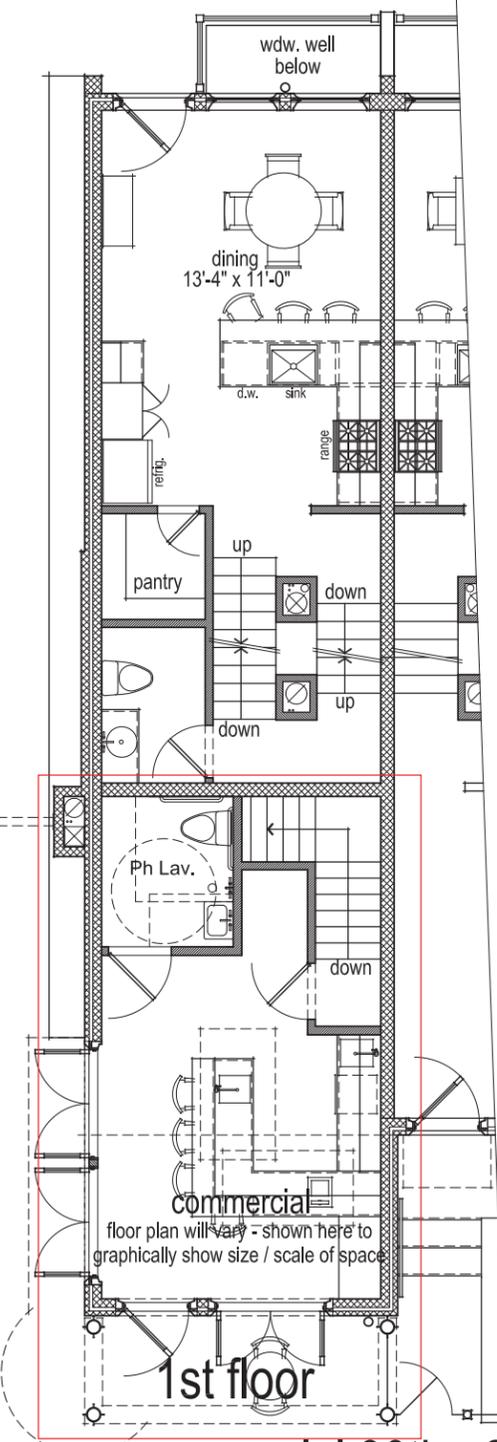


3rd floor

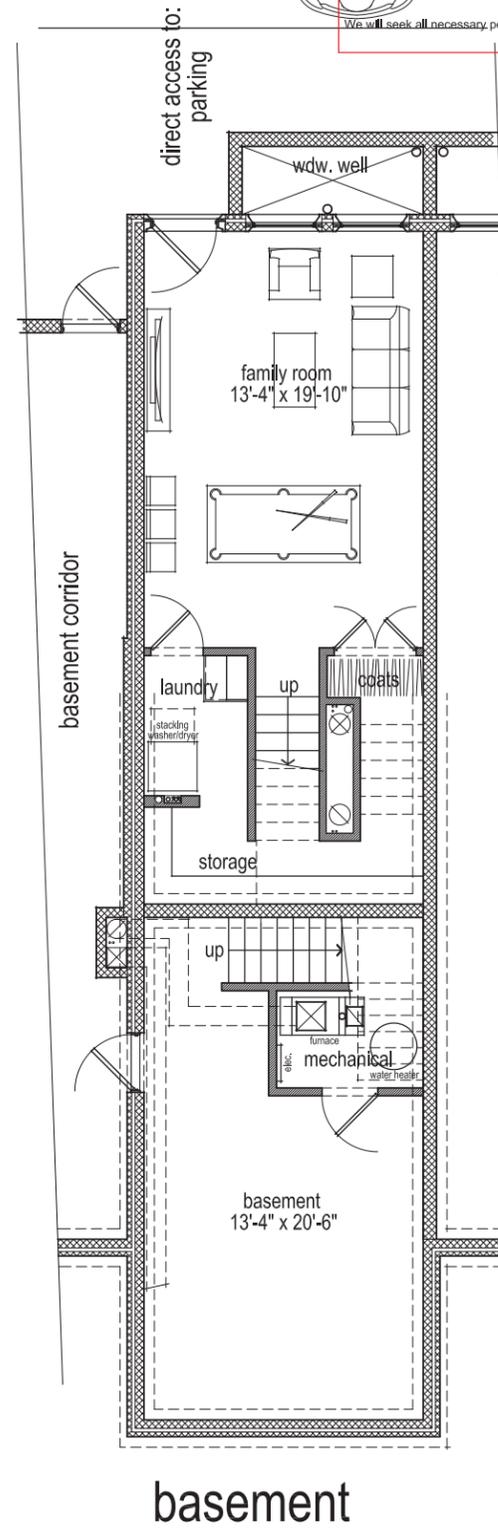


2nd floor

paseo



1st floor

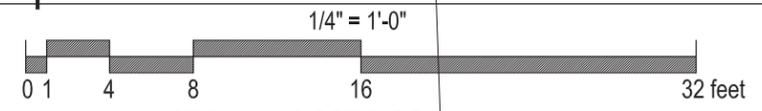


basement

area
typ. 2 bedroom unit
above grade
1,916 square feet
basement
482 square feet

area
commercial - end unit
(paseo)
above grade
366 square feet
(273 s.f. net)
basement
366 square feet

typical floor plans . paseo units . 2 bedroom + commercial 30th . 2 such



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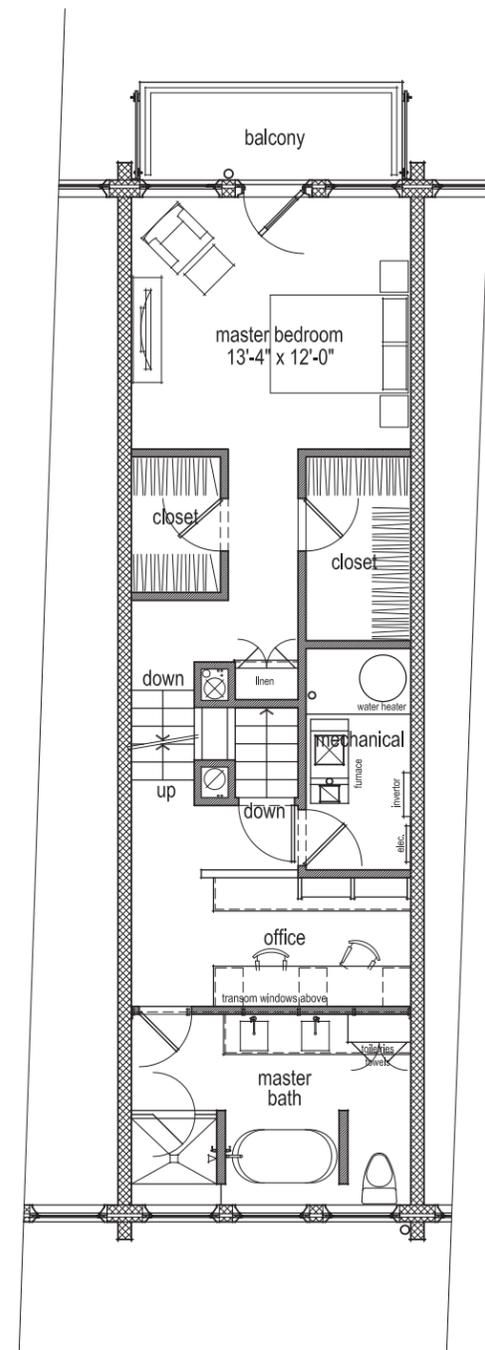


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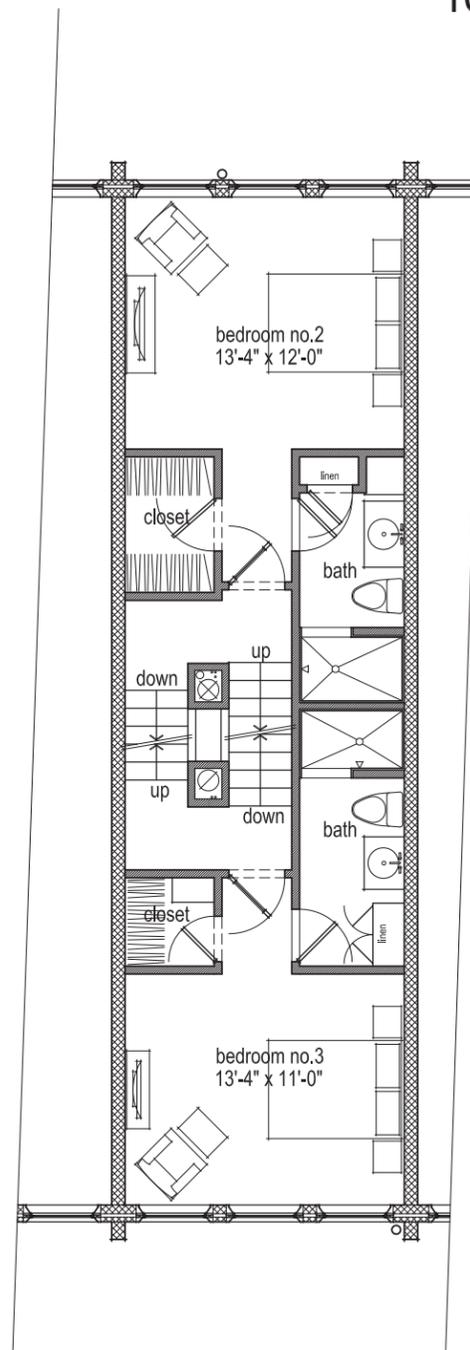
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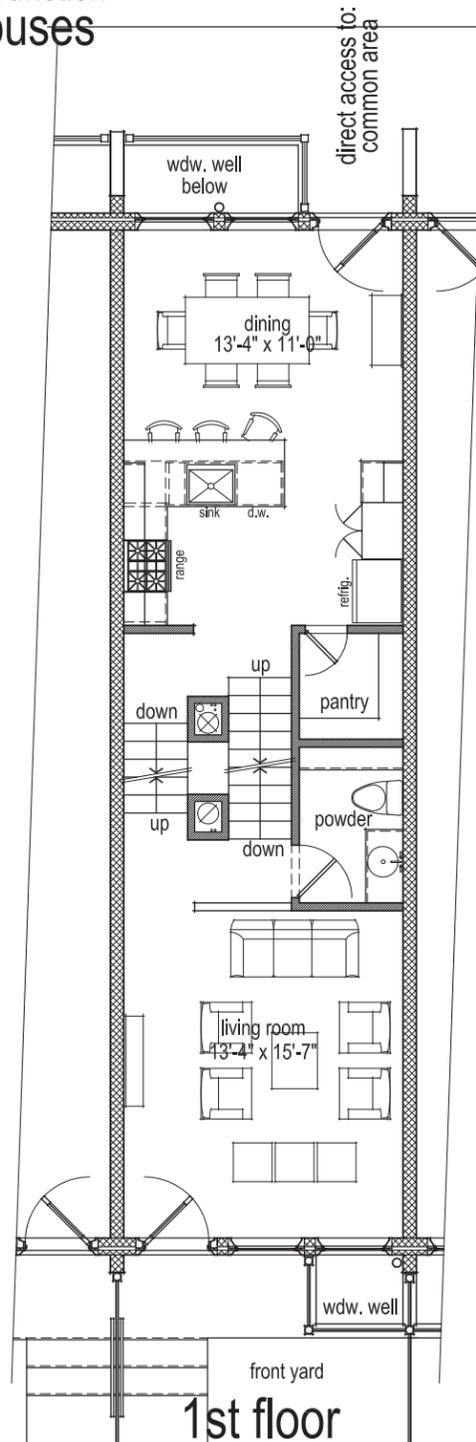
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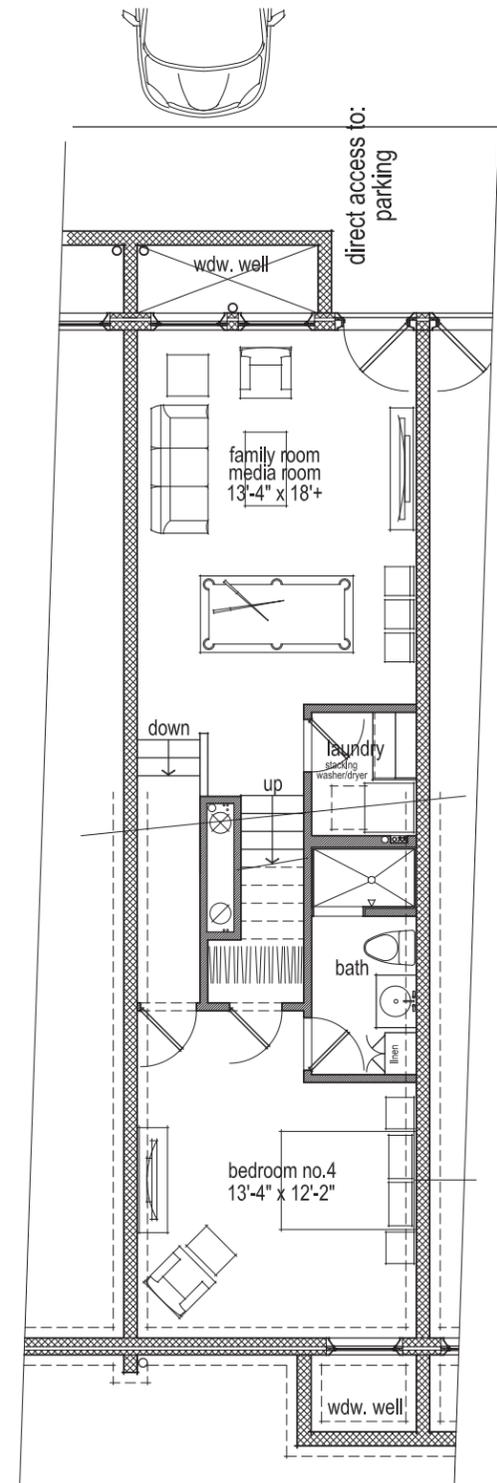
3rd floor



2nd floor



1st floor

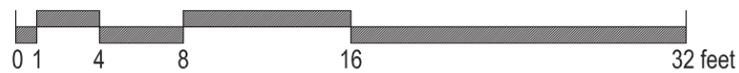


basement

area
typ. 4 bedroom unit
30th
above grade
2,079 square feet
basement
693 square feet

typical floor plans . 4 bedroom . 30th . 12 such

1/4" = 1'-0"



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Boulder, Colorado
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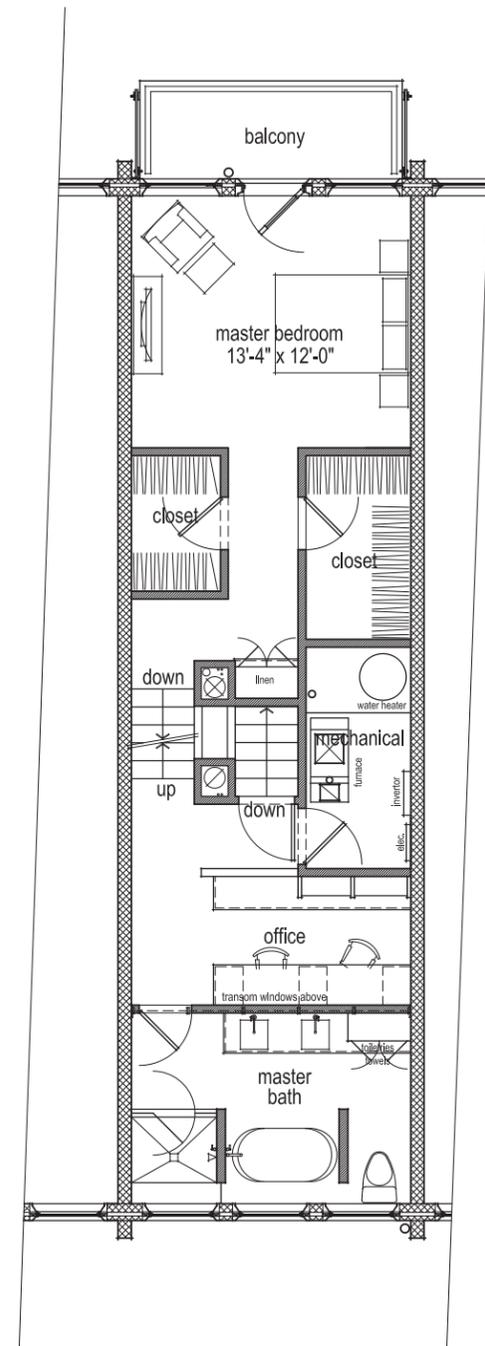
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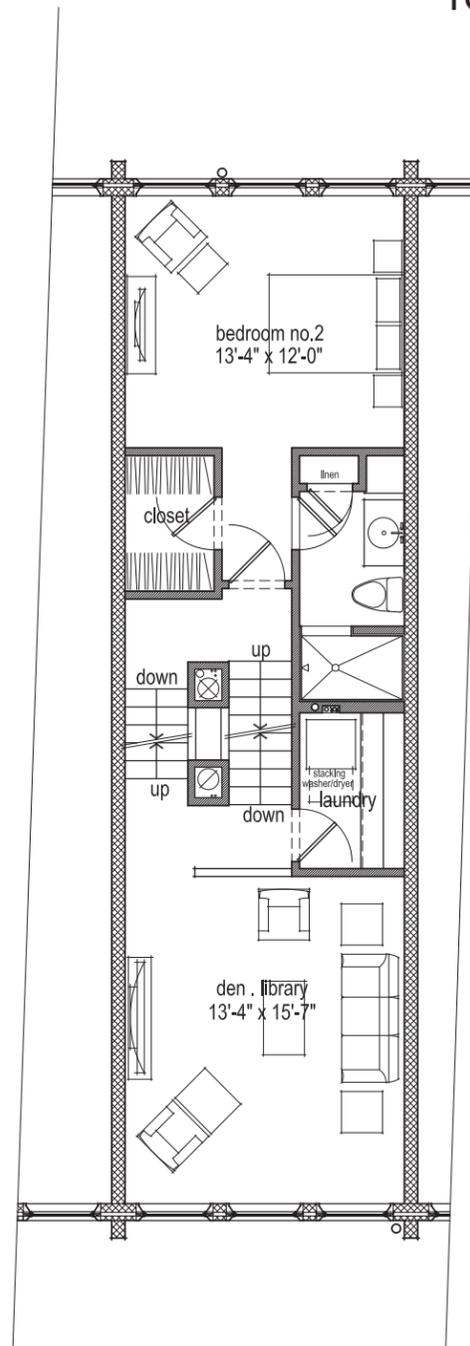
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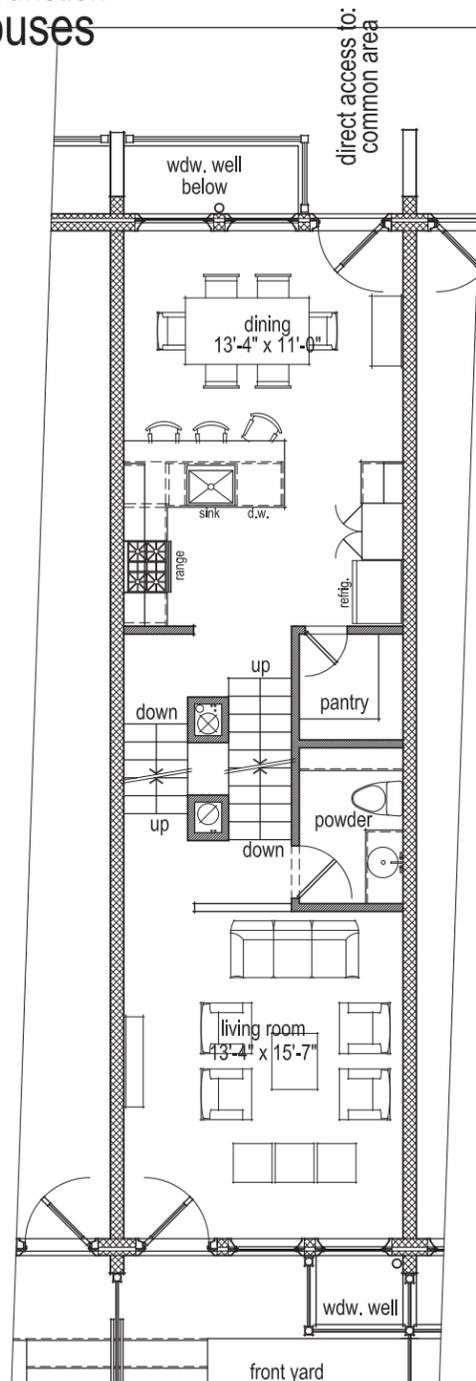
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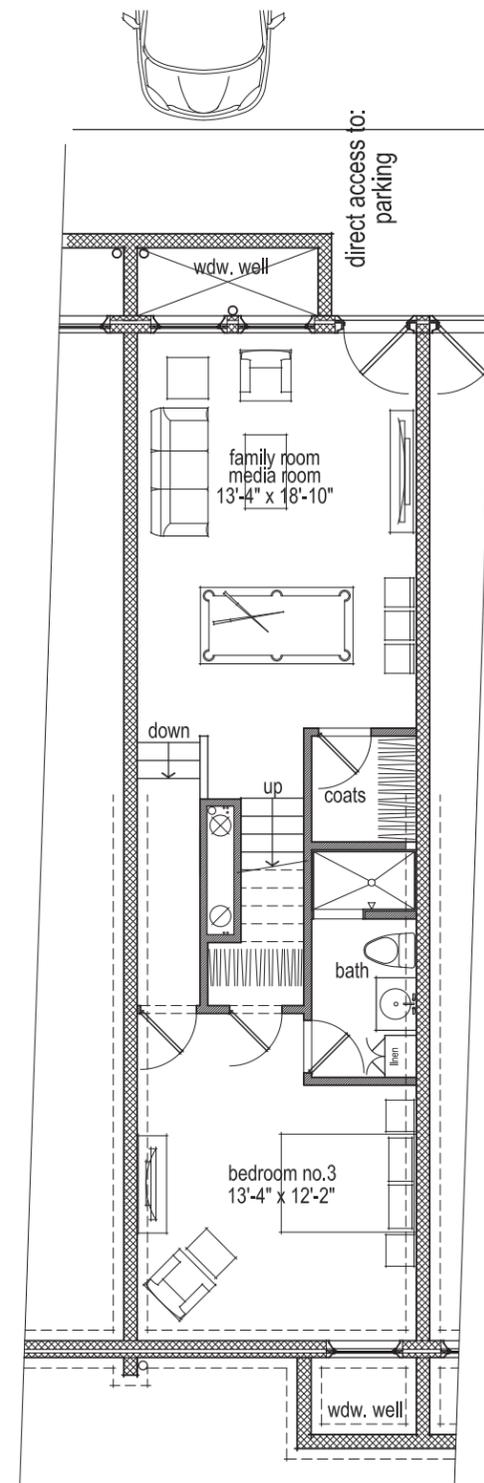
3rd floor



2nd floor



1st floor

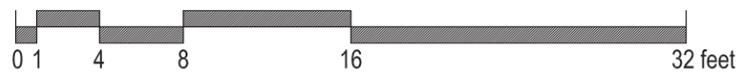


basement

area
typ. 3 bedroom unit
29 1/2
above grade
2,079 square feet
basement
693 square feet

typical floor plans . 3 bedroom . Alley . 16 such

1/4" = 1'-0"



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