

MEMORANDUM
February 3, 2016

TO: Landmarks Board

FROM: Lesli Ellis, Comprehensive Planning Manager
Debra Kalish, Senior Assistant City Attorney
James Hewat, Senior Historic Preservation Planner
Marcy Cameron, Historic Preservation Planner
Angela Smelker, Historic Preservation Intern

SUBJECT: Public hearing and consideration of a Landmark Alteration Certificate application to make improvements at the north end of Chautauqua Park including constructing a sidewalk along Baseline Road, improving accessibility at the King's Gate, and installing new lighting from Baseline to the Auditorium, pursuant to Section 9-11-18 of the Boulder Revised Code 1981 (HIS2015-00355).

STATISTICS:

- | | | |
|----|------------|-----------------------------------|
| 1. | Site: | 900 Baseline Rd., Chautauqua Park |
| 2. | Zoning: | RL-1 (Residential Low-1) |
| 3. | Applicant: | Brian Wiltshire, Project Manager |
| 4. | Owner: | City of Boulder |

STAFF RECOMMENDATION:

If the conditions below are met, the proposed public improvements in the Chautauqua Historic District will meet the standards for issuance of a Landmark Alteration Certificate specified in Section 9-11-18, Boulder Revised Code ("B.R.C.") 1981; and will be consistent with Section 2.5, "Sidewalks," of the *General Design Guidelines*; and the "Public Improvements" section of the *Chautauqua Design Guidelines*. Therefore, staff recommends that the Landmarks Board adopt the following motion:

The Landmarks Board approves the application for the construction of public improvements as shown on plans and specifications dated 02/03/2016, finding that, if constructed pursuant to the conditions below, the public improvements will meet the standards for issuance of a Landmark Alteration Certificate in Section 9-11-18, B.R.C. 1981, and will be consistent with the General Design Guidelines and the Chautauqua Park Historic District Design Guidelines, and

adopts the staff memorandum dated February 3, 2016 as the findings of the Board.

CONDITIONS OF APPROVAL:

1. The applicant shall be responsible for ensuring that the development will be constructed in compliance with the application dated 02/03/2016 on file in the City of Boulder Planning Housing & Sustainability Department, except as modified by these conditions of approval.

2. Prior to submitting a building permit application and receiving final issuance of the Landmark Alteration Certificate, the applicant shall submit revised plans showing:
 - a. A reduction in the width and amount of new sidewalk along Kinnikinic Road, including the potential of constructing sidewalk on one side of that road;
 - b. Plans for the reconstructed swale that shows for that feature to closely match the historic swales in terms of material, size, depth and finish; and
 - c. Simplified lighting posts that are more compatible with the rural character of Chautauqua,
 - d. Details showing the mounted lighting on the arbor and King's Gate Shelter so that they do the least amount of damage to those structures, all in a manner that will be consistent with *General Design Guidelines for Boulder's Historic Districts and Individual Landmarks* and the *Chautauqua Park Design Guidelines*.

3. Prior to submitting a building permit application and receiving final issuance of the Landmark Alteration Certificate, the applicant shall submit design revisions and details as required above that shall be reviewed and approved by the Landmarks design review committee, prior to the issuance of a building permit. The applicant shall demonstrate that the design details are in compliance with the intent of this approval and the *General Design Guidelines* and the *Chautauqua Park Historic District Design Guidelines*.

SUMMARY

- Because the application calls for a number of significant public improvements in the Chautauqua Park Historic District, review by the full Landmarks Board in a quasi-judicial hearing is required pursuant to Section 9-11-14(b) B.R.C. 1981.
- In 2012, the Boulder City Council adopted the *Colorado Chautauqua Guiding Principles for Place Management and Fiscal Sustainability*, which has been used as a guide in the planning of this project.
- Because a number of the public improvements are proposed outside of, but adjacent to the Chautauqua Park Historic District boundary, they are being provided as informational items and for comment by the Landmarks Board.
- In the fall of 2015, the Landmarks Board provided input on conceptual plans to construct new sidewalks, new retaining walls, improvements to the gates and drainage swales, and new lighting in the park.
- Staff recommends approval of the proposal for improvements in the historic district with a number of conditions to be reviewed and approved by the Landmarks design review committee (Ldrc) and that Public Works work closely with staff to ensure that the work outside the district not have and adverse effect on the character of the historic district. Historic preservation staff will provide information on these improvements to the Landmarks Board as they develop.

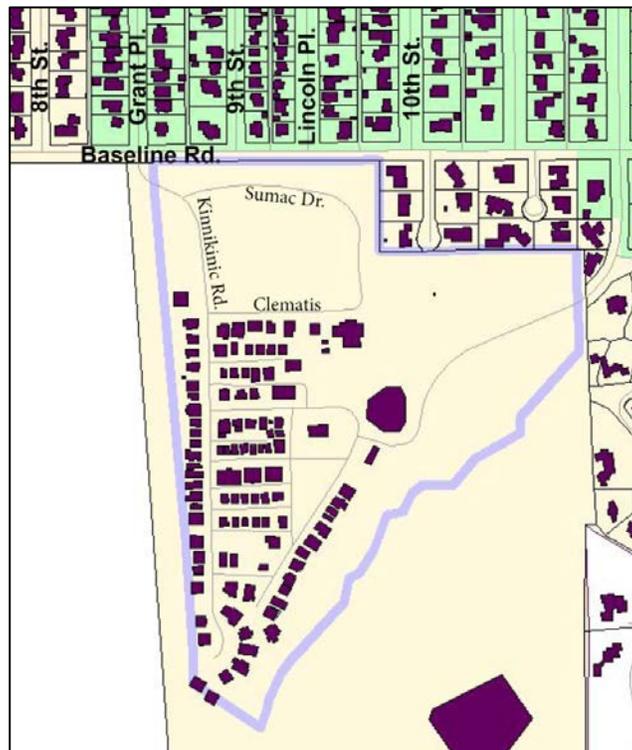


Figure 1. Map of Chautauqua. Purple line marks historic district boundary, 2015.

DISTRICT DESCRIPTION:

The Colorado Chautauqua is located in southwest Boulder at the foot of Green Mountain, bounded by City of Boulder Open Space and Mountain Parks land on three sides and Baseline Rd. on the north. The historic district, which encompasses 40 acres, includes five large public buildings, more than 100 residential cottages, and several landscaped open spaces. Chautauqua Park was designated as a local historic district in 1978 and was designated as a National Historic Landmark (NHL) in 2005 in recognition of its exceptional significance and intact historic integrity.¹ Currently, there are only 25 NHLs in the state of Colorado. It is widely recognized as being the best preserved surviving Chautauqua in the country.

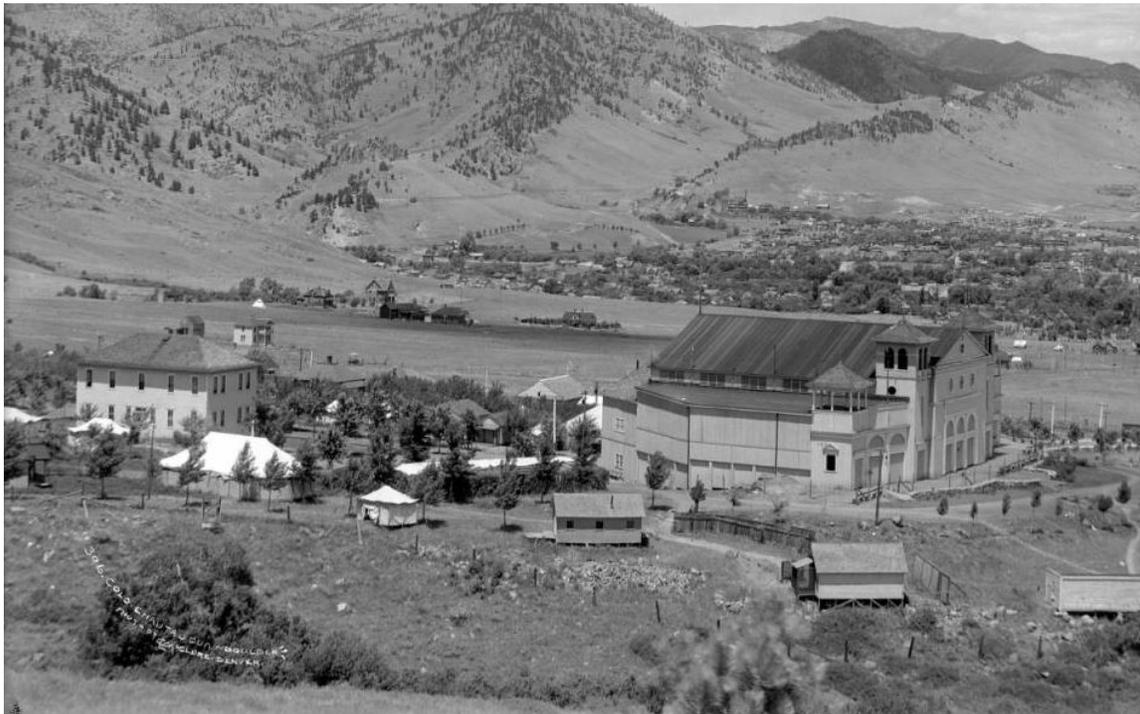


Figure 2. Chautauqua grounds looking northwest, c. 1900-1910. Denver Public Library.

The Chautauqua movement began in upstate New York in 1876 as a center for political, educational, and recreational programs. By 1924, nearly 40 million people were annually attending events at various Chautauquas across the country. In the late 1890s, a group of Texan educators, including the president of the University of Texas, wanted to open a Chautauqua and looked to the Rocky Mountains as a location. Boulder was chosen for the site and its citizens, thrilled to have a Chautauqua nearby, raised \$20,000 towards construction costs to further encourage the venture. The Texas-Colorado Chautauqua was opened on July 4, 1898 on 171 acres that the City of Boulder purchased

¹ Colorado Chautauqua. National Historic Landmark Nomination, 2005.

from the Bachelder Ranch. Since its inception, the City of Boulder and the Colorado Chautauqua Association have managed the site in partnership.

Chautauqua's layout has remained largely unchanged since its initial design in 1898. Chautauqua's streets were platted as a grid that was modified to accommodate the site's topography. Originally they were mostly narrow, paved with gravel, and some were accented with linear rows of trees. The 2005 National Historic Landmark nomination notes that "the near absence of sidewalks reinforces a rural, camp feeling, rather than lending an urban or suburban atmosphere."² A 1898 plat of the Chautauqua Park shows ten streets running east-west and two streets running north-south all of which were originally named after states, officials of the Colorado and Southern Railroad, officers of other Chautauquas, and Boulder municipal leaders. Early buildings and structures were located to allow ample room for temporary structures, gardens, walkways, and tent sites. Permanent buildings replaced tents between 1899 and World War I.

Baseline Road is located several feet north from the northern edge of the Chautauqua Park Historic District and is separated from the park by a change in grade, portions of which are retained with a stone wall. Early photographs show that Baseline Road was originally level with Chautauqua's northern border, the grade having changed sometime after 1917.³ The rubble stone retaining walls along the north edge of the district were likely constructed during this period (see Figure 4).



Figure 3. Chautauqua ticket booth and trolley station (King's Gate) looking north, 1899.

² Martha Vail, "National Historic Landmark Nomination: Colorado Chautauqua," *National Park Service* (2005): 5.

³ Mundus Bishop Design, Inc., "Chautauqua Park Historic District Cultural Landscape Assessment and Plan," *Colorado Chautauqua Association*, 57.

In 1898, an octagonal gatehouse was built along Baseline Road near 10th Street (in the current location of King's Gate), to mark the entry to the park. The ticket booth was consistent in design with the camp-like character of the park featuring a hipped roof, vertical siding on the lower portion of the building and horizontal lap siding above, and simple corner boards. In order to be sure that every admission fee was collected, a picket and post fence was erected alongside Baseline Road. This barrier continued around the perimeter of the park as a wire fence. The fence was removed in 1910 when a curfew and regular security patrols were established. By 1912, the octagonal ticket booth was moved to the northeast area of the grounds and used as an outhouse. ⁴



Figure 4. Existing Rubble Stone wall between 10th Street & Lincoln Place, just north of historic district boundary

From 1899 to 1926, King's Gate, located along Baseline Road near 10th Street, functioned as the main pedestrian and vehicular entrance and was also a stop along the streetcar line that ran from Chautauqua to downtown Boulder. In 1899, a boardwalk was constructed to make the linear walk from the entrance to the Dining Hall and Auditorium easier, especially during bad weather. The land adjacent to the south of Baseline Road (the northern part of what's now called Chautauqua Green) functioned as a buggy parking area, since cars were not allowed to park elsewhere on the grounds. ⁵

⁴ Vail, 6.

⁵ Mundus Bishop Design, Inc., 14.

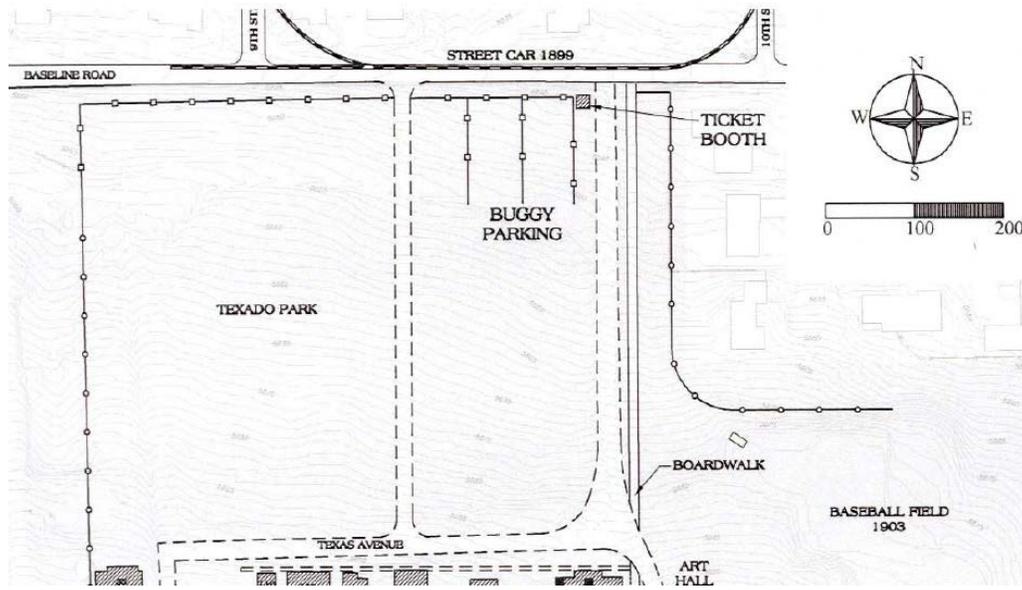


Figure 5. Map of Chautauqua 1899-1905, Chautauqua Landscape Assessment, pg. 16.

In 1912, Chautauqua’s vehicular entrance was shifted west from King’s Gate to Kinnikinic and Baseline roads, while King’s Gate remained to function as the main pedestrian entrance. In 1917, two eight-foot stone towers were constructed to mark King’s Gate and the Shelter House was constructed adjacent to these stone towers.⁶ The Shelter House is a one-story building with a hipped roof, overhanging eaves, stone rubble walls, and rubble quoins at each corner. The entrance is on the west elevation and a stone rubble bench is built into the north elevation. The Shelter House, as its name implies, sheltered passengers as they waited for the next trolley car.



Figure 6. Shelter House and King’s Gate, c. 1925-1940.

⁶ Vail, 7.



Figure 7. Shelter House and King's Gate, 2016

The rustic 1917 arbor is located on the walkway from King's Gate to the Dining Hall and was built as a resting stop for those walking up or down the path. Composed of several squared stone rubble piers that taper in width, the roof of the arbor is made up of an oval pergola of log beams, most of which have been replaced over the years. Stone benches are located in the interior.



Figure 8. Chautauqua arbor, c. 1915.



Figure 9. Chautauqua arbor, 2016

The Queen's Gate, located to the west of King's Gate along Baseline Road and directly across from Lincoln Street, was also constructed in 1917. While all of Chautauqua's streets were originally gravel, most were paved with asphalt in the 1940s.⁷



Figure 10. Queen's Gate, 2016

Chautauqua saw a number of improvements during the 1910s and 1920s. By 1910, the tents had been replaced with small frame cottages, which today define the residential area of the park. In addition, a few new public buildings were constructed, including the Mission House, Community House, and the Columbine Lodge. In 1923, a large reservoir was dug at the southern end of the park. Due to economic pressures and changing trends in recreation during the Great Depression, Chautauqua saw an overall decline in attendance and programs during the 1930s. As the buildings became increasingly dilapidated due to reduced use, the University of Colorado took over

⁷ Mundus Bishop Design, Inc., 47.

about half of Chautauqua's cottages as housing for students, which helped contribute to Chautauqua's survival. By 1955, Boulder's Chautauqua was one of only six remaining in the country. The programs and activities continued as they had from the beginning, but the buildings continued to deteriorate and by 1975 the City of Boulder considered demolishing Chautauqua's buildings. However, widespread concern about the future of the park resulted in the listing of Chautauqua in the National Register of Historic Places in 1978, sparking the park's revival. Today, Chautauqua is, arguably, the most distinctive area of Boulder as it continues to provide a variety of popular programs, concerts, debates, and recreational opportunities. Visitation to the park increases annually. In 2015, visitation to Chautauqua was estimated to have exceeded 500,000.

General Project Description:

The proposed project is part of the 2014 voter approved "Chautauqua Pedestrian Safety, Access, and Lighting Improvements Project," which addresses the south side of Baseline Road from the park boundary near 10th Street to west of 6th Street. In order to increase pedestrian safety and to add Americans with Disability Act compliant features, curbs and ramps will be added along Baseline Road and low-level lighting will be added along the walkway from the King's Gate entrance to the parking lot east of the tennis courts. The application describes the project as an interdepartmental City of Boulder project that seeks to honor the historic and natural character of the area while providing important safety improvements

Increased visitation to Chautauqua Park and the lack of sidewalk on the south side of Baseline Road from west of Sixth Street east to King's Gate have been identified as an increasing safety hazard. Pedestrian travel has increased pedestrian-vehicle and pedestrian-bicycle conflict. The lack of sidewalk in the area has been identified as impeding the mobility and access of area residents. Consequently, the project area has been identified in the City's missing sidewalk links program.

The application proposes a number of improvements, some of which are located just north of the Chautauqua Park Historic District Boundary. The following changes that proposed for the park are located outside the boundary and are **not** subject to Landmark Alteration Certificate (LAC) review:

- Construction of 5-foot wide sidewalks on the south side of Baseline Road from the park boundary near 10th Street to the end of on-street parking west of Sixth Street.
- Construction of a new stone retaining wall along the south side of Baseline Road.
- Transit stop improvements, including an Americans with Disabilities Act (ADA) compliant ramp at the King's and Queen's Gate.
- Improved ADA compliant pedestrian crossings along Baseline Road.
- Improved pedestrian connections from Baseline Road to Open Space and Mountain

Parks designated access trails and to existing Parks and Recreation paths.

- Native shrubs and plantings to replace non-native species and improve landscape conditions on the north edge of the Chautauqua meadow.

While these improvements are not subject to LAC review, they are being presented as an informational item to solicit comment from the Landmarks Board.

The following are improvements located within the historic district boundary and are subject to review by the Landmark Board through LAC review:

- Construction of a 5-foot concrete sidewalk with a flagstone valley pan to extend from the Kinnikinic entrance part way on Sumac Drive;
- Conversion of parallel parking to angular parking along northern edge of Sumac Drive;
- Construction of a soft surface path to connect Queen's Gate with Sumac Drive;
- Pedestrian lighting from the King's Gate entrance to the parking lot east of the tennis courts;
- Pedestrian lighting at the Shelter House (Trolley Station) and the Arbor;
- Installation of way-finding elements per the Colorado Chautauqua Association National Historic Landmark Master Signage Plan.

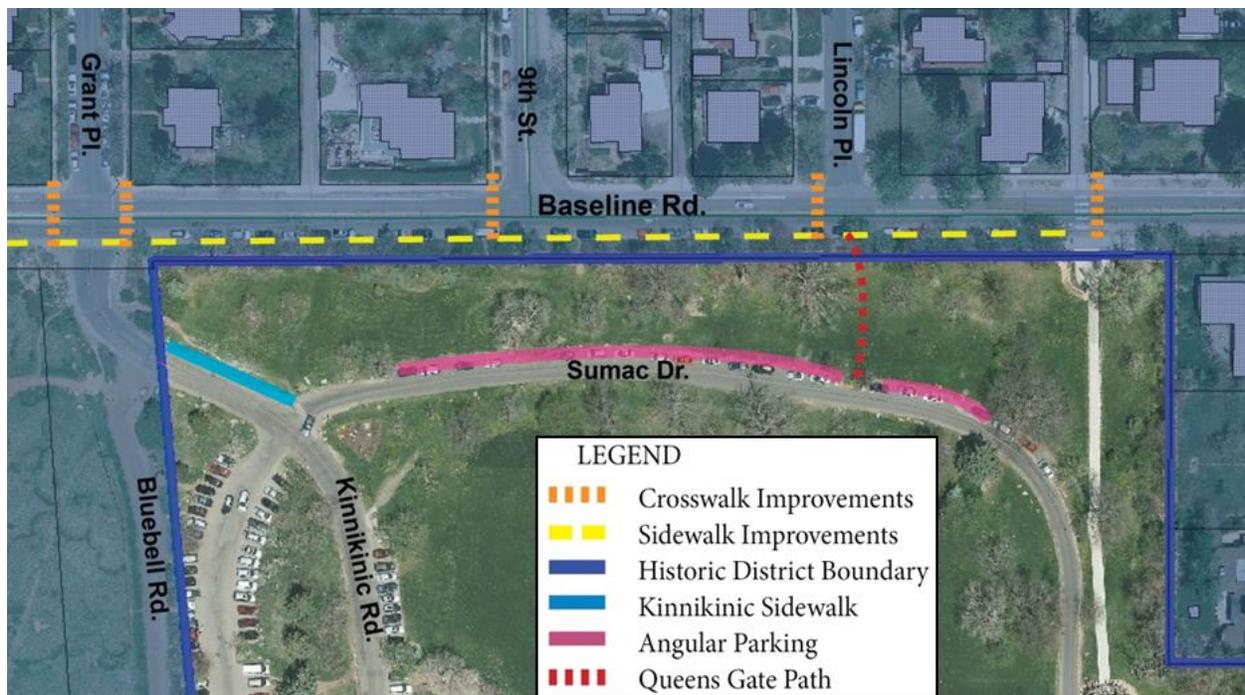


Figure 11. Map of Chautauqua showing areas for planned improvements, (unshaded area represents area subject to LAC review). Lighting and signage not shown on this plan.

Work Outside of District Boundary for Comment by the Landmarks Board

Sidewalks Along Baseline Road:

The lack of a sidewalks along the south side of Baseline Road, from west of 6th Street to east of King's Gate, is considered to be an increasing safety hazard as traffic in the area increases. The application notes "pedestrians travel in the street increasing pedestrian-vehicle and pedestrian-bicycle conflict. The lack of sidewalks impedes the mobility and access of area residents."

Plans show a 5 foot wide sidewalk to be constructed in the right-of-way along Baseline Road, just north of the Chautauqua Park Historic District. A section of grass that varies in width (from one to 6 feet), and varies in grade (at most a 50% upward slope) will connect the sidewalk to the higher existing grade of the Chautauqua grounds. Portions of this sloped area will be retained by a rubble stone wall to blend with the existing c.1917 rubble stone wall (outlined in yellow in plans). The submission does not include an elevation of proposed wall, but a section plan identifies it as ranging from 1 to 5 feet in height.

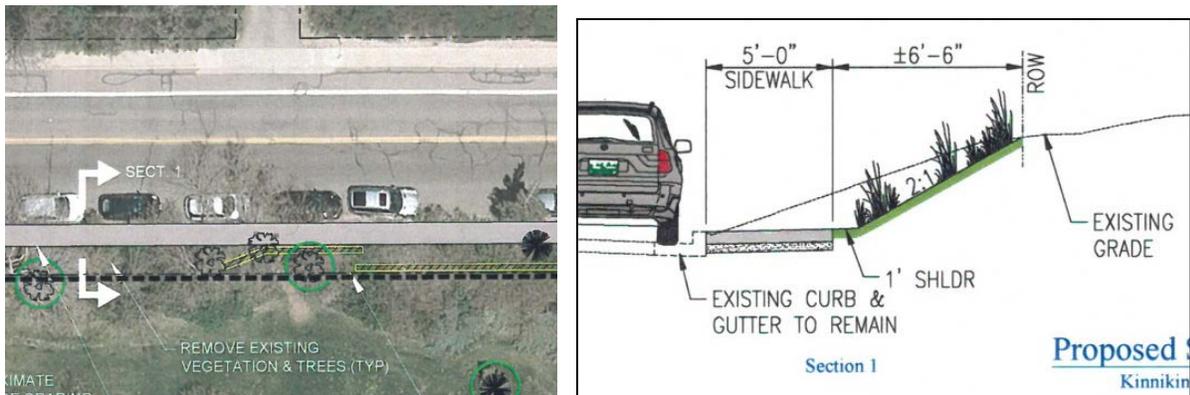


Figure 12 & 13. Design concept along Baseline Road (left) and details (right) between Grant Place and 9th Street.

Transit Stop Improvements at Kings Gate

Proposed changes at the King's Gate show reconstruction of the existing stairs to include a widened (bulb out) sidewalk area and an Americans with Disabilities Act compliant ramp to provide universal access into the park.



Figure 14 & 15 Existing and Proposed Change at King's Gate

Baseline Road Pedestrian Crossings

Five pedestrian crossings across Baseline Road: 6th Street, 7th Street, Grant Place, Queen's Gate (Lincoln Place), and at King's Gate (near 10th Street) are proposed. Curb ramps that are ADA-compliant will be installed at these crossings.

Removal of Trees and Shrubs Along Baseline Road

Much of the existing (non-significant) vegetation and trees along Baseline Road are proposed to be removed, including dead cherry trees and green ash trees in poor health. A number of large white and red oak trees will be preserved. The removal of much of this "volunteer" vegetation will open views into Chautauqua returning conditions closer to those during the early years at the north side of the area.

**Work Within the Chautauqua Historic District Boundary Subject to Landmark Alteration Certificate Review:
Kinnikinic Road Sidewalk and Gutter**



Figure 16. Proposal for sidewalk and gutter along Kinnikinic Road (dashed line marks district boundary), 2015.

Design concepts show the vehicular entrance to Chautauqua at Kinnikinic Road to have widened sidewalks and ADA-compliant ramps. The entrance is just outside of the Historic District boundary and not subject to LAC review, but the sidewalk improvements and the proposed installation of a flagstone drainage swale as they continue along the east side of Kinnikinic Road are located within the boundary just as the road begins to turn east (see map). Referring to the design concept details for the improved sidewalk located along Baseline from Grant Place to 9th Street (outside of the Historic District), it is assumed that the 160 lineal feet of sidewalk and swale that continue along Kinnikinic Road into the district will also be concrete and 5 feet in width. Plans show the mortared drainage swale to divide the concrete sidewalk from Kinnikinic Road.

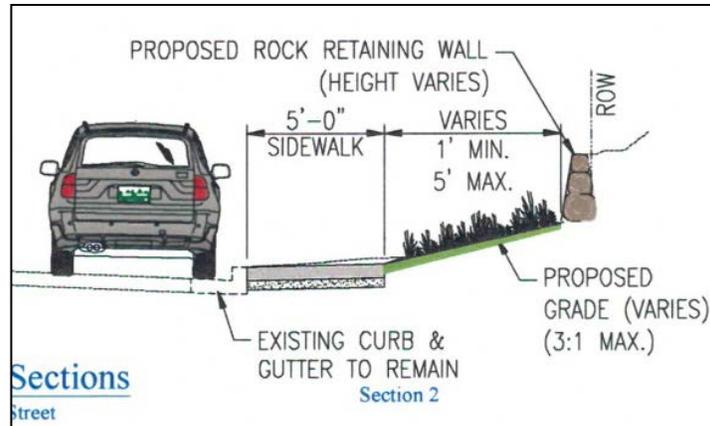


Figure 17. Section showing width & grade of sidewalk along Baseline Road between Grant Pl. and 9th Street, 2015.



Historic mortared stone swale along Kinnikinic Road, 2013



Figure 18. Proposed mortared stone swale along Kinnikinic Road, 2015.

ANGLED PARKING

The proposed reconfiguration of angular parking along the north side of Sumac Drive is intended to allow for more parking. Plans show that Sumac Drive is currently 13 feet in width with about 8 feet of parallel parking along the north side of the road. The

modification of the parallel parking spots to the angular parking will not change the width of Sumac Drive. The angular parking will be 21 feet and 4 inches in length, encroaching onto the lawn between Baseline Road and Sumac Drive by 12 feet and 7 inches. Instead of paving the extra 12 feet with asphalt, it will instead be composed of a permeable layer with a drainage aggregate (stabila-grid) beneath a grass covering. Plans note that existing parallel parking allows for about 18 spaces and the proposed angular parking will allow for 26 spaces.



Figure 19. Proposed parking along the eastern half of Sumac Drive, 2015.



Figure 20 & 21. Left: Proposed angular parking on Sumac Drive, 2015.
 Right: Example of proposed permeable parking on Sumac Drive, 2015.

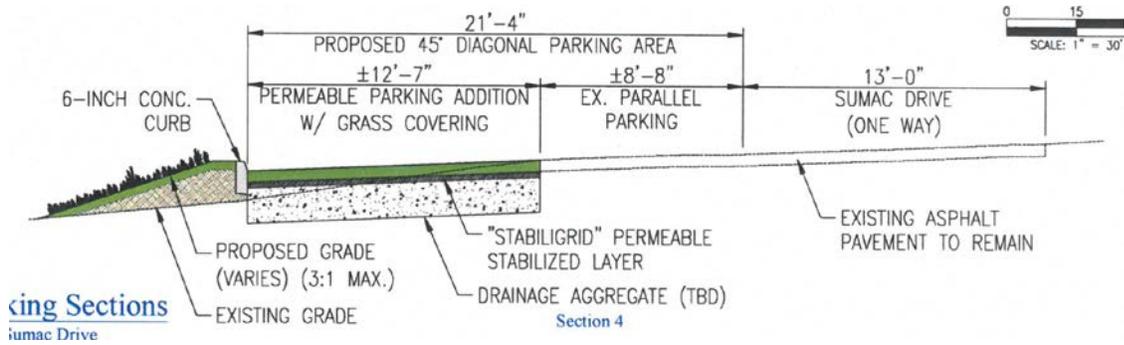


Figure 21. Design details for parking along Sumac Drive, 2015.

Queen’s Gate Path

The Queen’s Gate is located along Baseline Road directly across from Lincoln Place. While the gate and its proposed improvements are located outside the Historic District boundary, the proposal calls for the construction of a soft surface path to connect the gate to Sumac Drive in the historic district boundary. No historic photos of Queen’s Gate could be located and it is unclear what kind of path may have existed. Current photos show that there is no formal path. With the pedestrian improvements along Baseline Road and to Queen’s Gate, it may be safe to anticipate a higher level of foot-traffic at this historic entrance to the grounds.

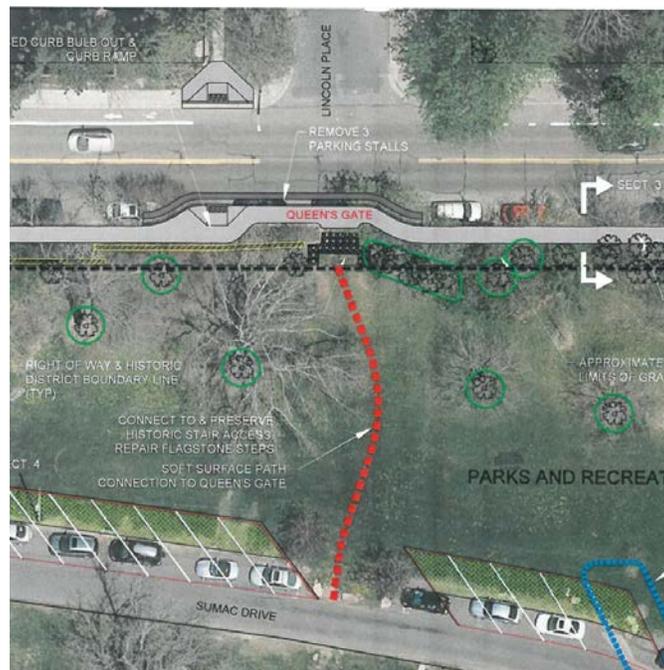
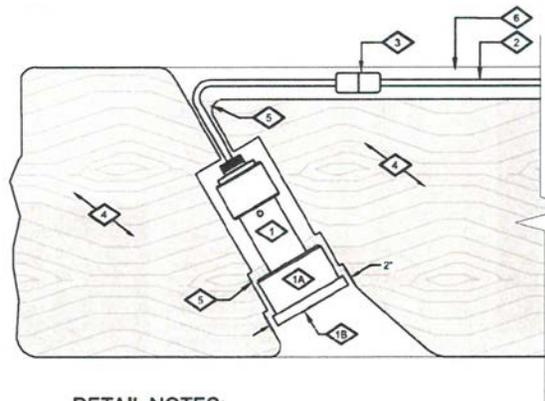


Figure 22. Proposed soft surface path from Queen’s Gate marked by red dashed line.

Lighting of the Arbor

Plans propose the installation of low level lighting along key access points for pedestrian safety and to increase accessibility from Baseline Road through the major park access walks. Currently very few structures in the park are lighted. The main path from King's Gate to the Auditorium (along which the historic arbor is located) can receive a substantial amount of pedestrian traffic at night. In the lighting plan for the arbor, exterior surfaces of the arbor's stone columns are proposed to be lit by way of flood light fixtures mounted of the underside of the arbor's structural members. The downward direction of the lights is intended to create a "glow" around the columns, making it a more inviting destination at night while also increasing a sense of safety. The proposed lighting will be controlled so that it will only be on during events or certain times of the evening.



DETAIL NOTES:

- 1 TYPE 'B', BK MINI-MICRO RECESSED LED DOWN LIGHT.
LED - 3 WATT, 3000K
SPOT DISTRIBUTION WITH SOFT FOCUS LENS.
- 1A MOUNTING COLLAR.
- 1B FLANGE, REMOVABLE FOR ACCESS TO LED MODULE.
- 2 CLASS II WIRING TO REMOTE 12VAC TRANSFORMER.
- 3 WET LOCATION CONNECTOR.
- 4 EXISTING WOOD BEAM.
- 5 SILICONE OR APPROPRIATE SEALANT/ADHESIVE.
- 6 CUT CHANNEL IN TOP OF BEAM FOR WIRING.

2 TYPE 'B' - ARBOR LIGHTING DETAIL
LE03 / NOT TO SCALE



Figure 23 & 24. Left: Rendering of Arbor with proposed lighting, 2015. Right: Detail notes for arbor lighting, 2015.

Lighting of the Trolley House (Shelter House)

Plans also call for the lighting of the interior of the historic Trolley House by way of an LED light fixture facing upwards to provide a glowing lighting visible through the station's windows with a small amount of light landing on the pathway outside. The request states the building is currently is not lighted and as a result it can be very dark and uninviting at night.

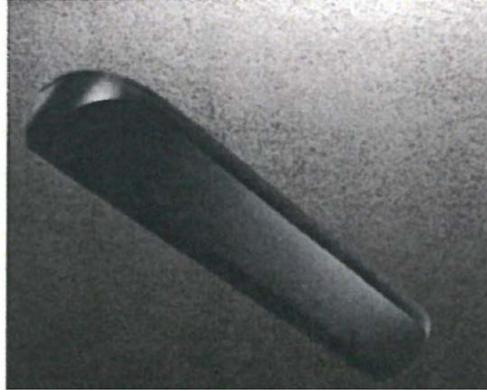


Figure 21 & 22. Left: Rendering of Arbor with proposed lighting, 2015. Right: Detail notes for arbor lighting, 2015.

Lighting of King's Gate Path and Sumac Drive

In addition to the arbor and the Trolley Station, the application requests the installation of nine lamp "acorn" lamp posts. Six of the proposed lamps have a 12 foot mounting height and are shown to be installed on the pathway south from King's Gate to the Tennis Courts. The proposed 12' lamp posts are similar to those installed in other locations, probably sometime during the 1980s. Two bell-shaped lamp posts that are 18.5 feet high to provide downward lighting, are proposed for to be installed on the eastern edge of the tennis court area, and a third bell-shaped lamp will be installed along the eastern portion of Sumac Drive near the angular parking (see map).

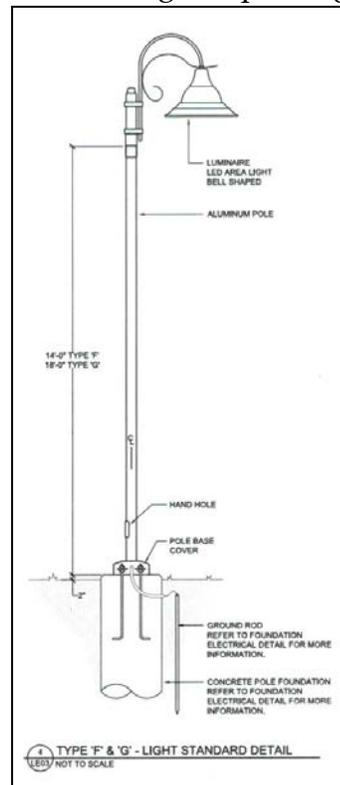
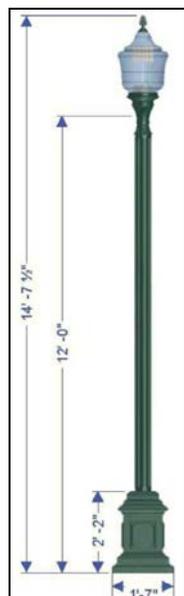


Figure 23 & 24. Left: Acorn-shaped proposed light fixture, 2015. Right: Bell-shaped proposed light fixture, 2015.



Figure 25. Proposed lighting plan, 2015.

CRITERIA FOR THE BOARD'S DECISION

Subsections 9-11-18(b) and (c), B.R.C. 1981, set forth the standards the Landmarks Board must apply when reviewing a request for a Landmark Alteration Certificate.

- (b) Neither the Landmarks Board nor the City Council shall approve a Landmark Alteration Certificate unless it meets the following conditions:
 - (1) The proposed work preserves, enhances, or restores and does not damage or destroy the exterior architectural features of the landmark or the subject property within an historic district;

- (2) The proposed work does not adversely affect the special character or special historic, architectural, or aesthetic interest or value of the landmark and its site or the district;
- (3) The architectural style, arrangement, texture, color, arrangement of color, and materials used on existing and proposed constructions are compatible with the character of the existing landmark and its site or the historic district;
- (4) With respect to a proposal to demolish a building in an historic district, the proposed new construction to replace the building meets the requirements of paragraphs (b)(2) and (3) above.

(c) In determining whether to approve a landmark alteration certificate, the Landmarks Board shall consider the economic feasibility of alternatives, incorporation of energy-efficient design, and enhanced access for the disabled.

ANALYSIS

1. *Does the proposed application preserve, enhance, or restore, and not damage or destroy the exterior architectural features of the landmark or the subject property within a historic district?*

Staff considers that provided the conditions listed are met, the proposal for changes in the historic district boundary will not damage or destroy the character of the Chautauqua Park.

2. *Does the proposed application adversely affect the special character or special historic, architectural, or aesthetic interest or value of the district?*

Provided the listed conditions are met, staff does not consider that the proposed work in the Chautauqua Park will adversely affect the historic, architectural, or environmental significance of the historic district.

3. *Is the architectural style, arrangement, texture, color, arrangement of color, and materials used on existing and proposed structures compatible with the character of the historic district?*

Staff considers that if the amount of new paving in the district (including drainage swale improvements), is limited and lighting installation is modified per the conditions the proposal will be compatible with the historic character of the Chautauqua Historic District

4. Does the proposal to demolish the building within the historic district and the proposed new construction to replace the proposed demolished building meet the requirements of paragraphs 9-11-18(b)(2) and 9-11-18(b)(3) and (4) of this section?

Not applicable.

DESIGN GUIDELINES

The Historic Preservation Ordinance sets forth the standards the Landmarks Board must apply when reviewing a request for a Landmark Alteration Certificate and the board has adopted the *Chautauqua Park Historic District Design Guidelines* and the *General Design Guidelines* to help interpret the ordinance. The following is an analysis of the submitted proposal with respect to relevant guidelines. It is important to emphasize that design guidelines are intended to be used as an aid to appropriate design, and not as a checklist of items for compliance.

The following is an analysis of the proposal's compliance with the applicable design guidelines:

CHAUTAUQUA DESIGN GUIDELINES

The following section is from the *Chautauqua Design Guidelines*. A more in-depth analysis is included below, in the *General Design Guidelines* section.

PUBLIC IMPROVEMENTS	
	<i>Public improvements are especially important to the character of Chautauqua, and include such elements as streets, public parking, curbs, gutters, stone walls, benches, street lights, water foundation, and signs. The addition of contemporary examples of these elements have a greater impact on Chautauqua than on some other historic areas because so much of the Chautauqua site remains little changed. Whenever considering public improvements or changes to existing improvements, every attempt should be made to fit them into the existing historic site character. This may require modification, waiver of change to standards applied elsewhere for public improvements.</i>
	<p>Analysis: The proposed installation of sidewalk at the Kinnikinic Road entrance to the park is necessary to provide universal pedestrian access to Chautauqua. The Chautauqua Park is a municipally owned place and providing accessibility in a manner that respects the historic character is an important goal. Ramping the King's Gate (just north of the district boundary) will provide access at this east (transit served) entrance. Likewise, lighting and drainage swale improvements in the park will improve safety and assist with ongoing maintenance.</p> <p>Staff considers that the amount of paving (sidewalk and swale) improvements</p>

	<p>should be limited, so as not to affect the character of the historic district. To this end, narrowing the sidewalk (and potentially limiting to one side of Kinnikinic Road) to the extent possible should be considered. Also, alternative paving materials that soften the appearance of the sidewalk should be explored.</p> <p>Details of improvements to the drainage swale at Kinnikinic Road should be revised to more closely match the appearance of the historic swales in material and detailing.</p> <p>Location, type and installation of light fixtures should be reviewed and approved by the Ldrc. Consideration might be given to choosing fixtures that are simpler (possibly smaller) and less ornate lamp posts.</p>
	<p>Public Parking:</p>
	<p><i>Public parking makes the greatest impact on residents of Chautauqua. Increased and reorganized parking in the public areas of the park are an attempt to limit the use of streets in the cottage area to residential parking. Every effort should be made to strengthen this separation of public and residential parking;</i></p>
	<p>Analysis: Staff considers the proposed parking improvements will allow for a modest number of new parking spaces to be provided in the park, without making a significant change to the character of the district. Details of the “Stabili-Grid” system should be reviewed and approved by the Ldrc.</p>
	<p>Sidewalks, Curbs, and Gutters:</p>
	<p><i>Streets in Chautauqua do not have the familiar sidewalks, curbs, and gutters; however, drainage and erosion is a significant problem in many locations. Drainage improvements have included special curb and gutter sections especially designed to fit the character of the park. Where drainage improvements are required, existing sandstone gutters or the special curb and gutter sections should be used as a reference. Standard formed concrete curb and gutter sections, and concrete sidewalks are generally inappropriate, and every attempt should be made to find appropriate substitutes;</i></p>
	<p>Analysis: The proposed addition of 5 foot concrete sidewalk into the historic district may be inconsistent with this guideline which suggests that sidewalks in Chautauqua do not have historic precedent, and their introduction may be inappropriate. The guideline does state that drainage is a problem in the park and that “special curb and gutter” sections should be used as reference. Limiting the amount of paving in the historic district while achieving universal accessible seems an important goal. Staff suggests that consideration should be given to reducing the width of the sidewalk on the east side of Kinnikinic Road should be considered or, alternatively, providing sidewalk only on one side (perhaps the west) to achieve accessibility and minimizing the visual effect of</p>

	<p>more paved area at this key view shed into the historic district.</p> <p>Likewise, staff recommends that a replacement curb and gutter prototype be developed by controlling entities at Chautauqua and reviewed and approved by the Landmarks Board. Long term, staff suggests comprehensive plan for drainage and pathways be developed for Chautauqua that balance the practical and aesthetic needs of the park. Such a plan and guidelines should be reviewed and approved by all entities at Chautauqua, including the Colorado Chautauqua Association Building and Grounds Committee, parks and Recreation, Opens Space and Mountain Parks as well as the Landmarks Board.</p>
	<p>Stone Walls</p>
	<p><i>Stone retaining walls are common throughout Chautauqua and are made of sandstone laid either in random rubble or random ashlar patterns. Individual stones vary in size, but are generally at least two to three inches in thickness. The stone colors range from buff to yellow ochre to red-brown. Mortar joints are frequently raked back, giving the appearance of dry-laid masonry. These characteristics should be carefully observed in new walls, and repairs to existing walls should match stone color and sizes, joint size, profile and mortar color as exactly as possible. Red colored Lyons sandstone with one to two inch cleavage planes should only be used in if mixed in small amounts with more characteristic stone.</i></p>
	<p>Analysis:</p> <p>While just north and outside of the district boundary, the construction of approximately 400 lineal feet of stone retaining wall will have a visual impact on this edge of Chautauqua. While submitted plans are do not show the extent of existing stone wall along Baseline Road, staff estimates the stretch between the King’s and Queen’s gates to span approximately 150 lineal feet. The proposed new retaining wall is described as ranging from 1 to 5 feet in height, but little information has been provided as to type of stone, pattern or mortar of the proposed wall. Staff recommends that details of the new wall be brought to the Ldrc for review to ensure compatibility with the existing character of this north edge of the Chautauqua Historic District.</p>
	<p>Street Lights, Street Signs, Benches, Foundation and Other Site Furniture:</p>
	<p><i>These elements were originally simple and utilitarian, not elaborate and ornate. There are a number of companies that manufacture historic reproductions from many periods. Since the development of Chautauqua spanned several decades, there may be a good selection of appropriate site furniture; however, simplicity is the most important consideration.</i></p>
	<p>Analysis:</p> <p>The introduction of new free-standing lighting should be simple and limited to preserve the rustic camp, like feel of Chautauqua. The introduction of the six lamp posts may have a visual effect, but that it most likely would not have an adverse effect on the overall character of the district. The introduction of the three 12 foot down lights will not negatively affect the park given their relative distance from each other. Lighting the arbor and trolley house with low wattage lighting should</p>

be done in a manner so that at it may be removed without damaging those historic structures. The proposal appears to remove material to install the lighting. Surface mounted lighting is more appropriate and the details of this should be reviewed by the Landmarks design review committee (Ldrc).

As with the drainage and paving, staff considers it important that the CCA, OSMP, Parks and Recreation jointly develop a comprehensive lighting plan for the park that can be used as the basis for the review of future lighting improvements, should they be necessary.

GENERAL DESIGN GUIDELINES FOR SITE DESIGN

2.5 Sidewalks			
<i>Many of Boulder’s older neighborhoods were originally paved with flagstone or aggregate concrete. These original walkway materials are important elements and contribute to the historic character of the neighborhood. The traditional pattern of walkways perpendicular from the public sidewalk to the front porches or main entries of the houses provides unity to the streetscape. New sidewalks must meet the Americans with Disabilities Act requirements.</i>			
	<i>Guideline</i>	<i>Analysis</i>	<i>Conforms?</i>
.2	<i>New walkways should be designed to be compatible in location, pattern, spacing, dimensions, materials and color with existing walkways that contribute to the overall historic character of the area. Walk design should be simple and traditional, reflecting the neighborhood and period of construction. Flagstone or traditional aggregate concrete is encouraged.</i>	Staff considers that the amount of paving (sidewalk and swale) improvements should be limited, so as not to affect the character of the historic district. To this end, narrowing the sidewalk (and potentially limiting to one side of Kinnikinic Road) to the extent possible should be considered. Also, alternative paving materials that soften the appearance of the sidewalk should be explored.	Maybe

The 2102 Colorado Chautauqua Guiding Principles for Place Management and Fiscal Sustainability collaborative work agreement has been used in planning the voter approved “Chautauqua Pedestrian Safety, Access, and Lighting Improvements Project”.

Public Works has been working with the Colorado Chautauqua Association, the city's Parks & Recreation, Open Space & Mountain Parks and the Historic Preservation program to develop plans. Conceptual plans were shared with the Landmarks Board in the fall of 2015 and the current proposal reflects input given at that time.

The guidance provided in the design guidelines counsel that any changes in the Chautauqua Park Historic District need to be considered carefully given the intact nature and fragility of this important place. However, given the compelling goal to provide universal access at the north entrances to the park, staff considers that the construction of a sidewalk along Kinnikinic Road can be undertaken without damaging the essential character of the district. Staff considers it will not detract from the historic character of the district provided final design details are reviewed by the Landmarks design review committee to ensure that the amount of paving is the minimum necessary to achieve ADA compliance. At the same time improvements to the drainage swales should be undertaken so that it closely matches the materiality, dimensions and overall appearance of the historic swales. Finally details of the proposed new lighting fixtures should be simply designed to ensure compatibility with the character of the district. All of these details should be reviewed and approved by the Ldrc prior to issuance of a final landmark alteration certificate.

In terms of the improvements outside of the district and not subject to a Landmark Alterations Certificate, staff considers that great care needs to be taken to ensure that the essential character of the north edge of Chautauqua not be adversely effected by the work. In particular, staff considers that the width and extent of the paving along Baseline Road (especially as it turns into Chautauqua at Kinnikinic Road) be minimized to the extent possible. Likewise, great care will need to be taken to ensure the construction of the new retaining walls will be compatible with and secondary to the historic walls along Baseline Road. Staff suggests that Public Works work with staff to develop designs that minimizes the impact in this area and that staff shares these designs, on an informational basis, with the Landmarks Board as they develop.

FINDINGS

Subject to the conditions stated in the February 3, 2016 staff memorandum, staff recommends that the Landmarks Board approve the application and adopt the following findings:

Providing the listed conditions are met, this decision will be consistent with the purposes and standards of the Historic Preservation Ordinance (Chapter 9-11, B.R.C. 1981) in that:

1. The proposed new construction will not adversely affect the special character or special historic, architectural, or aesthetic interest or value of the property or the historic district (9-11-18(b)(2), B.R.C. 1981).
 2. The proposed new construction will generally comply with Section 2.5 , “Sidewalks,” of the *General Design Guidelines* and with the “Public Improvements” section of the *Chautauqua Design Guidelines*, and Section 9-11-18(b)(3), B.R.C. 1981.
-

ATTACHMENTS:

- A: Historic Photographs
- B: Current Photographs
- C: Plans and Elevations

Attachment A: Historic Photographs



Photo 1. Chautauqua entry with ticket booth at right (at current location of King's Gate), c. 1898-1902.



Photo 2. View of northern grounds of Chautauqua looking northeast, 1898.



Photo 3. Chautauqua Boardwalk (path that led from King's Gate entry to Auditorium and Dining Hall), 1899.



Photo 4. View of Chautauqua's entrance taken from Baseline Road looking south, c. 1901-1902.



Photo 5. View looking north from Chautauqua grounds, c. 1902-1910. Path in center might lead to King's Gate. Mount St. Gertrude Academy in distance, center.

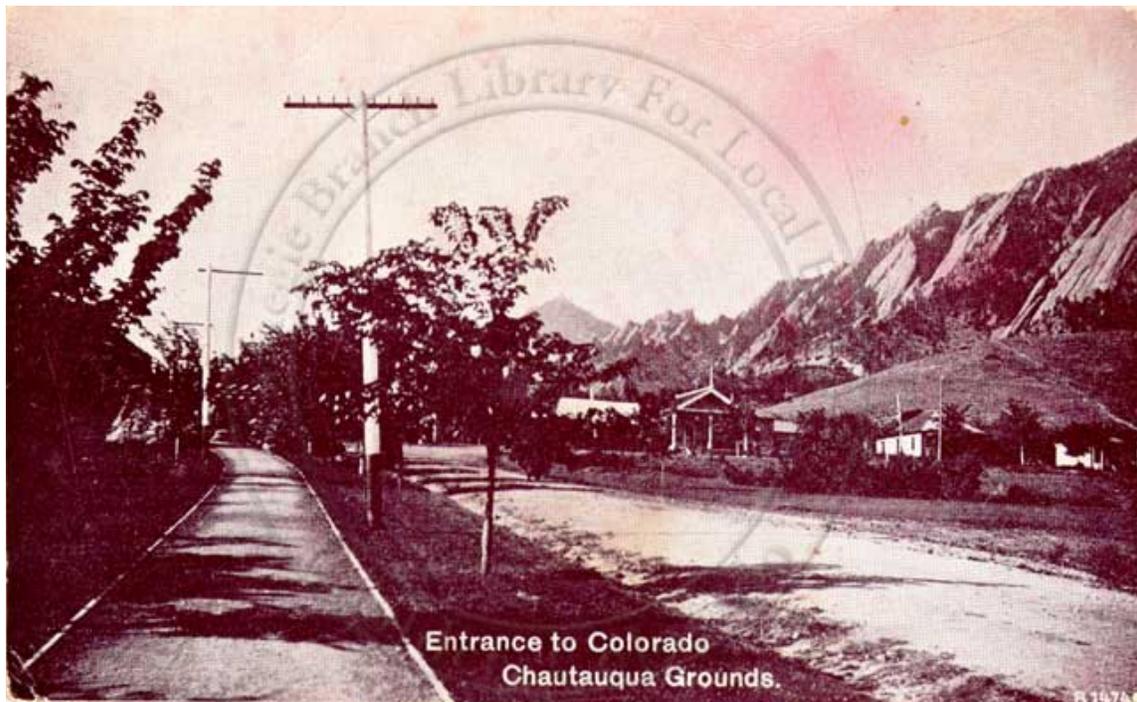


Photo 6. Postcard view of Chautauqua's entrance, c. 1908.



Baseline Rd. at Kinikininic looking south into park and looking southeast down Baseline Rd.
2016



Queens Gate (c.1917) and retaining wall along Baseline Road, 2016



King's Gate, 2016



c.1917 Transit Stop House at King's Gate, 2016



View south from King's Gate, 2016



c.1917 Arbor, Chautauqua 2016 (Looking North)



Playground at Tennis Courts, Chautauqua, 2016 (Looking east)

Currently, the lack of sidewalk on the south side of Baseline Road from west of Sixth Street east to King’s Gate creates a safety hazard. Pedestrians travel in the street increasing pedestrian-vehicle and pedestrian-bicycle conflict. The lack of sidewalks also impedes the mobility and access of area residents. Consequently, the project area has been identified in the City’s Missing Sidewalk Links program.

The Transportation Master Plan (TMP) prioritizes providing travel options by constructing multimodal facilities. The project supports this TMP goal through construction of sidewalks and transit stop amenities. Additionally, this project provides OSMP the opportunity to implement the Council-approved West Trail Study Area Plan, which includes recommendations to reduce the number of damaging social trails on the north side of the Chautauqua Meadow. This project also aligns with the Colorado Chautauqua Association 2004 Cultural Landscape Assessment and Plan’s recommendations to restore historic views “in and from Chautauqua Park.”

When will the Chautauqua Project begin?

Vegetation pruning and removal is scheduled to take place in fall, 2016, followed by construction in fall and winter 2016-2017.

	2015		2016		2017
	Sept	Oct	Fall	Winter	Spring
Board and Committee Presentations					
Public Meeting					
Preliminary and Final Design Development					
Vegetation Pruning & Removal					
Construction Start					
Construction Completion					

Construction Impacts

Project construction is anticipated to begin in the winter of 2016 and will take six months to complete.

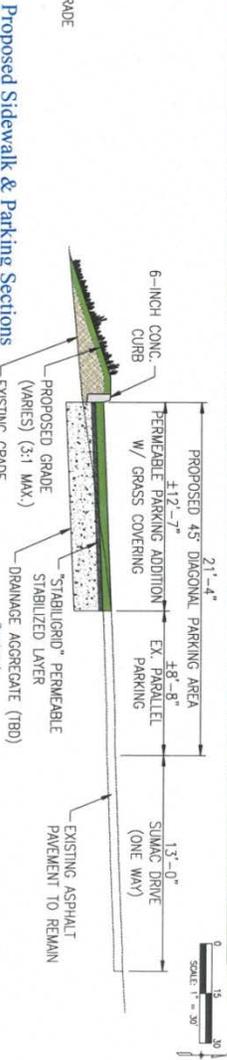
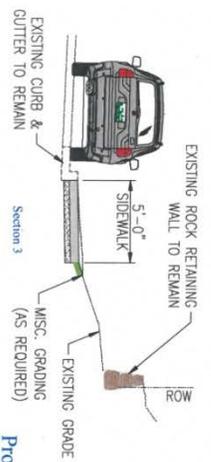
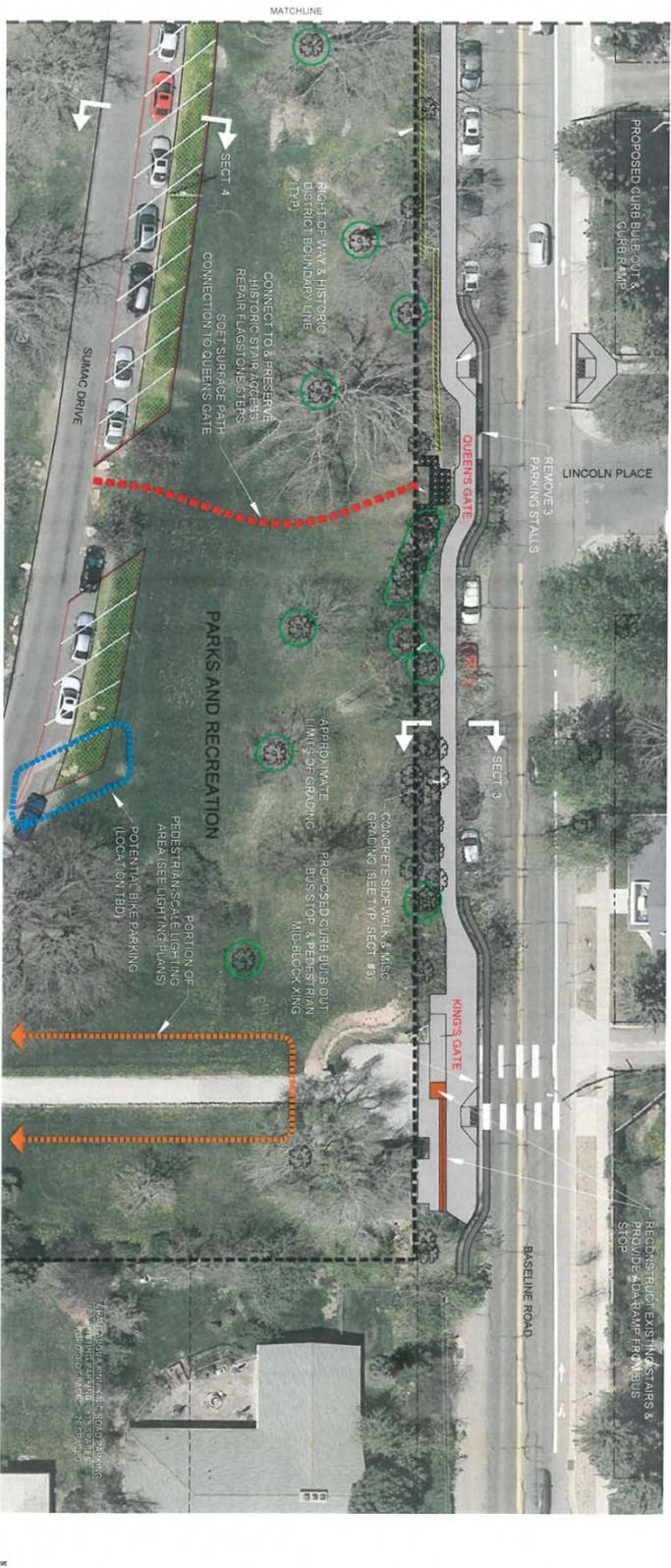
The Chautauqua Project will remove small- and medium-sized trees along Baseline Road, including cherry trees that died in the November 2014 freeze and green ash trees that are in poor health and are susceptible to emerald ash borer, a destructive insect pest.

Tree protection will be used for trees to be preserved, including the large white and red oaks on the north side of the park. The project will plant eight new trees in the park outside of the natural area.

Additional construction impacts are to be determined: parking restrictions, pedestrian detours, vehicular lane narrowing, and bicycle detours may be in effect during construction.

How can I get more information about this and other projects?

To find the latest information about this project, visit the [Chautauqua Project](#) webpage or contact Melanie Sloan at 303-441-4934 or by email at sloanm@bouldercolorado.gov.



Chautauqua Pedestrian Safety, Access, and Lighting Improvements

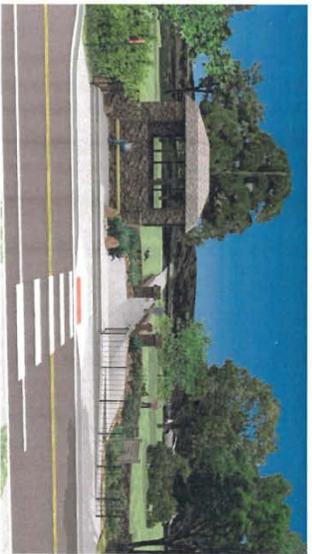
Design Concept
Kinnikinic to 10th
December, 2015



Plans for improvements along Baseline Road, 2015.



Chautauqua Kings Gate
Existing Conditions



Chautauqua Kings Gate
Proposed Conditions



Chautauqua Entry at Kinnikinick Rd
Existing Conditions



Chautauqua Entry at Kinnikinick Rd
Proposed Conditions



Chautauqua Parking on Sumac Dr
Existing Conditions

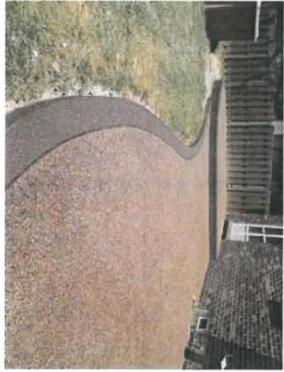


Chautauqua Parking on Sumac Dr
Proposed Conditions

Chautauqua Pedestrian Safety, Access,
and Lighting Improvements

Design Concept
Existing Conditions & Renderings
December, 2015

Design Concepts for Baseline Road improvements, 2015.



Exposed Aggregate Concrete Sidewalk
Chautauqua Entrance at Kinnikinic Road



Stone Valley Gutter Pan
Chautauqua Entrance at Kinnikinic Road



"Stabilgrid" Permeable Parking Area
Proposed Diagonal Parking on Sumac Drive



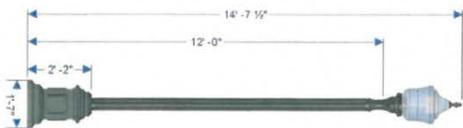
Rock Walls
Proposed Wall Construction at Baseline ROW/Historic District Boundary



Crusher Fines Trails
Proposed Trail Connection Material on West Side of Kinnikinic to Blue Bell Rd



Lighting Components
Proposed LED Lighting per Chautauqua Lighting Master Plan

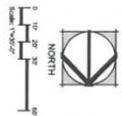
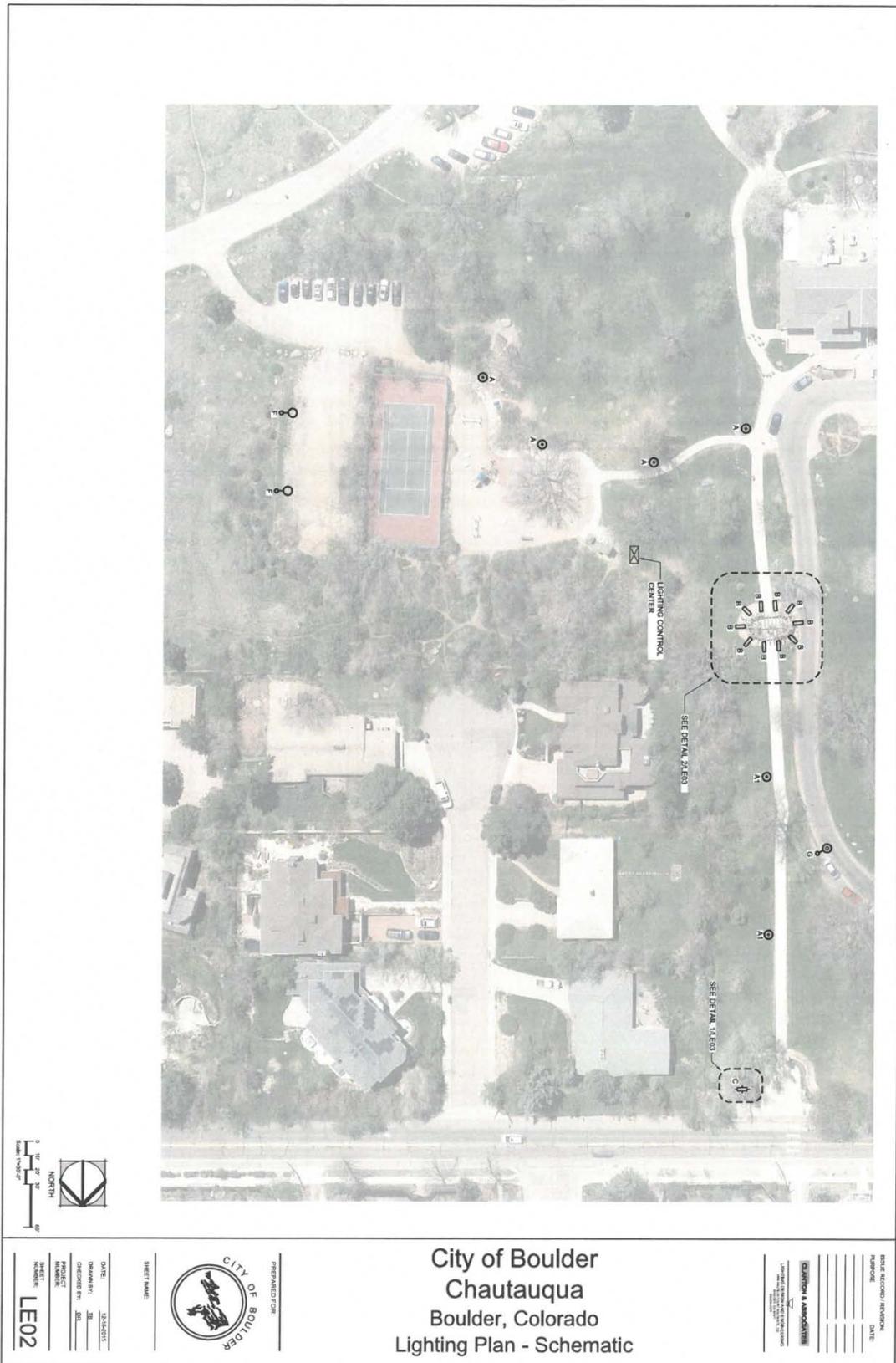


Chautauqua Pedestrian Safety, Access, and Lighting Improvements

Design Concept
Proposed Materials
December, 2015



Design Concepts for Baseline Road improvements, 2015.



DATE: 12.14.2011
 DRAWN BY: JLB
 CHECKED BY: JLB
 PROJECT NUMBER: 1400000
 SHEET NUMBER: LE02

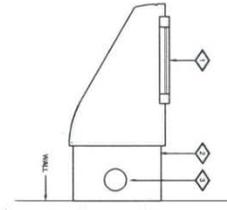


City of Boulder
 Chautauqua
 Boulder, Colorado
 Lighting Plan - Schematic

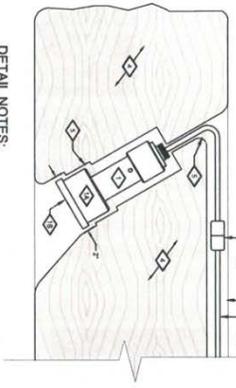
DATE	REVISION

CLARKSON & ASSOCIATES
 1400 13th Street, Suite 100
 Boulder, CO 80502
 303.440.1400
 www.clarksonandassociates.com

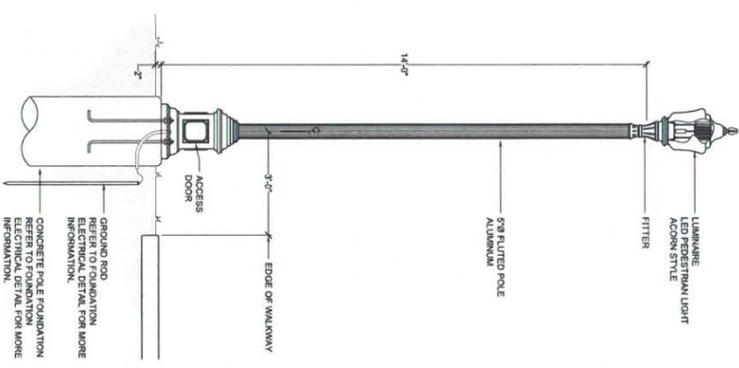
Lighting Plan, 2015.



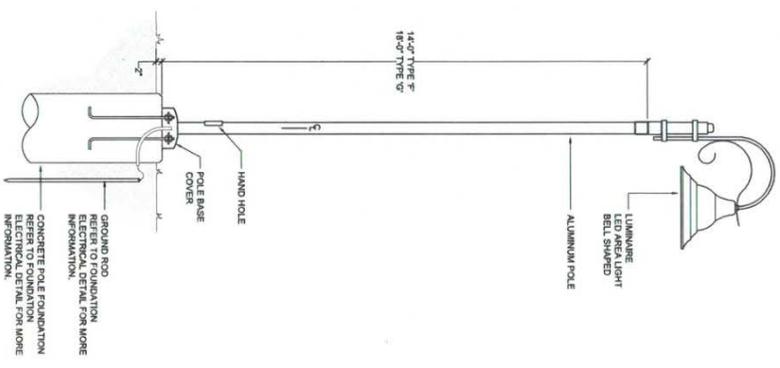
- DETAIL NOTES:**
- ◇ TYPE 'A' - Recessed LED LUMINAIRE
 - ◇ LED - 10 WATT, 3000K
 - ◇ MOUNT INVERTED FOR UPWARD DISTRIBUTION.
 - ◇ SURFACE MOUNTED BACK BOX.
 - ◇ SURFACE MOUNTED RIGID CONDUIT TO EXISTING JUNCTION BOX.
- 1 TYPE 'C' - TROLLEY STOP LIGHTING DETAIL**
 (LED) NOT TO SCALE



- DETAIL NOTES:**
- ◇ TYPE 'B' - 8K MINIMUM Recessed LED DOWN LIGHT
 - ◇ LED - 3 WATT, 3000K
 - ◇ SPOT DISTRIBUTION WITH SOFT FOCUS LENS.
 - ◇ MOUNTING COLLAR.
 - ◇ FLANGE, REMOVABLE FOR ACCESS TO LED MODULE.
 - ◇ CLAS II WIRING TO REMOTE TVAC TRANSFORMER.
 - ◇ WET LOCATION CONNECTOR.
 - ◇ EXISTING WOOD BEAM.
 - ◇ SILICONE OR APPROPRIATE SEALANT/ADHESIVE
 - ◇ CUT CHANNEL IN TOP OF BEAM FOR WIRING.
- 2 TYPE 'B' - ARBOR LIGHTING DETAIL**
 (LED) NOT TO SCALE



- 3 TYPE 'A' & 'A1' - LIGHT STANDARD DETAIL**
 (LED) NOT TO SCALE
- ◇ LUMINAIRE FROM LIGHT ARBOR STYLE
 - ◇ FITTER
 - ◇ 9\"/>



- 4 TYPE 'F' & 'G' - LIGHT STANDARD DETAIL**
 (LED) NOT TO SCALE
- ◇ LUMINAIRE LED AREA LIGHT BELL SHAPED
 - ◇ ALUMINUM POLE
 - ◇ 14'-0\"/>

SEE RECORD FOR REVISIONS

NO.	DATE	DESCRIPTION

City of Boulder
Chautauqua
 Boulder, Colorado
 Lighting Plan - Schematic



PREPARED BY: [Blank]
 CHECKED BY: [Blank]
 DATE: 12.14.2010
 DRAWN BY: JBL
 PROJECT NUMBER: [Blank]
 SHEET NUMBER: LE03

Details for proposed lighting fixtures, 2015.

